

# COMMERCIAL SITE DEVELOPMENT PLANS

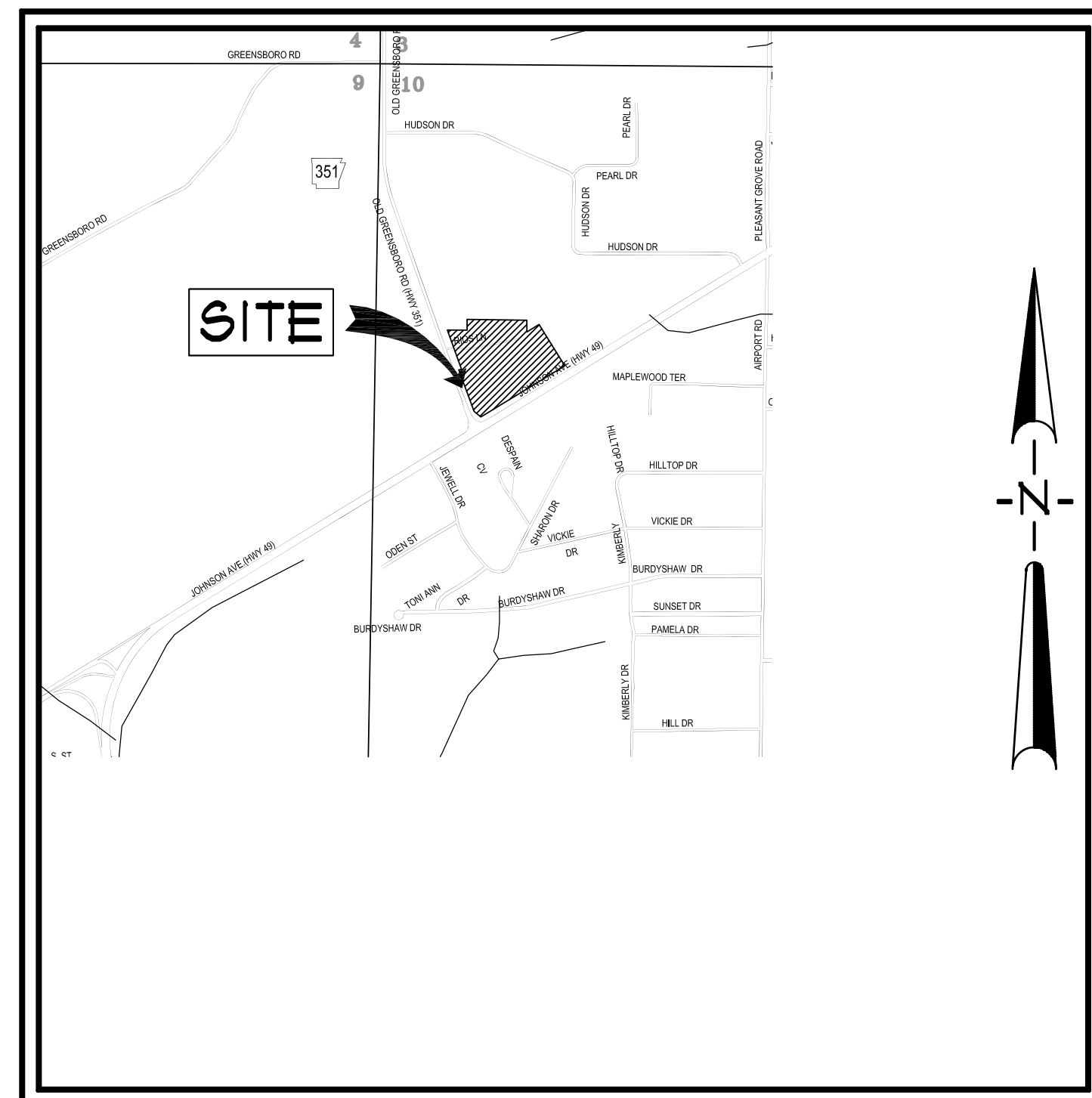
# FIRST NATIONAL BANK

3500 EAST JOHNSON AVENUE

PREPARED FOR  
FIRST NATIONAL BANK

JONESBORO, ARKANSAS  
JUNE, 2016

Sidewalks Required



VICINITY MAP

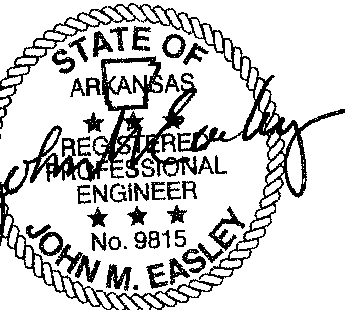
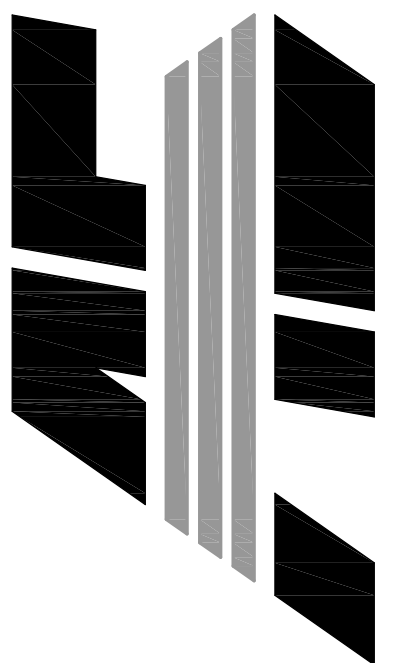
SCALE: 1" = 1000'

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FIRST NATIONAL BANK  
3500 EAST JOHNSON AVENUE  
HWY. 49 & HWY. 351  
JONESBORO, ARKANSAS

ASSOCIATED  
ENGINEERING, LLC  
CIVIL ENGINEERING • LAND SURVEYING  
LAND PLANNING  
103 SOUTH CHURCH STREET • P.O. BOX 1462  
JONESBORO, AR 72403  
PH: 870-932-3594 • FAX: 870-935-1263

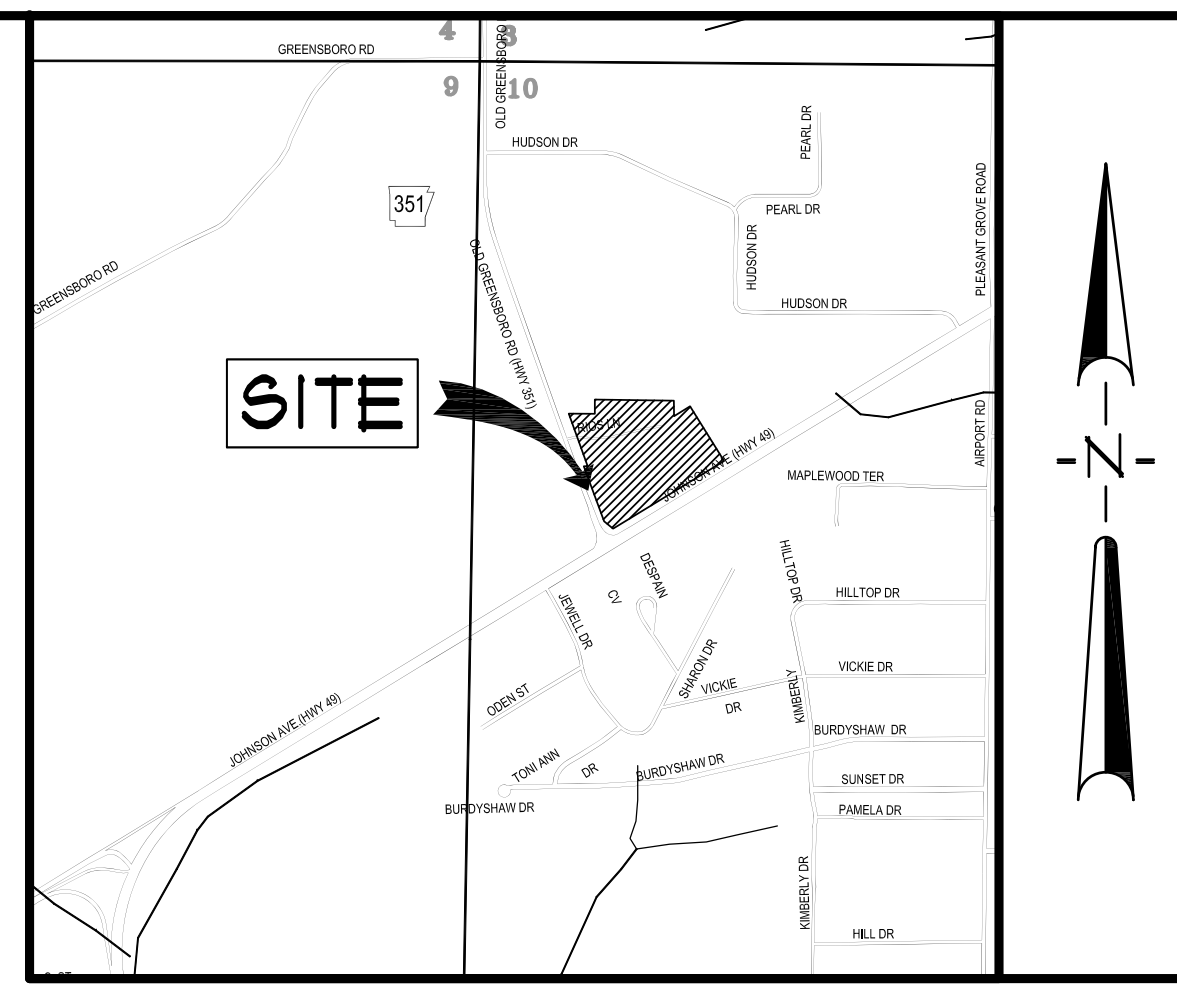
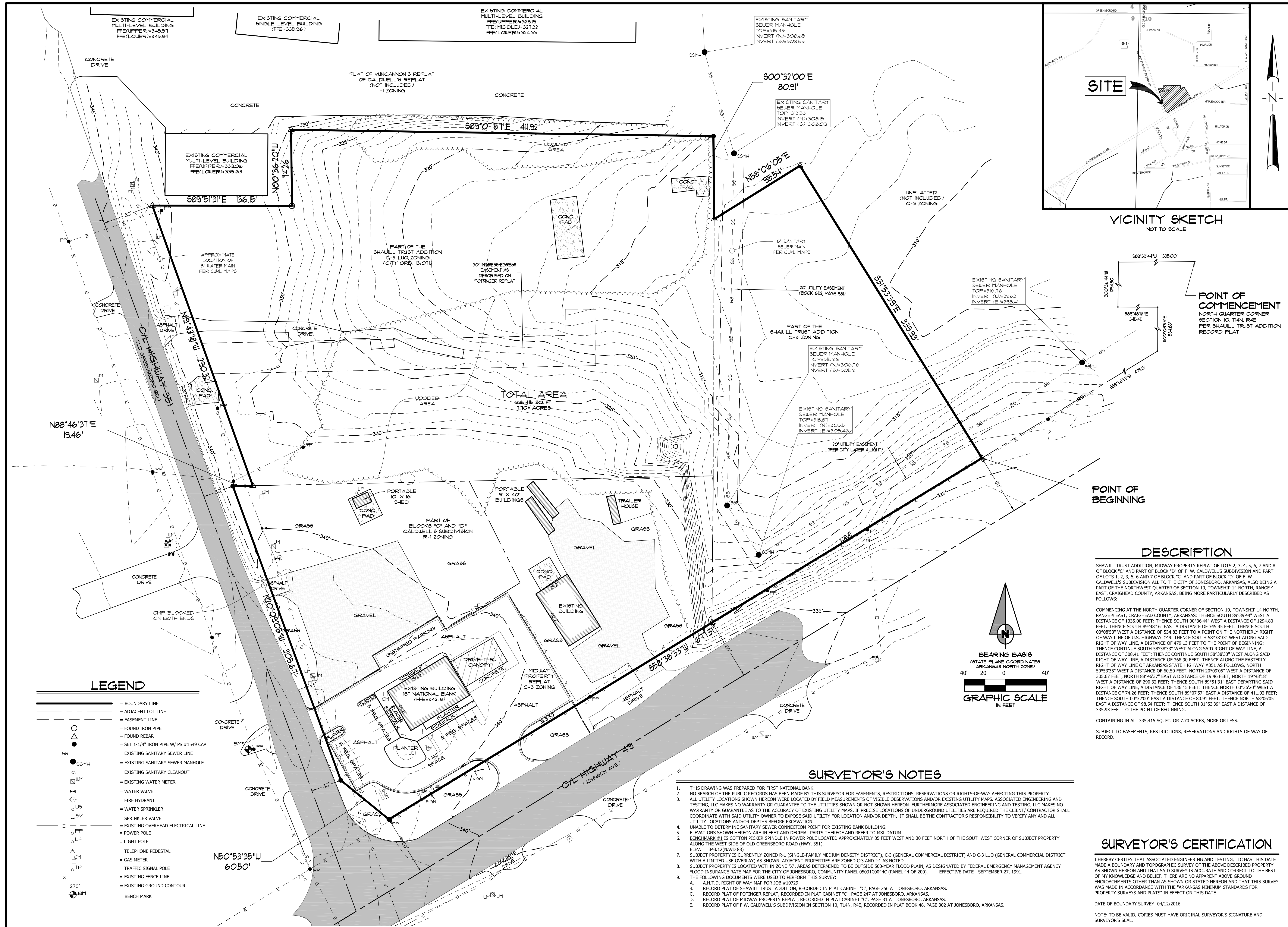


NO.	DESCRIPTION	DATE
1	ADEQ COMMENTS	07/13/16

INDEX SHEET

DATE: 06/27/16 DRAIN: C01  
CADD FILE: 15165-SDP CHECKED: JME  
DWG#: 0414104.000X SHEET  
SCALE: 1" = 40' C001





**POINT OF COMMENCEMENT**  
 NORTH QUARTER CORNER SECTION 10, T14N, R4E PER SHAWILL TRUST ADDITION RECORD PLAT

**POINT OF BEGINNING**

**DESCRIPTION**  
 SHAWILL TRUST ADDITION, MIDWAY PROPERTY REPLAT OF LOTS 2, 3, 4, 5, 6, 7 AND 8 OF BLOCK "C" AND PART OF BLOCK "D" OF F. W. CALDWELL'S SUBDIVISION AND PART OF LOTS 1, 2, 3, 5, 6 AND 7 OF BLOCK "C" AND PART OF BLOCK "D" OF F. W. CALDWELL'S SUBDIVISION ALL TO THE CITY OF JONESBORO, ARKANSAS, ALSO BEING A PART OF THE NORTHWEST QUARTER OF SECTION 10, TOWNSHIP 14 NORTH, RANGE 4 EAST, CRAIGHEAD COUNTY, ARKANSAS, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:  
 COMMENCING AT THE NORTH QUARTER CORNER OF SECTION 10, TOWNSHIP 14 NORTH, RANGE 4 EAST, CRAIGHEAD COUNTY, ARKANSAS; THENCE SOUTH 89°39'44" WEST A DISTANCE OF 1335.00 FEET; THENCE SOUTH 00°30'44" WEST A DISTANCE OF 1294.80 FEET; THENCE SOUTH 89°48'10" EAST A DISTANCE OF 345.45 FEET; THENCE SOUTH 00°08'53" WEST A DISTANCE OF 534.83 FEET TO A POINT ON THE NORTHERLY RIGHT OF WAY LINE OF U.S. HIGHWAY #49; THENCE SOUTH 58°38'33" WEST ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 479.13 FEET TO THE POINT OF BEGINNING; THENCE CONTINUE SOUTH 58°38'33" WEST ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 308.41 FEET; THENCE CONTINUE SOUTH 58°38'33" WEST ALONG SAID RIGHT OF WAY LINE, A DISTANCE OF 368.90 FEET; THENCE ALONG THE EASTERLY RIGHT OF WAY LINE OF ARKANSAS STATE HIGHWAY #351 AS FOLLOWS, NORTH 50°53'35" WEST A DISTANCE OF 60.50 FEET, NORTH 20°09'05" WEST A DISTANCE OF 305.67 FEET, NORTH 88°46'37" EAST A DISTANCE OF 19.46 FEET, NORTH 19°43'18" WEST A DISTANCE OF 290.32 FEET; THENCE SOUTH 89°51'31" EAST DEPARTING SAID RIGHT OF WAY LINE, A DISTANCE OF 136.15 FEET; THENCE NORTH 00°36'20" WEST A DISTANCE OF 74.26 FEET; THENCE SOUTH 89°07'57" EAST A DISTANCE OF 411.92 FEET; THENCE SOUTH 09°32'00" EAST A DISTANCE OF 80.91 FEET; THENCE NORTH 58°06'05" EAST A DISTANCE OF 98.54 FEET; THENCE SOUTH 51°53'39" EAST A DISTANCE OF 335.93 FEET TO THE POINT OF BEGINNING.

CONTAINING IN ALL 335,415 SQ. FT. OR 7.70 ACRES, MORE OR LESS.  
 SUBJECT TO EASEMENTS, RESTRICTIONS, RESERVATIONS AND RIGHTS-OF-WAY OF RECORD.

**BEARING BASIS**  
 (STATE PLANE COORDINATES ARKANSAS NORTH ZONE)

**GRAPHIC SCALE**  
 IN FEET

**SURVEYOR'S NOTES**

- THIS DRAWING WAS PREPARED FOR FIRST NATIONAL BANK.
- NO SEARCH OF THE PUBLIC RECORDS HAS BEEN MADE BY THIS SURVEYOR FOR EASEMENTS, RESTRICTIONS, RESERVATIONS OR RIGHTS-OF-WAY AFFECTING THIS PROPERTY.
- ALL UTILITY LOCATIONS SHOWN HEREON WERE LOCATED BY FIELD MEASUREMENTS OF VISIBLE OBSERVATIONS AND/OR EXISTING UTILITY MAPS. ASSOCIATED ENGINEERING AND TESTING, LLC MAKES NO WARRANTY OR GUARANTEE TO THE UTILITIES SHOWN OR NOT SHOWN HEREON. FURTHERMORE ASSOCIATED ENGINEERING AND TESTING, LLC MAKES NO WARRANTY OR GUARANTEE AS TO THE ACCURACY OF EXISTING UTILITY MAPS. IF PRECISE LOCATIONS OF UNDERGROUND UTILITIES ARE REQUIRED THE CLIENT/ CONTRACTOR SHALL COORDINATE WITH SAID UTILITY OWNER TO EXPOSE SAID UTILITY FOR LOCATION AND/OR DEPTH. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ANY AND ALL UTILITY LOCATIONS AND/OR DEPTHS BEFORE EXCAVATION.
- UNABLE TO DETERMINE SANITARY SEWER CONNECTION POINT FOR EXISTING BANK BUILDING.
- ELEVATIONS SHOWN HEREON ARE IN FEET AND DECIMAL PARTS THEREOF AND REFER TO MSL DATUM.
- BENCHMARK #1 IS COTTON PICKER SPINDLE IN POWER POLE LOCATED APPROXIMATELY 85 FEET WEST AND 30 FEET NORTH OF THE SOUTHWEST CORNER OF SUBJECT PROPERTY ALONG THE WEST SIDE OF OLD GREENSBORO ROAD (HWY. 351). ELEV. = 343.12 (NAVD 88)
- SUBJECT PROPERTY IS CURRENTLY ZONED R-1 (SINGLE-FAMILY MEDIUM DENSITY DISTRICT), C-3 (GENERAL COMMERCIAL DISTRICT) AND C-3 LUO (GENERAL COMMERCIAL DISTRICT WITH A LIMITED USE OVERLAY) AS SHOWN. ADJACENT PROPERTIES ARE ZONED C-3 AND I-1 AS NOTED.
- SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS DETERMINED TO BE OUTSIDE 500-YEAR FLOOD PLAIN, AS DESIGNATED BY FEDERAL EMERGENCY MANAGEMENT AGENCY FLOOD INSURANCE RATE MAP FOR THE CITY OF JONESBORO, COMMUNITY PANEL 50031C0044C (PANEL 44 OF 200). EFFECTIVE DATE - SEPTEMBER 27, 1991.
- THE FOLLOWING DOCUMENTS WERE USED TO PERFORM THIS SURVEY:
  - A.H.L.D. RIGHT OF WAY MAP FOR JOB # 10729.
  - RECORD PLAT OF SHAWILL TRUST ADDITION, RECORDED IN PLAT CABINET "C", PAGE 256 AT JONESBORO, ARKANSAS.
  - RECORD PLAT OF POTINGER REPLAT, RECORDED IN PLAT CABINET "C", PAGE 247 AT JONESBORO, ARKANSAS.
  - RECORD PLAT OF MIDWAY PROPERTY REPLAT, RECORDED IN PLAT CABINET "C", PAGE 31 AT JONESBORO, ARKANSAS.
  - RECORD PLAT OF F.W. CALDWELL'S SUBDIVISION IN SECTION 10, T14N, R4E, RECORDED IN PLAT BOOK 48, PAGE 302 AT JONESBORO, ARKANSAS.

**SURVEYOR'S CERTIFICATION**

I HEREBY CERTIFY THAT ASSOCIATED ENGINEERING AND TESTING, LLC HAS THIS DATE MADE A BOUNDARY AND TOPOGRAPHIC SURVEY OF THE ABOVE DESCRIBED PROPERTY AS SHOWN HEREON AND THAT SAID SURVEY IS ACCURATE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. THERE ARE NO APPARENT ABOVE GROUND ENCROACHMENTS OTHER THAN AS SHOWN OR STATED HEREON AND THAT THIS SURVEY WAS MADE IN ACCORDANCE WITH THE "ARKANSAS MINIMUM STANDARDS FOR PROPERTY SURVEYS AND PLATS" IN EFFECT ON THIS DATE.

DATE OF BOUNDARY SURVEY: 04/12/2016  
 NOTE: TO BE VALID, COPIES MUST HAVE ORIGINAL SURVEYOR'S SIGNATURE AND SURVEYOR'S SEAL.

**LEGEND**

—	= BOUNDARY LINE
- - -	= ADJACENT LOT LINE
- - - - -	= EASEMENT LINE
○	= FOUND IRON PIPE
○	= FOUND REBAR
○	= SET 1-1/4" IRON PIPE W/ PS #1549 CAP
SS	= EXISTING SANITARY SEWER LINE
SSMH	= EXISTING SANITARY SEWER MANHOLE
○	= EXISTING SANITARY CLEANOUT
WM	= EXISTING WATER METER
WV	= WATER VALVE
○	= FIRE HYDRANT
○	= WATER SPRINKLER
○	= SPRINKLER VALVE
○	= EXISTING OVERHEAD ELECTRICAL LINE
○	= POWER POLE
○	= LIGHT POLE
○	= TELEPHONE PEDESTAL
○	= GAS METER
○	= TRAFFIC SIGNAL POLE
○	= EXISTING FENCE LINE
- - - - -	= EXISTING GROUND CONTOUR
BM	= BENCH MARK

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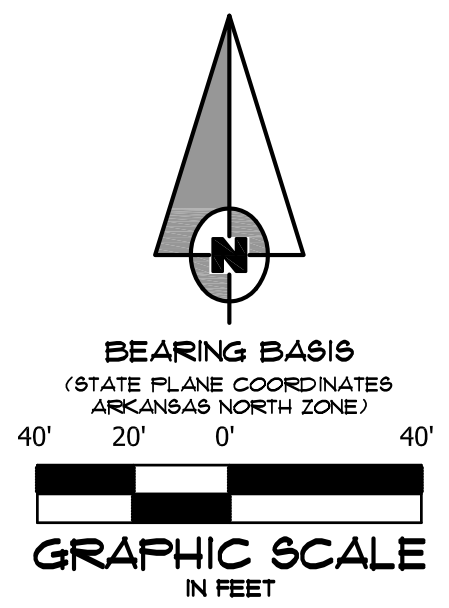
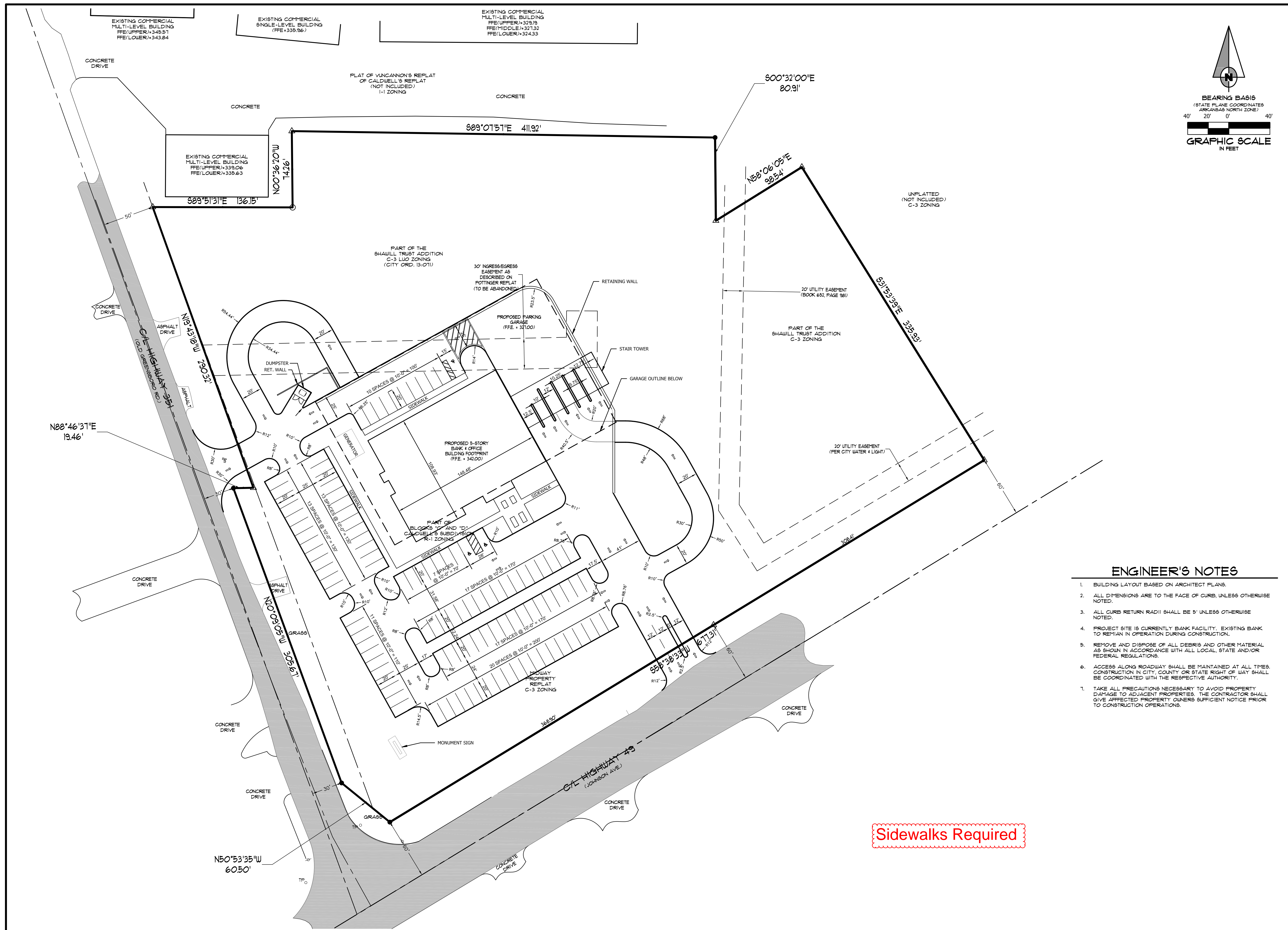
**REGISTERED STATE OF ARKANSAS**  
 NO. 289  
 ASSOCIATED ENGINEERING & TESTING, LLC

NO.	DESCRIPTION	DATE

**BOUNDARY & TOPOGRAPHIC SURVEY**

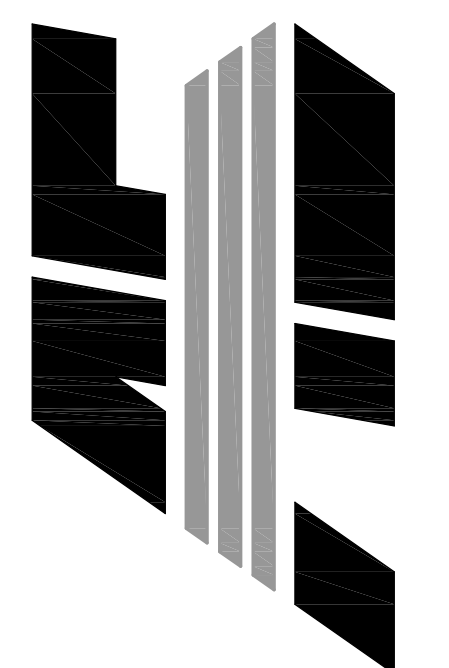
DATE: 04/12/16 DRAIN: CCH  
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 DWG#: 0414104.0045 SHEET  
 SCALE: 1" = 40' 1 OF 1

500-14N-04E-0-10-430-16-1549



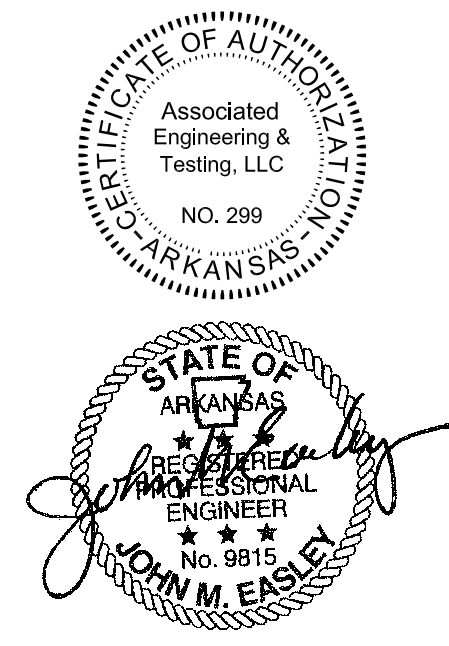
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**ENGINEER'S NOTES**

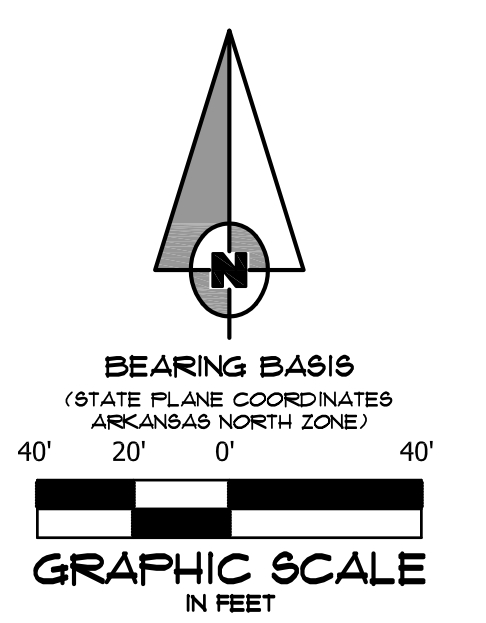
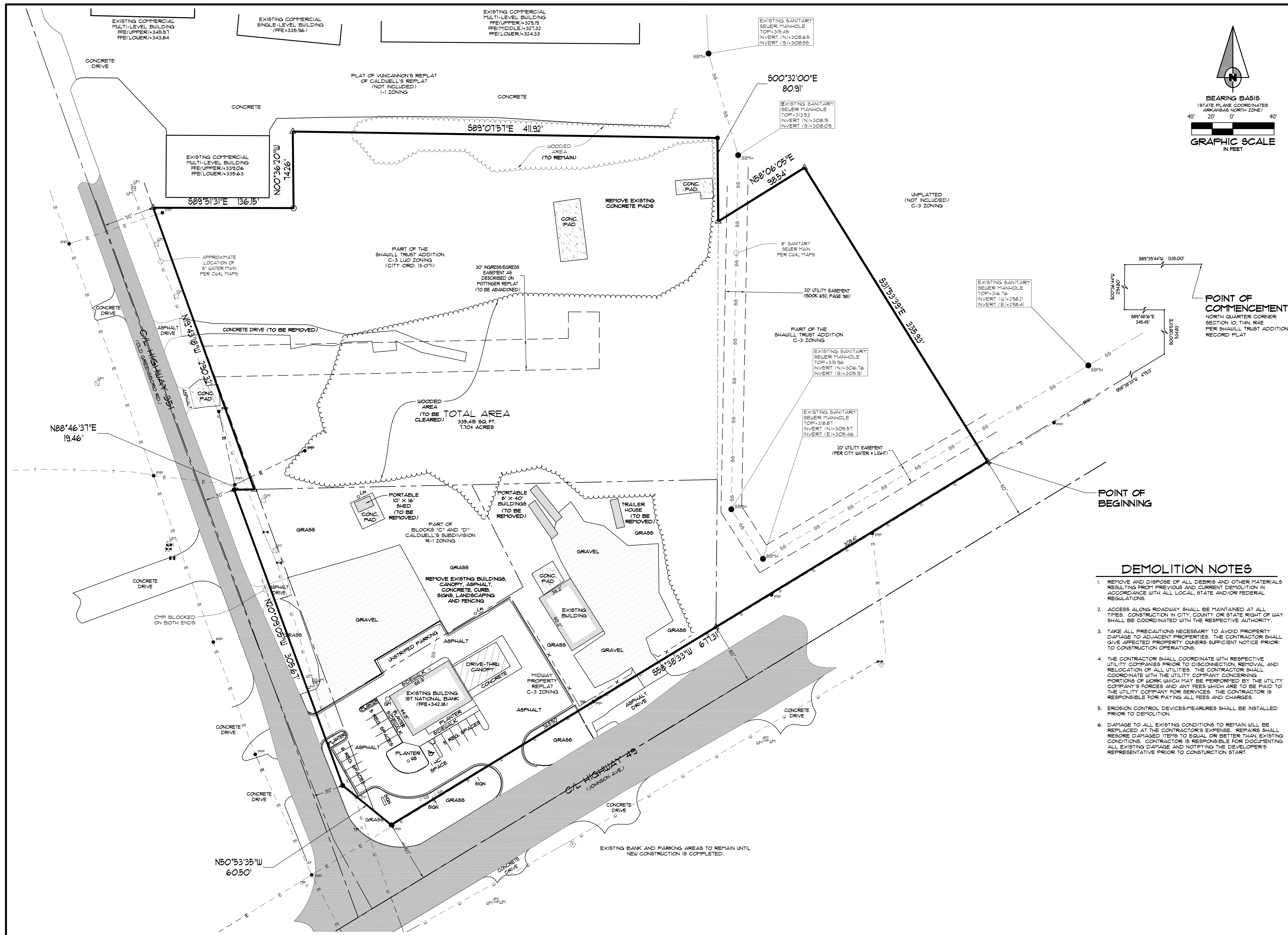
- BUILDING LAYOUT BASED ON ARCHITECT PLANS.
- ALL DIMENSIONS ARE TO THE FACE OF CURB, UNLESS OTHERWISE NOTED.
- ALL CURB RETURN RADIi SHALL BE 5' UNLESS OTHERWISE NOTED.
- PROJECT SITE IS CURRENTLY BANK FACILITY, EXISTING BANK TO REMIAN IN OPERATION DURING CONSTRUCTION.
- REMOVE AND DISPOSE OF ALL DEBRIS AND OTHER MATERIAL AS SHOWN IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS.
- ACCESS ALONG ROADWAY SHALL BE MAINTAINED AT ALL TIMES. CONSTRUCTION IN CITY, COUNTY OR STATE RIGHT OF WAY SHALL BE COORDINATED WITH THE RESPECTIVE AUTHORITY.
- TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES. THE CONTRACTOR SHALL GIVE AFFECTED PROPERTY OWNERS SUFFICIENT NOTICE PRIOR TO CONSTRUCTION OPERATIONS.



NO.	DESCRIPTION	DATE
1	ADEQ COMMENTS	07/18/16

**SITE PLAN**

DATE: 06/27/16    DRAWN: CCH  
 CADD FILE: 15165-SDP    CHECKED: JME  
 DWG#: 0414104.000X    SHEET  
 SCALE: 1" = 40'    C004



POINT OF BEGINNING

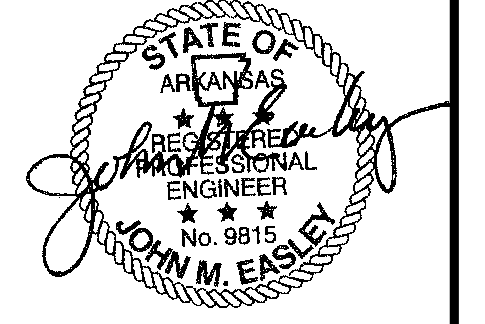
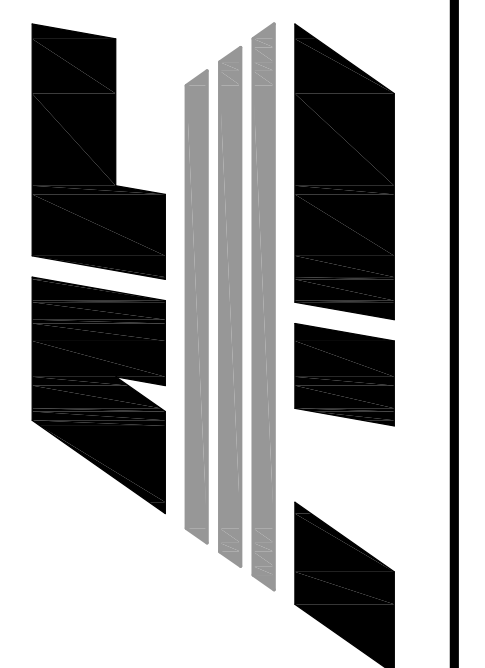
POINT OF COMMENCEMENT  
NORTH QUARTER CORNER SECTION 10, T4N, R4E PER SHAULL TRUST ADDITION RECORD PLAT

**DEMOLITION NOTES**

1. REMOVE AND DISPOSE OF ALL DEBRIS AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS.
2. ACCESS ALONG ROADWAY SHALL BE MAINTAINED AT ALL TIMES. CONSTRUCTION IN CITY, COUNTY OR STATE RIGHT OF WAY SHALL BE COORDINATED WITH THE RESPECTIVE AUTHORITY.
3. TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES. THE CONTRACTOR SHALL GIVE AFFECTED PROPERTY OWNERS SUFFICIENT NOTICE PRIOR TO CONSTRUCTION OPERATIONS.
4. THE CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO DISCONNECTION, REMOVAL AND RELOCATION OF ALL UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY'S FORCES AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR SERVICES. THE CONTRACTOR IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
5. EROSION CONTROL DEVICES/MEASURES SHALL BE INSTALLED PRIOR TO DEMOLITION.
6. DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE. REPAIRS SHALL RESORE DAMAGED ITEMS TO EQUAL OR BETTER THAN EXISTING CONDITIONS. CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING ALL EXISTING DAMAGE AND NOTIFYING THE DEVELOPER'S REPRESENTATIVE PRIOR TO CONSTRUCTION START.

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**DEMOLITION PLAN**

DATE: 06/27/16    DRAWN: CCH  
 CADD FILE: 15165-SDP    CHECKED: JME  
 DWG#: 0414104.000X    SHEET  
 SCALE: 1" = 40'    C005

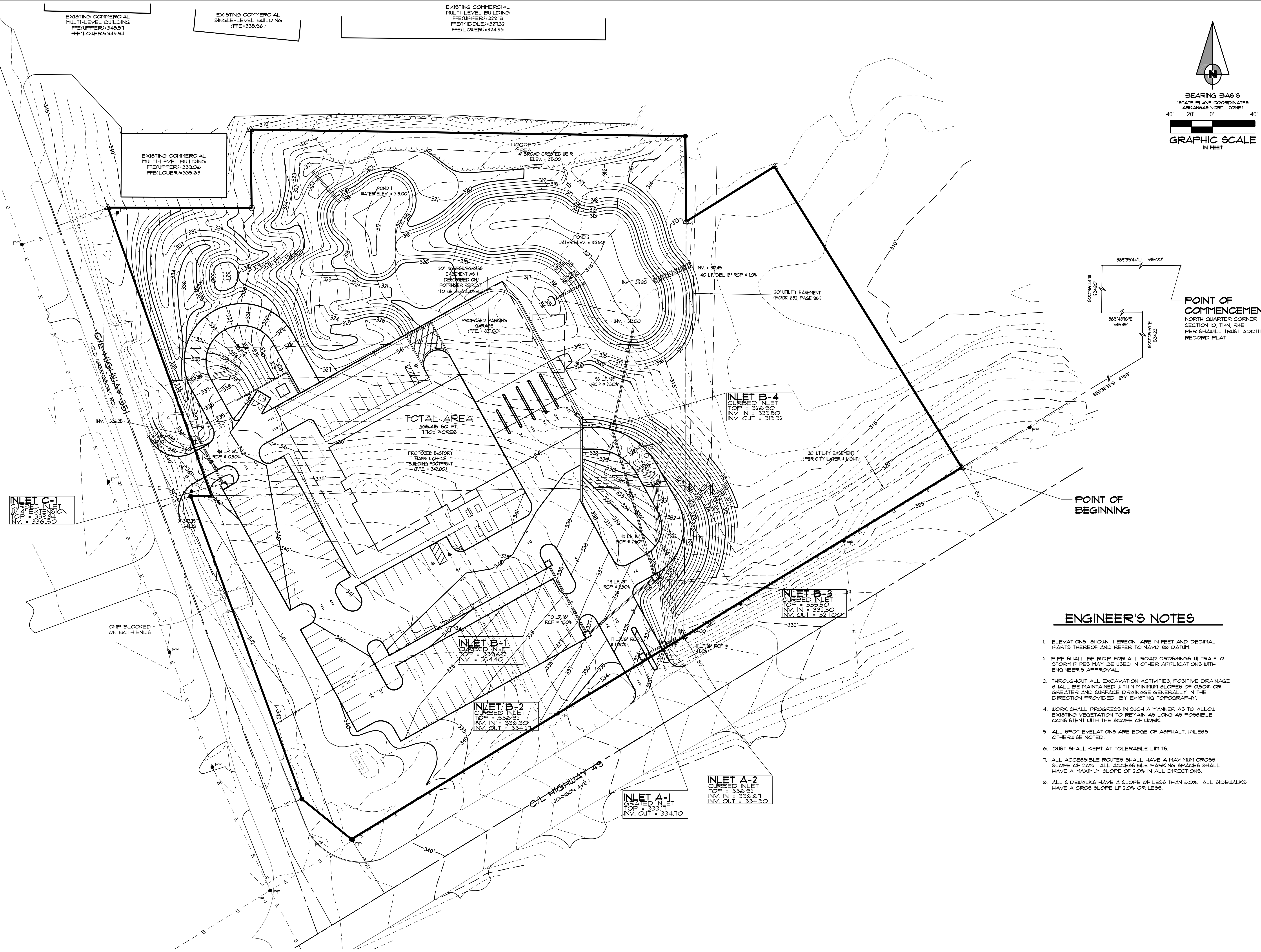
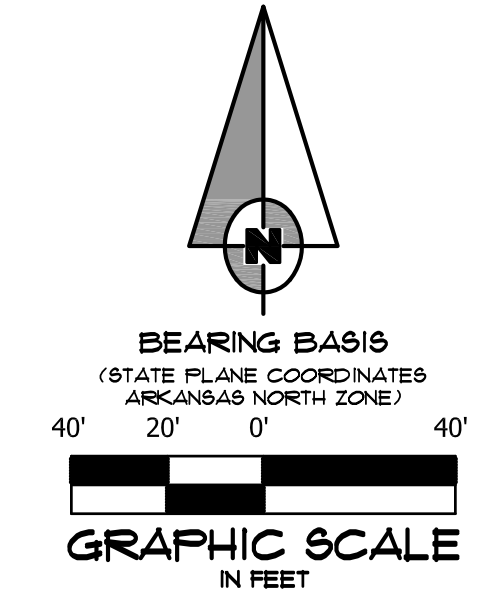


EXISTING COMMERCIAL  
MULTI-LEVEL BUILDING  
FFE(UPPER)=345.51  
FFE(LOWER)=343.84

EXISTING COMMERCIAL  
SINGLE-LEVEL BUILDING  
FFE=335.86

EXISTING COMMERCIAL  
MULTI-LEVEL BUILDING  
FFE(UPPER)=329.15  
FFE(MIDDLE)=321.32  
FFE(LOWER)=324.33

EXISTING COMMERCIAL  
MULTI-LEVEL BUILDING  
FFE(UPPER)=339.06  
FFE(LOWER)=335.63



**INLET C-1**  
COVERED INLET  
TOP = 339.06  
INV. IN = 336.00  
INV. OUT = 334.00

**INLET B-1**  
COVERED INLET  
TOP = 333.60  
INV. IN = 334.40

**INLET B-2**  
COVERED INLET  
TOP = 332.57  
INV. IN = 336.30  
INV. OUT = 334.32

**INLET A-1**  
GRADED INLET  
TOP = 333.11  
INV. OUT = 334.10

**INLET A-2**  
COVERED INLET  
TOP = 336.34  
INV. IN = 332.61  
INV. OUT = 334.50

**INLET B-4**  
COVERED INLET  
TOP = 326.50  
INV. IN = 323.50  
INV. OUT = 315.32

**INLET B-3**  
COVERED INLET  
TOP = 333.50  
INV. IN = 333.30  
INV. OUT = 327.00

**POINT OF COMMENCEMENT**  
NORTH QUARTER CORNER  
SECTION 10, T44N, R4E  
PER SHAULL TRUST ADDITION  
RECORD PLAT

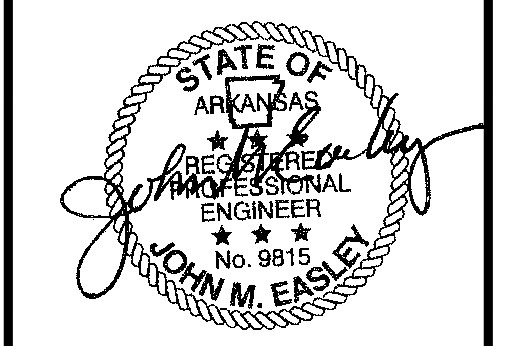
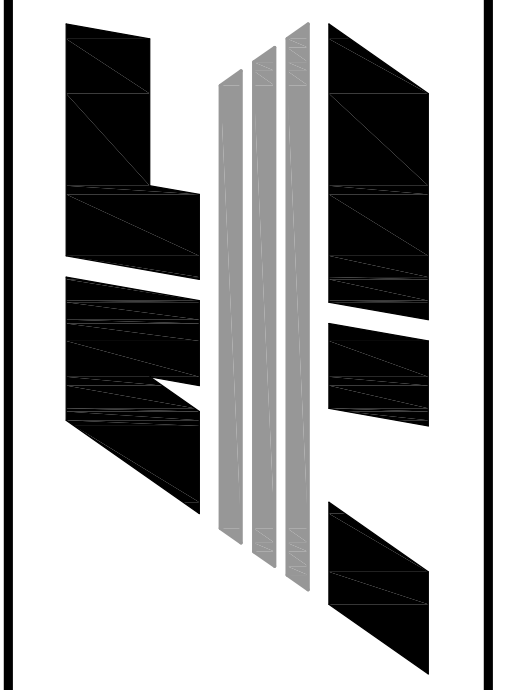
**POINT OF BEGINNING**

**ENGINEER'S NOTES**

- ELEVATIONS SHOWN HEREON ARE IN FEET AND DECIMAL PARTS THEREOF AND REFER TO NAVD 88 DATUM.
- PIPE SHALL BE R.C.P. FOR ALL ROAD CROSSINGS. ULTRA FLO STORY PIPES MAY BE USED IN OTHER APPLICATIONS WITH ENGINEER'S APPROVAL.
- THROUGHOUT ALL EXCAVATION ACTIVITIES, POSITIVE DRAINAGE SHALL BE MAINTAINED WITHIN MINIMUM SLOPES OF 0.50% OR GREATER AND SURFACE DRAINAGE GENERALLY IN THE DIRECTION PROVIDED BY EXISTING TOPOGRAPHY.
- WORK SHALL PROGRESS IN SUCH A MANNER AS TO ALLOW EXISTING VEGETATION TO REMAIN AS LONG AS POSSIBLE, CONSISTENT WITH THE SCOPE OF WORK.
- ALL SPOT ELEVATIONS ARE EDGE OF ASPHALT, UNLESS OTHERWISE NOTED.
- DUST SHALL BE KEPT AT TOLERABLE LIMITS.
- ALL ACCESSIBLE ROUTES SHALL HAVE A MAXIMUM CROSS SLOPE OF 2.0%. ALL ACCESSIBLE PARKING SPACES SHALL HAVE A MAXIMUM SLOPE OF 2.0% IN ALL DIRECTIONS.
- ALL SIDEWALKS HAVE A SLOPE OF LESS THAN 5.0%. ALL SIDEWALKS HAVE A CROSS SLOPE OF 2.0% OR LESS.

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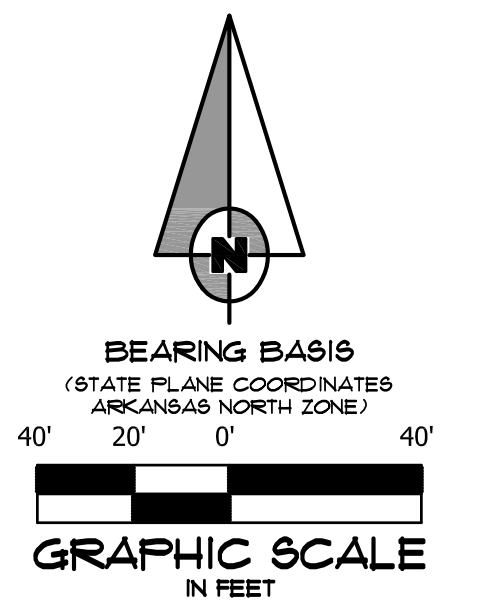
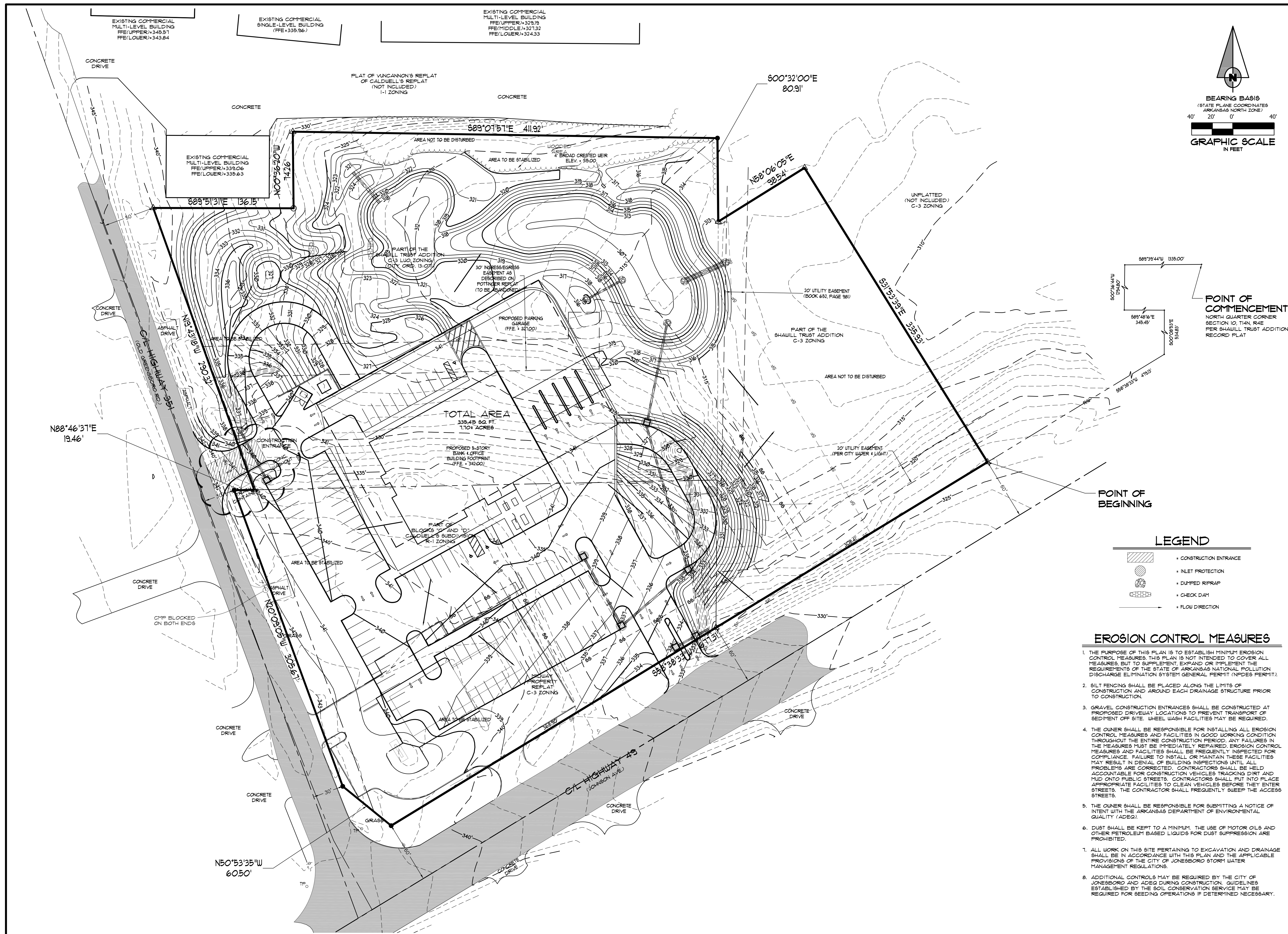
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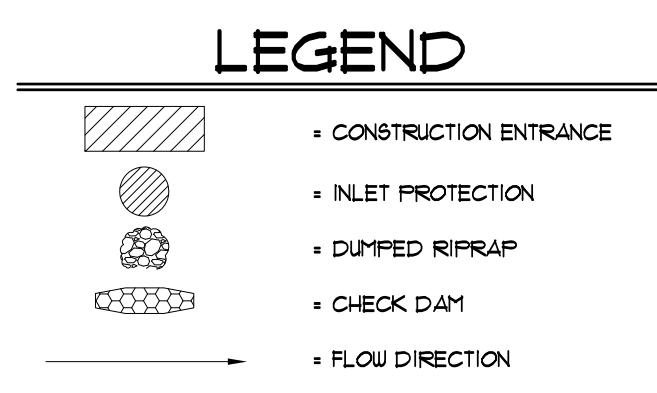
**GRADING PLAN**

DATE: 06/27/16    DRAWN: CCH  
 CADD FILE: 15165-SDP    CHECKED: JME  
 DWG#: 0414104.000X    SHEET  
 SCALE: 1" = 40'    0007



**POINT OF COMMENCEMENT**  
NORTH QUARTER CORNER SECTION 10, T4N, R1E PER SHAULL TRUST ADDITION RECORD PLAT

**POINT OF BEGINNING**

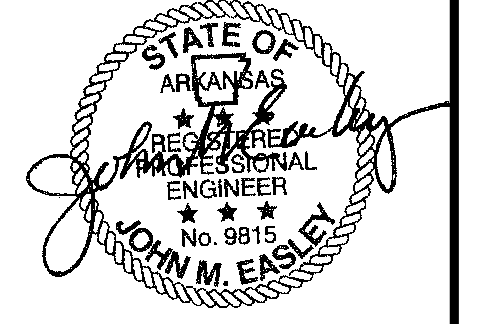
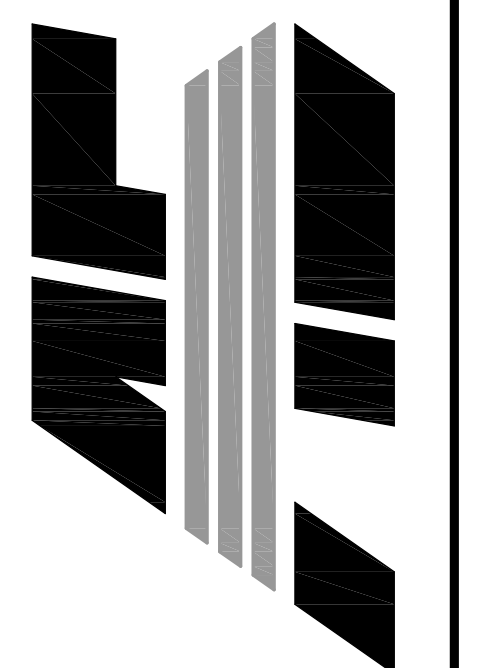


**EROSION CONTROL MEASURES**

1. THE PURPOSE OF THIS PLAN IS TO ESTABLISH MINIMUM EROSION CONTROL MEASURES. THIS PLAN IS NOT INTENDED TO COVER ALL MEASURES, BUT TO SUPPLEMENT, EXPAND OR IMPLEMENT THE REQUIREMENTS OF THE STATE OF ARKANSAS NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM GENERAL PERMIT (NPDES PERMIT).
2. SILT FENCING SHALL BE PLACED ALONG THE LIMITS OF CONSTRUCTION AND AROUND EACH DRAINAGE STRUCTURE PRIOR TO CONSTRUCTION.
3. GRAVEL CONSTRUCTION ENTRANCES SHALL BE CONSTRUCTED AT PROPOSED DRIVEWAY LOCATIONS TO PREVENT TRANSPORT OF SEDIMENT OFF SITE. WHEEL WASH FACILITIES MAY BE REQUIRED.
4. THE OWNER SHALL BE RESPONSIBLE FOR INSTALLING ALL EROSION CONTROL MEASURES AND FACILITIES IN GOOD WORKING CONDITION THROUGHOUT THE ENTIRE CONSTRUCTION PERIOD. ANY FAILURES IN THE MEASURES MUST BE IMMEDIATELY REPAIRED. EROSION CONTROL MEASURES AND FACILITIES SHALL BE FREQUENTLY INSPECTED FOR COMPLIANCE. FAILURE TO INSTALL OR MAINTAIN THESE FACILITIES MAY RESULT IN DENIAL OF BUILDING INSPECTIONS UNTIL ALL PROBLEMS ARE CORRECTED. CONTRACTORS SHALL BE HELD ACCOUNTABLE FOR CONSTRUCTION VEHICLES TRACKING DIRT AND MUD ONTO PUBLIC STREETS. CONTRACTORS SHALL PUT INTO PLACE APPROPRIATE FACILITIES TO CLEAN VEHICLES BEFORE THEY ENTER STREETS. THE CONTRACTOR SHALL FREQUENTLY SWEEP THE ACCESS STREETS.
5. THE OWNER SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE OF INTENT WITH THE ARKANSAS DEPARTMENT OF ENVIRONMENTAL QUALITY (ADEQ).
6. DUST SHALL BE KEPT TO A MINIMUM. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED LIQUIDS FOR DUST SUPPRESSION ARE PROHIBITED.
7. ALL WORK ON THIS SITE PERTAINING TO EXCAVATION AND DRAINAGE SHALL BE IN ACCORDANCE WITH THIS PLAN AND THE APPLICABLE PROVISIONS OF THE CITY OF JONESBORO STORM WATER MANAGEMENT REGULATIONS.
8. ADDITIONAL CONTROLS MAY BE REQUIRED BY THE CITY OF JONESBORO AND ADEQ DURING CONSTRUCTION. GUIDELINES ESTABLISHED BY THE SOIL CONSERVATION SERVICE MAY BE REQUIRED FOR SEEDING OPERATIONS IF DETERMINED NECESSARY.

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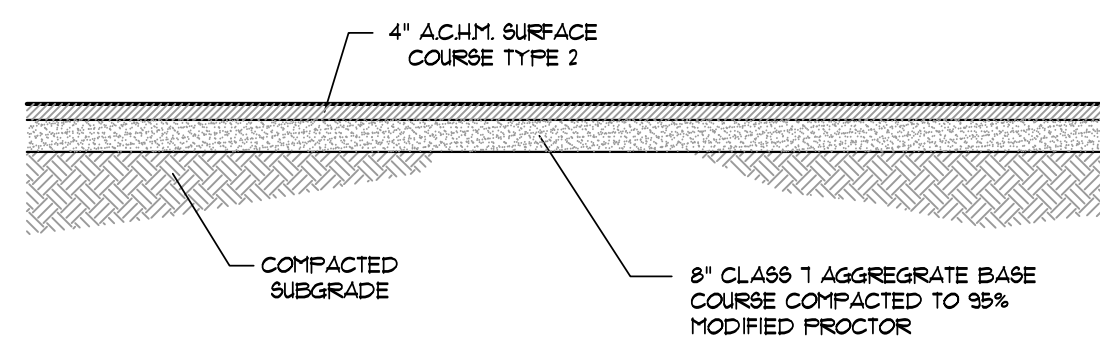
NO.	DESCRIPTION	DATE
1	ADEQ COMMENTS	07/16

**EROSION CONTROL PLAN**

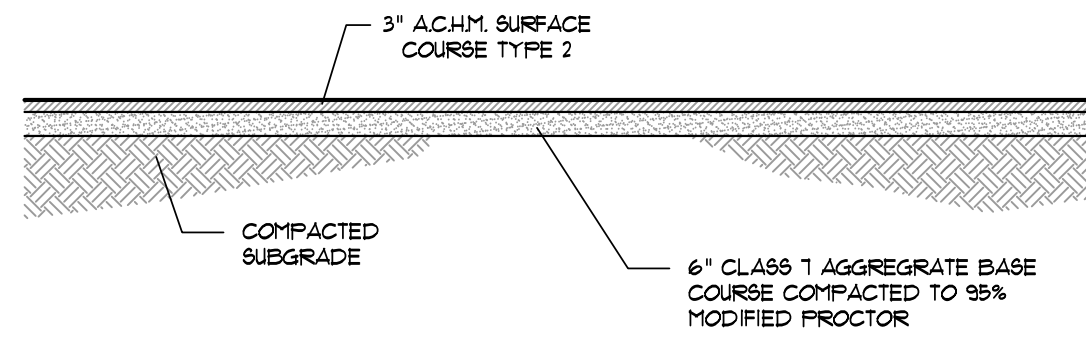
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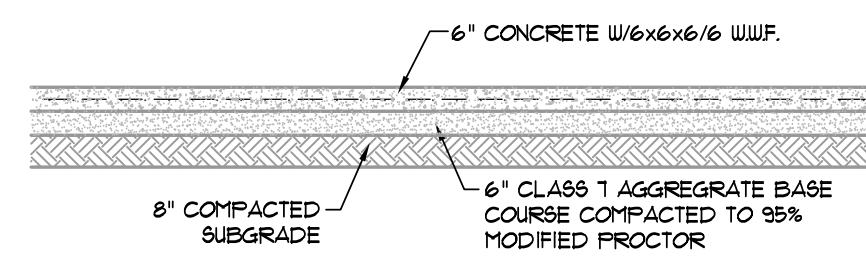




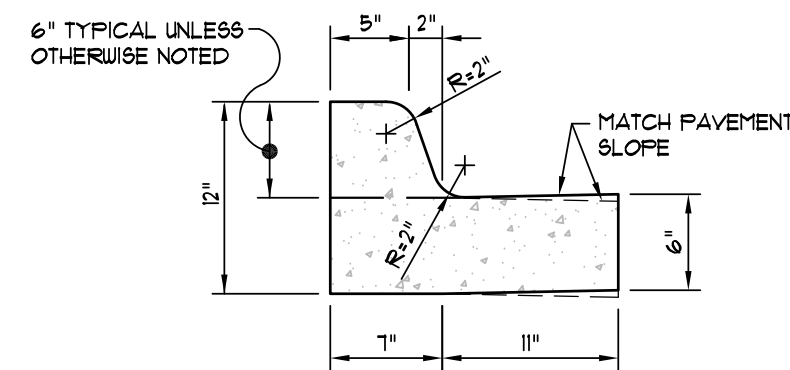
**SECTION - ASPHALT PARKING - HEAVY**  
N.T.S.



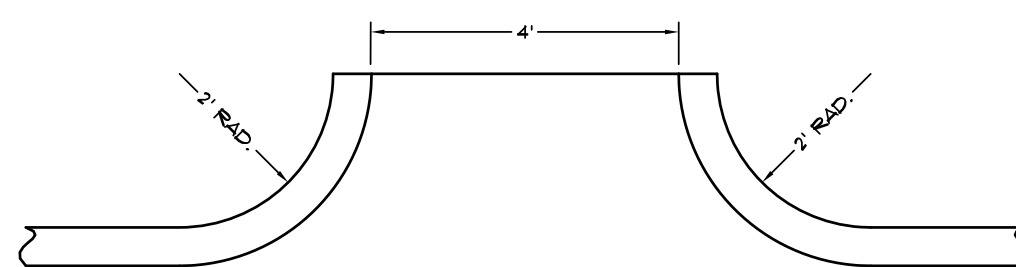
**SECTION - ASPHALT PARKING - LIGHT**  
N.T.S.



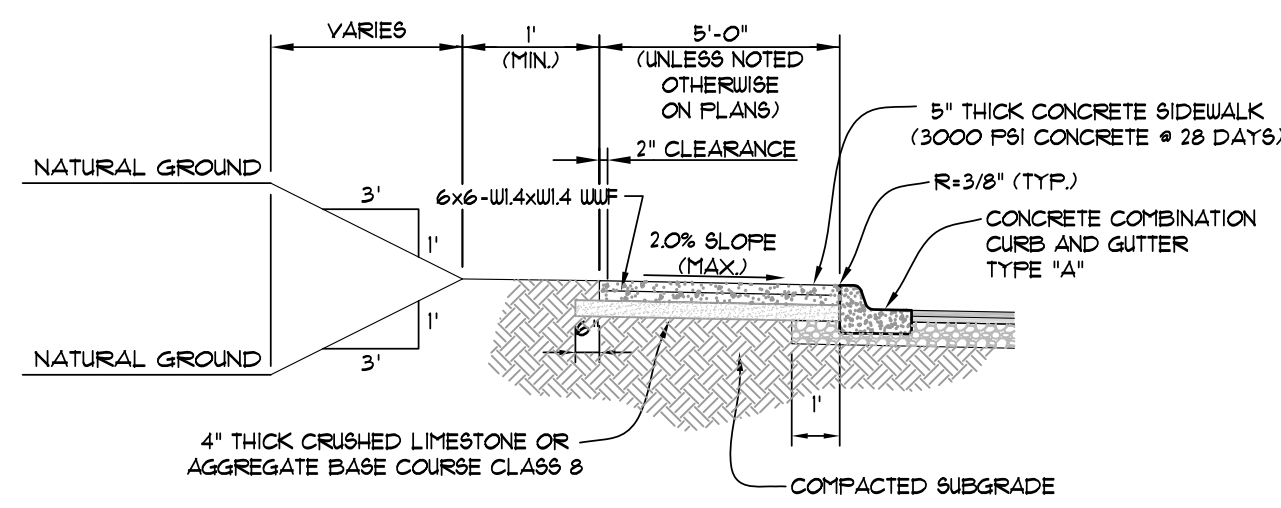
**SECTION - CONCRETE PAVING - LIGHT**  
N.T.S.



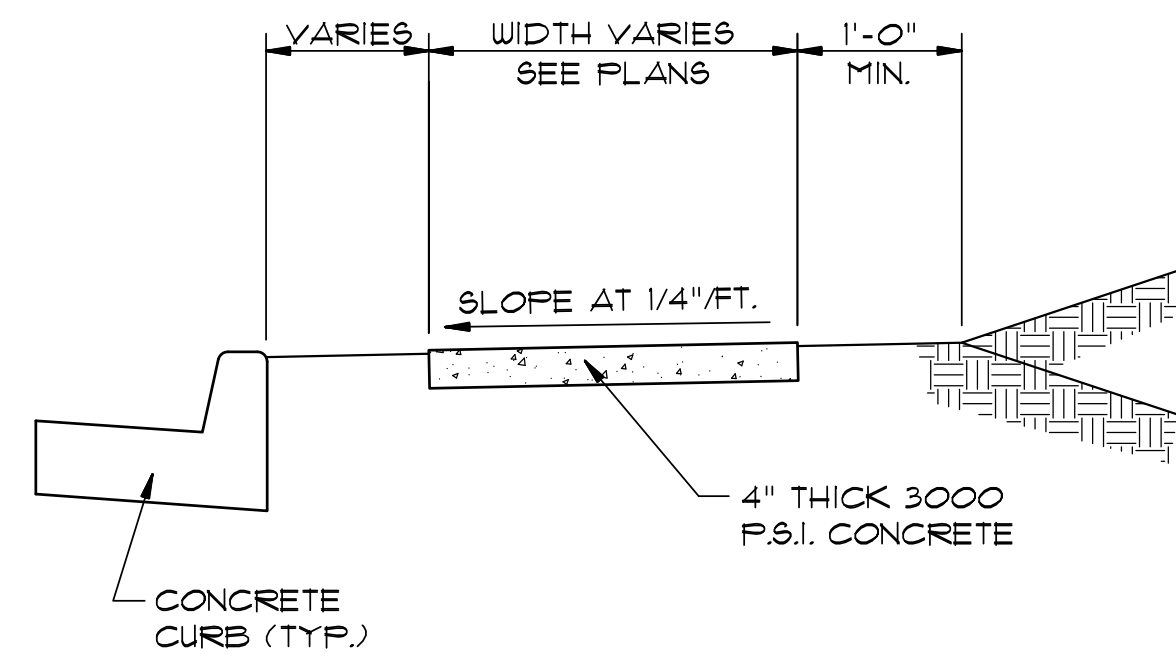
**TYPICAL CURB & GUTTER TYPE "A-18"**  
(NOT TO SCALE)



**CURB CHANNEL**  
(NOT TO SCALE)

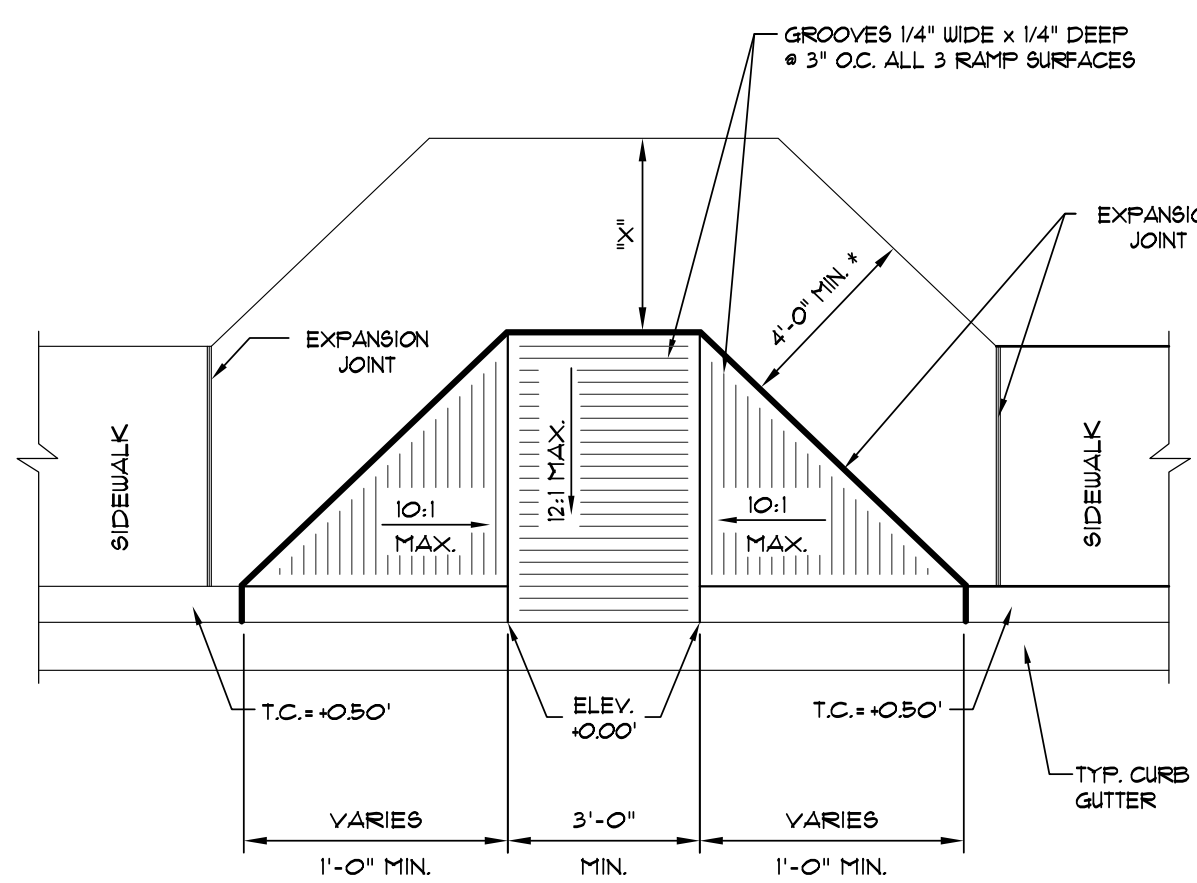


**TYPICAL SECTION - SIDEWALK AT CURB**  
(NOT TO SCALE)

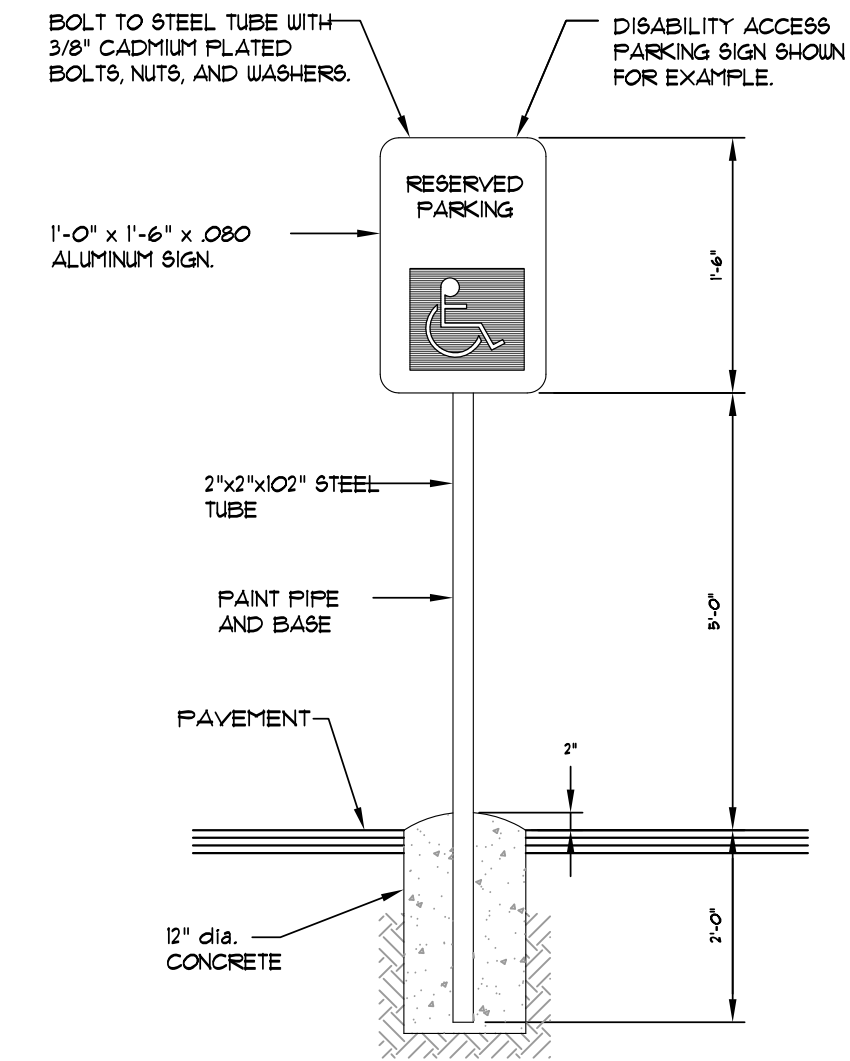


**TYPICAL SECTION - SIDEWALK**  
N.T.S.

\* MINIMUM WIDTH FOR SIDEWALK ADJACENT TO CURB IS 5' FOR CITY STREETS AND 6' FOR STATE OR U.S. HIGHWAYS.

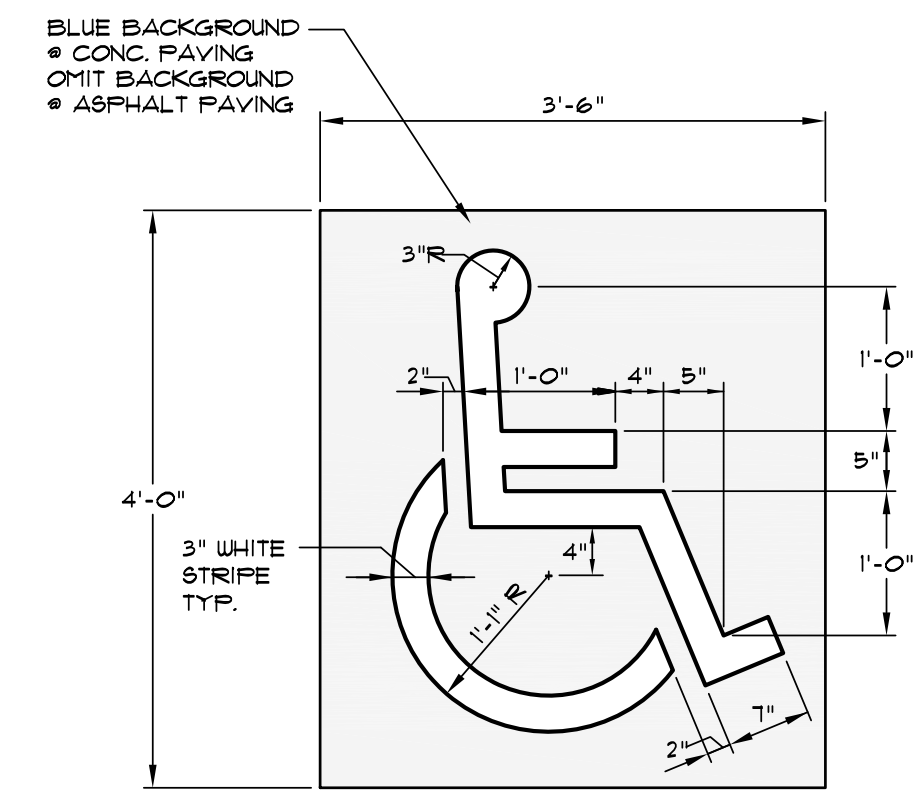


**TYPE "A" ACCESS RAMP**  
SCALE: 1/4" = 1'-0"



NOTE: ALL SIGNS SHALL CONFORM TO MUTCD AND SHALL BE HIGH INTENSITY.

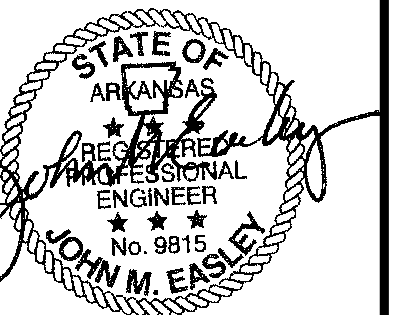
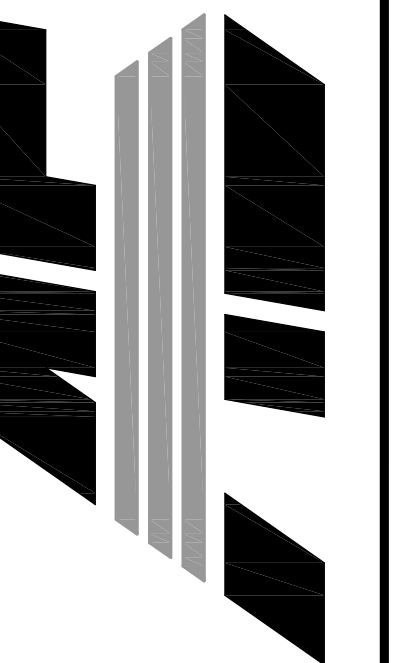
**SIGN DETAIL**  
SCALE: 1/2" = 1'-0"



**PARKING STALL SYMBOL**  
SCALE: 3/4" = 1'-0"

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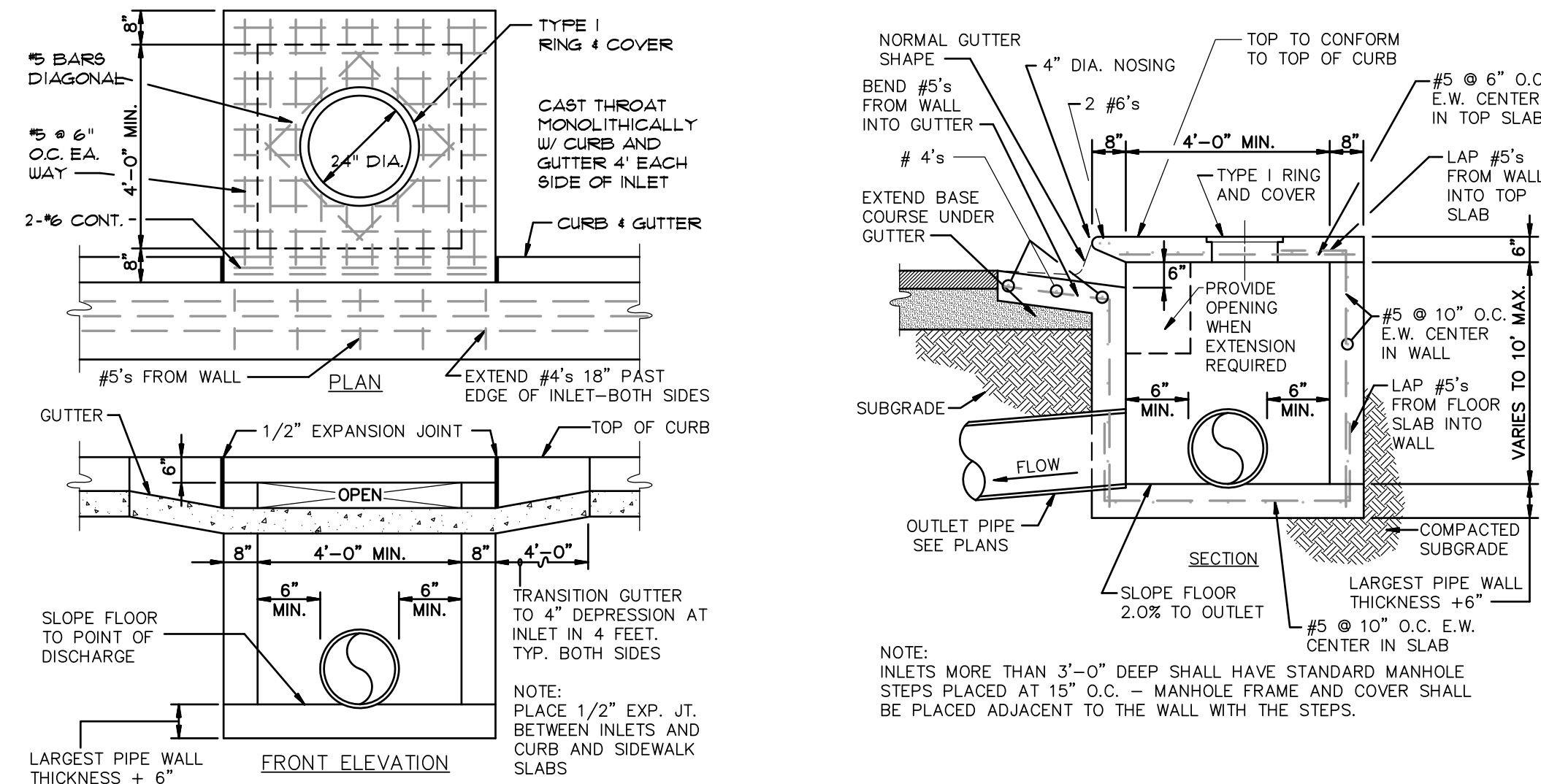
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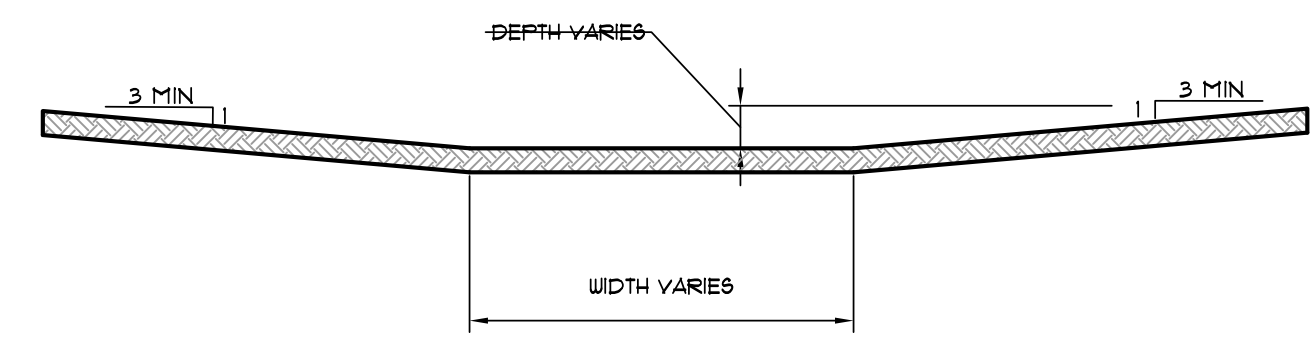
NO.	DESCRIPTION	DATE
1	ADEG COMMENTS	07/13/16

**STANDARD DETAILS**

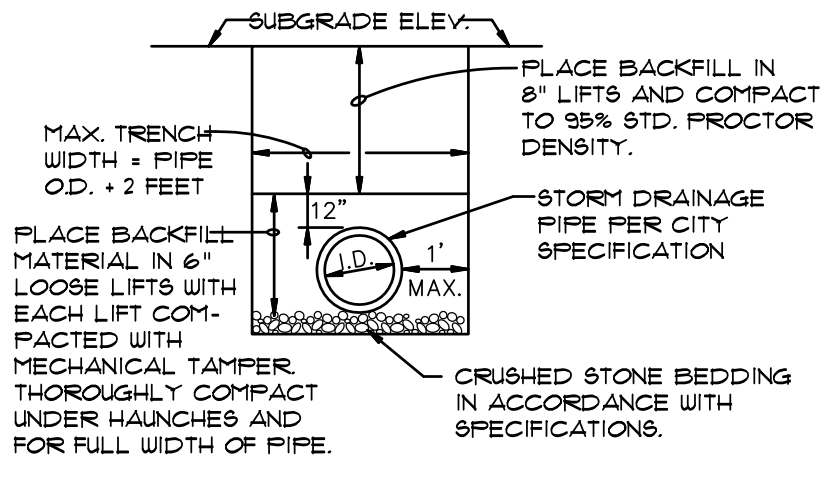
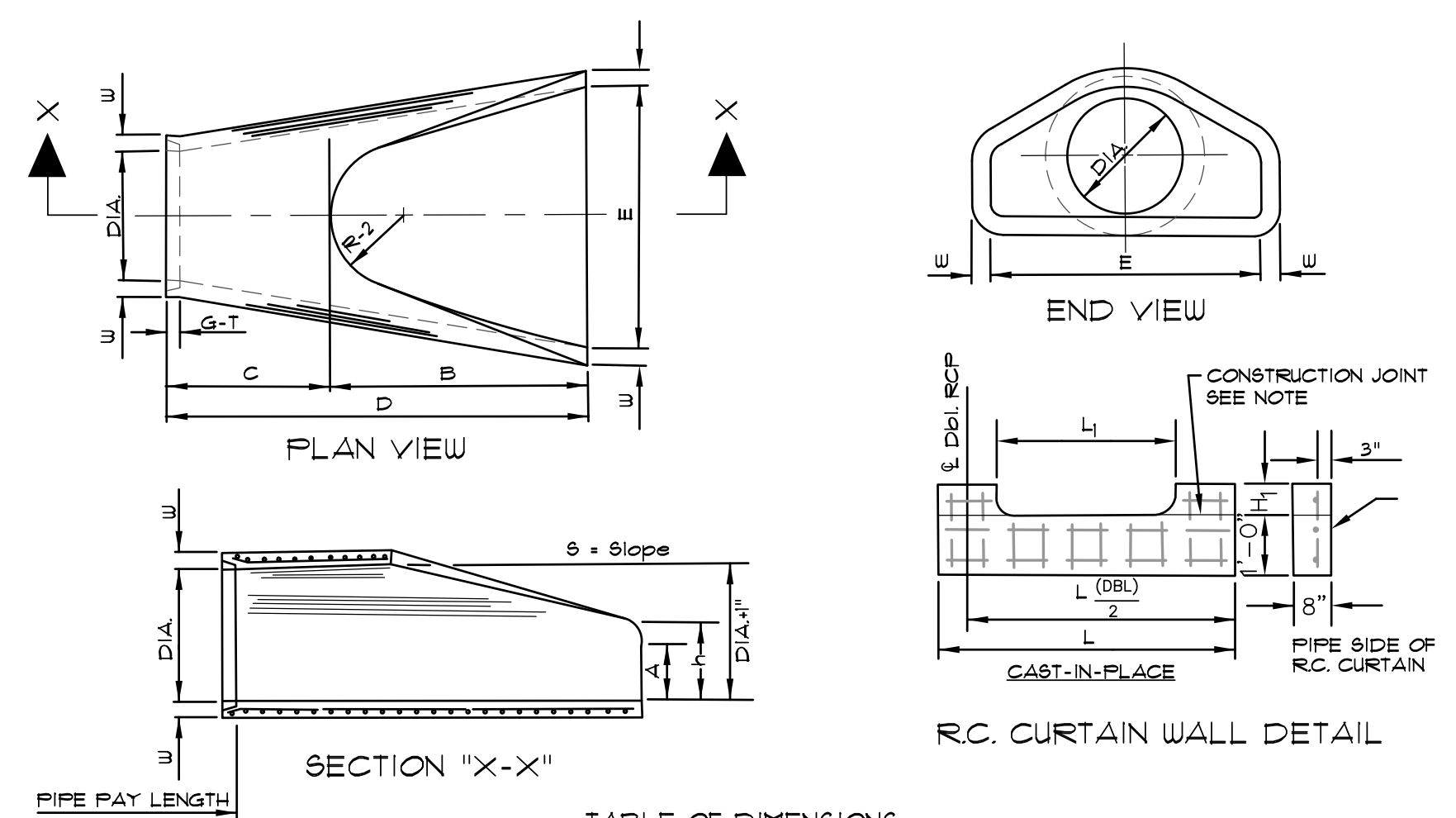
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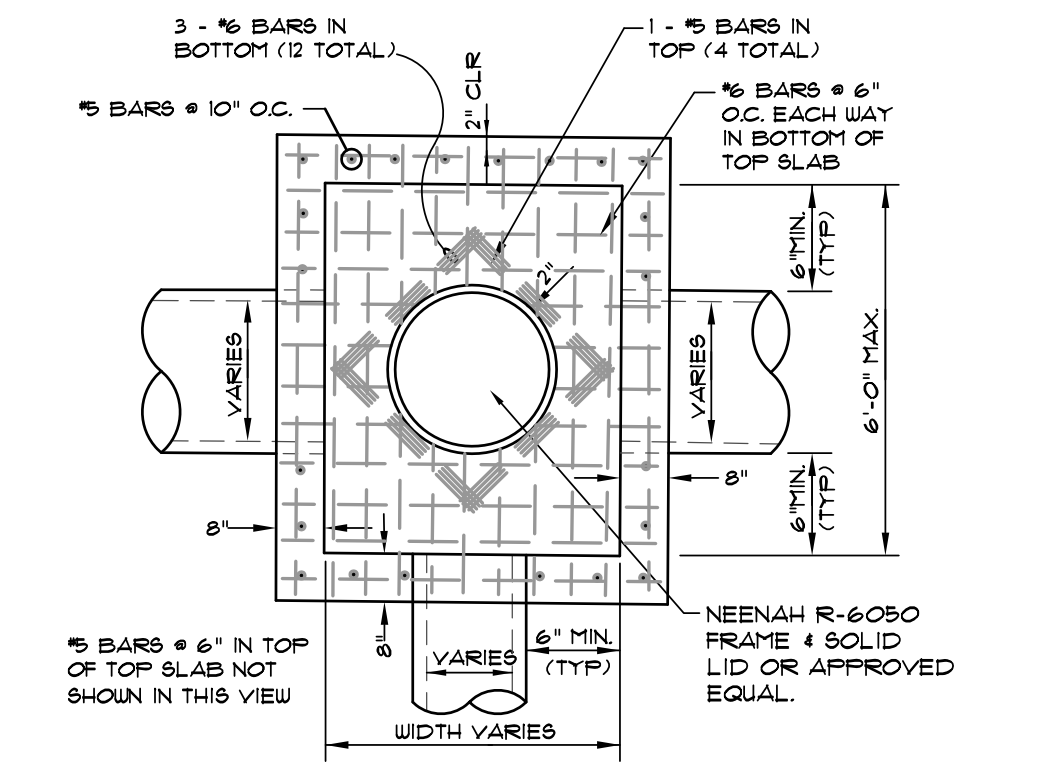
**CURB INLET - TYPE "A"**  
(NOT TO SCALE)



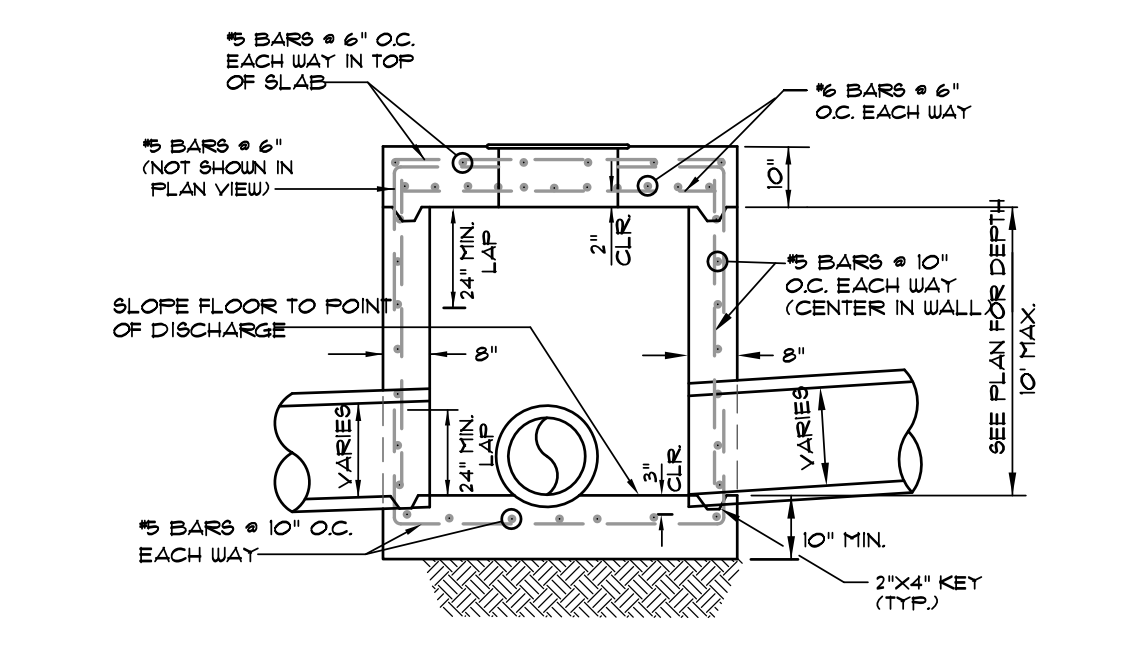
**DETAIL - DRAINAGE SWALE - FLAT BOTTOM**  
N.T.S.



**DRAINAGE PIPE TRENCH DETAIL**  
N.T.S.

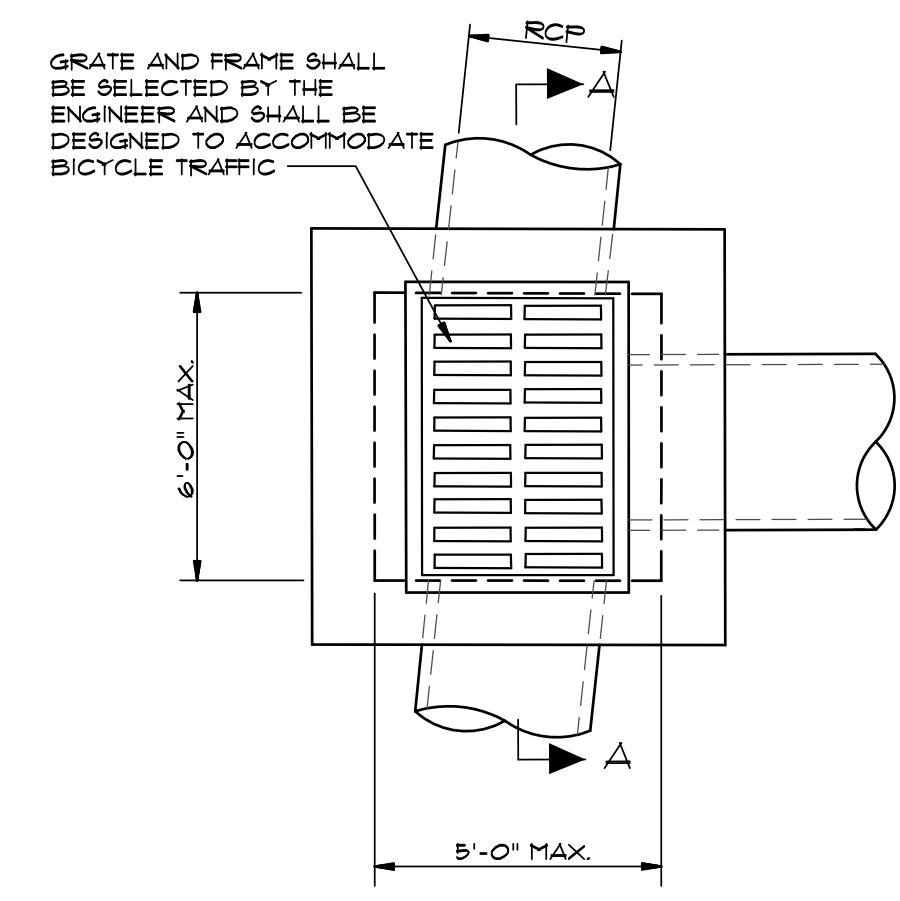


**PLAN - JUNCTION BOX - HEAVY**  
N.T.S.

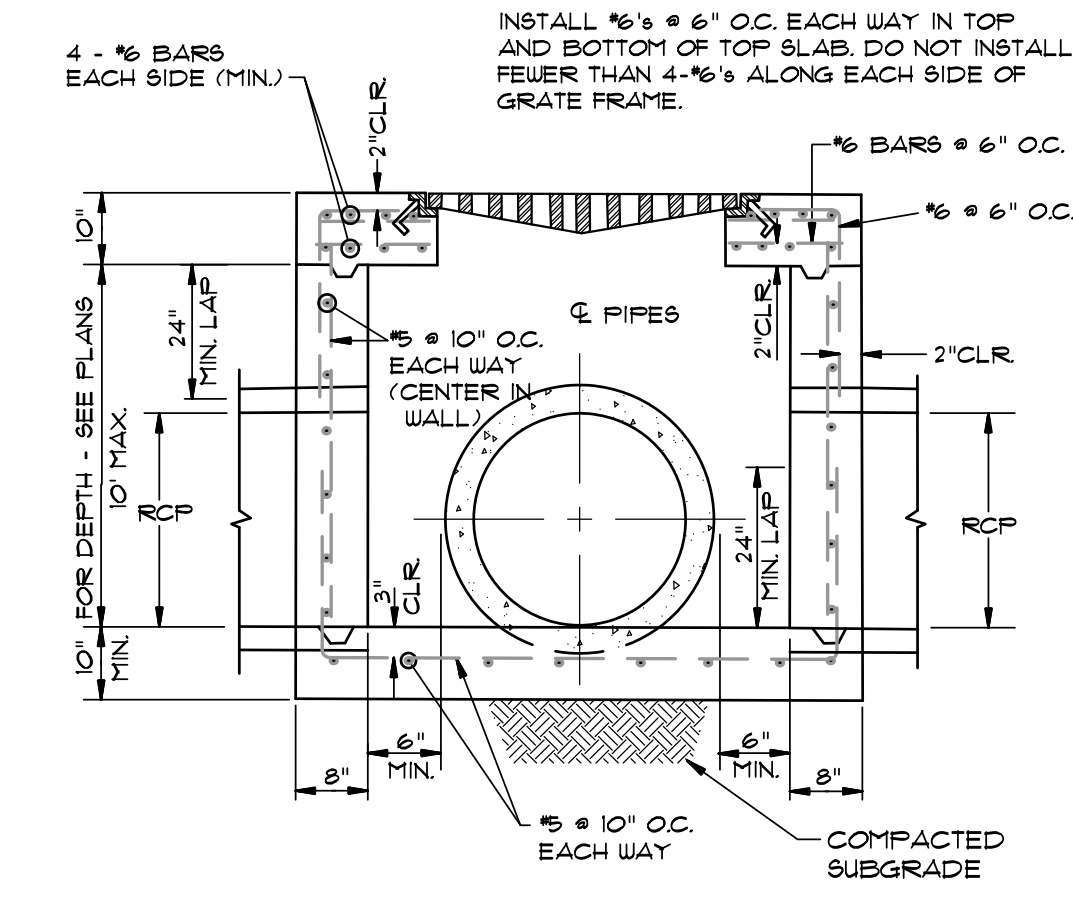


**SECTION - JUNCTION BOX - HEAVY**  
N.T.S.

- NOTES:
- SHOULD BE USED TO ACCOMMODATE VEHICULAR TRAFFIC.
  - JUNCTION BOXES MORE THAN 3 FEET DEEP SHALL HAVE STANDARD MANHOLE STEPS PLACED @ 15" O.C.
  - SEE REINFORCING DETAIL FOR OPENING IN WALL OR SLAB.



**PLAN - GRATE INLET - HEAVY**  
N.T.S.



**SECTION "A-A" - GRATE INLET - HEAVY**  
N.T.S.

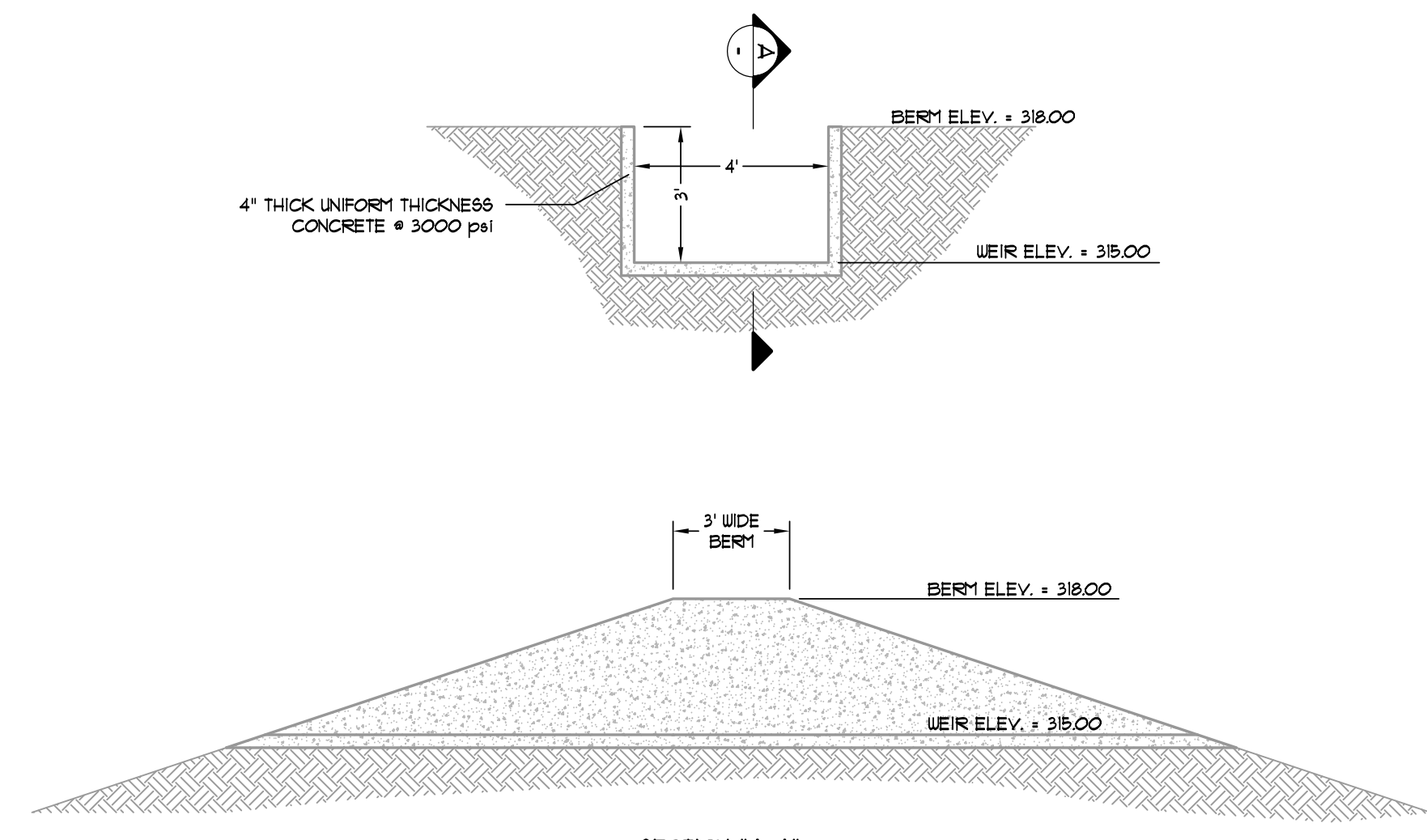
- NOTES:
- SHOULD BE USED TO ACCOMMODATE VEHICULAR TRAFFIC.
  - JUNCTION BOXES MORE THAN 3 FEET DEEP SHALL HAVE STANDARD MANHOLE STEPS PLACED @ 15" O.C.
  - SEE REINFORCING DETAIL FOR OPENING IN WALL OR SLAB.

TABLE OF DIMENSIONS

DIA.	WALL	A	B	C	D	E	S	DIA. #1"	P	R-1	R-2	G-T	WT	h
18"	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	3:1	19"	29"	15 1/2"	12"	2"	1000	1'-0 1/2"
24"	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3:1	25"	33 3/16"	16 13/16"	14"	2 1/2"	1600	1'-1 1/2"
30"	3 1/2"	1'-0"	4'-6"	1'-7 3/4"	8'-1 3/4"	5'-0"	3:1	31"	37"	18 1/2"	15"	3 1/4"	1940	1'-4 5/8"
36"	4"	1'-3"	5'-3"	2'-10 3/4"	8'-3 3/4"	6'-0"	3:1	39"	47 13/16"	24 5/16"	20"	3 1/2"	4100	1'-8"
42"	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	3:1	43"	53 7/8"	27 1/2"	22"	3 1/2"	5380	2'-2 1/2"
48"	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	3:1	49"	56 1/2"	28 1/2"	22"	3 1/2"	6550	2'-6"
54"	5 1/2"	2'-4"	6'-8"	1'-10"	8'-4"	7'-6"	3:1	55"	65 1/2"	33 1/8"	24"	4"	8750	2'-10 1/2"
60"	6"	2'-10"	6'-8"	1'-10"	8'-4"	8'-0"	3:1	61"	72 1/2"	36 11/16"	24"	4"	9270	3'-5"
72"	7"	3'-10"	6'-8"	1'-10"	8'-4"	9'-0"	3:1	73"	77 13/16"	38 15/16"	24"	5"	13250	4'-6"

- NOTES:
- THE PORTION OF THE R.C. CURTAIN WALL BENEATH THE FLARED END SECTION (LOWER 1'-0") SHALL BE PLACED MONOLITHICALLY. THE FLARED END SECTION SHALL THEN BE SET IN PLACE AND THE REMAINING PORTIONS OF THE R.C. CURTAIN WALL PLACED.
  - ALL REINFORCING STEEL ARE #4 BARS AT 6" O.C.
  - NO SEPARATE PAYMENT WILL BE MADE FOR THE CURTAIN WALLS. THEY SHALL BE CONSIDERED SUBSIDIARY TO THE FLARED END SECTIONS.
  - TONGUE END ON UPSTREAM SECTION. GROOVE END ON DOWNSTREAM SECTION.

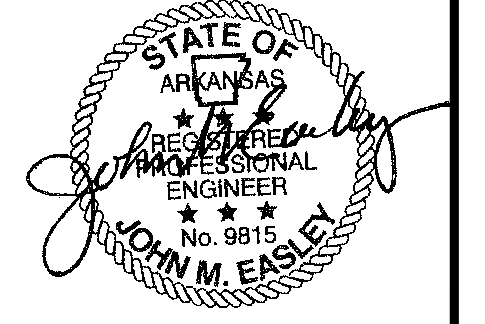
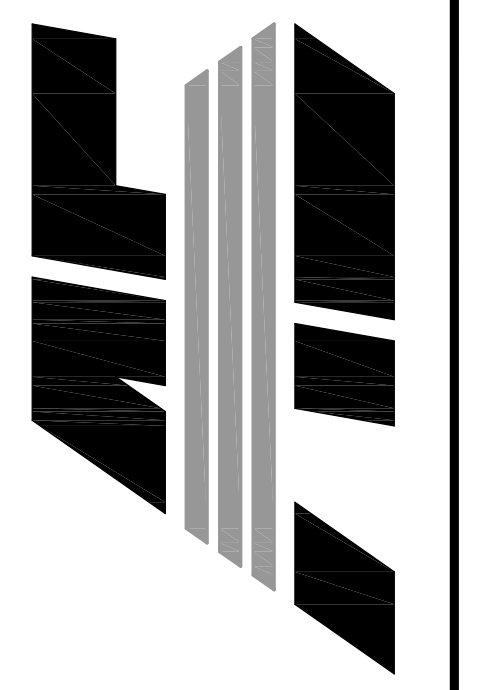
**FLARED END SECTION DETAILS FOR REINFORCED CONCRETE PIPE CULVERT**  
(NOT TO SCALE)



**BROAD CRESTED WEIR DETAIL**  
NOT TO SCALE

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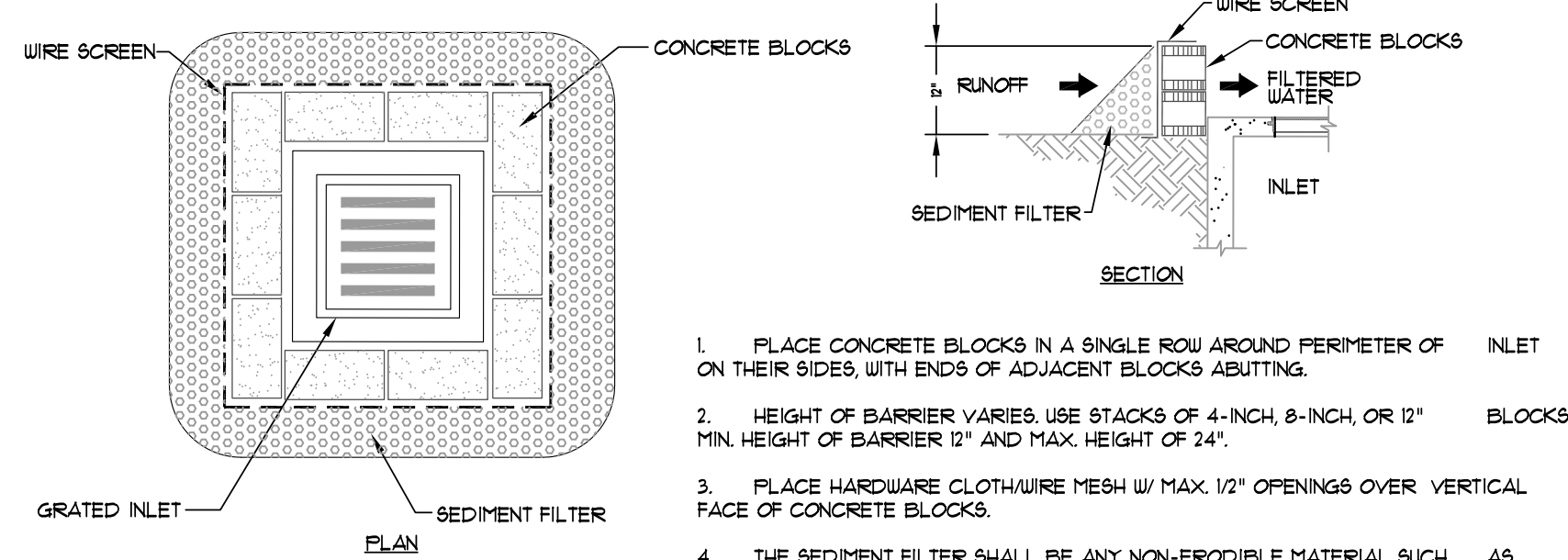
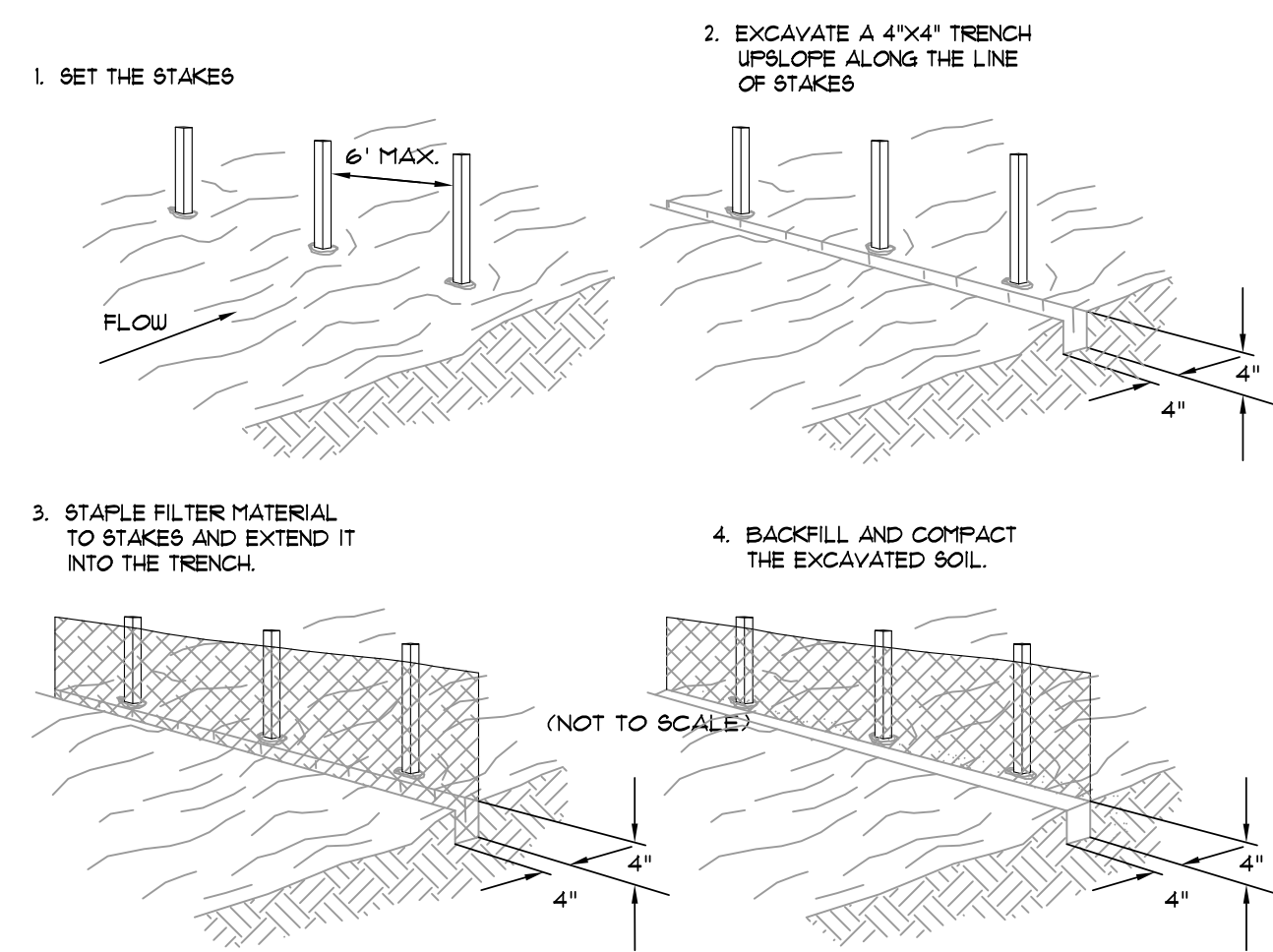
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NO.	DESCRIPTION	DATE
1	ADEQ COMMENTS	07/3/16

**STANDARD DETAILS**

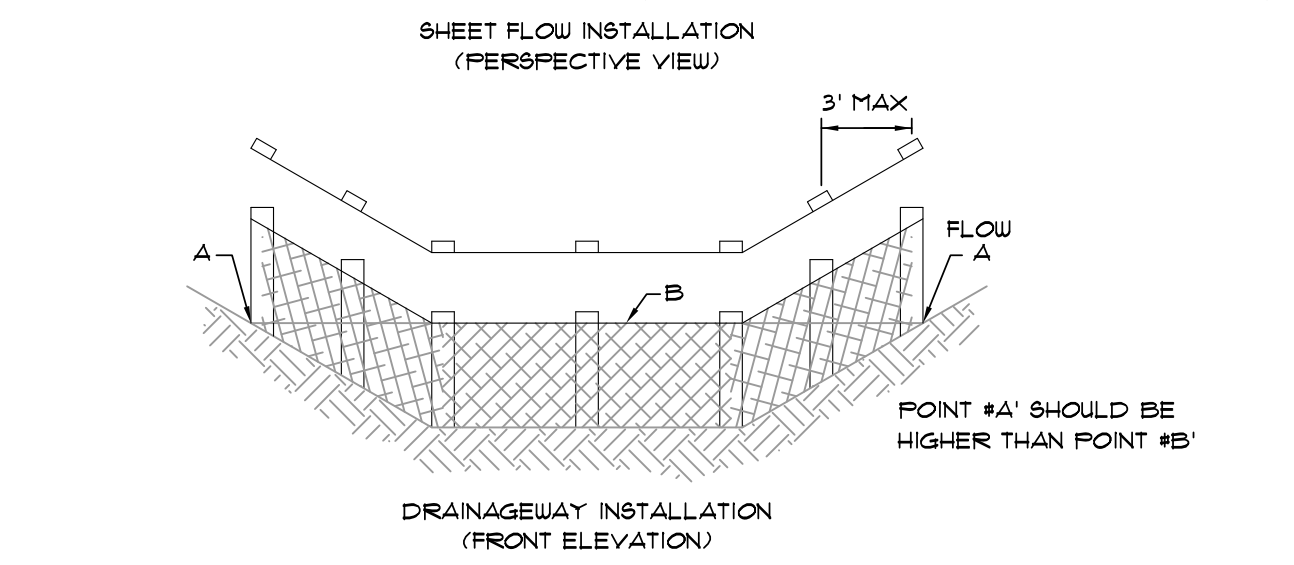
DATE: 06/27/16	DRAWN: CCH
CADD FILE: 15165-SDP	CHECKED: JME
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SCALE: 1" = 40'	C011



1. PLACE CONCRETE BLOCKS IN A SINGLE ROW AROUND PERIMETER OF INLET ON THEIR SIDES, WITH ENDS OF ADJACENT BLOCKS ABUTTING.
2. HEIGHT OF BARRIER VARIES. USE STACKS OF 4-INCH, 8-INCH, OR 12" BLOCKS. MIN. HEIGHT OF BARRIER 12" AND MAX. HEIGHT OF 24".
3. PLACE HARDWARE CLOTH/WIRE MESH W/ MAX. 1/2" OPENINGS OVER VERTICAL FACE OF CONCRETE BLOCKS.
4. THE SEDIMENT FILTER SHALL BE ANY NON-ERODIBLE MATERIAL SUCH AS LOOSE ROCK, BROKEN CONCRETE THAT WILL SLOW THE FLOW OF THE WATER AND ALLOW IT TO FILTER THROUGH AND OVER THE MATERIAL BEFORE ENTERING THE INLET.

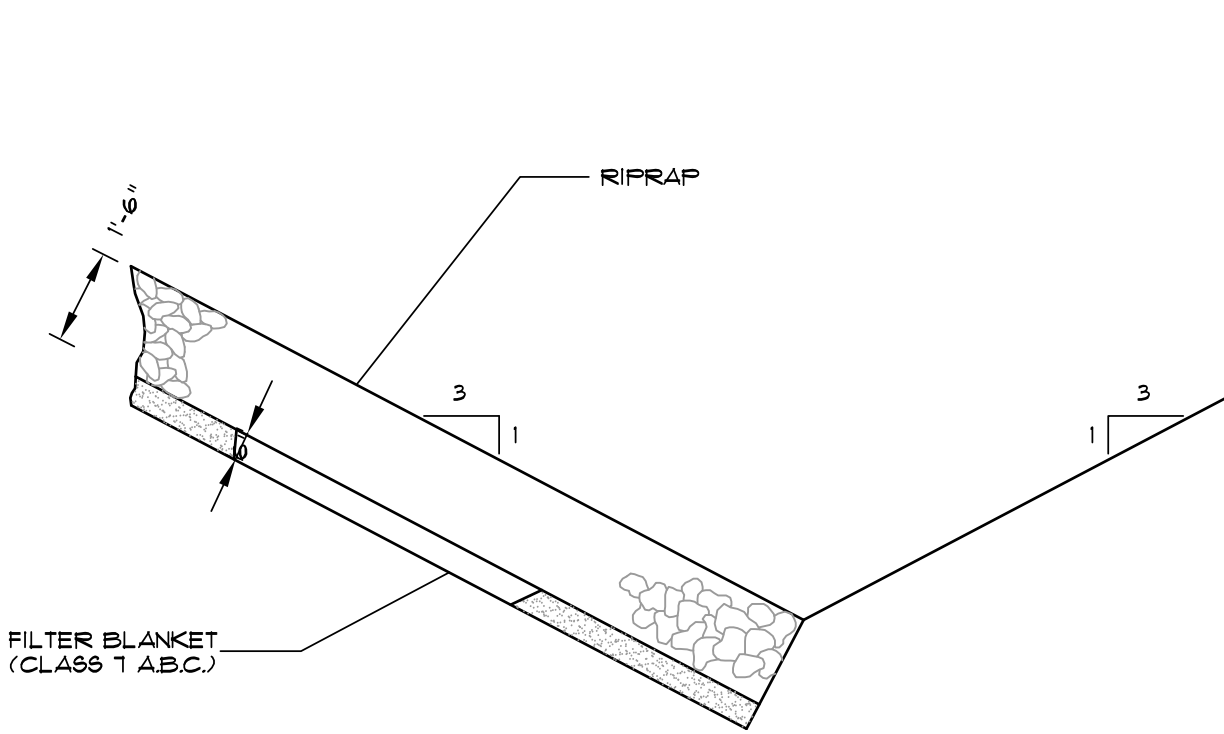
**BLOCK AND AGGREGATE INLET SEDIMENT FILTER**

SCALE: 1/2" = 1'-0"



**FILTER BARRIER**

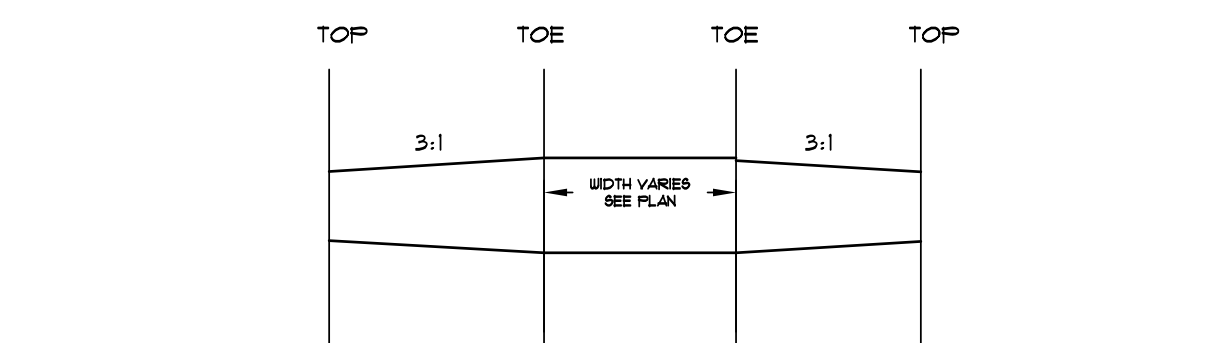
SCALE: 1/4" = 1'-0"



NOTE: IN LIEU OF AN AGGREGATE FILTER BLANKET, A SYNTHETIC FIBER GEOTEXTILE FABRIC MEETING THE REQUIREMENTS OF AASHTO M288 MAY BE USED. IN LIEU OF RIPRAP, AN EROSION CONTROL MAT MAY BE USED.

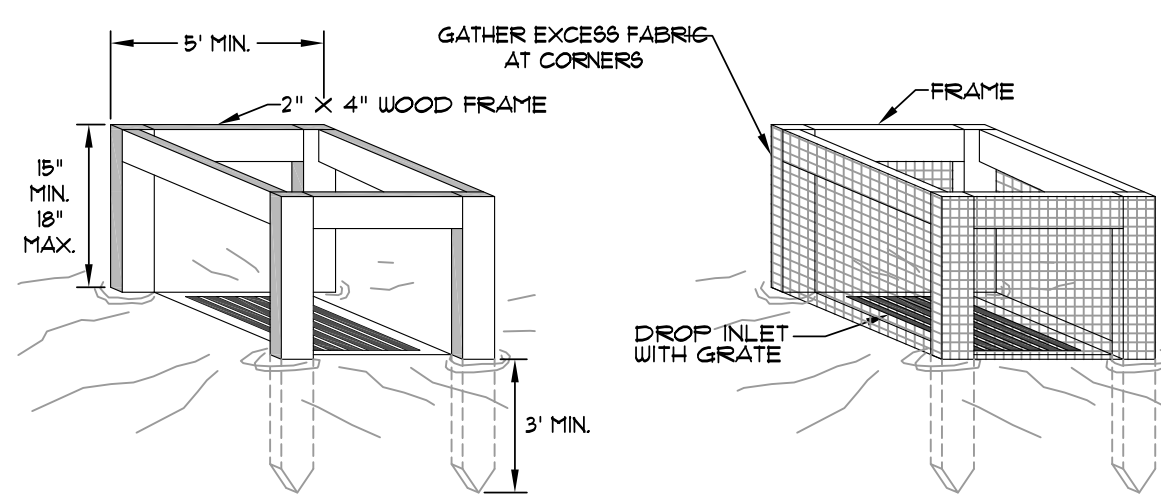
**SECTION - DUMPED RIPRAP**

TOE EXCAVATION IN SOIL



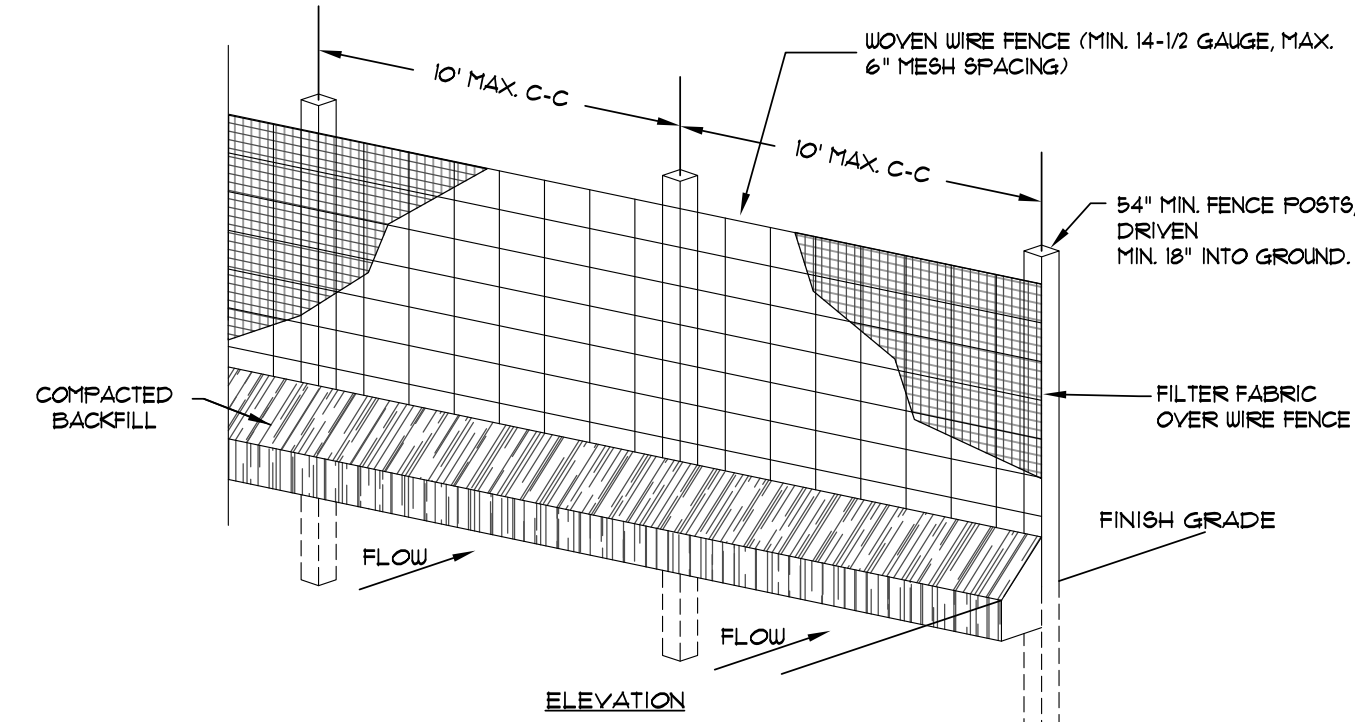
**ROCK CHECK DAM**

SCALE: 1/2" = 1'-0"



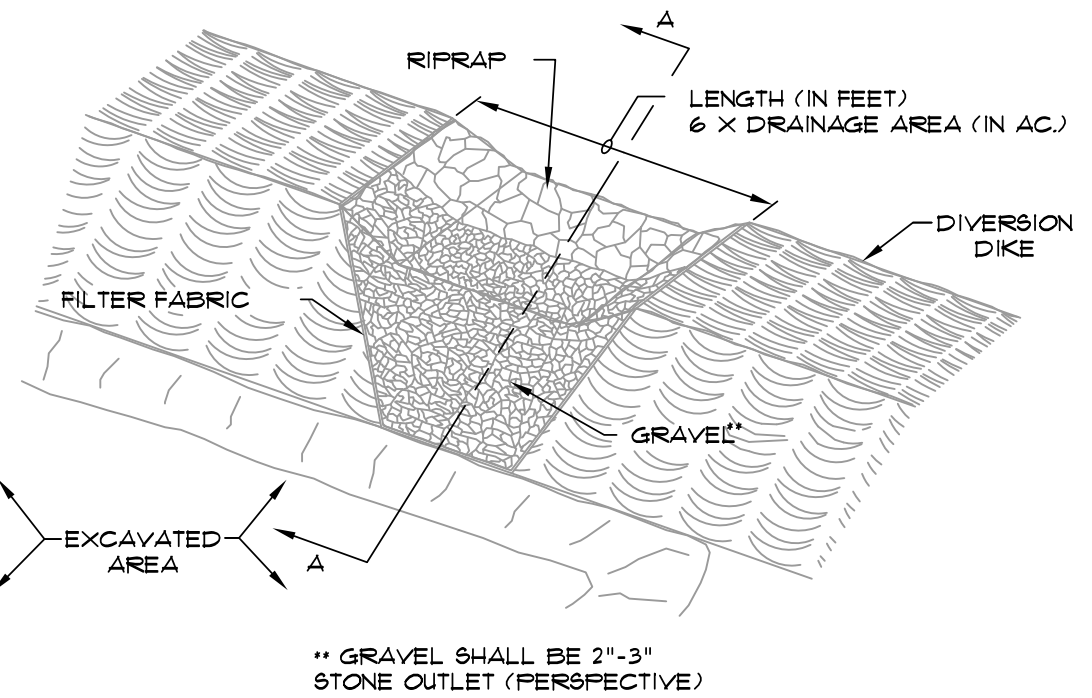
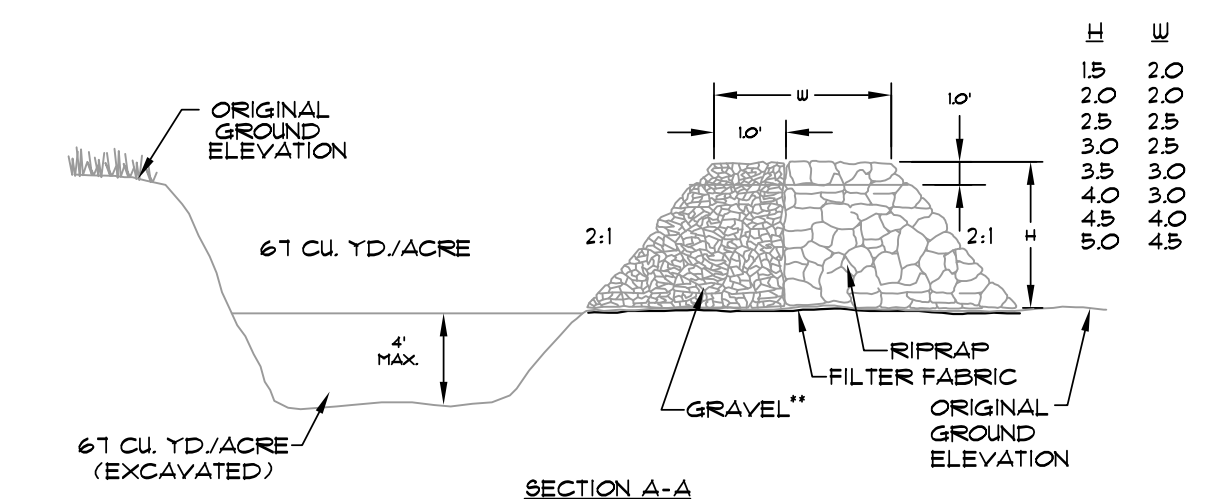
**SILT FENCE INLET PROTECTION**

SCALE: 1/2" = 1'-0"



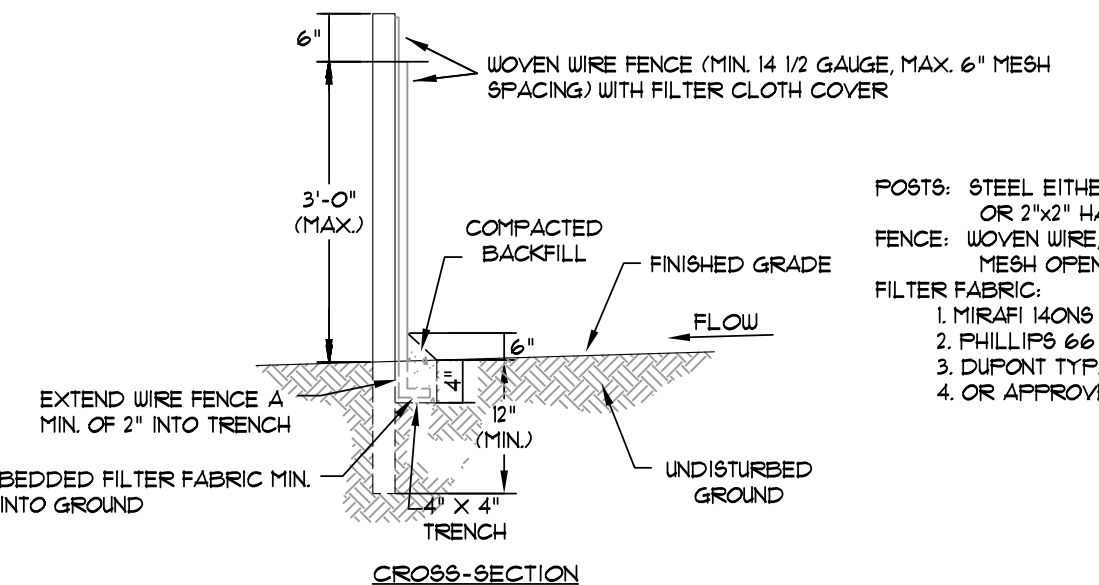
**GRAVEL AND WIRE MESH INLET SEDIMENT FILTER**

SCALE: 1/2" = 1'-0"



**TEMPORARY SEDIMENT TRAP**

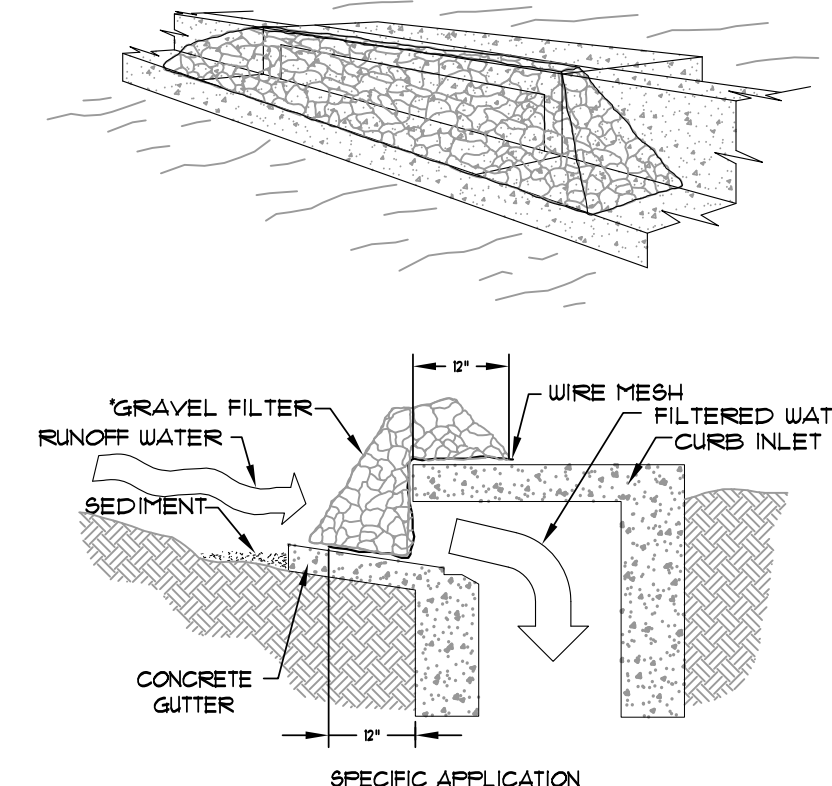
SCALE: 3/8" = 1'-0"



1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO FENCE POSTS WITH WIRE TIES OR STAPLES.
2. FILTER CLOTH TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID SECTION.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED BY SIX INCHES AND FOLDED.
4. MAINTENANCE SHALL BE PERFORMED AS NOTED IN THE EROSION CONTROL PLAN. COLLECTED MATERIAL SHALL BE REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE.

**SILT FENCE**

NOT TO SCALE



**GRAVEL CURB INLET SEDIMENT FILTER**

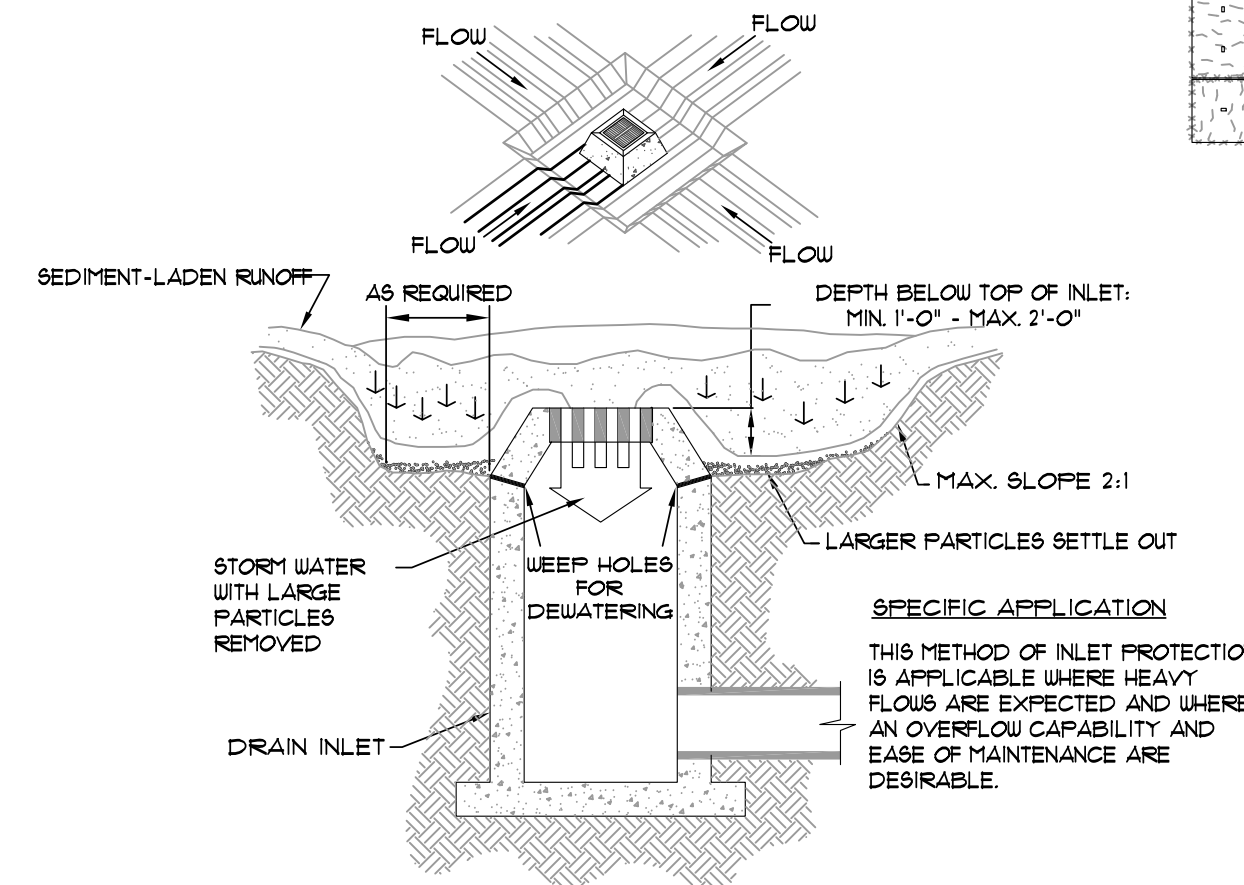
SCALE: 1/2" = 1'-0"

SPECIFIC APPLICATION: THIS METHOD OF INLET PROTECTION IS APPLICABLE AT CURB INLETS WHERE FLOWING IN FRONT OF THE STRUCTURE IS NOT LIKELY TO CAUSE INCONVENIENCE OR DAMAGE TO ADJACENT STRUCTURES AND UNPROTECTED AREAS.

\* GRAVEL SHALL BE 2"-3" STONE

**STRAW BALE INLET SEDIMENT FILTER**

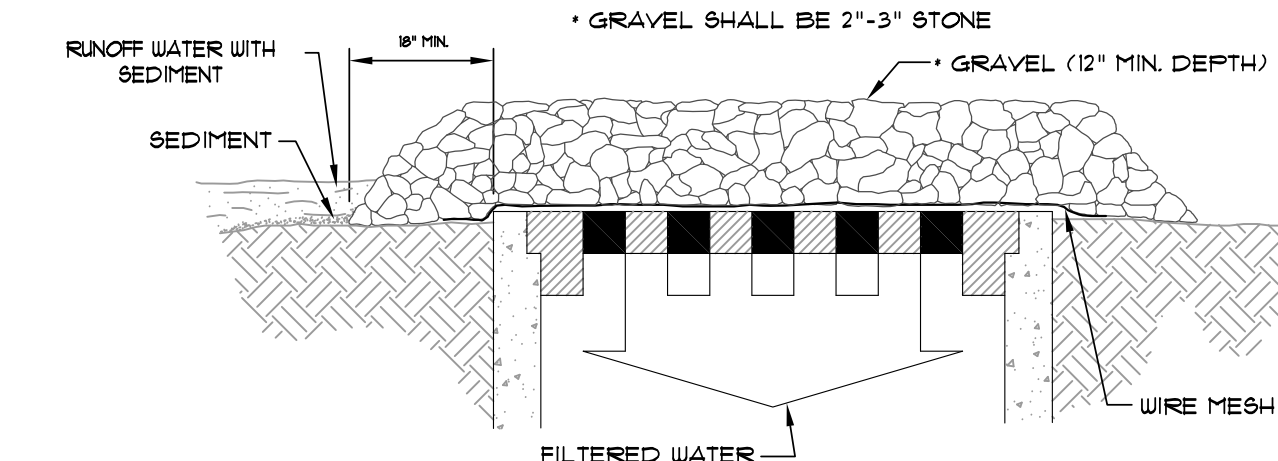
SCALE: 1/2" = 1'-0"



**EXCAVATED INLET SEDIMENT TRAP**

SCALE: 1/4" = 1'-0"

SPECIFIC APPLICATION: THIS METHOD OF INLET PROTECTION IS APPLICABLE WHERE HEAVY FLOWS ARE EXPECTED AND WHERE AN OVERFLOW CAPABILITY AND EASE OF MAINTENANCE ARE DESIRABLE.



**GRAVEL AND WIRE MESH INLET SEDIMENT FILTER**

SCALE: 1/2" = 1'-0"

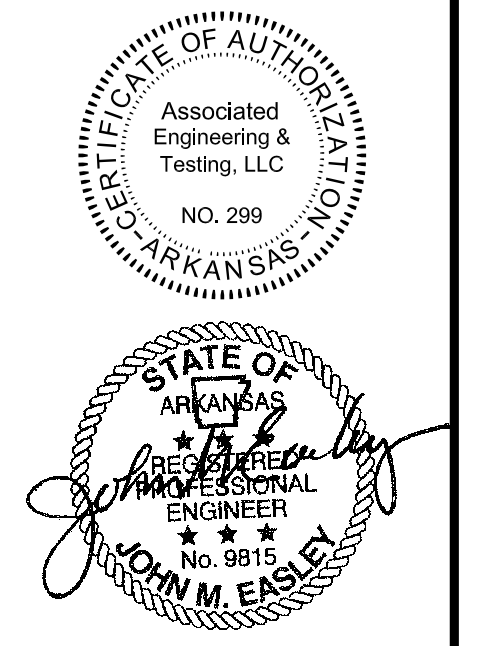
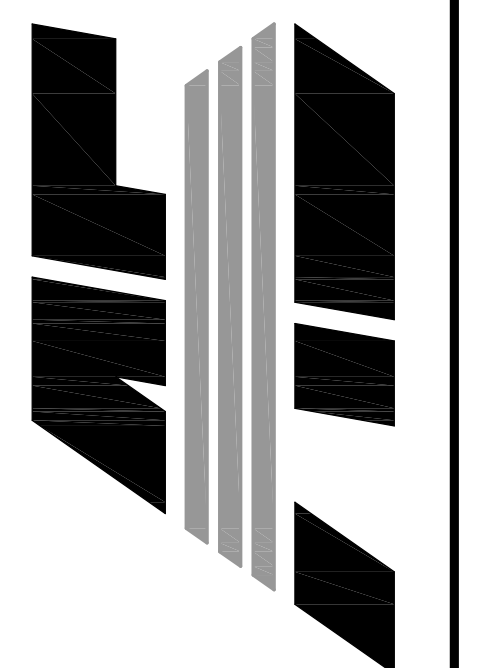
SPECIFIC APPLICATION: THIS METHOD OF INLET PROTECTION IS APPLICABLE WHERE HEAVY CONCENTRATED FLOWS ARE EXPECTED, BUT NOT WHERE FLOWING AROUND THE STRUCTURE MIGHT CAUSE EXCESSIVE INCONVENIENCE OR DAMAGE TO ADJACENT STRUCTURES AND UNPROTECTED AREAS.

GENERAL NOTES:

1. BALES SHALL BE EITHER WIRE-BOUND OR STRING-TIED WITH THE ENDINGS ORIENTED AROUND THE SIDES RATHER THAN OVER AND UNDER THE BALES.
2. BALES SHALL BE PLACED LENGTHWISE IN A SINGLE ROW SURROUNDING THE INLET WITH ENDS OF ADJACENT BALES PRESSED TOGETHER.
3. THE FILTER BARRIER SHALL BE ENTRENCHED AND BACKFILLED. A TRENCH SHALL BE EXCAVATED AROUND THE INLET THE WIDTH OF A BALE TO A MINIMUM DEPTH OF 4 INCHES. AFTER THE BALES ARE STAKED, THE EXCAVATED SOIL SHALL BE BACKFILLED AND COMPACTED AGAINST THE FILTER BARRIER.
4. EACH BALE SHALL BE SECURELY ANCHORED AND HELD IN PLACE BY AT LEAST TWO STAKES OR REBARS DRIVEN THROUGH THE BALE.
5. LOOSE STRAW SHALL BE WEDGED BETWEEN BALES TO PREVENT WATER FROM ENTERING BETWEEN BALES.

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NO.	DESCRIPTION	DATE
1	ADEQ COMMENTS	07/18/16

**EROSION CONTROL DETAILS**

DATE: 06/27/16 DRAIN: CCH  
 CADD FILE: 15165-SDP CHECKED: JME  
 DWG#: 0414104.000X SHEET  
 SCALE: 1" = 40' 0012