



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Signature Copy

Resolution: R-EN-051-2019

File Number: RES-19:054

Enactment Number: R-EN-051-2019

A RESOLUTION EXPRESSING THE WILLINGNESS OF THE CITY OF JONESBORO TO UTILIZE STATE AID STREET MONIES FOR THE FOLLOWING CITY PROJECT:
JONESBORO C.W. POST ROAD OVERLAY

WHEREAS, the City of JONESBORO understands that State Aid Street Program funds are available for certain city projects at the following participating ratios:

	<u>Work Phase</u>	<u>State Aid %</u>	<u>City %</u>
Reconstruction/Resurfacing Construction of City Projects	Preliminary Engineering	100%	- 0 -
	Right-of-Way	- 0 -	100%
	Utilities	- 0 -	100%
	Construction	90%	10%
	Construction Engineering	100%	- 0 -
City Projects programmed but not let to contract	All Phases	- 0 -	100%

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF JONESBORO, ARKANSAS, THAT:

SECTION I: The City will participate in accordance with its designated responsibilities in this project.

SECTION II: The Mayor, or his designee, is hereby authorized and directed to execute all appropriate agreements and contracts necessary to expedite the construction of this city project.

SECTION III: The City agrees upon completion of the project to assume the maintenance of the right-of-way by City Forces and/or others including utilities and individuals in accordance with the prevailing Arkansas Department of Transportation regulations.

SECTION IV: The City pledges its full support and hereby authorizes the Arkansas Department of Transportation to initiate action to implement this project.

PASSED AND APPROVED this 7th day of May, 2019.



ARKANSAS DEPARTMENT OF TRANSPORTATION

ArDOT.gov | IDriveArkansas.com | Scott E. Bennett, P.E., Director

STATE AID DIVISION

10324 Interstate 30 | P.O. Box 2261 | Little Rock, AR 72203-2261 | Phone: 501.569.2346 | Fax: 501.569.2348

April 23, 2019

The Honorable Harold Perrin
Mayor of Jonesboro
300 S. Church St.
Jonesboro, Arkansas 72401

Job C16008
Jonesboro C W Post Rd. Overlay
(Sel. Sec.) (S)
C W Post Rd.
Craighead County

Dear Mayor Perrin:

We are enclosing one set of plans on the above subject job.

These plans have been prepared generally in accordance with the items discussed and agreements reached. After review of these plans, please advise as soon as possible if you have any comments or suggestions. Your approval is requested.

In accordance with the State Aid City Street Program law, cities with a population greater than 25,000 are required to pay 10% of the cost of the Street Program project.

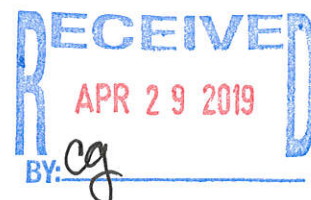
Enclosed is a sample resolution for the referenced job reflecting the required 10% matching City funding. In order to proceed, the resolution is required to be on file with the State Aid Division. Please return this resolution as soon as possible.

When the estimated cost of the project has been determined, we will forward a request for the City's portion of the cost.

Sincerely,

Bryan Freeling
State Aid Engineer

BEF:AF:caa
Enclosures



STATE AID CITY STREET PROGRAM PROJECT RESOLUTION

RESOLUTION NO. 19:054

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FOR THE FOLLOWING CITY PROJECT:

Jonesboro C W Post Rd. Overlay

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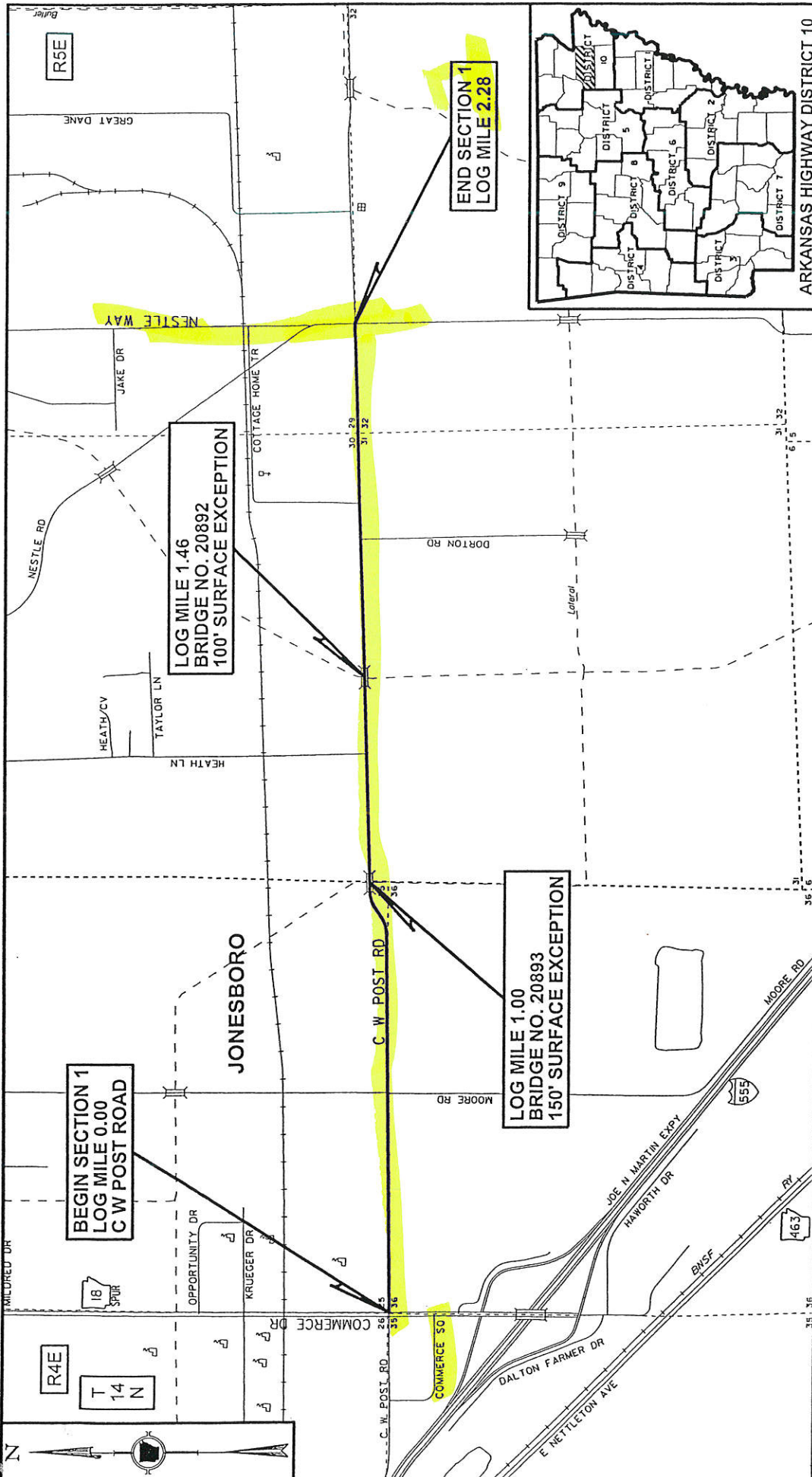
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THIS RESOLUTION adopted this 13th day of May, 2019.

Harold Perrin
Mayor Harold Perrin

ATTEST: April Goggett
(SEAL)



JOB C16008

JONESBORO C W POST RD. OVERLAY (SEL. SEC.) (S)
C W POST RD.
CRAIGHEAD COUNTY



APPROXIMATE CENTER OF SECTION 1
 LATITUDE: N 35° 48' 25"
 LONGITUDE: W 90° 36' 15"

INDEX OF SHEETS

TITLE

SHEET NO.

- 1 _____ TITLE SHEET
- 2 _____ INDEX OF SHEETS AND STANDARD DRAWINGS
- 3 _____ GOVERNING SPECIFICATIONS
- 4 _____ GENERAL NOTES
- 5 _____ TYPICAL SECTIONS OF IMPROVEMENT
- 6 - 9 _____ SPECIAL DETAILS
- 10 - 14 _____ QUANTITIES
- 15 _____ SUMMARY OF QUANTITIES AND REVISIONS

ROADWAY STANDARD DRAWINGS

TITLE

DRWG. NO.

DRWG. NO.	TITLE	DATE
PM-1	PAVEMENT MARKING DETAILS	06-01-17
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	04-13-17
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	09-02-15
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	09-02-15

GOVERNING SPECIFICATIONS

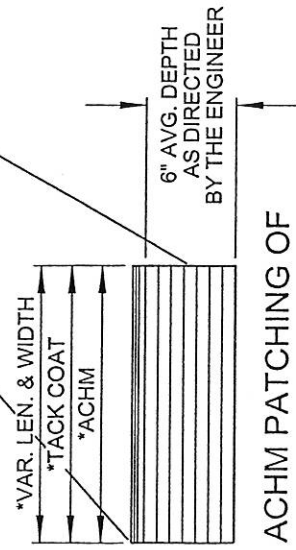
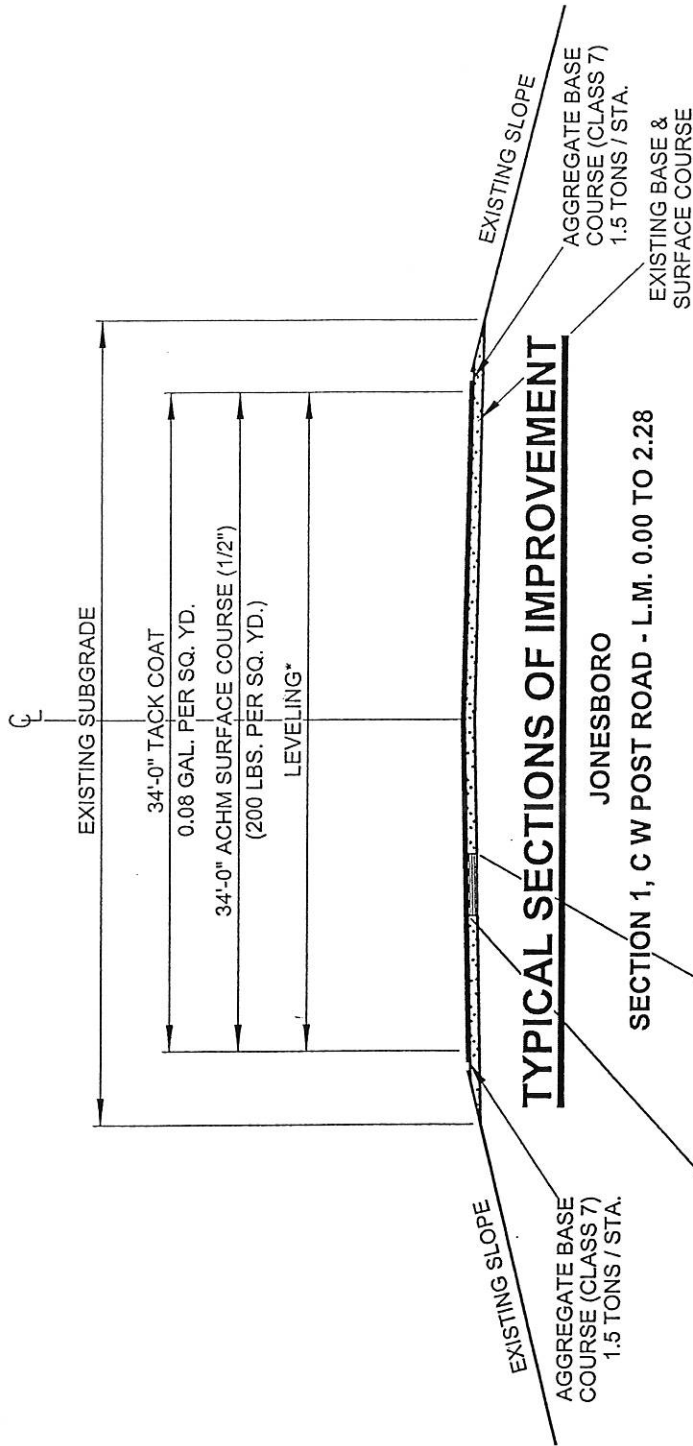
ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION,
EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS.

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
303-1	AGGREGATE BASE COURSE
306-1	QUALITY CONTROL AND ACCEPTANCE
400-1	TACK COATS
400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
400-6	LIQUID ANTI-STRIP ADDITIVE
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
410-2	DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB C16008	ADJUSTMENT OF QUANTITIES FOR STATE-AID STREET PROJECTS
JOB C16008	BIDDING REQUIREMENTS AND CONDITIONS
JOB C16008	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB C16008	DENSITIES FOR ACHM SURFACE COURSE
JOB C16008	FLEXIBLE BEGINNING OF WORK (PAVEMENT PRESERVATION, CITY AID, AND STATE AID PROJECTS)
JOB C16008	MANDATORY ELECTRONIC CONTRACT
JOB C16008	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB C16008	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB C16008	WARM MIX ASPHALT

GENERAL NOTES

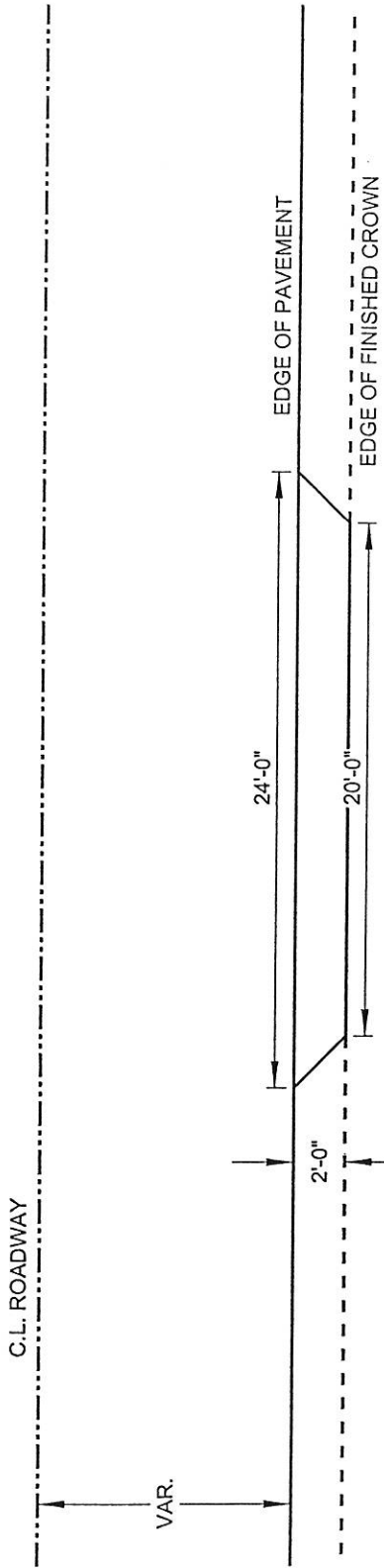
1. THE CONTRACTOR SHALL CLIP GRASS AND DEBRIS FROM THE EDGE OF THE EXISTING ROADWAY. THESE "CLIPPINGS" SHALL BE PULLED UP TO THE EDGE OF THE NEW PAVEMENT IN SECTIONS WHERE THE SHOULDERS ARE NOT BEING REBUILT WITH AGGREGATE OR BORROW. NO DIRECT PAYMENT WILL BE MADE FOR THIS WORK.
2. THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.
3. INSTALLATION OF PERMANENT TRAFFIC SAFETY SIGNS NOT SHOWN IN THE PLANS IS THE RESPONSIBILITY OF THE CITY OF JONESBORO.
4. CONTRACTOR TO COORDINATE WITH THE CITY OF JONESBORO FOR THE ADJUSTMENT OF MANHOLES AND WATER VALVE COVERS.

NOTE: DETAILS MAY BE MODIFIED TO MEET LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



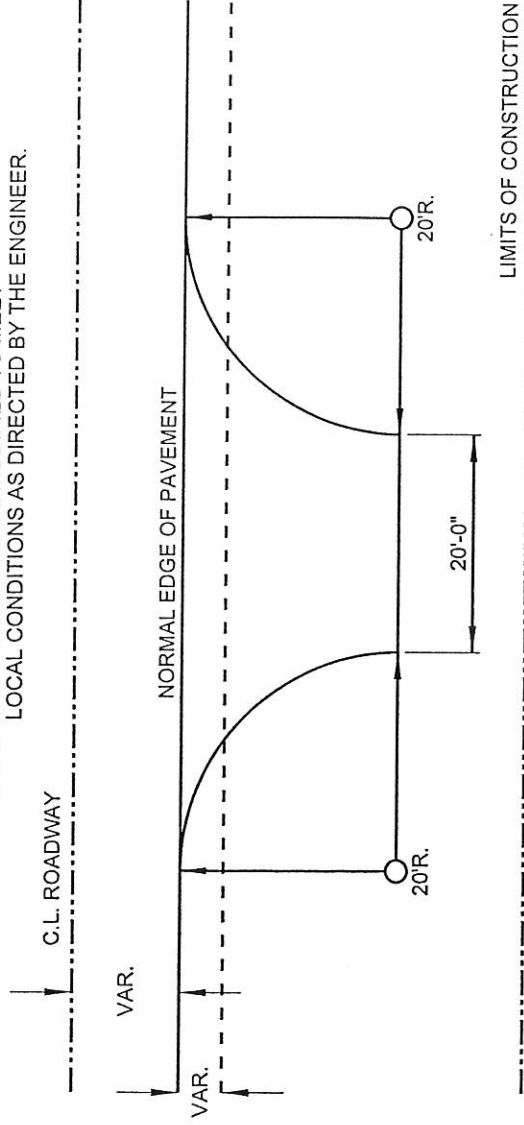
ACHM PATCHING OF EXISTING ROADWAY
 PAY ITEMS AND CONSTRUCTION REQUIREMENTS ARE AS DEFINED IN SECTION 415 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

* LOCATION AND APPLICATION RATES ARE AT THE DIRECTION OF THE ENGINEER. SEE QUANTITY SHEET FOR ESTIMATED AMOUNTS.



TYPICAL PRIVATE ENTRANCE
ADDITIONAL SURFACING AREA = 4.9 SQ. YDS.

NOTE: DIMENSIONS MAY BE MODIFIED TO MEET
 LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



TYPICAL CITY TURNOUT
ADDITIONAL SURFACING AREA = 63.5 SQ. YDS.

FINAL SURFACE

FINAL SURFACE

EXISTING SURFACE

EXISTING SURFACE

**BEGIN SECTION
END SECTION
BEGIN BRIDGE
END BRIDGE**

200 LBS. PER SQ. YD. ACHM

200 LBS. PER SQ. YD. ACHM

MILLED SURFACE

OVERLAY SECTION

MILLING SECTION - 100' TAPER 0" TO 1.82"

PROFILE VIEW

NOT TO SCALE

PAVEMENT TRANSITION DETAILS FOR COLD MILLING

SECTIONS 1

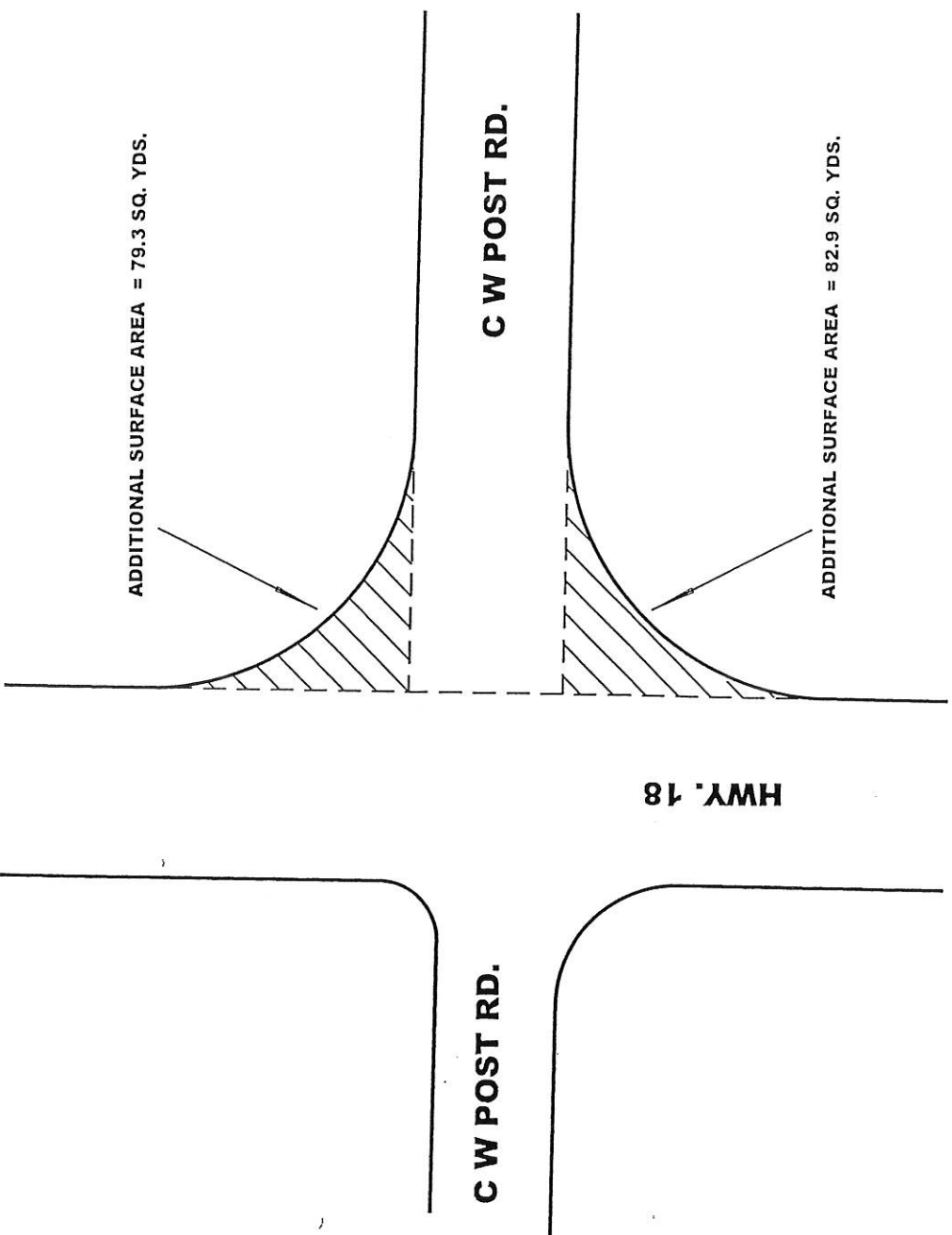
LOCATIONS

- L.M. 0.00 TO 0.02, BEGIN SECTION
- L.M. 0.97 TO 0.99, BEGIN BRIDGE
- L.M. 1.02 TO 1.04, END BRIDGE
- L.M. 1.43 TO L.M. 1.45, BEGIN BRIDGE
- L.M. 1.47 TO L.M. 1.49, END BRIDGE
- L.M. 2.26 TO 2.28, END SECTION

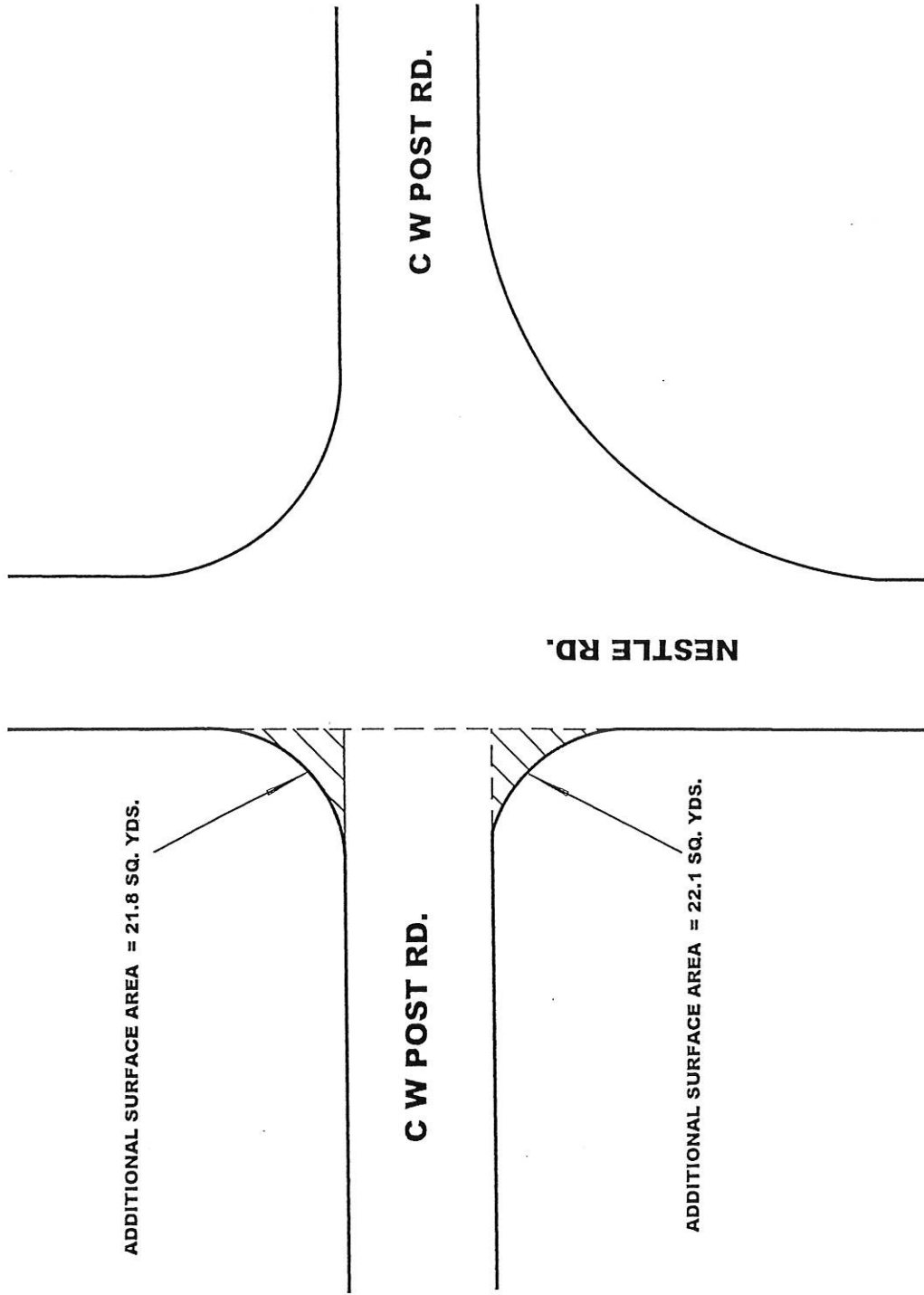
NOTE: DIMENSIONS ARE APPROXIMATED AND MAY BE MODIFIED AS DIRECTED BY THE ENGINEER.

NOTE: FOR COLD MILLING ASPHALT PAVEMENT TRANSITION, TAPER 100' OF FINAL SURFACE TO CREATE A 1.82" PAVING NOTCH, AT VARIOUS LOCATIONS, AS SHOWN ABOVE. OVERLAY MILLED AREAS USING 200 LBS. PER SQ. YD. OF ACHM.

NOTE: MATERIAL PRODUCED FROM COLD MILLING ASPHALT PAVEMENT SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.



TYPICAL CITY TURNOUT AT L.M. 0+00
ADDITIONAL SURFACING AREA = 162.2 SQ. YDS.



ADDITIONAL SURFACE AREA = 21.8 SQ. YDS.

C W POST RD.

ADDITIONAL SURFACE AREA = 22.1 SQ. YDS.

NESTLE RD.

C W POST RD.

TYPICAL CITY TURNOUT AT L.M. 2.28
ADDITIONAL SURFACING AREA = 43.9 SQ. YDS.

ACHM PATCHING OF EXISTING ROADWAY

LOCATION	LOG MILE		LENGTH LIN. FT.	ACHM PATCHING OF EXISTING ROADWAY* TON
	FROM	TO		
SECTION 1 - C W POST ROAD				
MAIN LANES	0.00	2.28	12038	100.0
TOTAL:				100.0
USE:				100

*QUANTITY ESTIMATED. TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER. SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

REFLECTORIZED PAINT PAVEMENT MARKING

DESCRIPTION	LOG MILE		YELLOW MARKING 4" LIN. FT.	WHITE MARKING 12" LIN. FT.
	FROM	TO		
SECTION 1 - C W POST ROAD				
CONTINUOUS	0.00	0.21	1109	
SKIP	0.00	0.81	1070	
CONTINUOUS	0.62	0.81	1003	
DOUBLE	0.81	1.03	2324	
CONTINUOUS	1.03	1.23	1056	
SKIP	1.03	2.28	1650	
CONTINUOUS	2.12	2.28	845	
STOP LINE	0.00	0.00		18
STOP LINE	2.28	2.28		18
TOTALS:			9057	36

THIS IS A LOW VOLUME ROAD AS DEFINED IN SECTION 604.03 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

SUMMARY OF QUANTITIES

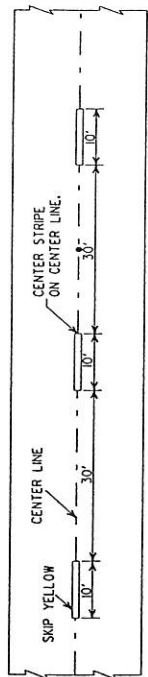
I T E M

ITEM NO.	I T E M	TOTAL QUANTITY	UNIT
SS & 303	AGGREGATE BASE COURSE (CLASS 7)	354	TON
SS & 401	TACK COAT	4289	GAL.
SP, SS, & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	4718	TON
SP, SS, & 407	ASPHALT BINDER (PG 76-22) IN ACHM SURFACE COURSE (1/2")	254	TON
412	COLD MILLING ASPHALT PAVEMENT	2473	SQ. YD.
SP, SS, & 415	ACHM PATCHING OF EXISTING ROADWAY	100	TON
601	MOBILIZATION	1.00	LUMP SUM
603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	36	SQ. FT.
719	THERMOPLASTIC PAVEMENT MARKING WHITE (12")	36	LIN. FT.
719	THERMOPLASTIC PAVEMENT MARKING YELLOW (4")	9057	LIN. FT.

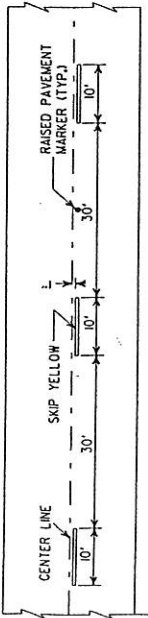
REVISIONS

DATE	REVISION	SHEET NO.

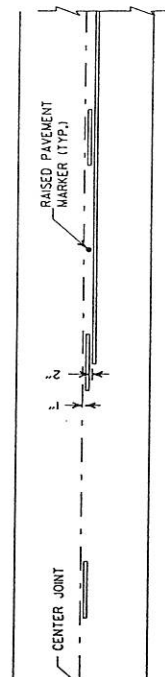
- NOTES:
1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
 2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
 3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.



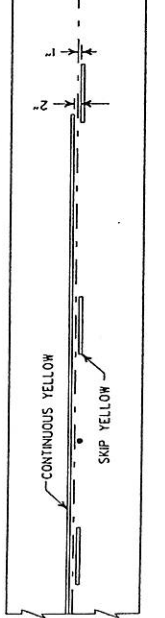
ASPHALT PAVEMENT



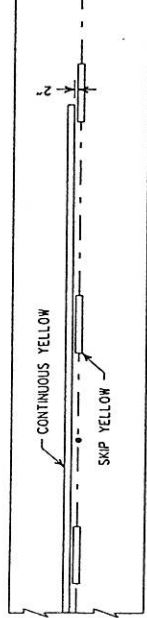
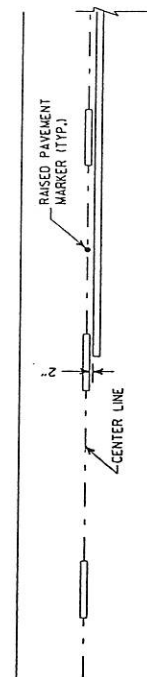
CONCRETE PAVEMENT



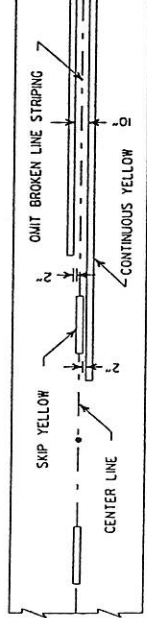
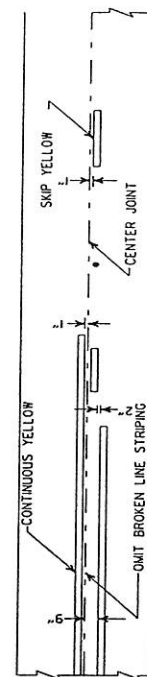
BROKEN LINE STRIPING



SOLID LINE STRIPING ON CONCRETE PAVEMENT



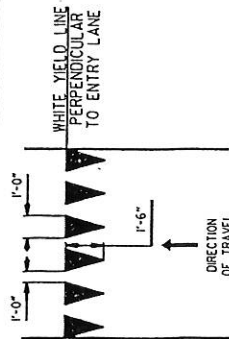
SOLID LINE STRIPING ON ASPHALT PAVEMENT



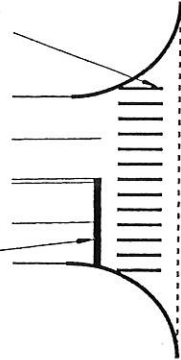
STRIPING AT ADJACENT NO PASSING LANES

12" STOPBAR
OFFSET STOPBAR 4'
FROM CROSSWALK

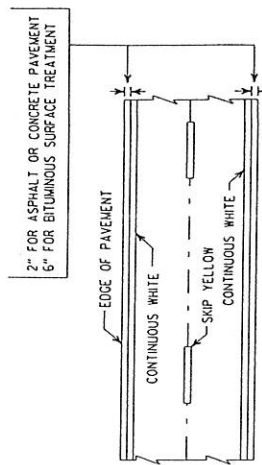
12" CROSSWALK STRIPES
10 FT. WIDE - PLACED 4 FT. O.C.
OFFSET NEAR EDGE OF CROSSWALK
3 FT. MIN. FROM LINE EDGE



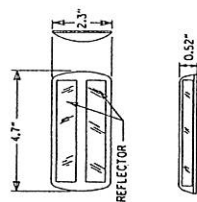
YIELD LINE DETAIL



CROSSWALK AND STOPBAR DETAILS



PAVEMENT EDGE LINE MARKING



NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR SHALL VERIFY THE MARKER DIMENSIONS AND OBTAIN THE APPROVAL OF THE ENGINEER. REQUESTING THE APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

DETAIL OF STANDARD RAISED PAVEMENT MARKERS

DATE	REVISION	FILED
6-11-17	ADDED YIELD LINE DETAIL	
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REMOVED PLOTTABLE PAINT MARKERS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DETAILS	
7-02-98	ADDED REFLECTOR DETAIL OF STD. RAISED PAVT. MARKERS	
4-25-96	REV. NOTES 3&4. ADDED R.P.M.	
9-30-80	DRAWN	
1-9-30-80	REVISION	

ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

STANDARD DRAWING PM-1

ADVANCE DISTANCES	
500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE
2000 FT	1 1/4 MILE
3000 FT	2 MILE

GENERAL NOTES:
 1. ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.

2. TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION AND REMOVED IMMEDIATELY AFTER THE WORK IS COMPLETED. IF DEVICES EXIST, THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
 3. EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE REPAIRED OR REPLACED IMMEDIATELY AFTER CONSTRUCTION IS COMPLETED. DURING CONSTRUCTION SIGNS SHALL BE CLEANED, REPAIRED, OR REPLACED.
 4. SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN TO 50 FT SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRIQUADE.
 5. SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4" X 4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED AND SHALL BE REPLACED, CLEANED, OR REPAIRED AS NEEDED DURING THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A TYPE III FOR PORTABLE SIGNS OR MORE THAN 4 POSTS IN ANY CHANNEL POST SPACE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.

















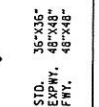
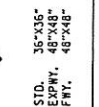
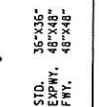
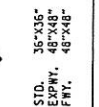
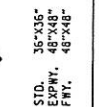
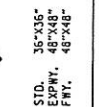
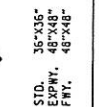
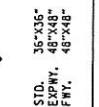
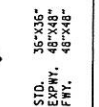
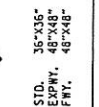
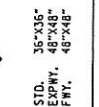
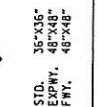
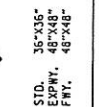
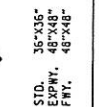
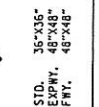
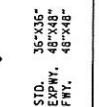
6. PORT MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FACING THE ROAD AND BE MOUNTED TO A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
 7. ALL POST AND BARRIQUADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRIQUADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. EXCEPT IN URBAN AREAS, PORT MOUNTED SIGNS SHALL BE MOUNTED ON PORTABLE SUPPORTS AT AN INTERMEDIATE TERM STATIONARY WORK SITE. PORTABLE SIGNS SHALL BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL UNLESS CONDITIONS PERMIT. PORTABLE SIGNS OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BILST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.

8. FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW SITUATIONS.
 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
 10. R55-SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK. THE SIGN SPEED LIMIT REDUCTION IS IN EFFECT. THE SIGN SPEED LIMIT REDUCTION IS IN EFFECT IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

* NOTE: SUPPORTS FOR SIGNS, BARRIQUADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT WHICH MEET THE REQUIREMENTS OF THIS DRAWING OR MANUAL FOR ASSESSING SAFETY HAZARDOUS (HAZ) - WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NC-HP-350 OR MANUAL FOR ASSESSING SAFETY HAZARDOUS (HAZ) IS REQUIRED FOR ALL PROJECTS.

K-017	DELETED SIGN & ADDED W1-50
K-22-1	REVISED SPEED LIMIT AHEAD SIGNS
K-26-1	REVISED SIGN & WORK NEXT 1/4 MILE
K-30-1	DELETED SIGN & ADDED W3-3
K-35-1	ADDED REFERENCE TO MARK & ADDED SIGN R2-4
K-40-1	REVISED SIGN DESIGNATIONS
K-45-1	REVISED NOTE 1
K-46-1	REVISED NOTE 7
K-47-1	REVISED NOTE
K-50-1	REVISED NOTE 5
K-51-1	REVISED NOTE 5
K-52-1	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7
K-53-1	ADDED R55-1
K-54-1	REVISED TO CORRECT SIGN ILLUSTRATIONS
K-55-1	REVISED PER PART 1, MATED SEPT. 2, 1993
K-56-1	DRAWN AND PLACED IN USE
	REVISION
DATE	FILED

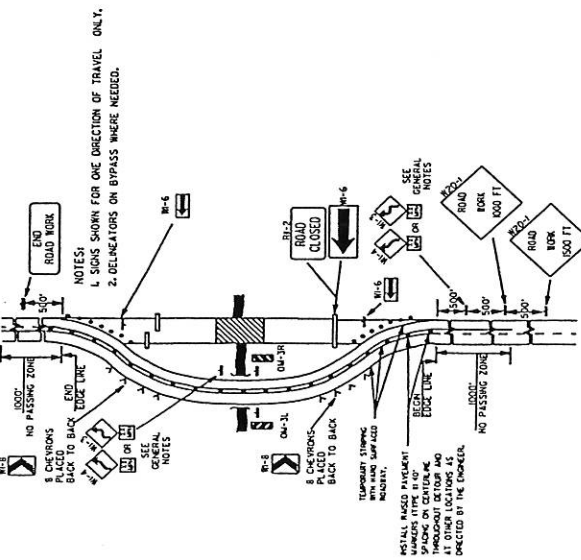
ARKANSAS STATE HIGHWAY COMMISSION
 STANDARD TRAFFIC CONTROL
 FOR HIGHWAY CONSTRUCTION
 STANDARD DRAWING TC-1

R4-1  STD. 24"x30" EXPMY. 36"x48" FWY. 48"x60"	W3-50  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	R4-2  STD. 24"x30" EXPMY. 36"x48" FWY. 48"x60"	W3-5  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	R2-1  STD. 24"x30" EXPMY. 36"x48" FWY. 48"x60"	R1-2  STD. 36"x36"x36" EXPMY. 48"x48"x48" FWY. 60"x60"x60"	R4-1  STD. 24"x30" EXPMY. 36"x48" FWY. 48"x60"	W3-50  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	R4-2  STD. 24"x30" EXPMY. 36"x48" FWY. 48"x60"	W3-5  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	R2-1  STD. 24"x30" EXPMY. 36"x48" FWY. 48"x60"	R1-2  STD. 36"x36"x36" EXPMY. 48"x48"x48" FWY. 60"x60"x60"	W3-50  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	W3-50  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	W3-50  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"	W3-50  STD. 36"x36" EXPMY. 48"x48" FWY. 48"x60"
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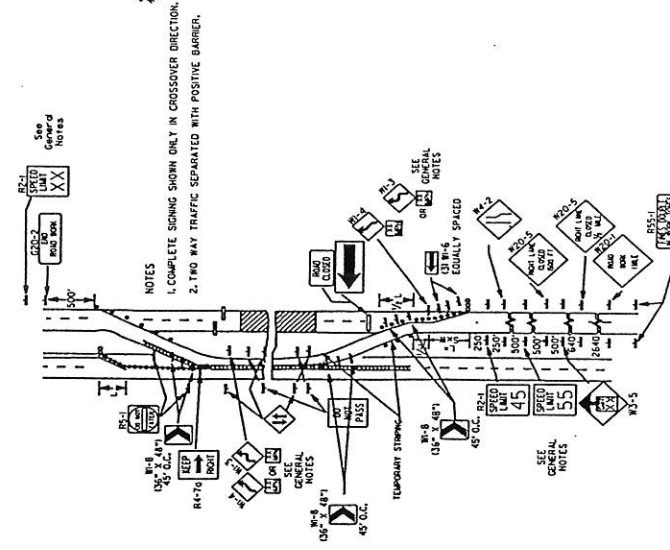


R55-1
FINES DOUBLE
IN WORK ZONES
WHEN WORKERS
ARE PRESENT
36"x60"

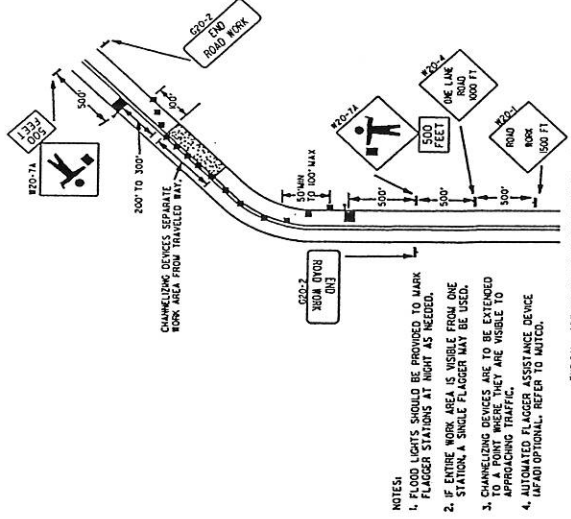
* USE 6" C LETTERS
 ** USE 4" D LETTERS



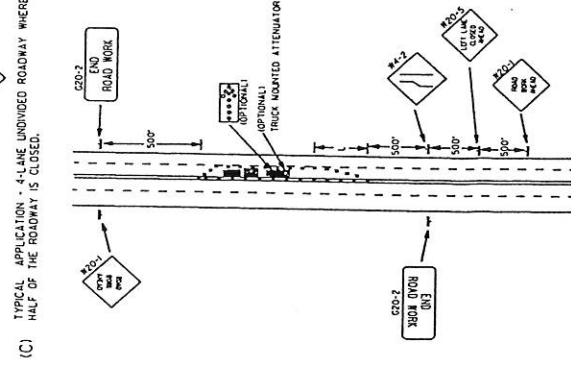
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



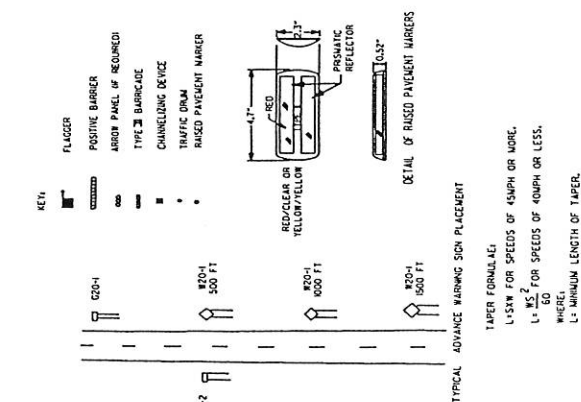
(B) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



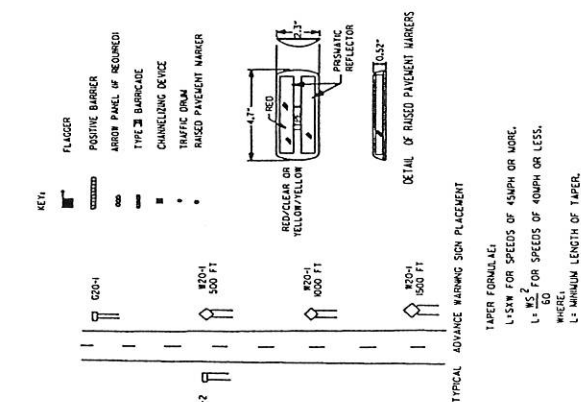
(C) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 4-LANE UNDIVIDED ROADWAY WHERE ONE HALF OF THE ROADWAY IS CLOSED.



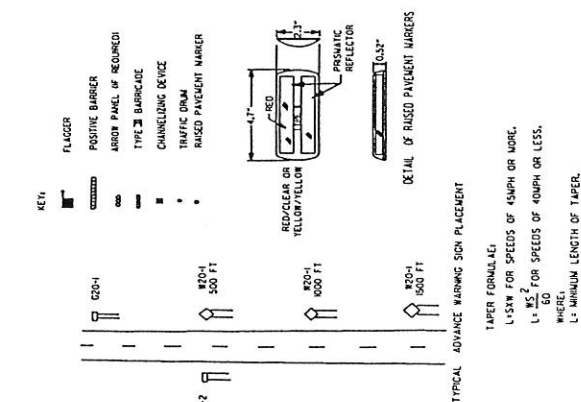
(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



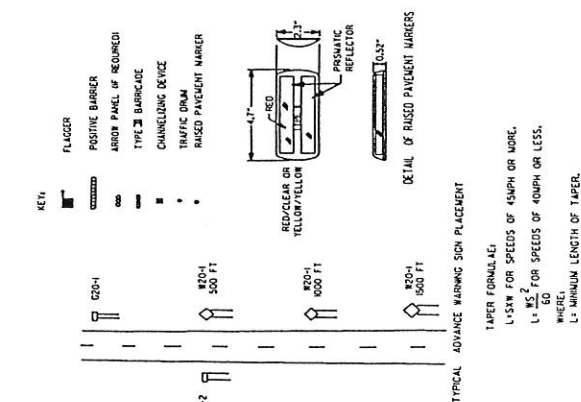
(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.



(F) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

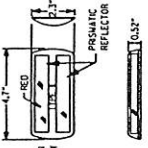


(G) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.



(H) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:**
- FLASHER
 - POSITIVE BARRIER
 - ARROW PANEL OF REQUIRED
 - TYPE B BARRICADE
 - CHANNELING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



DETAIL OF RAISED PAVEMENT MARKERS

TYPICAL ADVANCE WARNING SIGN PLACEMENT

- TAPER FORMULAE:**
- L = SW FOR SPEEDS OF 45MPH OR MORE.
 - L = W² FOR SPEEDS OF 40MPH OR LESS.
- WHERE:
- L = MINIMUM LENGTH OF TAPER.
 - S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85% PERCENTILE SPEED.
 - W = WIDTH OF OFFSET.

- GENERAL NOTES:**
1. ADVISORY SPEED POSTED ON W-3 OR W-4 CURVE WARNING SIGNS IS TO BE USED ONLY WHEN THE POSTED SPEED IS GREATER THAN 30MPH AND W-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 45MPH AND THE PLANS OMITTED AND A SPEED LIMIT OF 45MPH, THE R-2-55 SHALL BE INSTALLED. ADDITIONAL R-2-55MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT THE END OF THE WORK AREA A R-2-55 SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 60MPH AND THE PLANS OMITTED AND A SPEED LIMIT OF 60MPH, THE R-2-55MPH ADDITIONAL R-2-55MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R-2-55 SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN SIGNS SHALL BE 1/4 MILE BEYOND THE TAPER. MAXIMUM SPACING SHALL BE TWO TIMES THE TAPER LENGTH.
 5. MASHING LIGHTS OR AS DIRECTED BY THE ENGINEER.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. CHANNELING DEVICES SUCH AS ARROW PANELS AND PORTABLE CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE ROADWAY SHOULD BE INSTALLED TO MATCH ORIGINAL SPACING. PLACING FIVE (5) TRAFFIC DRUMS EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
 8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. DIMENSIONS FOR RAISED PAVEMENT MARKERS MAY BE MADE BY REFERRING TO THE AUTO QUALIFIED PRODUCTS LIST.

REVISION	DATE	BY	CHKD	APP'D
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ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-2

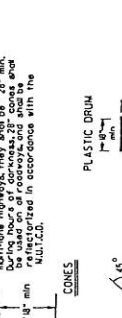
Channelizing devices

TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

TRAFFIC CONTROL LOCATIONS

1" to 3" Centerline, lane lines
1" to 3" Edge of shoulder
Greater than 3" Edge of traveled lane
Greater than 3" Edge of shoulder

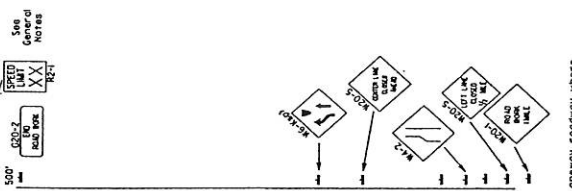
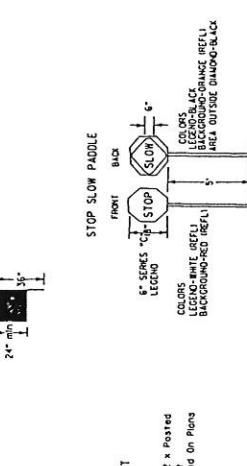
WB-9 Standard lane closure required
WB-9 Standard lane closure required
WB-9 Standard lane closure required
WB-9 Standard lane closure required



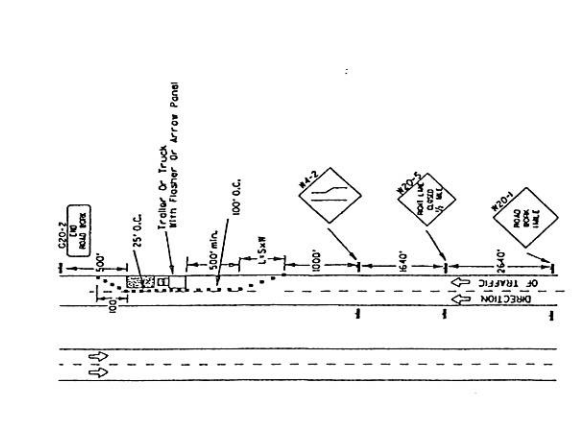
Multi-opening devices are used on freeways during hours of congestion. Cones and drums are reflected in accordance with the MUTCD.

NOTE: When shown on the plans concrete barrier will be used.

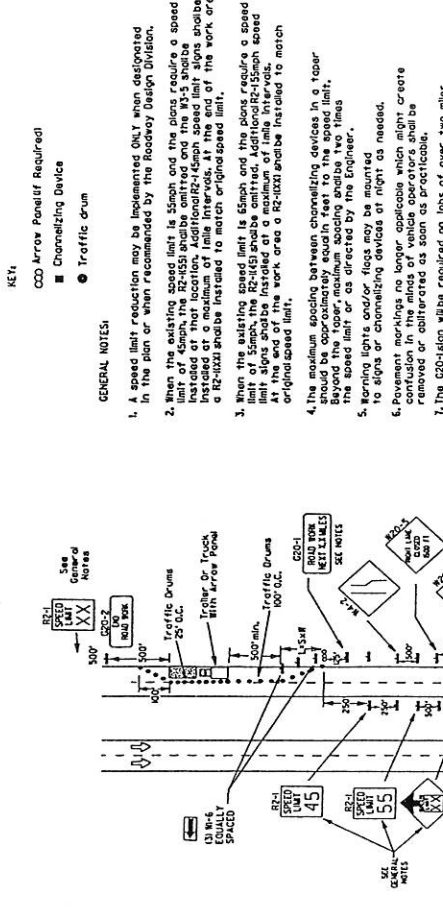
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



(B) Typical application - 3-lane one-way roadway where center lane is closed.



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



DATE	REVISION	REVISION
8-7-75	REVISED NOTE 2 & REPLACED R2-1 WITH R2-15	REVISION
8-22-88	ADDED REFERENCE TO WASH	REVISION
1-28-94	ADDED REFERENCE TO WASH	REVISION
1-28-94	ADDED NOTE	REVISION
10-11-98	ADDED UPTO WB-9 REVISED TRAFFIC CONTROL DEVICES NOTE	REVISION
8-22-93	ADDED UPTO WB-9 REVISED TRAFFIC CONTROL DEVICES NOTE	REVISION
6-8-93	REVISED PER PART 1A, UNCL. TEXT	REVISION
7-2-95	REVISED PER PART 1A, UNCL. SEPT. 3, 1993	REVISION
8-25-99	DRAWN AND PLACED IN USE	REVISION

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROL
FOR HIGHWAY CONSTRUCTION
TC-3
STANDARD DRAWING

- KEY:
- CCD Arrow Panel Required
 - Channelizing Device
 - Traffic Drum
- GENERAL NOTES:
- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
 - When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 45mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 30mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 25mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 20mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 15mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 10mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 5mph, the R2-15 shall be omitted and the W-5 shall be omitted.
 - When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 45mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 30mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 25mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 20mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 15mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 10mph, the R2-15 shall be omitted and the W-5 shall be omitted. If the existing speed limit is 5mph, the R2-15 shall be omitted and the W-5 shall be omitted.
 - The maximum spacing between channelizing devices in a taper shall be approximately equal in feet to the speed limit.
 - At the beginning of the project, the speed limit or as directed by the Engineer.
 - Warning lights on or fog may be mounted.
 - At night or in conditions of low visibility, the use of flashing lights on or fog may be mounted.
 - Pavement markings no longer applicable when they might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
 - The CD-15 shall be required on jobs of over two miles in length. The CD-15 shall be required at the beginning of the project, the CD-15 shall be required at the beginning of the project, the CD-15 shall be required at the beginning of the project. Additional W20-W24 signs are not required in advance of lane closures that begin inside the project limits.
 - Flagger shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
 - Plastic drums and cones shall meet the requirements of NCEP-350 or MASH-1995. Traffic drums shall meet the requirements of NCEP-350 or MASH-1995. Traffic drums shall meet the requirements of NCEP-350 or MASH-1995.
 - Cones shall be spaced on the face of the trolley. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be spaced at 100' intervals. Traffic drums shall be spaced along the traffic side of the device.

(D) Typical application - closing multiple lanes of a multilane highway.

STATE AID CITY STREET PROGRAM PROJECT RESOLUTION

RESOLUTION NO. _____

A RESOLUTION EXPRESSING THE WILLINGNESS OF
THE CITY OF JONESBORO
TO UTILIZE STATE AID STREET MONIES
FOR THE FOLLOWING CITY PROJECT:

Jonesboro C W Post Rd. Overlay

WHEREAS, the City of JONESBORO understands that State Aid Street Program funds are available for certain city projects at the following participating ratios:

	<u>Work Phase</u>	<u>State Aid %</u>	<u>City %</u>
Reconstruction/Resurfacing Construction of City Projects	Preliminary Engineering	100%	-0-
	Right-of-Way	-0-	100%
	Utilities	-0-	100%
	Construction	90%	10%
	Construction Engineering	100%	-0-
City projects programmed but not let to contract	All Phases	-0-	100%

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF
JONESBORO, ARKANSAS, THAT:

SECTION I: The City will participate in accordance with its designated responsibilities in this project.

SECTION II: The Mayor, or his designee, is hereby authorized and directed to execute all appropriate agreements and contracts necessary to expedite the construction of this city project.

SECTION III: The City agrees upon completion of the project to assume the maintenance of the right-of-way by City Forces and/or others including utilities and individuals in accordance with the prevailing Arkansas Department of Transportation regulations.

SECTION IV: The City pledges its full support and hereby authorizes the Arkansas Department of Transportation to initiate action to implement this project.

THIS RESOLUTION adopted this _____ day of _____, 2019.

Mayor Harold Perrin

ATTEST: _____

(SEAL)