

**IMPROVEMENTS TO EXISTING PUBLIC ROAD AT GRADE CROSSING
FOR PUBLIC PEDESTRIAN CROSSING, DOT 440571H
M.P. 238.44 WYNNE SUBDIVISION
JONESBORO, ARKANSAS**

THIS AGREEMENT, executed in duplicate this 6th day of July, 2017, by and between UNION PACIFIC RAILROAD COMPANY, a Delaware corporation ("Railroad"), and CITY OF Jonesboro, a municipal corporation of the State of Arkansas ("Political Body").

RECITALS:

Political Body has requested Railroad to improve the existing **Public Pedestrian** crossing, at grade, along, over and across Railroad's track and right of way at Railroad Mile Post **238.44**, on Railroad's **Wynne** Subdivision, DOT No. 440571, in City of **Jonesboro, Craighead** County, State of Arkansas (the "Crossing"), to which Railroad is agreeable, but solely upon terms and conditions hereinafter set forth.

The location of the Crossing is generally shown on the print marked **Exhibit A**, attached hereto and hereby made a part hereof.

AGREEMENT:

NOW THEREFORE, in consideration of the premises and of the promises and conditions hereinafter set forth, the parties hereto agree as follows:

1. Railroad shall furnish all labor, material, equipment and supervision for, and shall (a) remove the existing timber crossing, (b) install an 8 ft. precast concrete crossing, (c) renew the crossties, and (d) surface the track with new ballast.
2. Political Body agrees to reimburse the Railroad for one hundred percent (100%) of Railroad's actual labor and material costs associated with the work and materials described in Section 1 above. Railroad estimates such cost to be **Seventeen Thousand Nine Hundred and Ninety-Two Dollars (\$17,992.00)**, as set forth in Estimate of Material and Force Account dated April 01, 2017 marked **Exhibit B**, hereto attached and hereby made a part hereof. During the performance of such work Railroad will provide progressive billing to Political Body based on Railroad's actual costs. Actual costs to Railroad shall include customary additives (which includes its overhead and indirect construction costs) to materials, services and labor provided by Railroad. Within One Hundred Twenty (120) days after Railroad has completed its work, Railroad will submit a final billing to Political Body for any balance owed. Political Body shall pay Railroad within thirty (30) days of its receipt of progressive and final bills submitted by Railroad.
3. Railroad, at its cost, shall maintain the Crossing between the track tie ends and Political Body, at its cost, shall maintain all other portions of the Crossing. If, in the future, Political Body elects to have the surfacing material between the track tie ends replaced with paving or some surfacing material other than concrete, Railroad, at the Political Body's expense, shall install such replacement surfacing.
4. Political Body, at its cost, shall provide all labor, material and equipment associated with traffic control, barricades, detour signing, advanced warning signs and pavement markings. All such work shall be in compliance with the current Manual on Uniform Traffic Control Devices.
5. If Political Body's contractor(s) is/are performing any work described in Section 4 above, then Political Body shall require its contractor(s) to execute Railroad's standard and current form of

Contractor's Right of Entry Agreement. Political Body acknowledges receipt of a copy of the Contractor's Right of Entry Agreement and understanding of its terms, provisions, and requirements, and will inform its contractor(s) of the need to execute the Agreement and to obtain and provide to Railroad the insurance policies, binders, certificates and endorsements that are required in the Contractor's Right of Entry Agreement. Under no circumstances will the Political Body's contractor(s) be allowed onto the Railroad's premises without first executing the Contractor's Right of Entry Agreement and providing the aforesaid insurance documents.

6. Fiber optic cable systems may be buried on Railroad's property. Protection of the fiber optic cable systems is of extreme importance since any break could disrupt service to users resulting in business interruption and loss of revenue and profits. Political Body or its contractor(s) shall telephone Railroad during normal business hours (7:00 a.m. to 9:00 p.m., Central Time, Monday through Friday, except holidays) at 1-800-336-9193 (also a 24-hour number, 7 day number for emergency calls) to determine if fiber optic cable is buried anywhere on Railroad's premises to be used by Political Body or its contractor(s). If it is, Political Body or its contractor(s) will telephone the telecommunications company(ies) involved, arrange for a cable locator, and make arrangements for relocation or other protection of the fiber optic cable prior to beginning any work on Railroad's premises.

7. Political Body, for itself and for its successors and assigns, hereby waives any right of assessment against Railroad, as an adjacent property owner, for any and all improvements made under this Agreement.

8. Covenants herein shall inure to or bind each party's successors and assigns; provided, no right of Political Body shall be transferred or assigned, either voluntarily or involuntarily, except by express written agreement acceptable to Railroad.

9. Political Body hereby confirms that funds have been appropriated for the work set forth in this Agreement.

10. The person signing this Agreement on behalf of Political Body hereby confirms that he/she is authorized under Political Body policies and procedures to execute this Agreement and to bind Political Body to the obligations under this Agreement.

IN WITNESS WHEREOF, the parties hereto have duly executed this Agreement as of the date first herein written.

UNION PACIFIC RAILROAD COMPANY

By  _____

Title: Kevin D. Hicks
AVP. Engineering

CITY OF JONESBORO CONSTRUCTION PLANS

RACE ST. SIDEWALK & RR PED. CROSSING
(JONESBORO) (TAP-15) (S)

CRAIGHEAD COUNTY

JOB 100857

Engineering Department
P.O. Box 1845 Phone: (870) 933-4438
307 Vine Street Fax: (870) 933-4464
Jonesboro, AR 72401 Email: cmpr@engr.com

City of Jonesboro ARKANSAS



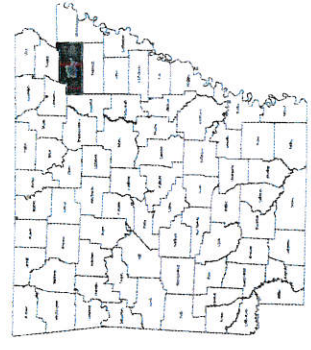
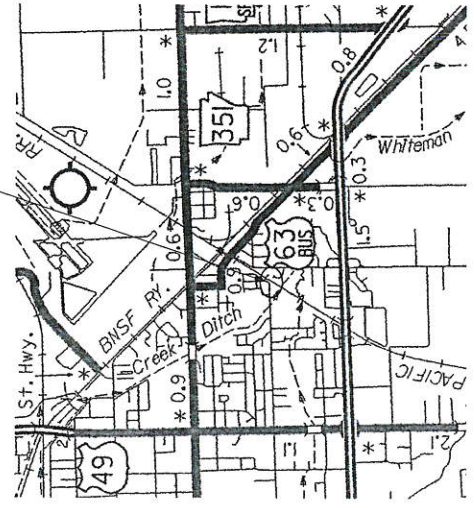
RACE ST. SIDEWALK AND RR PED. CROSSING



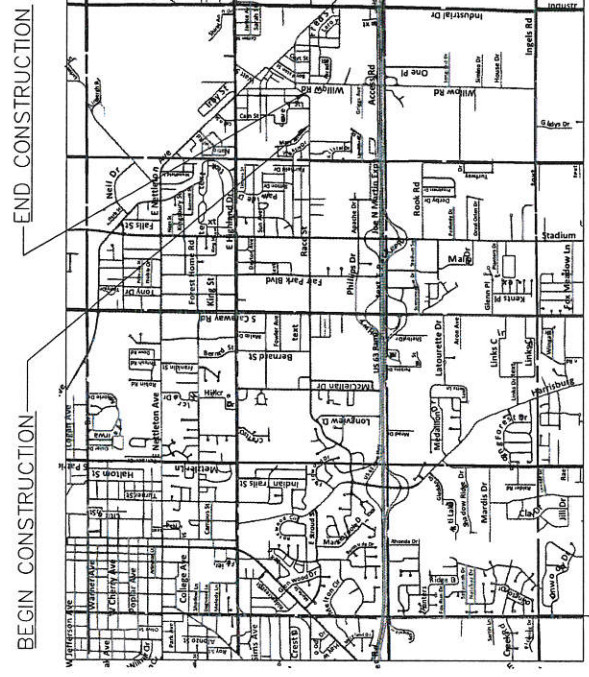
REVISIONS	
DATE	DESCRIPTION

DRAWING INFO.	
DRAWN BY:	DATE:

COVER	
SHEET NUMBER:	
1 of 23	



AHTD DISTRICT 10



Know what's below.
Call before you dig.

CITY OF JONESBORO

NOT TO SCALE

DATE REVISED	DATE PLUMED	DATE REVISION	FED. PROJ. NO.	STATE PROJ. NO.	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
			6	ARK.	100857	2	23
JOB NO. 100857 ② INDEX OF SHEETS, GOVNL. SPECS & GEN. NOTES							



GOVERNING SPECIFICATIONS
 ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER -----**TITLE**
 EMBALVA-----ERRATA FOR THE BOOK OF STANDARDS SPECIFICATIONS
 JOB 100857-----SIDEWALK DRAIN

GENERAL NOTES

- GRADE LINE DENOTES FINISHED GRADE OF SIDEWALKS WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U.S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND MAINTENANCE SHALL BE PROVIDED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATION.
- ALL PARTS OF THIS DESIGN & INSTALLATION SHALL BE IN ACCORDANCE WITH ASHTO, THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARDS AND DETAILS, AOA STANDARDS FOR ACCESSIBLE DESIGN, AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITIONS.
- CONTRACTOR SHALL NOTIFY ALL EXISTING UTILITY OWNERS BEFORE BEGINNING WORK ON THIS PROJECT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITY SERVICES LINE CROSSED OR EXPOSED BY CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OR REPAIR OF THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER, AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PROVIDE THE DIVERSION OF PEDESTRIANS AND VEHICLE DURING THE PROGRESS OF WORK IN A MANNER SATISFACTORY TO THE CONSTRUCTION ENGINEER. NO SEPARATE PAY ITEM.
- THE CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS AND SIDEWALKS ADJACENT TO PROJECT FREE OF MUD AND DEBRIS.
- ALL MODIFICATIONS TO THE CONSTRUCTION PLANS SHALL MEET THE LATEST VERSION OF AOA STANDARDS FOR ACCESSIBLE DESIGN AND BE APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT ARKANSAS ONE CALL (800)482-8989 PRIOR TO THE START OF ANY WORK.
- CONTRACTOR SHALL BE RESPONSIBLE TO ADJUST WATER VALVE TO GRADE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION STAKING. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- NEITHER THE OWNER NOR THE ENGINEER SHALL BE RESPONSIBLE FOR SUB-SURFACE CONDITIONS. THE CONTRACTOR SHALL MAKE HIS OWN DETERMINATION CONCERNING SUB-SURFACE CONDITIONS.
- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT CONTRACTOR'S OWN EXPENSE.
- IT SHALL BE THE DUTY OF THE CONTRACTOR TO CONTROL AND PROVIDE SAFETY SIGNAGE FOR ALL VEHICULAR TRAFFIC WHEN CONSTRUCTION IMPROVEMENTS IMPAIR NORMAL STREET USAGE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE AMOUNT OF BORROW MATERIAL USED ON THE PROJECT. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINAL SOIL STABILIZATION OF THE PROJECT LIMITS INCLUDING THE ESTABLISHMENT OF VEGETATIVE COVER AND INSTALLATION OF PERMANENT EROSION CONTROL MEASURES AS SPECIFIED.
- CONSTRUCTION LIMITS TO BE CLEARED BY THE CONTRACTOR. ALL REMAINING DEBRIS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. ALL DEBRIS FROM THE DEMOLISHED SITE SHALL BE REMOVED AND DISCARDED BY THE CONTRACTOR FROM THE SITE EACH DAY.
- ALL CURB AND GUTTER SHALL HAVE A MINIMUM RUNNING SLOPE OF 0.50%.
- USE EXPANSION JOINTS WHERE ANY PERMANENT OBJECTS (STREET LIGHTS, POWER POLES, INLETS, FIRE HYDRANTS, ETC.) ARE LOCATED.
- CONTRACTOR SHALL SAW CUT EXISTING ASPHALT AND/OR CONCRETE TO ACCOMMODATE PROPOSED IMPROVEMENTS.
- ALL SIDEWALKS ARE TO HAVE A MAXIMUM OF 2.0% CROSS SLOPE AND 5.0% RUNNING SLOPE. CURB RAMPS ARE TO HAVE A MAXIMUM OF 2.0% CROSS SLOPE AND RUNNING SLOPE BETWEEN 5.0% AND 8.33%.
- WHEN CONNECTING TO EXISTING SIDEWALK, TIE-INS ARE TO BE LOCATED AT EXISTING EXPANSION JOINT. IN THE EVENT THERE IS NO EXPANSION JOINT LOCATED WHERE PROPOSED CONSTRUCTION IS TO BE LOCATED, PROVIDE AN EXPANSION JOINT AND MATERIAL.

INDEX OF SHEETS

SHEET NO.	TITLE	DRWG NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS & GENERAL NOTES		
3	QUANTITY SHEET		
4	TYPICAL SECTION/SPECIAL DETAIL		
5	SURVEY CONTROL		
6	MAINTENANCE OF TRAFFIC		
7-8	SIDEWALK PLAN		
9-11	CROSS SECTIONS		
12	CURBING DETAILS	CG-1	11/29/07
13	DETAILS OF DRIVEWAYS & ISLANDS	DR-1	2/27/14
14	MAILBOX DETAILS	MB-1	11/18/04
15	DETAIL OF DROP INLETS	FCP-9	11/16/01
16	DETAIL OF DROP INLETS (TYPE C)	FCP-9E	8/22/02
17	STANDARD HIGHWAY SIGNS AND SUPPORT ASSEMBLIES	SHS-1	9/12/13
18	U-CHANNEL POST ASSEMBLIES	SHS-2	2/27/14
19	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	9/2/15
20	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	9/2/15
21	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	9/2/15
22	WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS	WR-1	11/10/05
23	WHEELCHAIR RAMPS ALTERATIONS ONLY	WR-2	10/9/03

SUMMARY OF QUANTITIES

Item No.	AHTD Ref	Description	Quantity	Unit
1	202	REMOVAL AND DISPOSAL OF ASPHALT PAVEMENT	18	SQ. YD.
2	202	REMOVAL AND DISPOSAL OF CONCRETE PAVEMENT	15	SQ. YD.
3	202	REMOVAL AND DISPOSAL OF CURB & GUTTER	100	LIN. FT
4	202	REMOVAL AND DISPOSAL OF CONCRETE WALK	550	EACH
5	210	BORROW	500	CU. YD.
6	303	AGGR. BASE COURSE (CLASS 7)	100	TON
7	505	PORTLAND CEMENT CONCRETE DRIVEWAY	40	SQ. YD.
8	601	MOBILIZATION	1	LUMP SUM
9	603	MAINTENANCE OF TRAFFIC	1	LUMP SUM
10	604	SIGNS	48	SQ. FT.
11	604	TRAFFIC DRUMS	20	EACH
14	624	SOLID SOD	600	SQ. YD.
15	633	CONCRETE WALKS	545	SQ. YD.
17	634	CC CURB & GUTTER - A(1'-6")	100	LIN. FT
18	640	MODIFY DROP INLET	2	EACH
21	SP	SIDEWALK DRAIN	6	EACH

DATE REVISED	DATE PLUMED	DATE REVISED	DATE PLUMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.	100857	3	23

② QUANTITY SHEET



ADVANCED WARNING SIGNS & DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	TOTAL QUANTITY REQUIRED	TOTAL SIGNS REQUIRED (SQ. FT.)	TRAFFIC DRUMS
G22-2	END ROAD WORK	48" x 24"	2	16	
W22-1	ROAD WORK AHEAD	48" x 48"	2	32	20
	TRAFFIC DRUMS				20
			TOTAL	48	20

NOTE: ADDITIONAL SIGNS AND DEVICES MAY BE REQUIRED BASED ON TRAFFIC VOLUME, CURB AND TYPING, OPERATIONS AND CONSTRUCTION WITH VARIOUS PHASES AND WILL NOT BE PAID FOR DIRECTLY BY THE CONTRACTOR. SEE LISTED BY VENDOR FOR ITEMS.

REMOVAL AND DISPOSAL OF ITEMS

STATION	PC CONCRETE DRIVEWAY		ASPHALT PAVEMENT		CONCRETE WALKS		CURB & GUTTER	
	50' SQ. YD.	15' SQ. YD.	50' SQ. YD.	15' SQ. YD.	50' SQ. YD.	15' SQ. YD.	LIN. FT.	LIN. FT.
411-13								
411-75								
441-38								
441-38								
451-67								
451-67								
462-25								
462-36								
491-28								
491-08								
491-08								
501-83								
501-83								
411-13	15	18	18	15	100	100	100	100

PC CONCRETE DRIVEWAY

STATION	DESCRIPTION	PC CONCRETE DRIVEWAY 50' SQ. YD.	PC CONCRETE DRIVEWAY 15' SQ. YD.
411-13	DRIVEWAY LEFT		21
462-31	DRIVEWAY LEFT		19
	TOTAL		40

BASE AND SURFACING

STATION	DESCRIPTION	AGGREGATE BASE COURSE (TON)
ENTIRE PROJECT	RIGHT OF CENTERLINE	100
	TOTAL	100

ENTIRE PROJECT IS TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

CONCRETE COMBINATION CURB AND GUTTER

STATION	DESCRIPTION	TYPE A LIN. FT.	TYPE B LIN. FT.
ENTIRE PROJECT	RIGHT OF CENTERLINE	92	92
	TOTAL	184	184

ENTIRE PROJECT IS TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

STRUCTURES

STATION	DESCRIPTION	MODIFY INLET EACH
411-13	MODIFY DRAIN	1
462-31	MODIFY DRAIN	1
	TOTAL	2

CONCRETE WALKS

STATION	DESCRIPTION	CONCRETE WALKS 50' SQ. YD.	CONCRETE WALKS 15' SQ. YD.	SIDEWALK DRAIN - EACH
411-13	CONCRETE WALK RT. CL.	8		
411-80	CONCRETE WALK RT. CL.	144		
441-38	SIDEWALK DRAIN			1
441-38	CONCRETE WALK RT. CL.	22		
451-67	CONCRETE WALK RT. CL.	11		
451-67	SIDEWALK DRAIN			1
462-25	CONCRETE WALK RT. CL.	17		
462-25	SIDEWALK DRAIN			1
462-36	CONCRETE WALK RT. CL.	6		
491-28	CONCRETE WALK RT. CL.	88		
491-28	CONCRETE WALK RT. CL.	78		
491-11	CONCRETE WALK RT. CL.	78		
491-08	CONCRETE WALK RT. CL.	109		
491-08	CONCRETE WALK RT. CL.	109		
	TOTAL	545	18	6

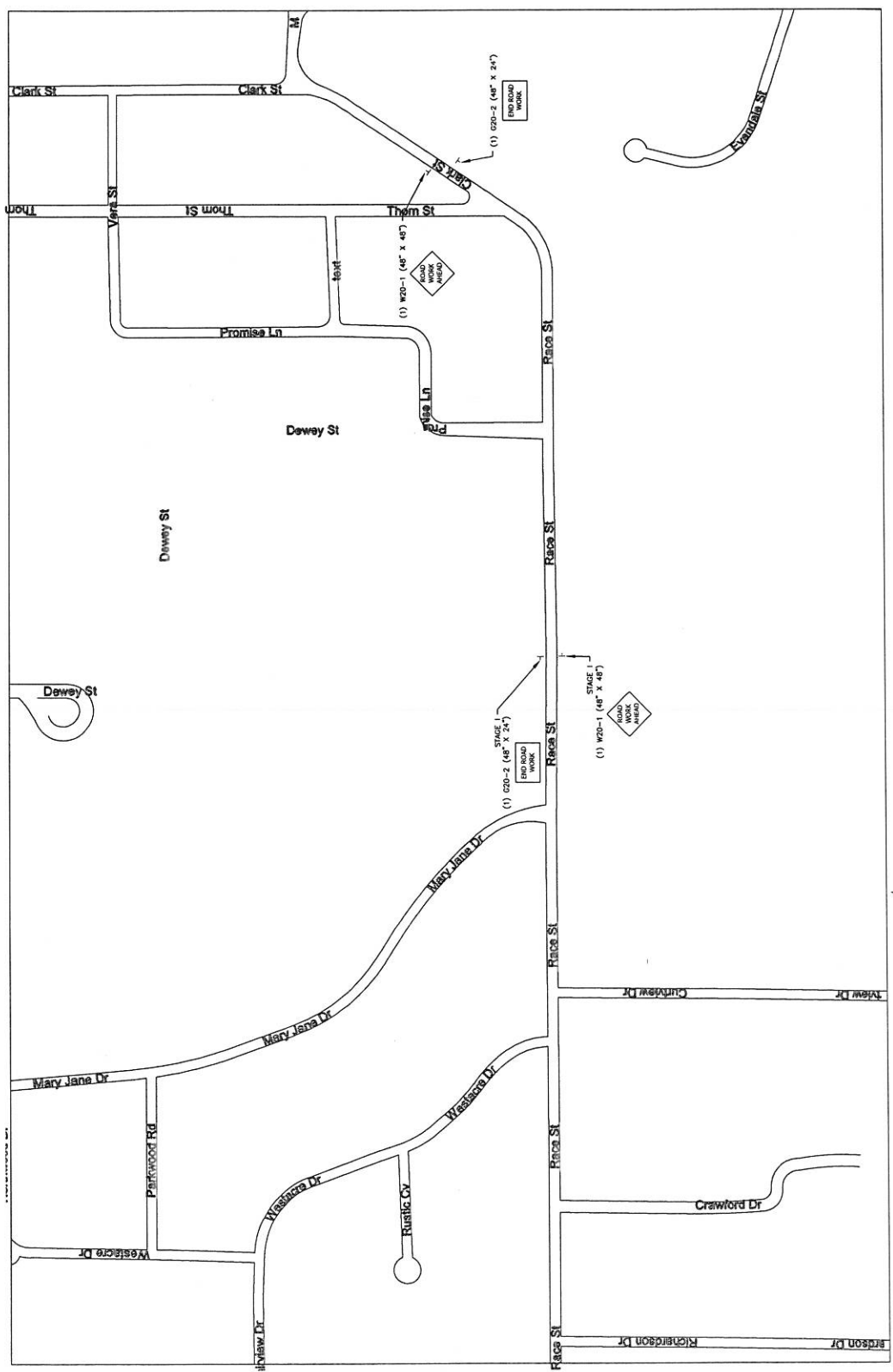
*TO BE USED IF AND WHERE DIRECTED BY ENGINEER. QUANTITIES BY WALKS.

DATE REVISION	DATE PLANNED	DATE REVISED	DATE PAID	DATE REVISION	DATE PAID	DATE REVISION	DATE PAID	DATE REVISION	DATE PAID	DATE REVISION	DATE PAID

PROJECT NO.	100857
PROJECT NAME	MAINTENANCE OF TRAFFIC
DATE	
STATE	ARK.
PROJECT NO.	6
DATE	
PROJECT NO.	100857
DATE	
PROJECT NO.	6
DATE	
PROJECT NO.	23



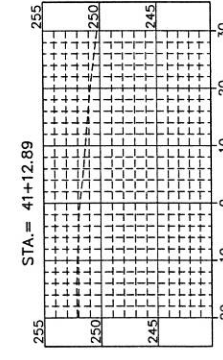
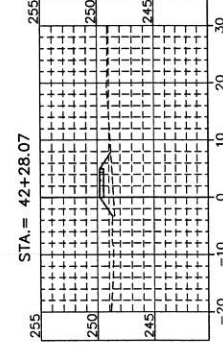
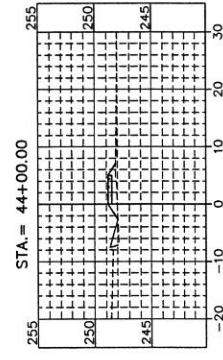
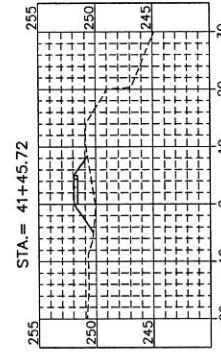
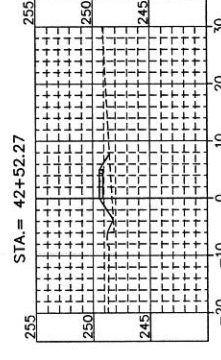
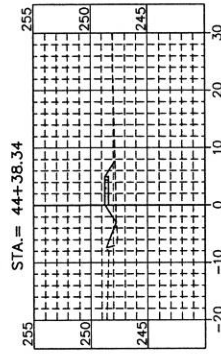
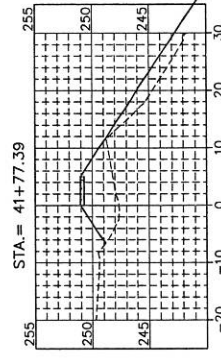
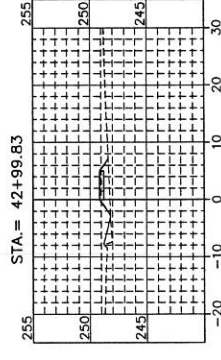
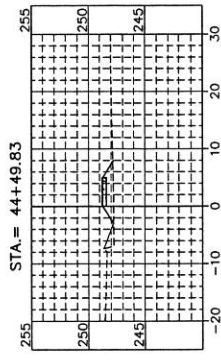
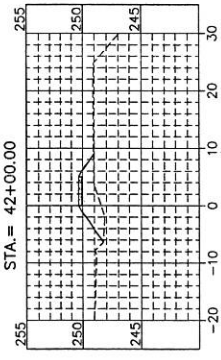
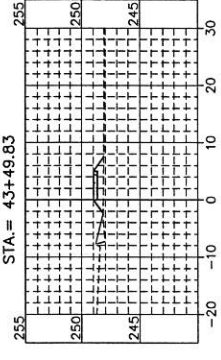
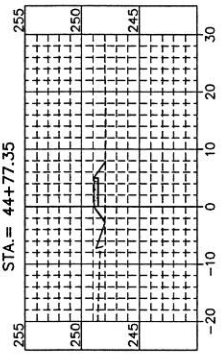
② MAINTENANCE OF TRAFFIC



MAINTENANCE OF TRAFFIC

DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION		
FED. ROAD DIST. NO.			STATE			JOB NO.			TOTAL SHEETS		
6			ARK.			100857			9		
									23		

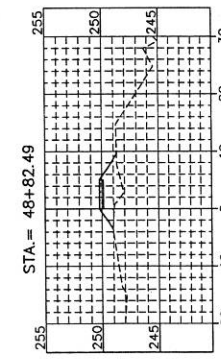
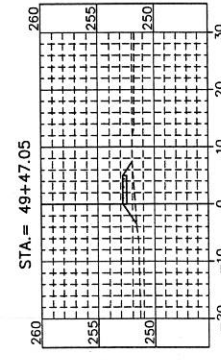
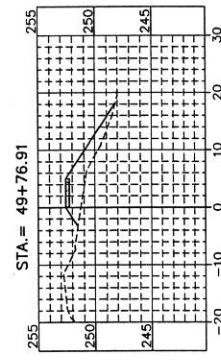
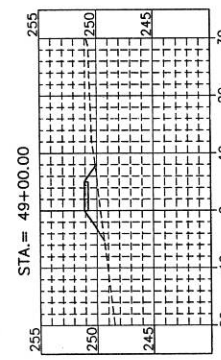
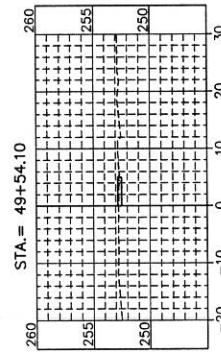
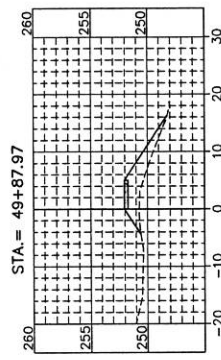
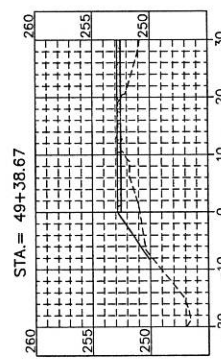
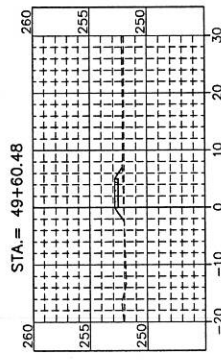
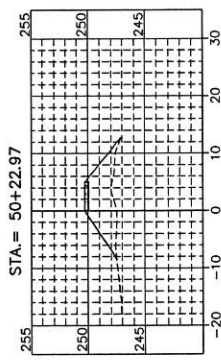
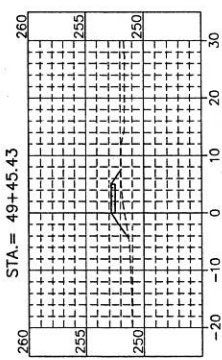
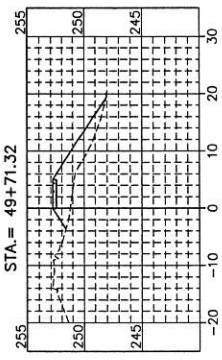
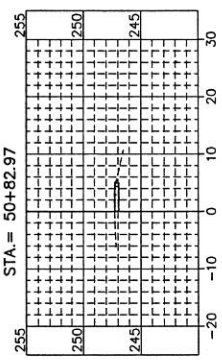
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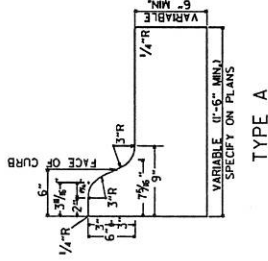


DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION		
FED. ROAD DIST. NO. 6			STATE ARK.			FED. ROAD PROJ. NO. 100857			SHEET NO. 11		
JOB NO. 100857			SECTION 2			TOTAL SHEETS 23					

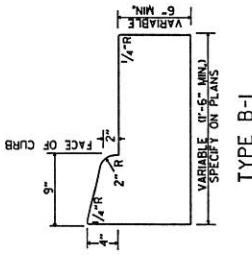


② CROSS SECTIONS

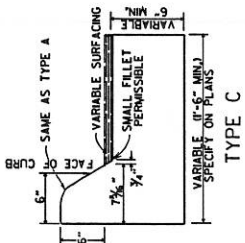




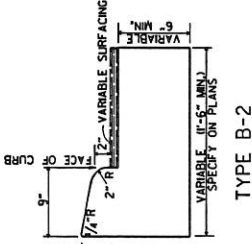
TYPE A



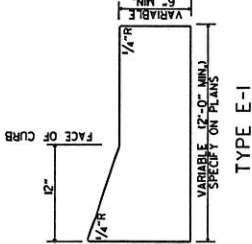
TYPE B-1



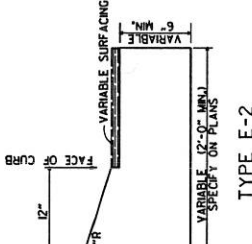
TYPE C



TYPE B-2

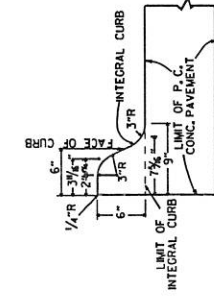
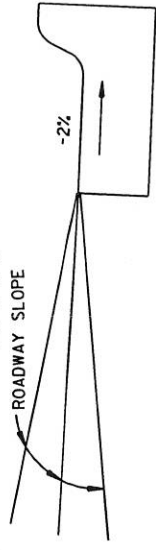


TYPE E-1

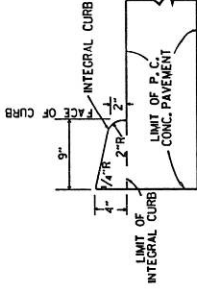


TYPE E-2

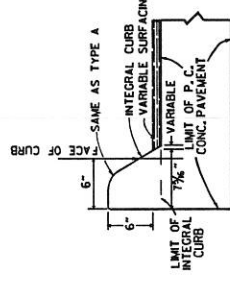
CONCRETE COMBINATION CURB AND GUTTER



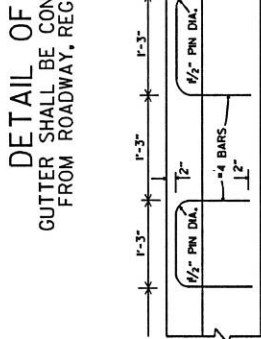
TYPE A



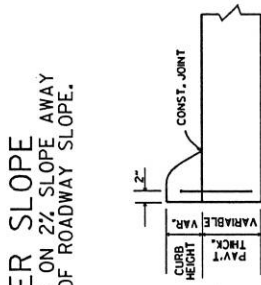
TYPE B



TYPE C



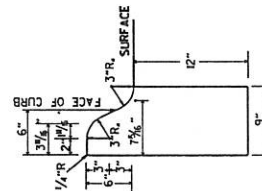
LONGITUDINAL SECTION



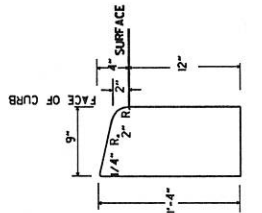
ELEVATION

DETAIL OF GUTTER SLOPE
GUTTER SHALL BE CONSTRUCTED ON 2% SLOPE AWAY FROM ROADWAY, REGARDLESS OF ROADWAY SLOPE.

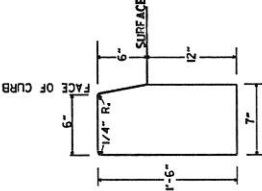
INTEGRAL CURB



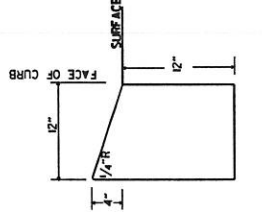
TYPE A



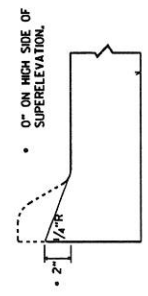
TYPE B



TYPE D



TYPE E



NOTE: USE MODIFIED CURB AS SPECIFIED ON STD. DR-1. COMPENSATION FOR MODIFIED CURB WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE TYPE OF CURB OR CURB AND GUTTER SPECIFIED.

CONCRETE CURB

DETAILS OF MODIFIED CURB

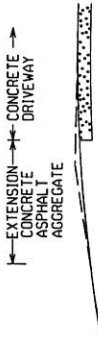
ALTERNATE CONSTRUCTION METHOD FOR INTEGRAL CURB

NO.	DATE	REVISION	DATE FILMED
1	12-20-57	REVISED GUTTER SLOPE & UNGRADED CURB DETAILS	
2	1-20-58	ADDED DETAILS OF TYPE CURB	
3	1-20-58	ADDED DETAIL OF TYPE CURB	
4	1-20-58	ADDED DETAIL OF TYPE CURB	
5	1-20-58	ADDED DETAIL OF TYPE CURB	
6	1-20-58	ADDED DETAIL OF TYPE CURB	
7	1-20-58	ADDED DETAIL OF TYPE CURB	
8	1-20-58	ADDED DETAIL OF TYPE CURB	
9	1-20-58	ADDED DETAIL OF TYPE CURB	
10	1-20-58	ADDED DETAIL OF TYPE CURB	
11	1-20-58	ADDED DETAIL OF TYPE CURB	
12	1-20-58	ADDED DETAIL OF TYPE CURB	
13	1-20-58	ADDED DETAIL OF TYPE CURB	
14	1-20-58	ADDED DETAIL OF TYPE CURB	
15	1-20-58	ADDED DETAIL OF TYPE CURB	
16	1-20-58	ADDED DETAIL OF TYPE CURB	
17	1-20-58	ADDED DETAIL OF TYPE CURB	
18	1-20-58	ADDED DETAIL OF TYPE CURB	
19	1-20-58	ADDED DETAIL OF TYPE CURB	
20	1-20-58	ADDED DETAIL OF TYPE CURB	

ARKANSAS STATE HIGHWAY COMMISSION

CURBING DETAILS

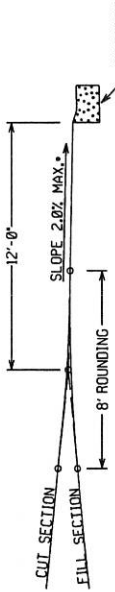
STANDARD DRAWING CG-1



- EXTENSION TYPICAL SECTIONS**
- 1: CONCRETE - 6" P.C. CONCRETE DRIVEWAY
 - 2: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
4" ACHM BINDER COURSE (1/2")
4" ACHM BASE COURSE (1-1/2")
 - 3: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
7" AGGREGATE BASE COURSE
 - 4: AGGREGATE - 6" AGGREGATE BASE COURSE

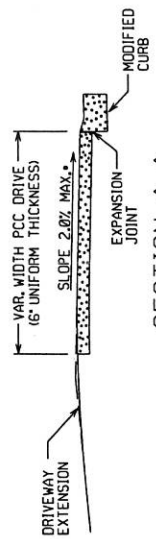
THE TYPE OF EXTENSION SHALL BE AS SHOWN IN THE PLANS. THE CONTRACTOR MAY, WITH THE APPROVAL OF THE ENGINEER, SUBSTITUTE A LOWER NUMBERED TYPE OF EXTENSION IN LIEU OF THE TYPE SPECIFIED IN THE PLANS, BUT AT NO ADDITIONAL COST TO THE DEPARTMENT.

DRIVEWAY EXTENSION DETAILS

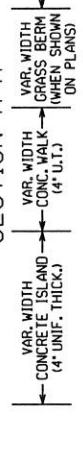


DRIVEWAY VERTICAL ALIGNMENT DETAILS

NOTE: DRIVEWAYS MAY NOT BE SLOPED AWAY FROM THE ROADWAY UNLESS APPROVED BY THE ENGINEER.

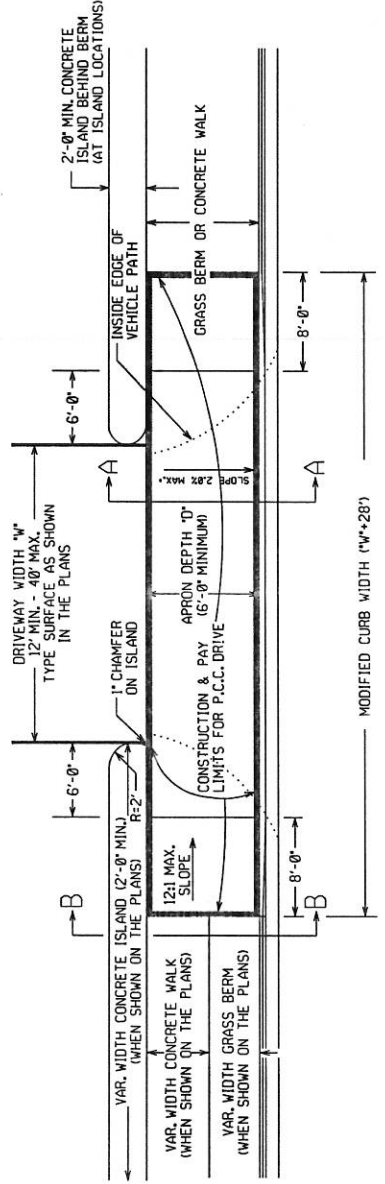


SECTION A-A



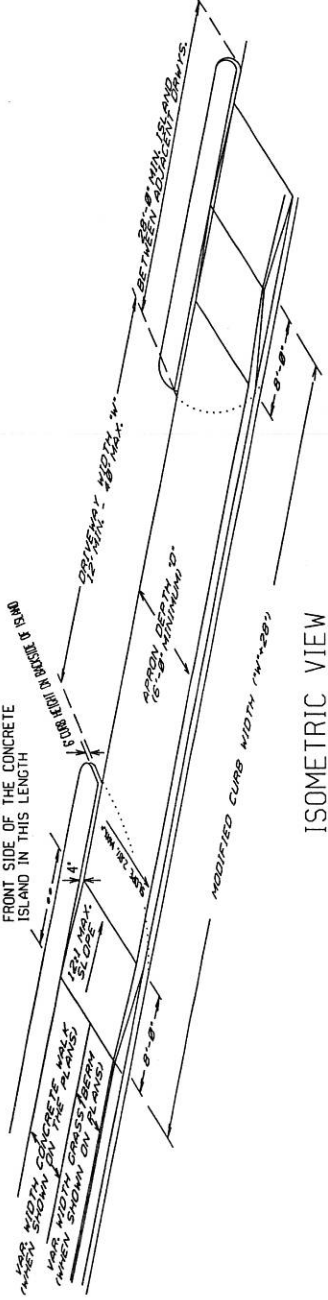
USE TYPE 'D' CURB FACE ON ALL SIDES OF CONC. ISLAND

SECTION B-B CURBED ISLAND BEHIND WALK



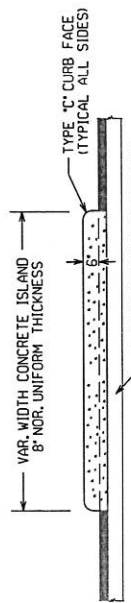
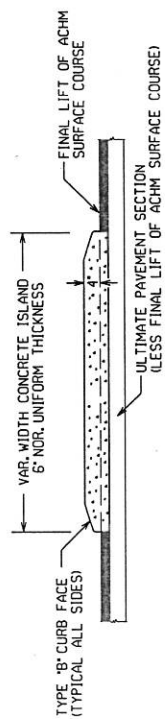
PLAN VIEW

** TRANSITION FROM A 0' TO A 4' TYPE 'D' CURB FACE ON THE FRONT SIDE OF THE CONCRETE ISLAND IN THIS LENGTH



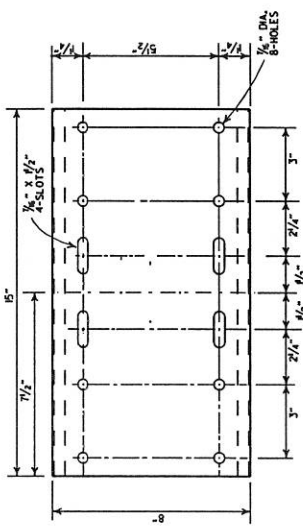
ISOMETRIC VIEW

REFER TO PLANS FOR TYPE OF CURB FACE TO BE USED. NO DIRECT PAYMENT WILL BE MADE FOR THE CURB FACES SHOWN ON THE ISLAND DETAILS. PAYMENT FOR THE CURB FACE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEM 'CONCRETE ISLAND'.

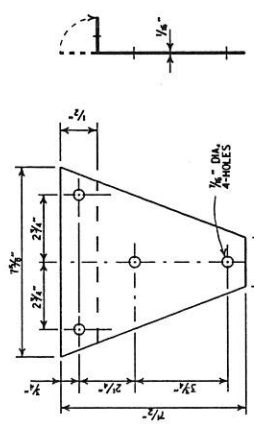


CURBED ISLANDS FOR CHANNELIZATION

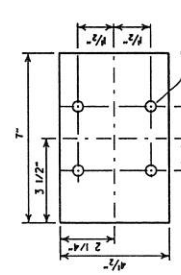
REVISION	DATE	DESCRIPTION
2-27-14		DATE REVISION FILLED
11-29-07		REVISION FOR TYPE C CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE
11-30-08		REVISION FOR TYPE 'D' CURB FACE & REVISED DRIVEWAY SLOPE NOTE



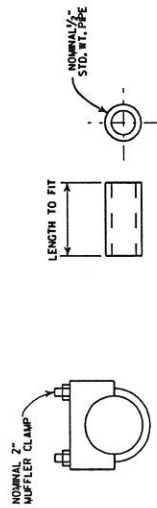
SHELF



BRACKET

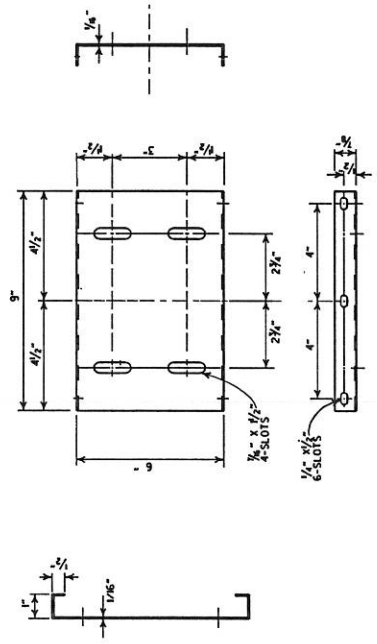


ANTI-TWIST PLATE



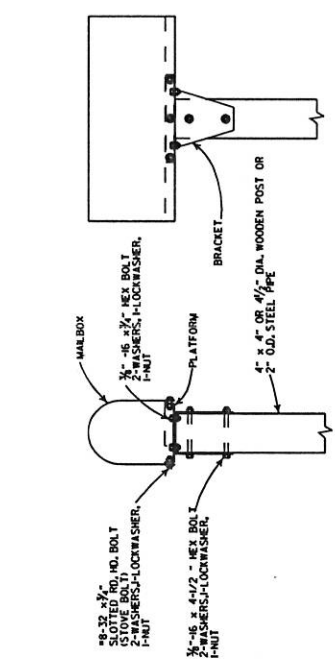
CLAMP

SPACER

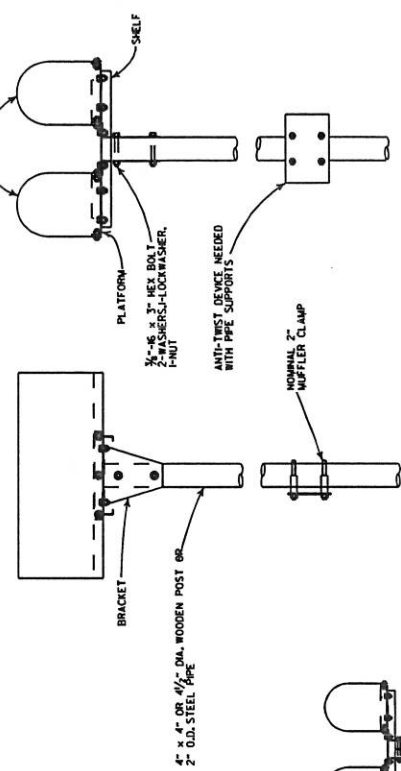


PLATFORM

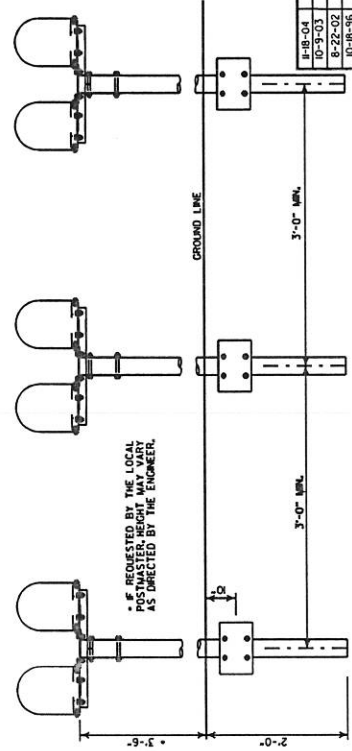
- GENERAL NOTES**
- MALBOX POSTS MAY BE WOOD OR METAL. WOOD POSTS SHALL BE SECTION 631.02 OF THE STANDARD SPECIFICATIONS.
 - ANTI-TWIST PLATES SHALL BE USED ON ALL POSTS.
 - OR PAINTED STEEL, HOWEVER TREATED WOOD MAY BE USED WITH WOODEN POSTS. WOODEN SHELVES SHALL BE ASSEMBLED WITH BOARDS OF THE APPROPRIATE LENGTH WITH SIX 8 x 2 1/2 PLAINHEAD WOOD SCREWS USED TO ATTACH THE MALBOX TO THE PLATFORM.
 - STANDARD SIZE MALBOXES, THE SHELF AND PLATFORM SIZE SHALL BE MODIFIED TO FIT MALBOXES OF A DIFFERENT SIZE.
 - DIALECTER STEEL WITH A WALL THICKNESS OF .045" AND A HEIGHT OF 2.72 LBS PER FT. OUTSIDE DIAMETER AND WEIGHT SHALL BE USED FOR MALBOXES. MALBOXES SHALL BE MANUFACTURED TO A TOLERANCE OF +/- .031, ACCORDING TO ASHTO M 88.
 - MALBOX SUPPORT SYSTEM DIFFERING FROM THOSE SHOWN MAY BE USED PROVIDED CLEARANCE ON THE AHD QUALIFIED PRODUCTS LIST FOR MALBOX SUPPORTS.



SINGLE INSTALLATION

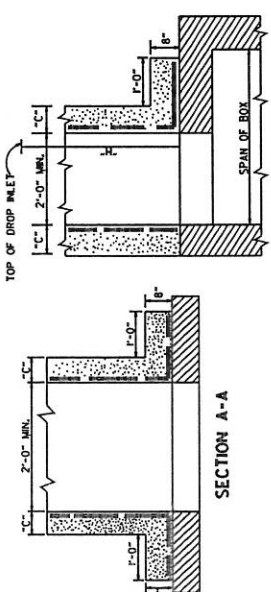
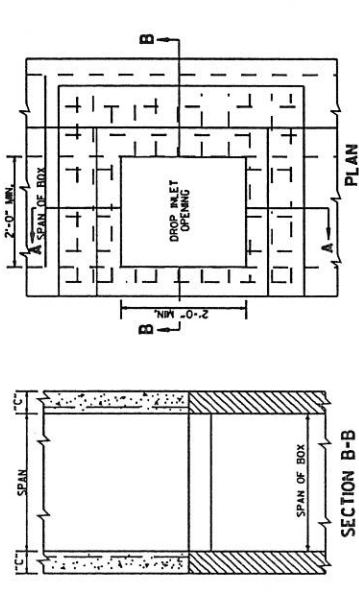


DOUBLE INSTALLATION

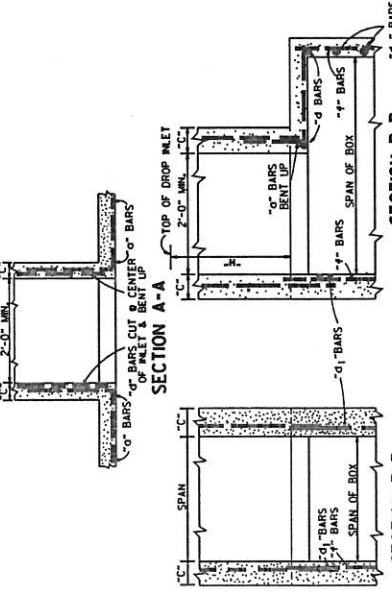


SPACING FOR MULTIPLE POST INSTALLATION

REVISION	DATE	DESCRIPTION
1	01-19-92	DELETED SLOTS FROM SHELF & PLATE ADJUSTED DIMENSIONS OF STEEL POSTS
2	01-19-92	ADJUSTED HEIGHT & ADDED NOTE
3	01-19-92	ADJUSTED HEIGHT & ADDED NOTE
4	01-19-92	ADJUSTED HEIGHT & ADDED NOTE
5	01-19-92	ADJUSTED HEIGHT & ADDED NOTE
6	01-19-92	ADJUSTED HEIGHT & ADDED NOTE
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100	01-19-92	ADJUSTED HEIGHT & ADDED NOTE

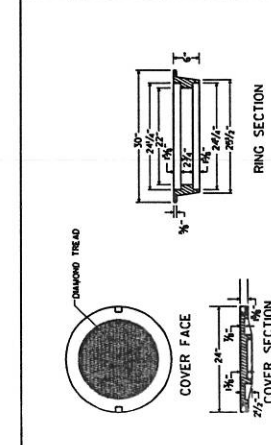
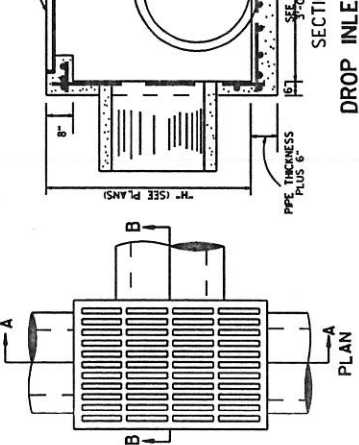


METHOD OF CONSTRUCTING DROP INLET ON EXISTING R.C. BOX CULVERT

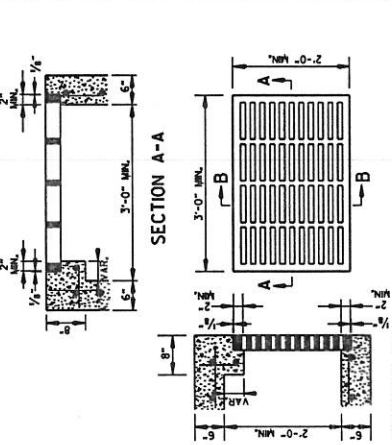


METHOD OF CONSTRUCTING DROP INLET ON NEW R.C. BOX CULVERT

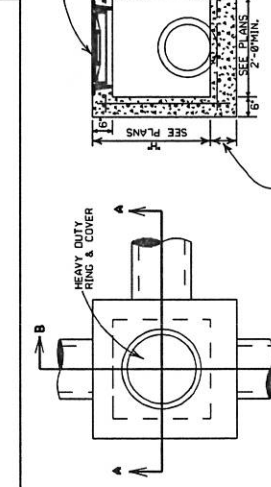
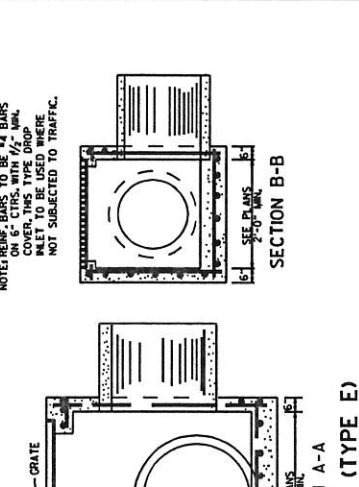
NOTE: "C" DIMENSIONS AND REINFORCING BAR SIZES, SHOWN CONFORM TO THOSE SHOWN ON STANDARD DRAWING FOR DROP INLET.



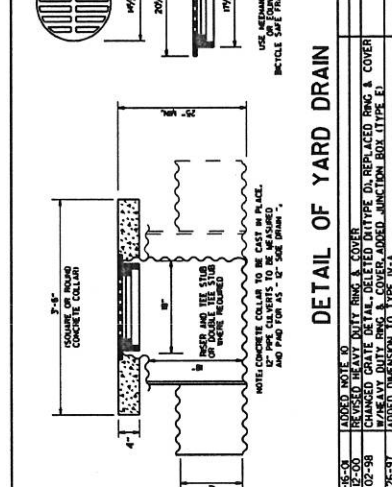
DETAIL OF DROP INLET (TYPE E)



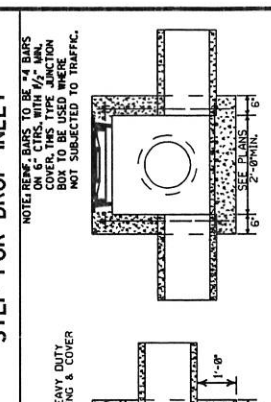
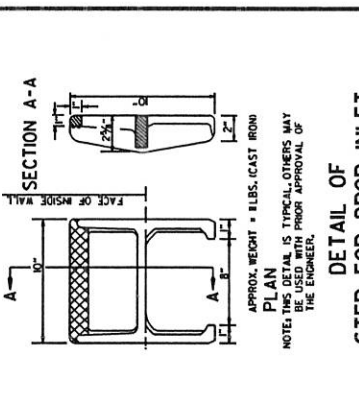
DETAIL OF DROP INLET (TYPE E)



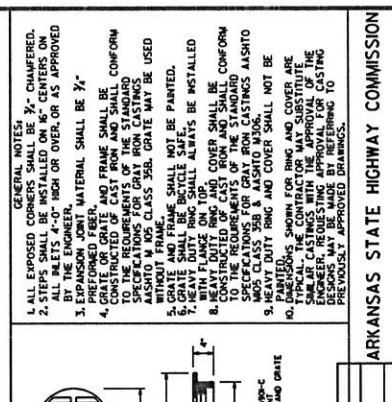
DETAIL OF DROP INLET (TYPE E)



DETAIL OF DROP INLET (TYPE E)



DETAIL OF DROP INLET (TYPE E)



DETAIL OF DROP INLET (TYPE E)

NOTE: REIN. BARS TO BE #4 BARS ON 6" CTRS. WITH 1/2" MIN. COVER. THIS TYPE DROP INLET TO BE USED WHERE NOT SUBJECTED TO TRAFFIC.

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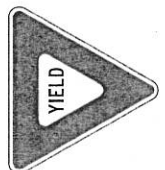
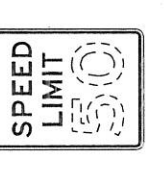



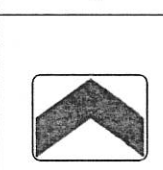



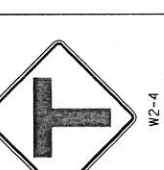
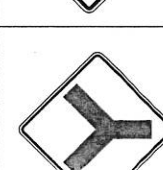


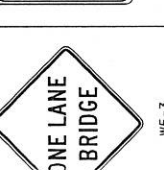



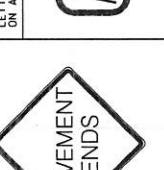
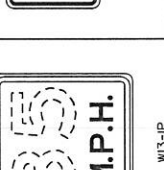
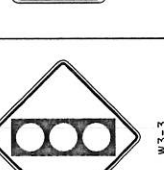
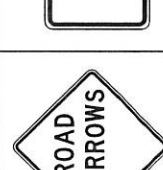
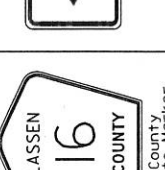
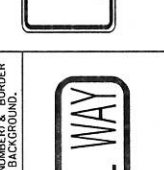
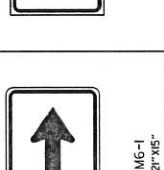
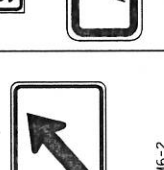
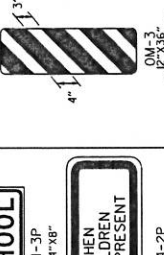

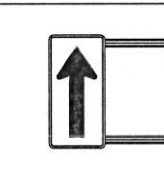
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 RI-1 30"x30"	 RI-2 36"x36"x36"	 RI-1 24"x30"	 WI-3 30"x30" (L.T. OR RT.)	 WI-4 30"x30" (L.T. OR RT.)	 WI-5 30"x30" (L.T. OR RT.)	 WI-8 48"x24"	 W2-1 30"x30"	 W2-2 30"x30"	 W2-3 30"x30" (L.T. OR RT.)	 W2-4 30"x30"	 W2-5 30"x30"	 SI-1 36"x36"	 W5-2 36"x36"	 W5-3 36"x36"	 W10-1 36" DIAMETER	 W5-1 36"x36"	 MI-6 24"x24" County Route Marker	 RI-3P 18"x6"	 M6-1 24"x15" NOTE: ALL M6 SIGNS TO BE MADE TO BE MADE WITH REFLECTORIZED YELLOW ARROW & BORDER WITH BLUE BACKGROUND.	 M6-2 24"x15"	 M6-3 24"x15"	 M6-4 24"x15"	 M6-5 24"x15"	 M6-6 24"x15"	 S4-3P 24"x8" SCHOOL WHEN CHILDREN ARE PRESENT S4-2P 24"x10"	 4" x 11" OM 3-2 (L.T. OR RT.)	 TYPE B	 TYPE C	MINIMUM WEIGHT TYPE A & B = 3 LBS./FT. TYPE C = 2 LBS./FT.
<p>MINIMUM DIMENSIONS SHOWN</p> <p>SUPPORT SECTION</p> <p>(U-CHANNEL) STANDARD SUPPORT ASSEMBLIES</p> <p>NOTE: LENGTH OF SIGN POSTS SHALL BE DETERMINED SO AS TO PROVIDE FOR MINIMUM VERTICAL CLEARANCES AS CALLED FOR IN THE SPECIFICATIONS PLUS A MINIMUM VERTICAL PENETRATION OF 30" IN THE SOIL.</p>																													
<p>ARKANSAS STATE HIGHWAY COMMISSION STANDARD HIGHWAY SIGNS AND SUPPORT ASSEMBLIES STANDARD DRAWING 5HS-1</p>																													
<p>DATE: _____ REVISION: _____</p> <p>DESIGNED BY: _____ CHECKED BY: _____</p> <p>DRAWN BY: _____</p> <p>DATE FILMED: _____</p>																													

ADVANCE DISTANCES
(XXXX)

500 FT
1000 FT
1500 FT

1/2 MILE
3/4 MILE
1 MILE

1 MILE
AHEAD

GENERAL NOTES:

1. ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE STANDARD TRAFFIC CONTROL MANUAL, LATEST EDITION, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.

2. TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS SHALL BE MAINTAINED AS NEARLY AS POSSIBLE TO THE CONSTRUCTION AREA EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.

3. EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.

4. SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE "B" BARRICADE.

5. SIGN POSTS DIRECT BURRED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS; CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE HEAVILY CONSTRUCTED, AND SHALL BE REFLUMED, CLEANED, OR REPAIRED AS NECESSARY. SIGN POSTS SHALL BE SET AT AN ANGLE OF 90 DEGREES TO THE ROAD AND 2 POSTS IN A T-PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.

6. POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.

7. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. WARNING SIGNS, TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR AN INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR AN INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURRED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE SIGN POSTS SHALL BE HEAVILY REINFORCED STOP-SLOW SITUATIONS. FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW SITUATIONS. FLAGGERS, FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.














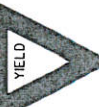
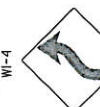
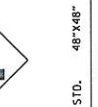

9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT, HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE HIGHWAY IS ONE WAY, ONE WAY TRAFFIC, MOTORISTS THE PROPER DIRECTION OF MOVEMENT.

10. R65-A SIGNS SHALL BE PLACED AT LEAST 500 FT BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGNS SHALL BE PLACED AT LEAST 100 FT IN ADVANCE OF THE REDUCED SPEED AHEAD SIGN.

*NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE MENU RECOMMENDED BY THIS MANUAL OR THIS MANUAL, FOR ASSESSING SAFETY HARDWARE THE REQUIREMENTS OF SECTION 200 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

R-1-1	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-2	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-3	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-4	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-5	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-6	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-7	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-8	REVISOR REduced ROAD WORK NEXT XX MILES
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R-1-98	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-99	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-100	REVISOR REduced ROAD WORK NEXT XX MILES

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROL
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-1

R4-1	DO NOT PASS	STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"	WI-1		WD-2	36"x36" FWY. 48"x48"	R4-2	PASS WITH CARE	STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"	WI-2		WD-2	36"x36" FWY. 48"x48"	W3-50	XX MPH SPEED ZONE AHEAD	STD. 36"x36" EXPWY. 48"x48" FWY. 48"x60"	RSP-1	SHOULDER CLOSED	48"x30"	W3-1		W3-2		W3-1	STD. 36"x36" SPECIAL 48"x48"	W2-0-1	ROAD WORK XXXX	STD. 48"x48"	W24-1		W4-9	DETOUR	STD. 30"x24" SPECIAL 48"x36" 60"x48"	M4-10		48"x18"
R4-2	PASS WITH CARE	STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"	WI-2		WD-2	36"x36" FWY. 48"x48"	W3-50	XX MPH SPEED ZONE AHEAD	STD. 36"x36" EXPWY. 48"x48" FWY. 48"x60"	RSP-1	SHOULDER CLOSED	48"x30"	W3-1		W3-2		W3-1	STD. 36"x36" SPECIAL 48"x48"	W2-0-1	ROAD WORK XXXX	STD. 48"x48"	W24-1		W4-9	DETOUR	STD. 30"x24" SPECIAL 48"x36" 60"x48"	M4-10		48"x18"							
W3-5		STD. 36"x36" EXPWY. 48"x48" FWY. 48"x60"	RH-4	ROAD CLOSED TO THRU TRAFFIC	60"x30"	WI-8	18"x24" SPECIAL 30"x36" FWY. 36"x48"	W9-2	LANE ENDS MERGE RIGHT	STD. 36"x36" FWY. 48"x48"	W21-2	FRESH OIL	STD. 30"x30" SPECIAL 36"x36"	G20-2	END ROAD WORK	48"x24"																				
R2-1	SPEED LIMIT 50	STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"	RH-3A	ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY	60"x30"	WI-6	48"x24" SPECIAL 60"x30"	W8-7	LOOSE GRAVEL	EXPWY. 36"x36" FWY. 48"x48"	W20-7g		500 FEET AHEAD	STD. 36"x36" FWY. 48"x48"	G20-1	ROAD WORK NEXT XX MILES	60"x24"																			
R1-2		STD. 36"x36"x36" EXPWY. 36"x48" FWY. 60"x60"x60"	RH-2	ROAD CLOSED	48"x30"	WI-4		W8-3		EXPWY. 36"x36" SPECIAL 48"x48"	W20-5	RIGHT LANE CLOSED XXXX	STD. 48"x48"	W8-9	LOW SHOULDER	STD. 36"x36" FWY. 48"x48"																				
R1-1	STOP	STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"	R5-1	DO NOT ENTER	STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"	WI-3		W5-1	ROAD NARROWS	STD. 48"x48"	W20-4	ONE LANE ROAD CLOSED XXXX	STD. 48"x48"	W8-II	UNEVEN LANES	STD. 36"x36" FWY. 48"x48"																				

FINES DOUBLE
IN WORK ZONES
WHEN WORKERS
ARE PRESENT **
36"x60"
USE 6" C LETTERS
** USE 4" D LETTERS

DETOUR
48"x18"

SHOULDER WORK
12"x36"

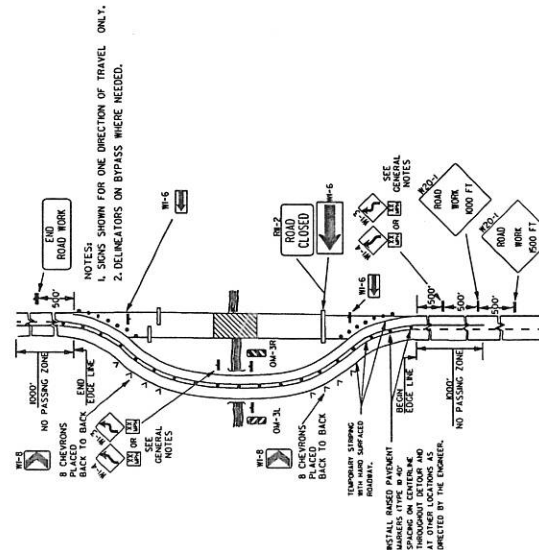
END ROAD WORK
48"x24"

ROAD WORK NEXT XX MILES
60"x24"

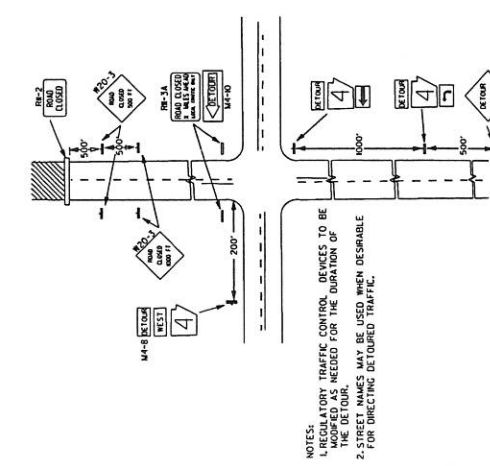
LOW SHOULDER
STD. 36"x36"
FWY. 48"x48"

UNEVEN LANES
STD. 36"x36"
FWY. 48"x48"

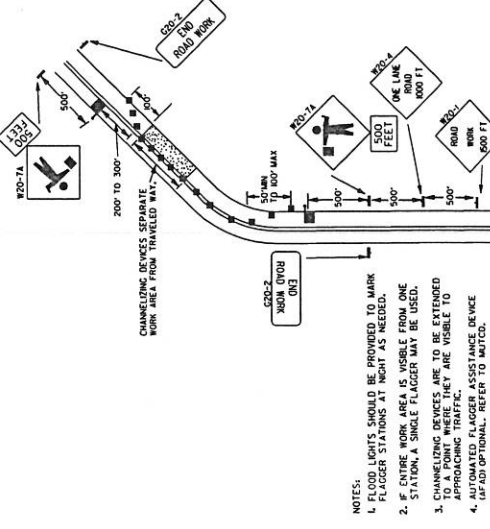
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R-1-47	REVISOR REduced ROAD WORK NEXT XX MILES
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R-1-67	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-68	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-69	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-70	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-71	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-72	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-73	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-74	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-75	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-76	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-77	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-78	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-79	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-80	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-81	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-82	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-83	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-84	REVISOR REduced ROAD WORK NEXT XX MILES
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R-1-87	REVISOR REduced ROAD WORK NEXT XX MILES
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R-1-96	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-97	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-98	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-99	REVISOR REduced ROAD WORK NEXT XX MILES
R-1-100	REVISOR REduced ROAD WORK NEXT XX MILES



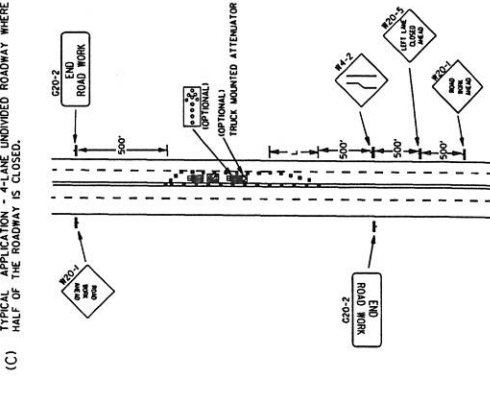
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



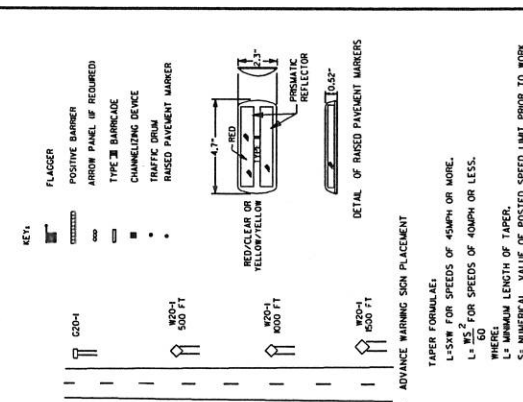
(B) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.



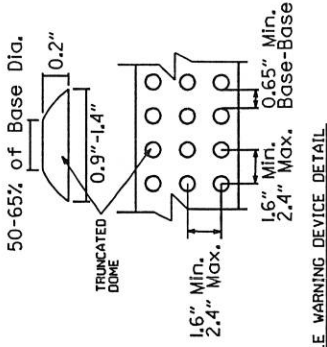
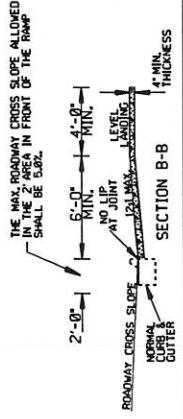
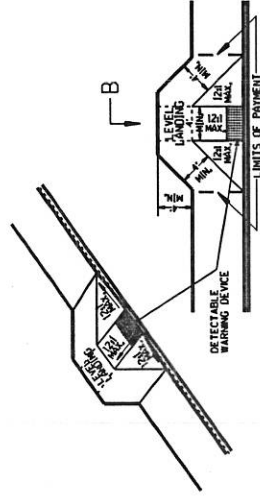
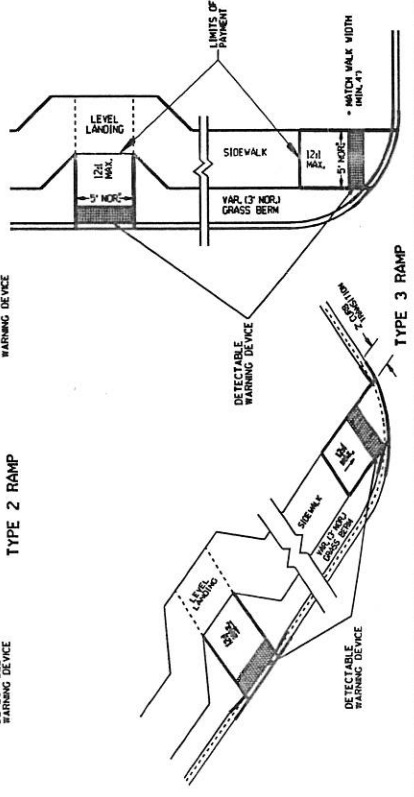
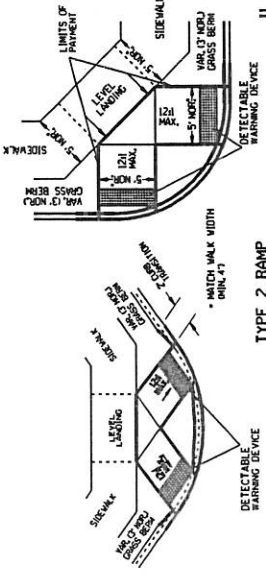
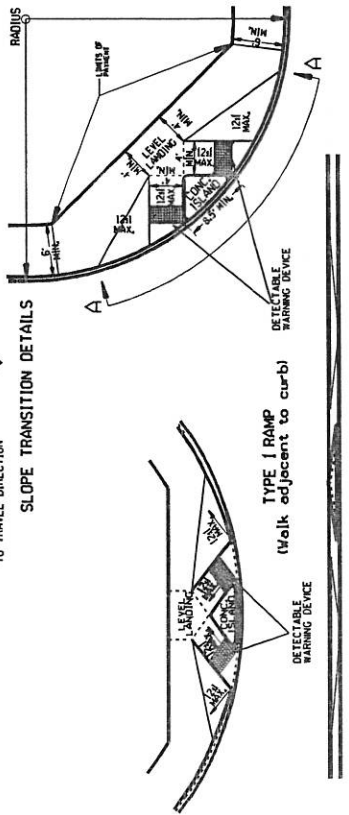
ARKANSAS STATE HIGHWAY COMMISSION
 STANDARD TRAFFIC CONTROLS
 FOR HIGHWAY CONSTRUCTION
 STANDARD DRAWING TC-2

NO.	DATE	REVISIONS
9-2-46		REVISED NOTE 8. REVISED 8.
9-2-46		REVISED DETAIL OF RAISED PAVEMENT MARKERS
3-1-49		ADDED W1-4
3-20-50		REVISED SIGN REVISIONS
1-22-51		ADDED W1-5
1-22-51		ADDED W1-6
1-22-51		ADDED W1-7
1-22-51		ADDED W1-8
1-22-51		ADDED W1-9
1-22-51		ADDED W1-10
1-22-51		ADDED W1-11
1-22-51		ADDED W1-12
1-22-51		ADDED W1-13
1-22-51		ADDED W1-14
1-22-51		ADDED W1-15
1-22-51		ADDED W1-16
1-22-51		ADDED W1-17
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1-22-51		ADDED W1-19
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1-22-51		ADDED W1-58
1-22-51		ADDED W1-59
1-22-51		ADDED W1-60
1-22-51		ADDED W1-61
1-22-51		ADDED W1-62
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1-22-51		ADDED W1-64
1-22-51		ADDED W1-65
1-22-51		ADDED W1-66
1-22-51		ADDED W1-67
1-22-51		ADDED W1-68
1-22-51		ADDED W1-69
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1-22-51		ADDED W1-72
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1-22-51		ADDED W1-74
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1-22-51		ADDED W1-77
1-22-51		ADDED W1-78
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1-22-51		ADDED W1-82
1-22-51		ADDED W1-83
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1-22-51		ADDED W1-87
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1-22-51		ADDED W1-89
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1-22-51		ADDED W1-92
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1-22-51		ADDED W1-94
1-22-51		ADDED W1-95
1-22-51		ADDED W1-96
1-22-51		ADDED W1-97
1-22-51		ADDED W1-98
1-22-51		ADDED W1-99
1-22-51		ADDED W1-100

TYPE 1 RAMP DIMENSIONS AND QUANTITIES

RADIUS	DISTANCE	LENGTH	RAMP AREA
FEET	FEET	FEET	SQ. FT.
15	16.7	18.8	26.21
20	22.2	25.8	36.87
25	27.7	32.8	47.53
30	33.3	39.7	58.20
35	38.8	46.7	68.86
40	44.4	53.7	79.53
45	50.0	60.6	90.20
50	55.6	67.6	100.86
55	61.1	74.6	111.53
60	66.7	81.6	122.20
65	72.2	88.6	132.86
70	77.8	95.6	143.53
75	83.3	102.6	154.20
80	88.9	109.6	164.86
85	94.4	116.6	175.53
90	100.0	123.6	186.20
95	105.6	130.6	196.86
100	111.1	137.6	207.53
105	116.7	144.6	218.20
110	122.2	151.6	228.86
115	127.8	158.6	239.53
120	133.3	165.6	250.20
125	138.9	172.6	260.86
130	144.4	179.6	271.53
135	150.0	186.6	282.20
140	155.6	193.6	292.86
145	161.1	200.6	303.53
150	166.7	207.6	314.20

GENERAL NOTES FOR DETECTABLE WARNING DEVICES
 THE DETECTABLE WARNING DEVICES SHALL BE CAST-IN-PLACE
 TRUNCATED DOWNS TO THE FACE OF THE CURB,
 6 TO 8 INCHES FROM THE FACE OF THE CURB.
 GEOMETRIC CONFIGURATION SHOWN.
 DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE
 PREDOMINANT DIRECTION OF TRAVEL TO PERMIT
 DETECTABLE WARNING DEVICES TO BE 24 INCHES
 IN THE DIRECTION OF TRAVEL AND 4 INCHES
 THE FULL WIDTH OF THE CURB RAMP OR FLUSH
 DETECTABLE WARNING DEVICE SHALL BE ON THE AHD
 DUAL-FASE PRODUCTS LIST FOR CAST-IN-PLACE
 TACTILE PANELS (ADA DETECTABLE WARNING).



GENERAL NOTES
 SECTION 105.05 OVERLAP INDICATED ON THE PLANS WHEN CHAIR RAMPS
 IN NEW AREAS TO BE PROVIDED AT CORNERS OF CURBED STREET INTERSECTIONS AND
 MID-BLOCK CROSSWALK LOCATIONS.
 IN ALTERATIONS WHEELCHAIR RAMPS ARE TO BE PROVIDED AT CURBED STREET INTER-
 SECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS.
 THE FINISH SURFACE OF THE RAMP SHALL CONFORM TO A CLASS 6
 FINISH ACCORDING TO SECTION 802.14.
 THE NORMAL GUTTER GRADE SHALL BE MAINTAINED THROUGH THE AREA
 OF THE RAMP.
 ALL DIMENSIONS SHALL BE IN ACCORDANCE WITH THE LATEST
 EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES
 PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION.
 THE MINIMUM WIDTHS OF THE RAMP SHALL BE 4' OR 5',
 WHICHEVER IS GREATER.
 RAMPS SHALL BE MODIFIED AS NECESSARY TO INSURE THAT THEY ARE PARALLEL
 TO A LINE DRAWN FROM THE CENTER OF ONE RAMP TO THE CENTER OF THE
 RAMP ON THE OPPOSITE SIDE OF THE INTERSECTION.
 THE DIMENSIONS AND QUANTITIES SHOWN ARE FOR
 INTERSECTIONS WILL VARY, AND ARE TO BE DETERMINED BY THE ENGINEER.

RAMP SELECTION CRITERIA

FIRST CHOICE	SECOND CHOICE	THIRD CHOICE	FOURTH CHOICE
TYPE 1	TYPE 5	TYPE 4	TYPE 6
TYPE 2	TYPE 6	TYPE 3	TYPE 2
TYPE 3	TYPE 4	TYPE 5	TYPE 6

NOTE:
 IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED
 SHALL BE DETERMINED BY THE ENGINEER, WITH THE WALK OFFSET FROM THE CURB AT DISTANCE INSUFFICIENT
 TO ALLOW THE REQUIRED RAMP SLOPE BOTH NEW CONSTRUCTION AND ALTERATIONS.
 TO ALLOW THE REQUIRED RAMP SLOPE BOTH NEW CONSTRUCTION AND ALTERATIONS.
 TANGENT LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE
 RAMP CANNOT BE PLACED AT THE ENDS OF THE RADIUS.
 IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF ANY OF THE TYPES LISTED,
 PROVIDE ACCESS TO THE STREET LEVEL THROUGH ALTERATIONS ONLY.
 THE SLOPE CAN BE STEEPER TO A MAXIMUM OF 2% FOR A MAXIMUM LENGTH OF 5' OR A 8% MAX.
 FOR A MAXIMUM LENGTH OF 2'. SLOPES STEEPER THAN 8% ARE NOT ALLOWED UNDER ANY
 CIRCUMSTANCES.

APRANKANS STATE HIGHWAY COMMISSION

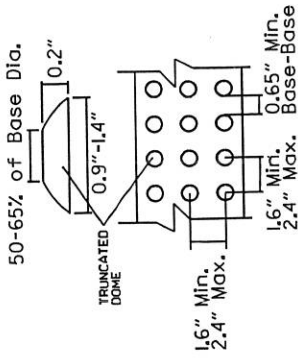
WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS

STANDARD DRAWING WR-1

NO.	DATE	DESCRIPTION
1	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
2	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
3	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
4	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
5	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
6	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
7	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
8	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
9	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
10	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
11	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
12	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
13	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
14	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
15	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
16	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
17	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
18	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
19	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
20	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
21	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
22	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
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46	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
47	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
48	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
49	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP
50	12-20-82	REVISED TO ADD DETAIL FOR 12" MIN. RAMP

GENERAL NOTES FOR DETECTABLE WARNING DEVICES

THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SUCH THAT THE SURFACE OF THE TRUNCATED DOME IS 1/8" TO 1/4" FROM THE FACE OF THE CURB. SURFACE DOME IN THE DETECTABLE WARNING GEOMETRIC CONFIGURATION SHALL MEET THE REQUIREMENTS OF THE DOME GEOMETRIC CONFIGURATION ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES. DETECTABLE WARNING DEVICES SHALL BE 24 INCHES IN LENGTH AND 24 INCHES IN WIDTH. THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE. DETECTABLE WARNING DEVICES SHALL BE ON THE AHEAD DETECTABLE WARNING PRODUCTS LIST FOR CAST-IN-PLACE TACTILE PANELS (ADA DETECTABLE WARNING).



DETECTABLE WARNING DEVICE DETAIL

GENERAL NOTES:

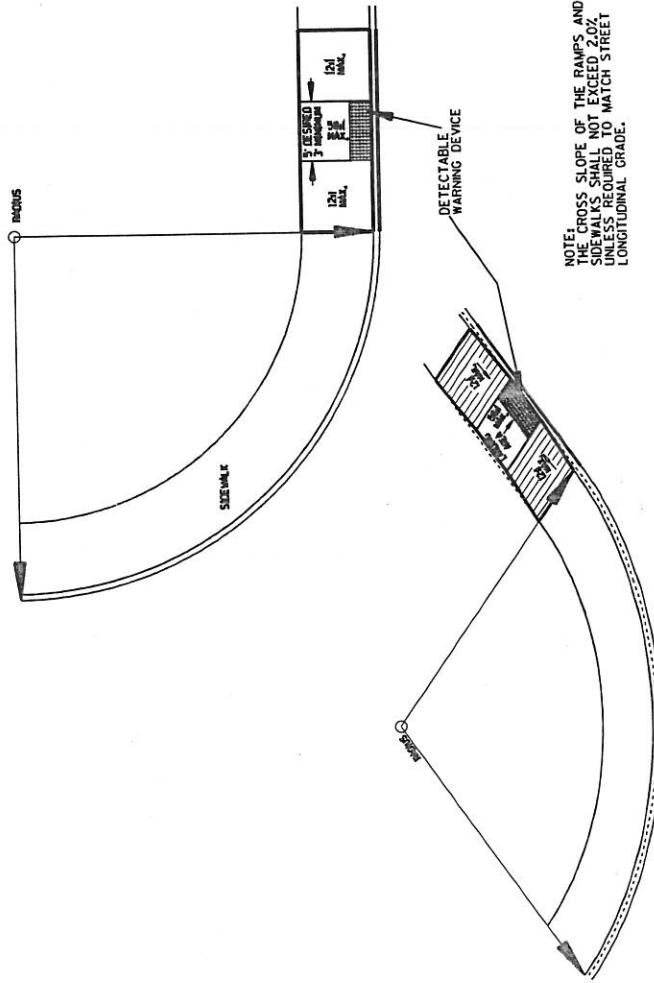
IN ALTERATIONS WHEELCHAIR RAMPS ARE TO BE PROVIDED AT CURBED STREET INTERSECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS. THE LENGTH OF THE RAMP SHALL BE SUCH THAT THE SLOPE DOES NOT EXCEED 1:21. THE SURFACE TEXTURE OF THE RAMP SHALL CONFORM TO A CLASS 6 FINISH ACCORDING TO SECTION 802.01.

THE NORMAL CUTTER GRADE SHALL BE MAINTAINED THROUGH THE AREA.

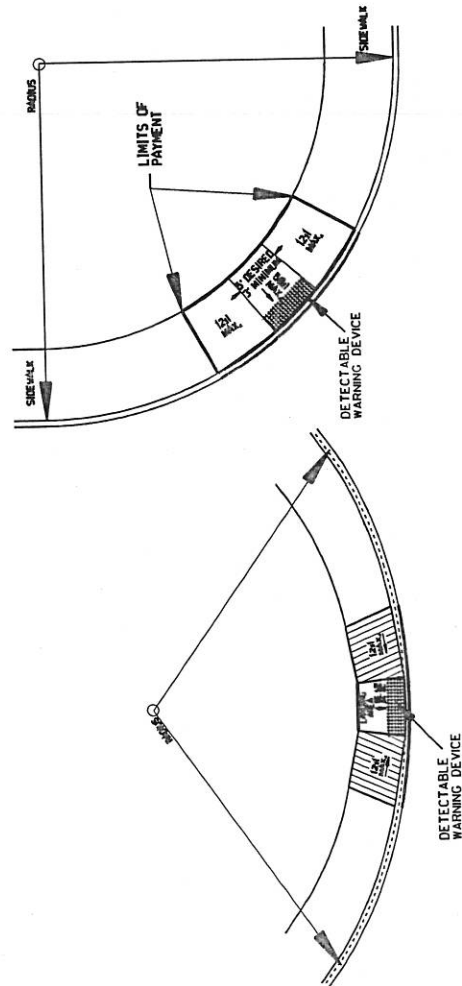
ALL PAVER MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION SHALL BE 4".

THE MINIMUM WIDTH OF THE RAMPS SHALL BE THE EXISTING WALK WIDTH OR 36", WHICHEVER IS GREATER.

MINOR MODIFICATIONS OF THESE DETAILS, AS APPROVED BY THE ENGINEER, MAY BE MADE TO ADJUST TO LOCAL CONDITIONS.



TYPE 5 RAMP



TYPE 6 RAMP

RAMP SELECTION CRITERIA

TYPE 1 CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE INSUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE WITH NEW CONSTRUCTION AND ALTERATIONS.	TYPE 2 CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE SUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE WITH NEW CONSTRUCTION AND ALTERATIONS.	TYPE 3 TANGENT LOCATIONS (BOTH NEW CONSTRUCTION AND ALTERATIONS).	TYPE 4 TANGENT LOCATIONS (ALTERATIONS ONLY).	TYPE 5 CORNER LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE TYPE 6 RAMP CANNOT BE PLACED AT THE ENDS OF THE RAMPS.	TYPE 6 IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF THE TYPE 5 RAMP, THEN ONLY THE TYPE 6 RAMP MAY BE EXCEEDED TO PROVIDE THE REQUIRED RAMP SLOPE. THE STREET LEVEL (ALTERATIONS ONLY) FOR A MAX. LENGTH OF 5' OR A BI-MANUAL FOR A MAX. LENGTH OF 2'. SLOPES STEEPER THAN 8:1 ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.
FIRST CHOICE	FIRST CHOICE	FIRST CHOICE	FIRST CHOICE	FIRST CHOICE	FIRST CHOICE
SECOND CHOICE	SECOND CHOICE	SECOND CHOICE	SECOND CHOICE	SECOND CHOICE	SECOND CHOICE
THIRD CHOICE	THIRD CHOICE	THIRD CHOICE	THIRD CHOICE	THIRD CHOICE	THIRD CHOICE
FOURTH CHOICE	FOURTH CHOICE	FOURTH CHOICE	FOURTH CHOICE	FOURTH CHOICE	FOURTH CHOICE

NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE, AND ON THE PRESENCE OF OTHER SITE CONSTRAINTS (UTILITIES, BUILDINGS, ETC.). THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED.

AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) THAT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. PROJECTS THAT REQUIRE CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.

ARKANSAS STATE HIGHWAY COMMISSION	
WHEELCHAIR RAMPS ALTERATIONS ONLY	
DATE	DATE REV
10-3-03	REVISED GENERAL NOTES
7-30-03	REVISED DETAIL PANEL
8-22-02	ADDED DETECTABLE WARNING
8-28-98	REVISED DETAIL NOTE
7-02-98	ISSUED
DATE	REVISION

DATE: 2017-04-01

ESTIMATE OF MATERIAL AND FORCE ACCOUNT WORK
BY THE
UNION PACIFIC RAILROAD

THIS ESTIMATE GOOD FOR 6 MONTHS EXPIRATION DATE IS :2017-09-30

DESCRIPTION OF WORK:

JONESBORO, AR / PEDESTRIAN XING / DOT#440571H-1 / WYNNE SUB MP 238.44
INSTALL 8' CROSSING SURFACE TO FUNCTION AS A PEDESTRIAN CROSSING,
INCLUDING TIES AND OTM
PROJECT WAS BUILT USING FED ADDITIVE W/ OVERHEAD AND INDIRECT 234%
UPRR WILL BE REIMBURSED FOR 100% OF THE PROJECT COSTS BY CITY/STATE/FED.

FID: 100352 AWO: MP,SUBDIV: 238.42, WYNNE
SERVICE UNIT: 06 CITY: JONESBORO STATE: AR

DESCRIPTION	QTY	UNIT	LABOR	MATERIAL	RECOLL	UPRR	TOTAL
ENGINEERING WORK							
ENGINEERING			640		640		640
FLAGGING			859		859		859
LABOR ADDITIVE 234%			3837		3837		3837
TRACK			140		140		140
TOTAL ENGINEERING			5476		5476		5476
SIGNAL WORK							
LABOR ADDITIVE 234%			1106		1106		1106
MATL STORE EXPENSE				21	21		21
SALES TAX				21	21		21
SIGNAL			644	549	1193		1193
TOTAL SIGNAL			1750	591	2341		2341
TRACK & SURFACE WORK							
BILL PREP FEE				900	900		900
ENVIRONMENTAL PERMIT				10	10		10
FOREIGN LINE FREIGHT				101	101		101
HOMELINE FREIGHT				900	900		900
LABOR ADDITIVE 234%			3340		3340		3340
MATL STORE EXPENSE				28	28		28
OTM			351	60	411		411
RDXING	8.00	TF	415	1789	2204		2204
SALES TAX				100	100		100
XTIE	6.00	EA	1498	683	2181		2181
TOTAL TRACK & SURFACE			5604	4571	10175		10175
LABOR/MATERIAL EXPENSE			12830	5162			
RECOLLECTIBLE/UPRR EXPENSE					17992	0	
ESTIMATED PROJECT COST							17992

THE ABOVE FIGURES ARE ESTIMATES ONLY AND SUBJECT TO FLUCTUATION. IN THE EVENT OF AN INCREASE OR DECREASE IN THE COST OR QUANTITY OF MATERIAL OR LABOR REQUIRED, UPRR WILL BILL FOR ACTUAL CONSTRUCTION COSTS AT THE CURRENT EFFECTIVE RATE.