

## 1.0 Purpose and Intent

The purpose of the Downtown Jonesboro Development Code (DJDC) is to guide future development and redevelopment in the Downtown Jonesboro area by building on the success of the historic core to implement the vision for a more walkable, vibrant, mixed use neighborhood by:

- a) Preserving and enhancing the existing development character of downtown and adjoining neighborhoods through a comprehensive form-based approach to achieve mixed use, transitions, and pedestrian-oriented design.
- b) Creating appropriate Overlays to implement the vision for different areas of downtown - Core Mixed Use, Commercial Mixed Use, Industrial Arts and Neighborhood Transition, which implements the vision of The City of Jonesboro.
- c) Establishing appropriate urban design standards associated with each overlay zone including building form, use, and public open space standards with a focus on implementing pedestrian-oriented design.
- d) Establishing a clear review, approval, and appeals process for all development.

### 1.1 DJDC District Map

Within any district subject to the approved area of the DJDC district, City Code Section 117 becomes the mandatory regulations, unless specified in Subsection 2.2 (Relationship to other City ordinances). It shall establish the following development standards for all properties within the DJDC area:

- a) **Establishment of DJDC Districts:** The DJDC area is distinguished into different districts. Each district is intended to respond to distinct urban aspects within Downtown Jonesboro and is based on the illustrative vision for the different districts within the DJDC area. Each zone shall establish use and building form standards including standards for building height, mass, placement, functional design and parking. The DJDC districts classifies all lots within the DJDC area into one of the following four districts:
  - i. Core Mixed Use (COR) - The purpose of the Core Mixed Use District is to highlight the historic core of Downtown Jonesboro and expand the core to include key locations for mixed-use development, compatible with the downtown core. The intent is to mix commercial uses with some residential and public uses. Focusing on pedestrian facilities and unique experience destinations will be a key to establishing Downtown Jonesboro as a place for public interaction.
  - ii. Commercial Mixed Use (COM) - The purpose of the Commercial Mixed Use District is to be compatible with the Core Mixed Use through flexibility of uses, continuing to allow retail, education and commercial as primary uses, but also including residential as a core use to be located within Downtown Jonesboro. The focus is to transition this area as a more livable location, by upgrading infrastructure for development and redevelopment while also making this area friendlier to residential uses.
  - iii. Industrial Arts (IA) - The purpose of the Industrial Arts District is to focus this area on the goal to establish local artisanal manufacturing, light industrial and businesses close by downtown. These developments will take advantage of being accessible to rail and trails. The intent is to mix manufacturing, residential, and retail in a walkable yet flexible format.
  - iv. Neighborhood Transition (NBT) - The purpose of the Neighborhood Transition District is to support appropriate transition between existing residential neighborhoods and open space and the more active commercial and urban residential areas. The intent is to support the character of the neighborhoods around downtown. Additional transition requirements will also be included in the regulations for the other three districts.
- b) **Street Designations:** The streets within the DJDC area shall be classified by their street type in coordination with the context of the surrounding development and their use. Street cross sections shall address vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards. The streets sections complement the Complete

Streets Plan for the City of Jonesboro. Construction and composition requirements for streets are located in Section 58

- c) **Building Frontage Standards:** Building Frontage designations shall classify different block frontages based on the pedestrian priority goals of the vision and are labeled on the DJDC District Map (Attachment 1). Building frontages are categorized into one of the three types:
  - i. Pedestrian Priority Frontage has the highest quality standard for pedestrian-oriented building design. Focus is on wider sidewalks, ample clear space, appropriate street furniture, adequate lighting and limited vehicular conflict points within the streetscape area and by providing other reasonable accommodations for necessary utilities. Buildings will be required to address the pedestrian area and provide a higher quality of commercial design standards.
  - ii. Pedestrian-Friendly Frontage has a middle quality standard for pedestrian-oriented building design. Focus is on adequate sidewalks, ample clear space, appropriate street furniture, adequate lighting and limited vehicular conflict points within the streetscape area and by providing other reasonable accommodations for necessary utilities. Pedestrian-Friendly Frontages lead to Pedestrian Priority Frontages and expand connections to these areas into adjacent neighborhoods.
  - iii. General Frontage has a basic quality standard for pedestrian-oriented building design. Any frontage not labeled as a Pedestrian Priority or Pedestrian-Friendly is considered a General Frontage. Focus is on required pedestrian access, adequate clear space, select locations for street furniture, adequate lighting and safe vehicular-pedestrian interactions within the streetscape area.
- d) **Public Open Space Designation:** Public Open Space within the DJDC is denoted on the DJDC District Map (Attachment 1). Detailed standards for any additional Public Open Space or improvements to existing Public Open Space are included in Subsection 7 (Open Space Standards). These standards include general character, typical size, frontage requirements and typical uses within these public open spaces.

## 2.0 Administration and Development Phasing

### 2.1 Applicability

- a) The uses and buildings on all properties within the DJDC district classification shall conform exclusively to this section. Where this section is silent, the other appropriate sections of the City Code shall apply.
- b) Table 2-1 (Applicability Matrix) shall determine which Subsections of the DJDC might apply to any proposed development. Not all Subsections will apply to every circumstance, but review each Subsection to determine need to adhere to those Subsections as necessary and to understand all required conditions of the DJDC.
- c) Terms used throughout this Chapter are defined in Subsection 9 (Definitions). For those terms not defined in this Section, definitions within the general City Code shall apply. In the event of conflict with definitions within the general City Code, the definitions of this Section shall take precedence.
- d) Where in conflict, numerical metrics shall take precedence over graphic metrics.

### 2.2 Relationship to other City Ordinances

The development and subdivision standards under Chapter 113 & 117 shall not apply to projects within the DJDC except as specifically referenced herein.

Building and Fire Code, Drainage, Utility, Stormwater and other related technical requirements shall be applicable to the extent that it does not directly disrupt the intent of the code.

**Table 2-1: Applicability Matrix**

DJDC Section	Subsection 3: Schedule of Uses	Subsection 4: Building Form and Development Standards				Subsection 5: Building Design Standards	Subsection 6: Street Design Standards	Subsection 7: Open Space Standards	Subsection 8: Streetscape and Landscape Standards			
		Build to Zones and Setbacks	Building Frontage	Building Height	Parking and Service Access				Sidewalk, Street Trees and Streetscape	Street Screen	Street Lighting and Furniture	Utilities
<b>Type of Development</b>												
<b>Commercial, Mixed-Use, Lodging and Multi-Family</b>												
New Construction	•	•	•	•	•	•	•	•	•	•	•	•
Change of Use (without expansion of conditioned building area)	•				•					•		•
Expansion of Existing Buildings (see Attachment 3)		•	•	•	•	•		•	•	•	•	•
Existing Building Remodel (without expansion)	•											

n of condition ed building area)												
Expansio n of Parking Area		•	•		•	•	•	•	•	•		
<b>Single-Family Residential</b>												
New Construct ion	•	•	•	•	•	•	•	•	•	•	•	•
Change of Use (without expansio n of condition ed building area)	•				•							
Expansio n of Existing Buildings (see Attachme nt 3)	•	•	•	•	•	•		•	•	•		
Existing Building Remodel (without expansio n of condition ed building area)	•					•						

• = required to meet the requirements of the Subsection

**2.3 Development Review Process**

- a) The plan review process shall be required and administered in the same manner as required for the large scale development plans under Chapter 117
- b) Allowable Adjustments: The Director of the Department of Planning and Zoning or his or her designee or designee may approve allowable adjustments within the limits and per the criteria listed in Table 2-2 (Allowable Adjustments Table). All allowable adjustments shall be considered "meeting the requirements of the DJDC" in accordance with City and State legislative requirements. In no circumstance shall the Director of the Department of Planning and Zoning or his or her designee or designee approve an allowable adjustment that results in:
  - i) An increase in overall project intensity or density;
  - ii) A change in permitted uses or mix of uses;
  - iii) A change in the required frontage designation; or
  - iv) A change in any required element of the DJDC District Map and the DJDC beyond the thresholds established in Table 2-2 below.

<b>Table 2-2: Allowable Adjustments Table</b>		
<b>DJDC Standard</b>	<b>Extent of Allowable Adjustment Permitted</b>	<b>Criteria for Adjustment</b>
<b>DJDC District Map</b>		
Area/Boundary of DJDC Districts	No more than a 20% change (increase or decrease) in the area of any DJDC District (aggregate).	Lots being changed shall be a part of a larger development needing common zoning for the project.
<b>Building Form and Development Standards</b>		
Build to District and Setbacks	No more than a 20% change in the maximum or minimum setback applicable or 5 feet whichever is greater.	Changes to the Build to Zones and setbacks may only occur when they are caused by one or more of the following: <ol style="list-style-type: none"> <li>1. Need to accommodate existing buildings and structures on the lot that meet the overall intent and vision for redevelopment in the DJDC area; or</li> <li>2. Need to accommodate other required modes of transportation (transit, bike, pedestrian), stormwater drainage, water quality, or low impact development (LID) elements on the site; or</li> <li>3. Need to accommodate overhead or underground utilities and/or easements; or</li> <li>4. Need to preserve existing trees on the property.</li> </ol> This adjustment is not mandatory for tree preservation, but for the allowance for preservation; or

		5. Need to provide public amenities along the sidewalk (outdoor dining/seating, larger sidewalk, or other similar public amenities).
Building Frontage	No more than a 15% reduction in the required building frontage along each subject lot with Pedestrian-Friendly or General Frontage designation.	Any reduction in the required building frontage shall be to address one or more of the following to accommodate: 1. Existing buildings and site elements; or 2. Other required transit, bike and pedestrian, stormwater drainage, or water quality elements on the site.
Building Heights	Increase in height of building for providing a public amenity.	Height may be increased by one additional story in COM and IA Districts if a public open space is provided on-site. The public open space must meet the standards of the Public Open Space Types in Attachment 4 of this Section. Public Open Space may be privately owned and maintained.
Sidewalk and Streetscape Standards	Sidewalks, Street Trees, Street Lighting, and other streetscape standards may be adjusted based on the development context, street cross section or larger public infrastructure plan.	Any changes to the streetscape standards shall be based on specific development context such as existing vegetation, natural features, utilities, drainage and fire safety.
Required Parking Spaces	Reduction in the number of required parking spaces.	Reduction in the number of parking spaces shall be based on one or more of the following: 1. A shared parking plan for parking within 1,000 feet of the subject property; or 2. A parking study for the uses proposed on the site; or 3. A combination of the above.
<b>Other</b>		
Any other numerical standard in the Section	An adjustment up to 10% (increase or decrease).	An adjustment of a numerical standard is needed to accommodate existing conditions. The proposed development still meets the intent of the DJDC District.
Phased Developments	Deferment of building frontage standards.	Phased developments may defer building frontage requirements as long as they meet the Build to Zone and parking setback requirements and utility requirements.

- c) Modifications:
  - i) The Jonesboro Board of Zoning Adjustments (BZA) shall first review projects that request a modification beyond the standards in the DJDC authorized above in Table 2-2 (Allowable Adjustments Table).
  - ii) The BZA shall review only the modification that exceeds the allowable adjustment within the DJDC and shall rule only in the case that this modification meets the following criteria:
    - 1. The goals, intent and vision of the adopted Downtown Initiative Plan;
    - 2. The extent to which the proposal fits the adjoining design context by providing appropriate building scale and use transitions;
    - 3. The extent to which the proposal provides public benefits such as usable public open space, livable streets, affordable housing, structures and/or shared parking, and linkages to transit;
    - 4. The extent to which the proposal does not hinder future opportunities for high intensity development; and
    - 5. Consideration of health and welfare of the general public.
  - iii) If BZA denies the requested modification, applicant must perform a redesign of the plan in order to meet the standards or request other modifications through BZA.
  - iv) If BZA approves the requested modification, application is passed to the Metropolitan Area Planning Commission (MAPC) for review of the remainder of the plan. During review of the plan, MAPC must review the plan for its merits with the consideration that the modification has been approved.
  - v) Review Considerations for Metropolitan Area Planning Commission - In reviewing any plans after a modification request to standards in this Section, the Metropolitan Area Planning Commission shall use the following criteria:
    - 1. The goals, intent and vision of the adopted Downtown Initiative Plan;
    - 2. The extent to which the proposal fits the adjoining design context by providing appropriate building scale and use transitions;
    - 3. The extent to which the proposal provides public benefits such as usable public open space, livable streets, affordable housing, structured and/or shared parking, and linkages to transit;
    - 4. The extent to which the proposal does not hinder future opportunities for higher intensity development; and
    - 5. Consideration of health and welfare of the general public.

#### **2.4 Plat Approval Process**

The applicant shall follow the process for the plat approval subject to the requirements for Subdivision in Chapter 113.

#### **2.5 Nonconforming Uses**

Shall meet standards in Section 117, Nonconforming lots, uses of land, structures, and uses of structures and premises.

#### **2.6 Noncomplying Structures and Sites**

A noncomplying structure or site may be altered or enlarged, provided that such alteration or enlargement shall neither create any new nonconformity, nor shall increase the degree of the existing nonconformity of

all or any part of such structure or site. Attachment 3 shall provide examples of acceptable additions to noncomplying structures in the DJDC area.

**2.7 Amendments to the DJDC**

Amendments and changes to the DJDC District Map, text and property boundaries beyond those expressly permitted under this Section shall be in accordance with the procedure set out in the City Code.

**3.0 Schedule of Permitted Uses**

**3.1 Applicability**

Due to the emphasis on urban form over land uses in the DJDC, general use categories have been identified by district (Table 3-1: Schedule of Uses).

- a) Use Determination: The Director of the Department of Planning and Zoning or his or her designee or designee is responsible for categorizing all uses. If a proposed use is not listed in a use category, but is similar to a listed use, the Director of the Department of Planning and Zoning or his or her designee or designee shall place the proposed use under that use category. A use not specifically listed shall be prohibited unless it is found to be similar to another listed use as described using the criteria below. The Director of the Department of Planning and Zoning or his or her designee or designee 's decision may be appealed to the MAPC for final determination. When determining whether a proposed use is similar to a listed use, the Director of the Department of Planning and Zoning or his or her designee or designee may consider any other following relevant criteria, especially as it relates to any direct impacts on adjoining roadways and/or neighborhoods:
  - i) The actual or projected characteristics of the proposed use;
  - ii) The relative number of employees;
  - iii) Hours of operation;
  - iv) Buildings and site arrangement;
  - v) The relative amount of site area or floor area and equipment devoted to the proposed use or uses;
  - vi) Types of vehicles used and their parking requirements;
  - vii) Relative amounts of sales;
  - viii) Transportation demand by all modes, including the number of vehicle, transit, and bike/walk trips generated and whether existing or proposed infrastructure is adequate to serve those trips;
  - ix) The likely impact on surrounding properties; or
  - x) Whether the activity is likely to be found independent of the other activities on the site.

**3.2 Uses Permitted with Criteria**

Certain uses are permitted with criteria established in Table 3-2. These uses will only be permitted to documented adherence to the design criteria and by approval of the Director of the Department Planning and Zoning or his or her designee or designee.

<b>Table 3-1: Schedule of Uses</b>			
<b>P = Permitted</b>	<b>— = Not Permitted</b>	<b>P/C = Permitted with criteria</b>	<b>A = Accessory Use</b>



DJDC District	Core Mixed Use (COR)	Industrial Arts (IA)	Commercial Mixed Use (COM)	Neighborhood Transition (NBT)
<i>Land Use</i>				
<b>Commercial Uses (Office, Retail, Sales and Service Uses)</b>				
Art, antique, museum, furniture or galleries (retail, repair or fabrication; excludes auto or electronics sales or service)	P	P	P	P
Auto-related sales establishments	—	—	—	—
Finance, insurance, and real estate establishments including banks, credit unions, real estate, and property management services, with no drive through facility	P	P	P	P
Fitness, recreational sports, gym, athletic club, dance or yoga studio	P	P	P	—
Food service uses such as full-service restaurants, cafeterias, bakeries catering and snack bars with no drive through facilities. Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages.	P	P	P	P/C
Offices for business, professional, administrative, and technical services such as accountants, architects, lawyers, doctors, etc.	P	P	P	P
Pet and animal sales or service, with no outside kennels	P	P	P	—
Retail sales or service with no drive through facility	P	P	P	—
Retail sales or service with drive through facility	—	—	—	—
Theater, cinema, or music venue	P	P	P	—
<b>Educational, Public Administration, Health Care and Other Institutional Uses</b>				

Business associations and professional membership organizations	P	P	P	P
Child day care and preschools	P	P	P	P
Funeral homes	P	P	P	—
Hospitals and nursing establishments	P	P	P	—
Public administration uses (including local, state, and federal government uses, public safety, health and human services and utilities)	P	P	P	P
Schools, libraries, community/civic facilities and religious institutions	P	P	P	P
Social, fraternal and philanthropic organizations	P	P	P	—
Universities and colleges and technical, trade, and specialty schools	P	P	P	—
<b>Residential and Lodging Uses</b>				
Accessory building residential unit (garage apt.)	—	P	—	P
Bed and breakfast (10 or fewer rooms and owner managed)	P	P	P	P
Hotel	P	P	P	—
Manufactured housing	—	—	—	—
Multi-family residential ground floor *See Subsection 5.4 - Commercial Ready Standards	P*	P	P	P
Multi-family residential upper floors	P	P	P	P
Multi-unit home	P	P	P	P
Single-family residential, attached dwelling unit (townhomes)/patio home, duplex	P	P	P	P
Single-family residential, detached	—	—	—	P

<b>Manufacturing, Transportation, Communication, and Utility Uses</b>				
Auto service establishment	—	—	—	—
Brewery, distillery and winery	P/C	P/C	P/C	—
Commercial food, textile and product manufacturing	—	P	—	—
Heavy manufacturing that may produce hazardous waste	—	—	—	—
Mini-storage	—	—	—	—
Miscellaneous light manufacturing (manufacturing processes that do not create hazardous waste)	P	P	—	—
Transportation services (air, rail, road, truck and freight)	P	P	—	—
Telecommunications and broadcasting (radio, TV, cable, wireless communications, telephone, etc.)	P	P	P	—
Utilities and utility services (electric, natural gas, alternative)	P/C	P/C	P/C	P/C
Wholesale trade establishment/warehouse and storage services	A	A	—	—
<b>Other Uses</b>				
Food trucks, food truck parks (See City of Jonesboro Code of Ordinances for standards on food trucks and food truck parks)	P	P	P	—
Home occupations	A	A	A	A
Parking, structured	P	P	P	—
Temporary use, other	P/C	P/C	P/C	P/C
Temporary use, surface parking lot	P/C	P/C	P/C	—
Veterinary clinic with outdoor services	—	—	—	—

<b>Table 3-2: Use Criteria Table</b>		
<b>Use</b>	<b>DJDC Zone</b>	<b>Use Criteria</b>
<b>Manufacturing, transportation, communication, and utility uses</b>		
Brewery, Distillery and Winery	Core Mixed Use Commercial Mixed Use Industrial Arts	All brewing, distillery or winery equipment and process shall be conducted indoors at all times.
Utilities and utility services (electric, natural gas, alternative)	All DJDC Districts	<p>i) Outdoor storage of fleet vehicles, service areas, utility boxes and equipment may be permitted located along Pedestrian Priority Streets with proper screening approved by the Jonesboro Metropolitan Area Planning Commission.</p> <p>ii) All such areas along other streets shall be screened with a required street screen (at least as high as the equipment being screened) (see Subsection 8.3 for standards). Any street trees or shrubbery planted in a public right-of-way, platted easement must meet the applicable requirements under Section 117-326.</p>
<b>Other Uses</b>		
Temporary Use, Other	All DJDC Districts	Any temporary use shall require a time limit not to exceed two years and be approved by the Metropolitan Area Planning Commission. After the two-year time limit, the applicant may receive a renewal of the temporary use.
Temporary Use, Surface Parking Lot	Core Mixed Use Commercial Mixed Use Industrial Arts	<p>i) New surface parking lots shall be permitted as an interim use of property if they are the primary use of property.</p> <p>ii) Applications for new surface lots shall include in-fill building concepts on the lot with a site plan that meets the build-to-zone and building frontage standards of the specific character zone.</p> <p>iii) New surface parking shall be set back a minimum of 30' from the edge of the right-of-way of Pedestrian Priority Streets.</p> <p>iv) New surface parking shall not be located at a street intersection (of any Pedestrian Priority and Pedestrian-Friendly Streets only) for minimum of 30' from the intersection along each street.</p>

**4.0 Building Form and Development Standards**

Building form and development standards applicable to each DJDC district are described in this Subsection. The images and graphics through this Subsection are provided as illustrative of intent and are advisory only. Refer to the standards on the following pages for the specific standards.

In addition, the graphics used to illustrate the standards in each DJDC district are NOT intended to indicate exact conditions within each DJDC district. Rather, illustrations are conceptual and standards are to be applied based on the specific frontage types designated along the subject property or site. Also, the illustrations may depict other site elements to establish context and only the standards regulated by the specific Subsection shall apply.

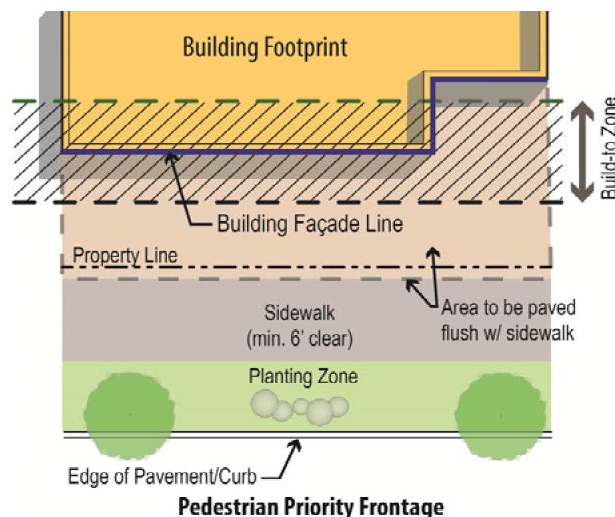
Building form graphics in this Subsection are NOT TO SCALE.

#### 4.1 General to All DJDC Districts

a) **Building Frontage Designations:** The Building Frontage designations are applied as depicted on the DJDC District Map to specify certain building form and site development standards. Building frontages are classified into one of the following three categories:

i) Pedestrian Priority:

1. Pedestrian Priority Frontages are intended to provide the most pedestrian-friendly and contiguous development context. Buildings and sites along Pedestrian Priority Frontages shall be held to the highest standard of pedestrian-oriented design and few, if any, gaps shall be permitted in the "street wall." Breaks in the street wall may be permitted for courtyards, forecourts, sidewalk cafés, and pedestrian connections between the individual sites and the public sidewalk. These street frontages are the main retail, restaurant, and entertainment streets or are important neighborhood connectors as identified in the DJDC District Map.
2. The area between the building façade and property line or edge of any existing sidewalk along any street with Pedestrian Priority Frontage shall be designed such that the sidewalk width shall be the minimum of the designated street cross section and the remainder of any setback area shall be paved flush with the public sidewalk. Sidewalk cafés, public art, landscaping within tree-wells, or planters may be incorporated within this area.

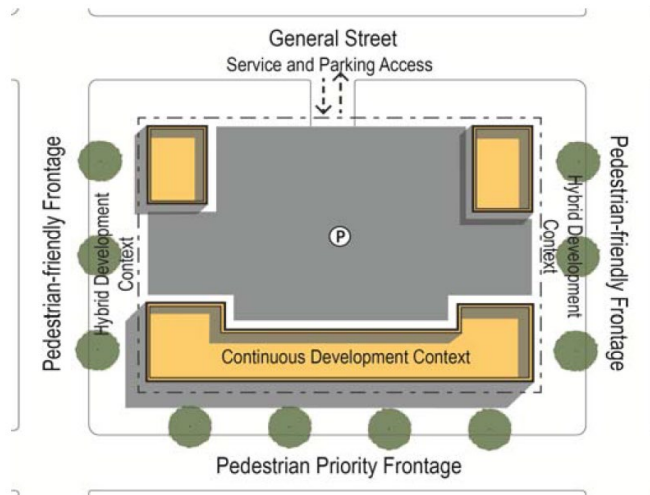


*Illustration showing requirements along Pedestrian Priority Frontages*

ii) Pedestrian-Friendly - Pedestrian-Friendly Frontages are also intended to be pedestrian-oriented with a mostly contiguous development context. However, in some locations, where access to a General Frontage street or Alley is not available, Pedestrian-Friendly Frontages may need to

accommodate driveways, parking, service/utility functions, and loading and unloading. In such cases, Pedestrian-Friendly Frontages may balance pedestrian orientation with automobile accommodation. Typically, they shall establish a hybrid development context that has a more pedestrian-supportive development context at street intersections and accommodates auto-related functions and surface parking in the middle of the block. Surface parking shall be screened from the roadway per Subsection 8.3 of this Section. Pedestrian-Friendly Frontages are designated on the DJDC District Map.

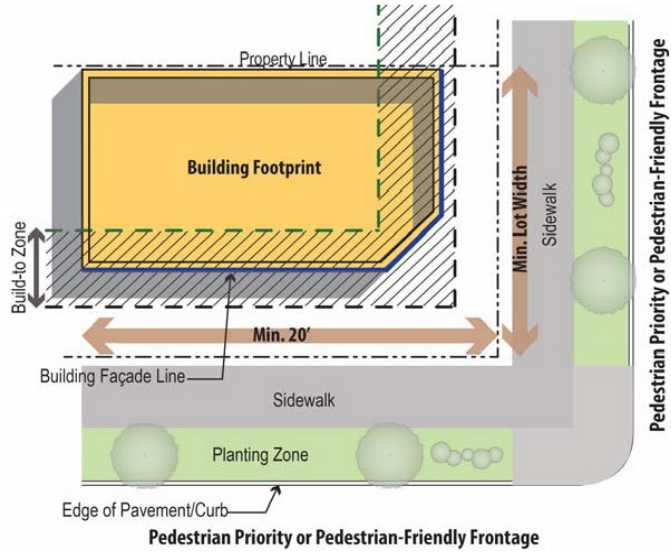
- iii) General - General Frontages are intended to accommodate more auto-oriented uses, surface parking, and service functions on a site with a more suburban/automobile orientation. The General Frontages shall be any frontage not designated as either a Pedestrian Priority or Pedestrian-Friendly Frontage on the DJDC District Map.



*Illustration showing the application of Frontage Types and their related development context*

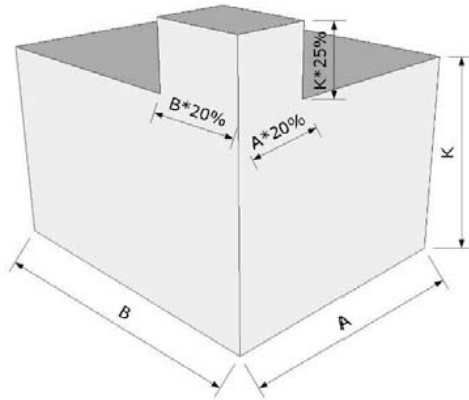
**b) Treatment of Street Intersections:**

- i) At the intersections of Pedestrian Priority and Pedestrian-Friendly Frontages, corner building street façades shall be built within the Build-to-Zone (BTZ) for a minimum of 30 feet from the intersection, along each street, or the width of the lot, whichever is less.
- ii) Corners of buildings are permitted to have curved or chamfered forms, recessed entries, or public open space at these corners.
- iii) In the case of public open space at the corner, the building shall wrap along the public open space.



*Illustration showing minimum frontage requirements at an intersection of Pedestrian Priority and Pedestrian-Friendly Frontages*

- iv) Corner buildings may exceed the maximum building height by 25% along no more than 20% of the building frontage along each corresponding street façade.



*Illustration showing Corner Building Height Allowance*

**c) Parking and Service Access:**

- i) Location of parking (structured and surface lots) shall be per DJDC district form standards, Subsections 4.2 through 4.5.
- ii) Required off-street parking spaces shall be calculated per Table 4-1 below.

Table 4-1: Parking Ratios			
DJDC District	COR, COM, IA	NBT	Additional Criteria
<b>Minimum Off-Street Vehicular Parking Requirements</b>			

All non-residential uses and ground floor commercial spaces	No requirement	1 space per 300 square feet of building area	<ol style="list-style-type: none"> <li>1. Landscaping within surface parking lots shall meet standards in Subsection 8 of this Section.</li> <li>2. A shared parking plan or alternative parking plan may be approved by the Director of Planning and Zoning or his or her designee or designee as an Allowable Adjustment (Subsection 2).</li> </ol>
Residential Uses	1.0 spaces per dwelling unit	1.5 spaces per dwelling unit	
Lodging Uses (bed and breakfast and hotels)	0.5 spaces per guest room; all other areas shall be parked at the non-residential rate above.	1.0 spaces per guest room; all other areas shall be parked at the non-residential rate above.	
<b>Minimum Bicycle Parking Requirements (in addition to vehicular parking)</b>			
All non-residential uses and ground floor commercial spaces	2 spaces for up to 3,000 square feet and 1 space for every 1,000 square feet additional	N/A	<ol style="list-style-type: none"> <li>1. Bicycle Parking may be accommodated with design appeal and within key locations on site.</li> <li>2. Bicycle parking located on-street counts towards bicycle parking requirement.</li> </ol>
Multi-Family Residential	15% of all required vehicular parking	20% of all required vehicular parking	

iii) Driveways, Alleys and Service Access:

1. Unless otherwise specified in the specific DJDC district standards in Subsections 4.2 through 4.5, driveways and off-street loading and unloading may be located with access along a Pedestrian-Friendly Frontage street only if the property has no access to either an alley, General Frontage street or joint use easement to an adjoining property with direct driveway access to any other street.
2. Unless otherwise specified in the specific DJDC district standards in Subsections 4.2 through 4.5, driveways and off-street loading and unloading may be located with access along a Pedestrian Priority Frontage street only if the property has no access to either an alley, Pedestrian-Friendly or General Frontage Street or joint use easement to an adjoining property with direct access to any other Street.
3. Along Pedestrian Priority and Pedestrian-Friendly Frontages, driveway spacing shall be limited to one driveway per each block face or per 200 feet of block face for blocks greater than 400 feet in length.
4. Shared driveways, joint use easements or joint access easements shall be required for adjoining properties when driveway and service access is off a Pedestrian Priority Frontage or Pedestrian-Friendly Frontage.
5. Service and loading/unloading areas shall be screened per standards in Subsection 8.3.
6. Unless required to meet minimum fire access or service access standards all commercial and mixed use driveways/alleys shall be a maximum of 20 feet in width. Service



driveways/alleys shall be a maximum of 30 feet in width. Driveways wider than 24 feet in width shall only be located off of General Frontage streets. Driveways/alleys along State controlled roadways shall meet Arkansas Department of Transportation (ArDOT) Standards or the City's adopted ordinances and regulations.

7. Residential Driveways:

- a. Unless required to meet minimum fire access or service access standards, driveways for Residential Use Buildings shall be a maximum of 12 feet in width.
- b. Garages for Residential Buildings shall be located on streets with General Frontage, Alleys or at the rear of residential buildings with pull-through garages where the garage door is set back behind the rear façade of the main structure. If front-loaded garages or carports are utilized on residential uses, the garages and carports shall be no greater than to fit two cars and set back at least 20 feet measured from the Building Façade Line of the primary structure. This restriction does not apply for J-swing garages.
- c. Front-loaded garages on residential lots less than 40 feet wide shall not be allowed. Townhomes and courtyard apartments shall utilize garages with access from streets with General Frontage, Alleys, or joint use easement to an adjoining property with direct access to any other Street.

8. Utilities may be located along a Predestrian-Friendly street if the utility has no other reasonable access to either an alley, General frontage street or joint use easement to an adjoining property.

d) **Measuring heights:**

- i) Chimneys, vents, elevator and stair enclosures, screened HVAC equipment, other mechanical enclosures, tanks, solar energy systems and similar elements are exempt from the height limit.
- ii) Internal building height shall be measured from finished floor to the bottom of the structural members of the ceiling.
- iii) Floor-to-floor heights shall not apply to parking structures or civic buildings.
- iv) Permitted corner tower elements are exempt from the height limit, except to the extent of the permitted tower height as denoted in Subsection 4.1(b) of this Section.

e) **Encroachments:**

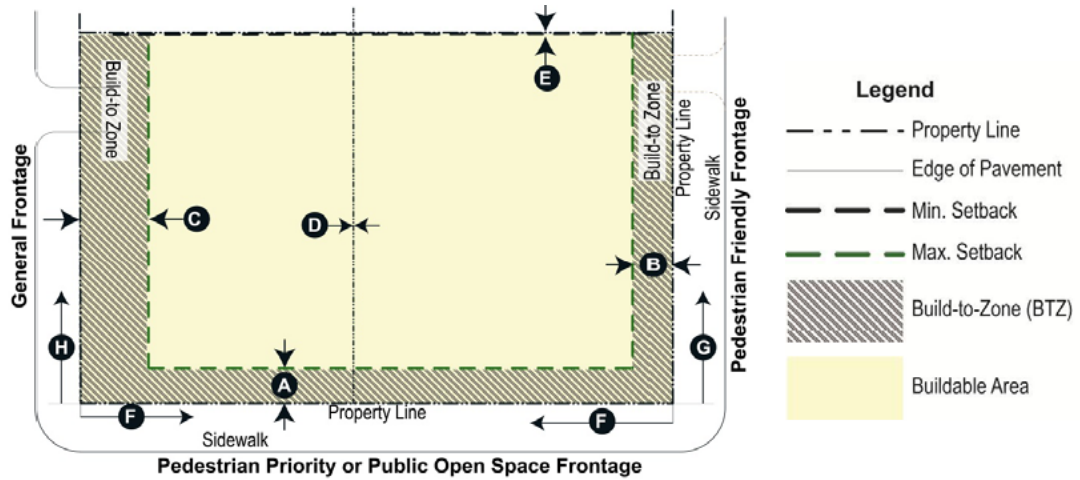
- i) Encroachments into R-O-W:
  1. Maximum of 50% of the depth of the sidewalk when there is no vertical support for the object (except blade signs which shall encroach no more than 6 feet from the building façade line).
  2. Maximum of 100% of the depth of the sidewalk when using a gallery, arcade or colonnade system.
  3. Minimum vertical clearance from the finished sidewalk shall be 8 feet.
  4. In no case shall an encroachment be located over an on-street parking area, travel lane or landscaping/street trees.
  5. Encroachments over ArDOT roadways shall adhere to ArDOT policy.
- ii) Overhangs within Required Setbacks: Canopies, awnings, galleries, and balconies may overhang within any required setback area per standards established in each DJDC district as long as the vertical clearance is a minimum of 8 feet from the finished sidewalk elevation.

- f) **Phased Developments:** Due to the infill nature of development within the DJDC, certain building form and site development standards may be deferred for phased development projects meeting the following criteria:
  - i) Submission of a site plan that illustrates how development and any related private improvements will be phased over time. Each phase of the site plan shall independently comply with all applicable standards of the DJDC unless an Allowable Adjustment is granted.
  - ii) Required private landscaping and open space amenities may also be phased with the building to permit final landscaping when adjacent buildings are finished.
  - iii) A letter of credit will be required for any phased development.
- g) **Set Back Lines:** Set back lines may be adjusted with each district as necessary to allow placement of utilities.

#### 4.2 Core Mixed Use (COR)

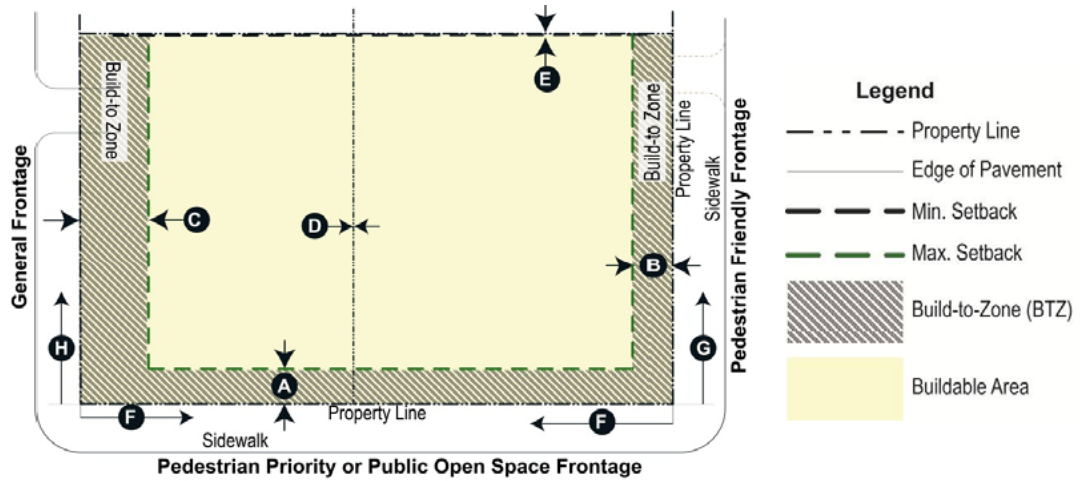
- a) **Illustrations and Intent:** The purpose of the Core Mixed Use Zone is to highlight the historic core of Downtown Jonesboro and to expand the core to include key locations for mixed-use development that are compatible with the downtown core. The intent is to mix commercial uses with some residential and public uses. Focusing on pedestrian facilities and unique experience destinations will be a key to establishing Downtown Jonesboro as a place for public interaction.

Development Standards within this DJDC district specifically address the unique aspects of redevelopment in this area by retaining the existing development pattern while allowing higher density, pedestrian-oriented development to occur. In addition, the standards allow phasing of redevelopment and encourage the use of existing buildings and parking lots to the extent possible.



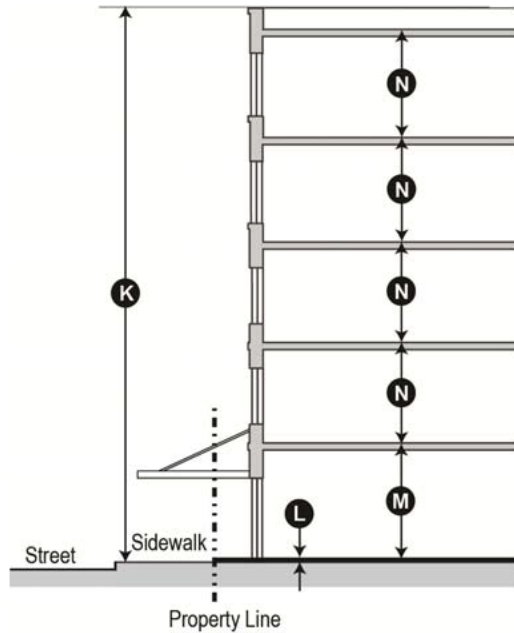
<b>(i) Build-to Zones (BTZs) and Setbacks (Distance from property line to edge of the zone)</b>		
Pedestrian Priority Frontage /Open Space (BTZ)	0' min. setback – 15' max. setback	<b>A</b>
Pedestrian-Friendly Frontage (BTZ)	0' min. setback – 25' max. setback	<b>B</b>
General Frontage	0' min. setback – 30' max. setback	<b>C</b>
Side	0' min. setback; no max. setback	<b>D</b>
Rear	0' min. setback; no max. setback	<b>E</b>
<b>(ii) Building Frontage</b>		
Pedestrian Priority /Open Space Frontage	90% min.	<b>F</b>
Pedestrian-Friendly Frontage	70% min.	<b>G</b>
General Frontage	30% min.	<b>H</b>

b) **Building Placement:**



<b>{i) Build-to Zones (BTZs) and Setbacks (Distance from property line to edge of the zone)</b>		
Pedestrian Priority Frontage /Open Space (BTZ)	0' min. setback – 15' max. setback	<b>A</b>
Pedestrian-Friendly Frontage (BTZ)	0' min. setback – 25' max. setback	<b>B</b>
General Frontage	0' min. setback – 30' max. setback	<b>C</b>
Side	0' min. setback; no max. setback	<b>D</b>
Rear	0' min. setback; no max. setback	<b>E</b>
<b>{ii) Building Frontage</b>		
Pedestrian Priority /Open Space Frontage	90% min.	<b>F</b>
Pedestrian-Friendly Frontage	70% min.	<b>G</b>
General Frontage	30% min.	<b>H</b>

c) **Building Height:**



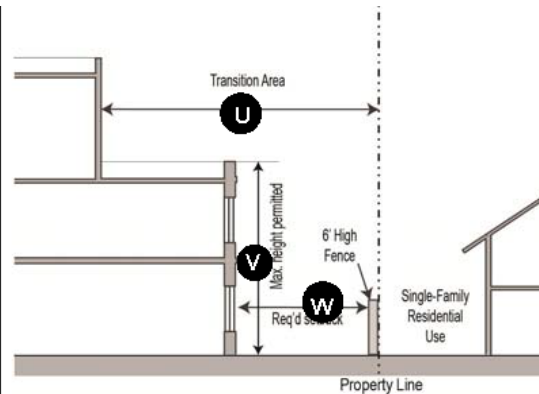
<b>(i) Principal Building Standards</b>		
Building height	<ul style="list-style-type: none"> <li>Maximum height limited by the Airport Hazard Zone</li> <li>Transition requirement if adjacent to detached single-family zoned residential (Subsection 4.2 (d))</li> </ul>	<b>K</b>
First floor to ceiling height (fin. floor to structure of ceiling)	<ul style="list-style-type: none"> <li>12' min. for all frontages</li> </ul>	<b>M</b>
Ground floor finish level	<ul style="list-style-type: none"> <li>Maximum 12" rise for commercial ground floor.</li> </ul>	<b>L</b>
Upper floor(s) height (fin. floor to structure of ceiling)	<ul style="list-style-type: none"> <li>9' min.</li> </ul>	<b>N</b>
<b>(ii) Accessory Building Standards</b>		
Accessory buildings shall meet the standards for Principal Building standards in the Core Mixed Use Zone.		

d) **Residential Transition Standards:**

The following transition standards shall apply to all new building construction and all upper story additions to existing buildings located adjacent to any existing single-family detached residentially zoned lots. This requirement shall NOT apply if an alley or other similar R-O-W separates the subject lot and the existing single-family detached residential lot.

i.	Transition Area	30 feet min.	<b>U</b>
ii.	Max. Building Height at/within Transition Area	3.5 feet max.	<b>V</b>
iii.	Required setback	10 feet min.	<b>W</b>

A Residential Transition Area fence (minimum 6 feet and maximum 8 feet high) shall be required when adjacent to any existing single-family detached residentially zoned lot and shall be optional for all other adjacencies. The required fence shall be constructed of masonry or block. A 6-foot wide landscape buffer with evergreen shrubs planted at 3' on center and 6' min. in height at maturity shall also be required to be planted within the landscape buffer parallel to any single-family residential lot line. (see surface parking setbacks illustration; Subsection 4.2 (e) i)



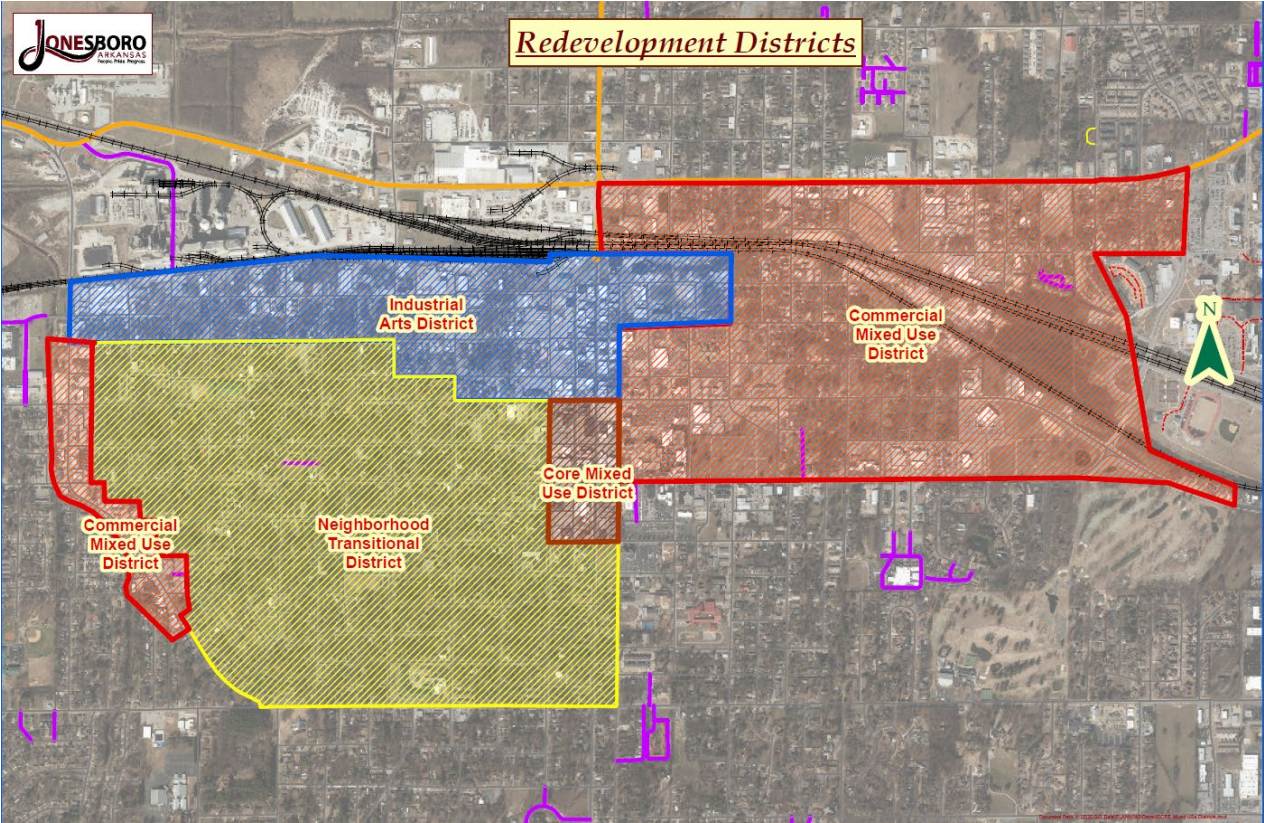
e) **Parking & Service Access:**

<b>(i) Surface Parking Setbacks</b>																					
<p>Existing Single-Family Detached Building Residential Zone Lot</p> <p>Req'd 6' Landscape Buffer</p> <p>Surface Parking Area</p> <p>Parking setback line</p> <p>Building</p> <p>Property Line</p> <p>Sidewalk</p> <p>Building facade line</p> <p>General Frontage</p> <p>Pedestrian Priority or Civic/Open Space Frontage</p> <p>Pedestrian-Friendly Frontage</p>	<table border="1"> <tr> <td>Pedestrian Priority or Civic/Open Space Frontage</td> <td> <ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul> </td> <td>O</td> <td>O*</td> </tr> <tr> <td>Pedestrian-Friendly Frontage</td> <td> <ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul> </td> <td>Q</td> <td>Q'</td> </tr> <tr> <td>General Frontage</td> <td> <ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul> </td> <td>Q*</td> <td></td> </tr> <tr> <td>Side</td> <td> <ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul> </td> <td>P</td> <td></td> </tr> <tr> <td>Rear</td> <td> <ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul> </td> <td>R</td> <td></td> </tr> </table>	Pedestrian Priority or Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul>	O	O*	Pedestrian-Friendly Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul>	Q	Q'	General Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul>	Q*		Side	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	P		Rear	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	R	
	Pedestrian Priority or Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul>	O	O*																	
	Pedestrian-Friendly Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul>	Q	Q'																	
	General Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul>	Q*																		
	Side	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	P																		
Rear	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	R																			
<b>(ii) Structured Parking or Below Grade Parking Setbacks</b>																					
<p>Upper Floor or Below Grade Structured Parking Area</p> <p>Street Level/Ground floor non-parking uses</p> <p>Property Line</p> <p>Sidewalk</p> <p>Building facade line</p> <p>Pedestrian-Friendly or General Frontage</p> <p>Pedestrian Priority or Civic/Open Space Frontage</p>	<table border="1"> <tr> <td>Pedestrian Priority / Civic/Open Space Frontage</td> <td> <ul style="list-style-type: none"> <li>Ground floor - Shall be min. of 30' from the property line</li> <li>Upper Floors – may be built to the façade line along that street</li> </ul> </td> <td>O</td> <td></td> </tr> <tr> <td>Pedestrian-Friendly Frontage or General Frontage</td> <td> <ul style="list-style-type: none"> <li>May be built up to the building façade line along that street; or</li> <li>If no building is located along the street frontage; then the structured parking shall meet the setback standards along that specific frontage.</li> </ul> </td> <td></td> <td></td> </tr> <tr> <td>Side</td> <td> <ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul> </td> <td>P</td> <td></td> </tr> <tr> <td>Rear</td> <td> <ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul> </td> <td>R</td> <td></td> </tr> </table> <p>Standards for <b>S</b> and <b>T</b> in the illustrations above are referenced in standards for Driveways, Alleys and Service Access in Subsection 4.1 (c) iii.</p>	Pedestrian Priority / Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Ground floor - Shall be min. of 30' from the property line</li> <li>Upper Floors – may be built to the façade line along that street</li> </ul>	O		Pedestrian-Friendly Frontage or General Frontage	<ul style="list-style-type: none"> <li>May be built up to the building façade line along that street; or</li> <li>If no building is located along the street frontage; then the structured parking shall meet the setback standards along that specific frontage.</li> </ul>			Side	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	P		Rear	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	R					
	Pedestrian Priority / Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Ground floor - Shall be min. of 30' from the property line</li> <li>Upper Floors – may be built to the façade line along that street</li> </ul>	O																		
	Pedestrian-Friendly Frontage or General Frontage	<ul style="list-style-type: none"> <li>May be built up to the building façade line along that street; or</li> <li>If no building is located along the street frontage; then the structured parking shall meet the setback standards along that specific frontage.</li> </ul>																			
	Side	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	P																		
Rear	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	R																			
<b>(iii) Partially Below Grade Parking</b>																					
<p>May be built up to the Building Façade Line along all Pedestrian-Friendly Frontages and General Frontages.</p>																					

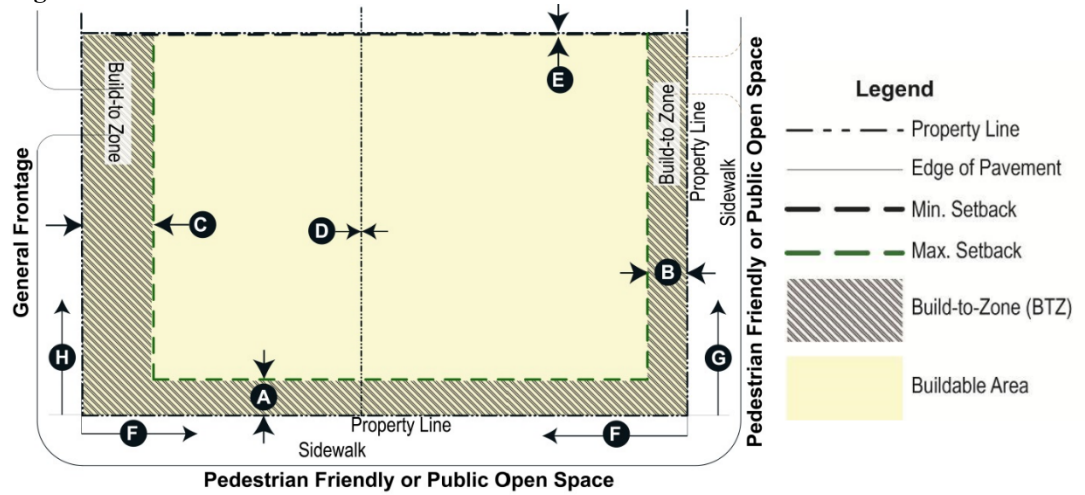
**4.3 Commercial Mixed Use (COM)**

a) **Illustrations and Intent:** The purpose of the Commercial Mixed Use District is to be compatible with the Core Mixed Use through flexibility of uses, continuing to allow retail, education and commercial as primary uses, but also including residential as a core use to be located within Downtown Jonesboro. The focus is to transition this area as a more livable location, by upgrading infrastructure for development while also making this area friendlier to residential uses.

Development Standards within this DJDC district specifically address the unique aspects of redevelopment in this area by retaining the existing development pattern while allowing higher density, pedestrian-oriented development to occur. In addition, the standards allow phasing of redevelopment and encourage the use of existing buildings and parking lots to the extent possible.



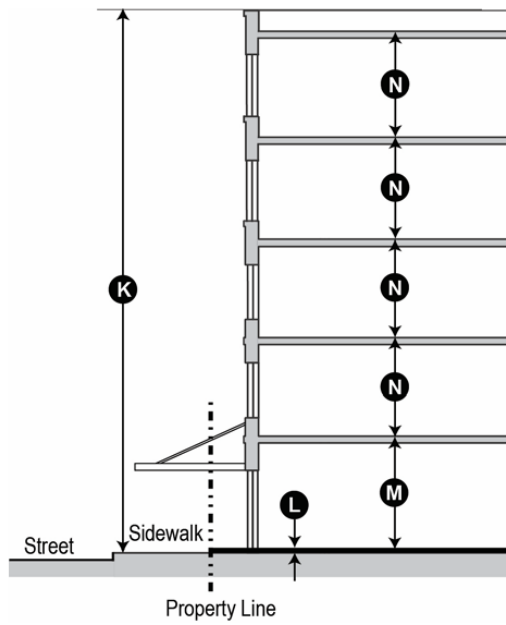
b) Building Placement:



<b>(i) Build-to Zones (BTZs) and Setbacks (Distance from property line to edge of the zone)</b>		
Pedestrian-Friendly Frontage / Public Open Space	0' min. setback – 25' max. setback	<b>A / B</b>
General Frontage	10' min. setback – 45' max. setback	<b>C</b>
Side	0' min. setback; no max. setback	<b>D</b>
Rear	0' min. setback; no max. setback	<b>E</b>
<b>(ii) Building Frontage</b>		
Pedestrian-Friendly Frontage / Public Open Space Frontage	70% min.	<b>F / G</b>
General Frontage	60% min.	<b>H</b>



c) Building Height



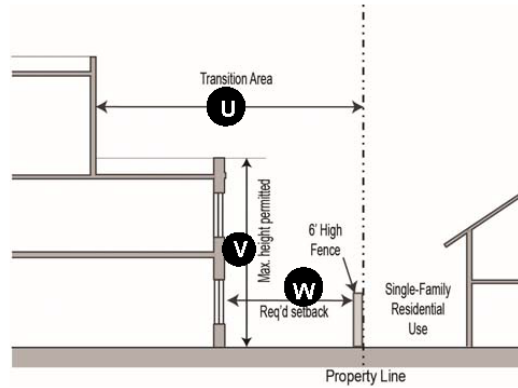
(i) Principal Building Standards		
Building height	<ul style="list-style-type: none"> <li>5 stories maximum by right</li> <li>Additional floor per Allowable Adjustments (Subsection 2)</li> <li>Transition requirement if adjacent to detached single-family zoned residential (Subsection 4.2 (e))</li> </ul>	<b>K</b>
First floor to ceiling height (fin. floor to structure of ceiling)	<ul style="list-style-type: none"> <li>12' min. for all frontages</li> </ul>	<b>M</b>
Ground floor finish level	<ul style="list-style-type: none"> <li>Maximum 12" rise for commercial ground floor.</li> </ul>	<b>L</b>
Upper floor(s) height (fin. floor to structure of ceiling)	<ul style="list-style-type: none"> <li>9' min.</li> </ul>	<b>N</b>
(ii) Accessory Building Standards		
Accessory buildings shall meet the standards for Principal Building standards in the Commercial Mixed Use Zone.		

**d) Residential Transition Standards:**

The following transition standards shall apply to all new building construction and all upper story additions to existing buildings located adjacent to any existing single-family detached residentially zoned lots. This requirement shall NOT apply if an alley or other similar R-O-W separates the subject lot and the existing single-family detached residential lot.

i.	Transition Area	30 feet min.	<b>U</b>
ii.	Max. Building Height at/within Transition Area	35 feet max.	<b>V</b>
iii.	Required setback	10 feet min.	<b>W</b>

A Residential Transition Area fence (minimum 6 feet and maximum 8 feet high) shall be required when adjacent to an existing single-family detached residentially zoned lot and shall be optional for all other adjacencies. The required fence shall be constructed of masonry or block. A 6-foot wide landscape buffer with evergreen shrubs planted at 3' on center and 6' min. in height at maturity shall also be required to be planted within the landscape buffer parallel to any single-family residential lot line. (see surface parking setbacks illustration; Subsection 4.3 (e) i)



e) **Parking & Service Access:**

<b>(i) Surface Parking Setbacks</b>			
<p>Existing Single-Family Detached Building Residential Zone Lot</p> <p>Surface Parking Area</p> <p>Req'd Landscape Buffer</p> <p>Parking setback line</p> <p>Building</p> <p>Property Line</p> <p>Sidewalk</p> <p>Building facade line</p> <p>General Frontage</p> <p>Pedestrian Priority or Civic/Open Space Frontage</p> <p>Pedestrian-Friendly Frontage</p>	Pedestrian Priority or Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul>	O O#
	Pedestrian-Friendly Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul>	Q Q'
	General Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul>	Q#
	Side	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	P
	Rear	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	R
<b>(ii) Structured Parking or Below Grade Parking Setbacks</b>			
<p>Upper Floor or Below Grade Structured Parking Area</p> <p>Street Level/Ground floor non-parking uses</p> <p>Property Line</p> <p>Sidewalk</p> <p>Building facade line</p> <p>General Frontage</p> <p>Pedestrian-Friendly or General Frontage</p>	Pedestrian Priority / Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Ground floor - Shall be min. of 30' from the property line</li> <li>Upper Floors – may be built to the façade line along that street</li> </ul>	O
	Pedestrian-Friendly Frontage or General Frontage	<ul style="list-style-type: none"> <li>May be built up to the building façade line along that street; or</li> <li>If no building is located along the street frontage; then the structured parking shall meet the setback standards along that specific frontage.</li> </ul>	
	Side	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	P
	Rear	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	R
<b>(iii) Partially Below Grade Parking</b>			
May be built up to the Building Façade Line along all Pedestrian-Friendly Frontages and General Frontages.			

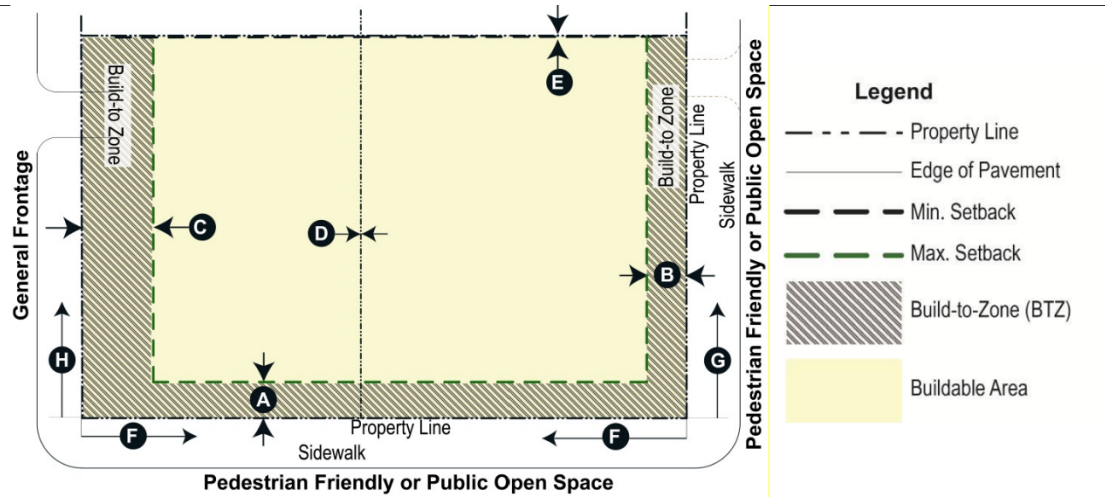
Standards for **S** and **T** in the illustrations above are referenced in standards for Driveways, Alleys and Service Access in Subsection 4.1 (c) iii.

**4.4 Industrial Arts (IA)**

a) **Illustrations and Intent:** The purpose of the Industrial Arts District is to focus on the goal to establish local artisanal manufacturing, light industrial and businesses close to Downtown. These developments will take advantage of being accessible to rail, trails and Cate Street and Aggie Road. The intent is to mix manufacturing, residential, and retail in a walkable yet flexible format.

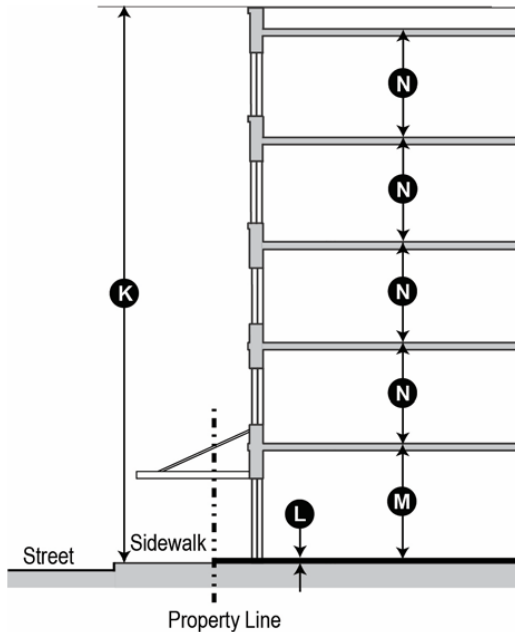
Development Standards within this DJDC district specifically address the unique aspects of redevelopment in this area by introducing a new development pattern while allowing higher quality, pedestrian-oriented development to occur. In addition, the standards allow phasing of redevelopment and emphasized connectivity to Downtown Jonesboro.

b) **Building Placement:**



(i) <b>Build-to Zones (BTZs) and Setbacks</b> (Distance from property line to edge of the zone)		
Pedestrian Priority Frontage /Open Space (BTZ)	0' min. setback – 15' max. setback	<b>A</b>
Pedestrian-Friendly Frontage (BTZ)	0' min. setback – 25' max. setback	<b>B</b>
General Frontage	0' min. setback – 30' max. setback	<b>C</b>
Side	0' min. setback; no max. setback	<b>D</b>
Rear	0' min. setback; no max. setback	<b>E</b>
(ii) <b>Building Frontage</b>		
Pedestrian Priority /Open Space Frontage	70% min.	<b>F</b>
Pedestrian-Friendly Frontage	50% min.	<b>G</b>
General Frontage	30% min.	<b>H</b>

c) **Building Height:**



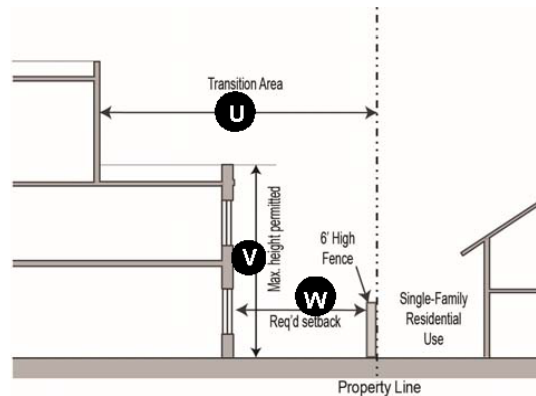
<b>(i) Principal Building Standards</b>		
Building height	<ul style="list-style-type: none"> <li>5 stories maximum by right *</li> <li>Additional floor per Allowable Adjustments (Subsection 2)</li> </ul>	*
	<ul style="list-style-type: none"> <li>Transition requirement if adjacent to detached single-family zoned residential (Subsection 4.2 (e))</li> </ul>	<b>K</b>
First floor to ceiling height (fin. floor to structure of ceiling)	<ul style="list-style-type: none"> <li>12' min. for all frontages</li> </ul>	<b>M</b>
Ground floor finish level	<ul style="list-style-type: none"> <li>Maximum 12" rise for commercial ground floor.</li> </ul>	<b>L</b>
Upper floor(s) height (fin. floor to structure of ceiling)	<ul style="list-style-type: none"> <li>9' min.</li> </ul>	<b>N</b>
<b>(ii) Accessory Building Standards</b>		
Accessory buildings shall meet the standards for Principal Building standards in the Industrial Arts Zone.		

d) **Residential Transition Standards:**

The following transition standards shall apply to all new building construction and all upper story additions to existing buildings located adjacent to any existing single-family detached residentially zoned lots. This requirement shall NOT apply if an alley or other similar R-O-W separates the subject lot and the existing single-family detached residential lot.

i. Transition Area	30 feet min.	<b>U</b>
ii. Max. Building Height at/within Transition Area	35 feet max.	<b>V</b>
iii. Required setback	10 feet min.	<b>W</b>

A Residential Transition Area fence (minimum 6 feet and maximum 8 feet high) shall be required when adjacent to an existing single-family detached residentially zoned lot and shall be optional for all other adjacencies. The required fence shall be constructed of masonry or block. A 6-foot wide landscape buffer with evergreen shrubs planted at 3' on center and 6' min. in height at maturity shall also be required to be planted within the landscape buffer parallel to any single-family residential lot line. (see surface parking setbacks illustration; Subsection 4.4 (e) i)



\* Principal Building Standard building height is subject to the limitations imposed by the Airport Hazard Zone

e) **Parking & Service Access:**

(i) Surface Parking Setbacks			
<p style="text-align: center;">Existing Single-Family Detached Building Residential Zone Lot</p> <p style="text-align: center;">Req'd 6' Landscape Buffer</p> <p style="text-align: center;">Surface Parking Area</p> <p style="text-align: center;">Parking setback line</p> <p style="text-align: center;">Building</p> <p style="text-align: center;">Property Line</p> <p style="text-align: center;">Sidewalk</p> <p style="text-align: center;">Building facade line</p> <p style="text-align: center;">Pedestrian Priority or Civic/Open Space Frontage</p>	Pedestrian Priority or Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul>	<p><b>O</b></p> <p><b>O*</b></p>
	Pedestrian-Friendly Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul>	<p><b>Q</b></p> <p><b>Q'</b></p>
	General Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul>	<b>Q*</b>
	Side	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	<b>P</b>
	Rear	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	<b>R</b>
(ii) Structured Parking or Below Grade Parking Setbacks			
<p style="text-align: center;">Upper Floor or Below Grade Structured Parking Area</p> <p style="text-align: center;">Street Level/Ground floor non-parking uses</p> <p style="text-align: center;">Property Line</p> <p style="text-align: center;">Sidewalk</p> <p style="text-align: center;">Building facade line</p> <p style="text-align: center;">Pedestrian Priority or Civic/Open Space Frontage</p>	Pedestrian Priority / Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Ground floor - Shall be min. of 30' from the property line</li> <li>Upper Floors – may be built to the façade line along that street</li> </ul>	<b>O</b>
	Pedestrian-Friendly Frontage or General Frontage	<ul style="list-style-type: none"> <li>May be built up to the building façade line along that street; or</li> <li>If no building is located along the street frontage; then the structured parking shall meet the setback standards along that specific frontage.</li> </ul>	
	Side	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	<b>P</b>
	Rear	<ul style="list-style-type: none"> <li>10' (adjacent to any single-family detached residential zoned lots)</li> <li>0' (all other adjacencies)</li> </ul>	<b>R</b>
(iii) Partially Below Grade Parking			
May be built up to the Building Façade Line along all Pedestrian-Friendly Frontages and General Frontages.			

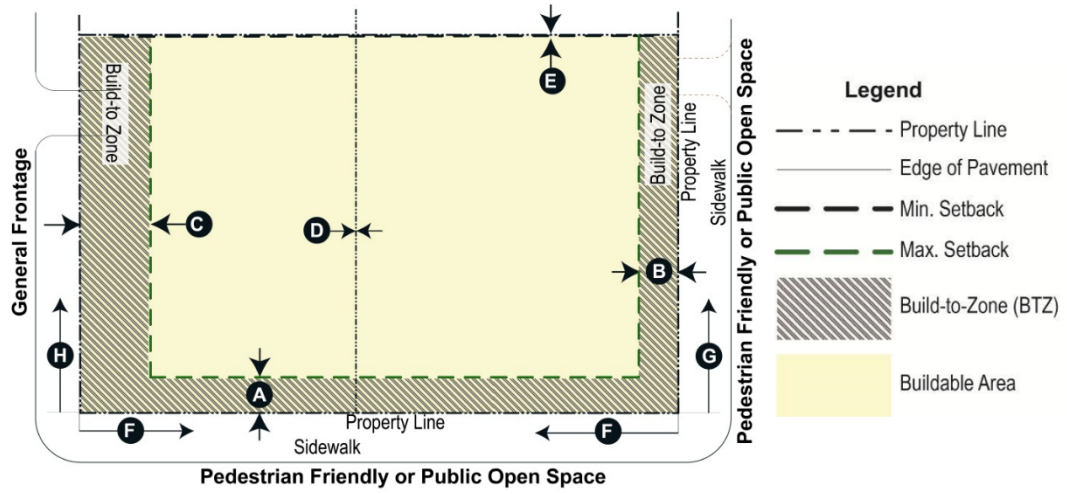
Standards for **S** and **T** in the illustrations above are referenced in standards for Driveways, Alleys and Service Access in Subsection 4.1 (c) iii.

**4.5 Neighborhood Transition (NBT)**

- a) **Illustrations and Intent:** The purpose of the Neighborhood Transition Zone is to support appropriate transition between existing residential neighborhoods and open space and the more active commercial and urban residential areas. The intent is to support the character of the neighborhoods around downtown.

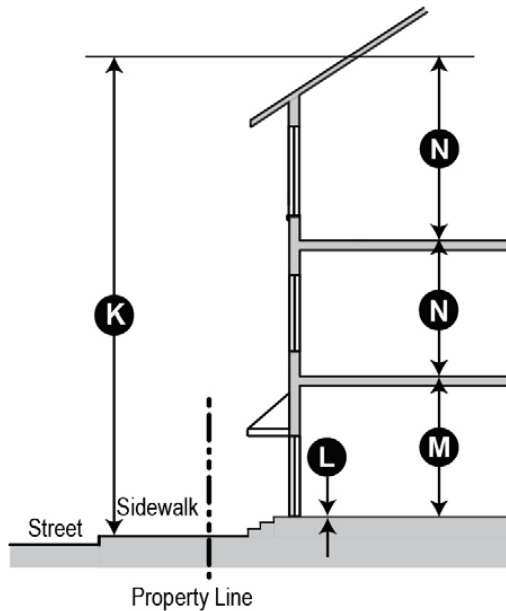
Development Standards within this DJDC zone specifically address the unique aspects of redevelopment in this area by retaining the existing development pattern while allowing higher density, pedestrian-oriented development to occur. In addition, the standards allow phasing of redevelopment and encourage the use of existing buildings and parking lots to the extent possible.

b) **Building Placement:**



(i) <b>Build-to Zones (BTZs) and Setbacks</b> (Distance from property line to edge of the zone)		
Pedestrian-Friendly Frontage / Public Open Space	0' min. setback – 20' max. setback	<b>A / B</b>
General Frontage	0' min. setback – 30' max. setback	<b>C</b>
Side	0' min. setback; no max. setback	<b>D</b>
Rear	See Parking Standards Subsection 4.5 (e)	<b>E</b>
(ii) <b>Building Frontage</b>		
Pedestrian-Friendly Frontage	50% min.	<b>F / G</b>
General Frontage	30%	<b>H</b>

c) **Building Height:**



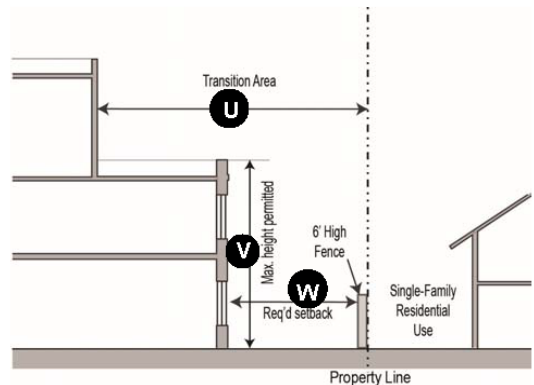
<b>(i) Principal Building Standards</b>		
Building height	3 stories maximum by right	*
	Transition requirement if adjacent to detached single-family zoned residential (Subsection 4.2 (e))	<b>K</b>
First floor to ceiling height (fin. floor to structure of ceiling)	12' min. for all frontages	<b>M</b>
Ground floor finish level	Maximum 12" rise for commercial ground floor.	<b>L</b>
Upper floor(s) height (fin. floor to structure of ceiling)	9' min.	<b>N</b>
<b>(ii) Accessory Building Standards</b>		
Accessory buildings shall meet the standards for Principal Building standards in the Neighborhood Transition Zone.		

d) **Residential Transition Standards:**

The following transition standards shall apply to all new building construction and all upper story additions to existing buildings located adjacent to any existing single-family detached residentially zoned lots. This requirement shall NOT apply if an alley or other similar R-O-W separates the subject lot and the existing single-family detached residential lot.

i.	Transition Area	30 feet min.	<b>U</b>
ii.	Max. Building Height at/within Transition Area	35 feet max.	<b>V</b>
iii.	Required setback	10 feet min.	<b>W</b>

A Residential Transition Area fence (minimum 6 feet and maximum 8 feet high) shall be required when adjacent to an existing single-family detached residentially zoned lot and shall be optional for all other adjacencies. The required fence shall be constructed of masonry or block. A 6-foot wide landscape buffer with evergreen shrubs planted at 3' on center and 6' min. in height at maturity shall also be required to be planted within the landscape buffer parallel to any single-family residential lot line. (see surface parking setbacks illustration; Subsection 4.5 (e) i)





e) **Parking & Service Access:**

(i) Surface Parking Setbacks for Non-Residential																
<p style="text-align: center;">Existing Single-Family Detached Building Residential Zone Lot</p>	<table border="1"> <tr> <td>Pedestrian Priority or Civic/Open Space Frontage</td> <td> <ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul> </td> <td>O O*</td> </tr> <tr> <td>Pedestrian-Friendly Frontage</td> <td> <ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul> </td> <td>Q Q'</td> </tr> <tr> <td>General Frontage</td> <td> <ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul> </td> <td>Q*</td> </tr> <tr> <td>Side</td> <td> <ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul> </td> <td>P</td> </tr> <tr> <td>Rear</td> <td> <ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul> </td> <td>R</td> </tr> </table>	Pedestrian Priority or Civic/Open Space Frontage	<ul style="list-style-type: none"> <li>Shall be located behind the principal building along that street frontage; or</li> <li>Min. 3' behind the building façade line along that street</li> </ul>	O O*	Pedestrian-Friendly Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that street or</li> <li>Min. 6' behind the property line along that street (if no buildings along the street frontage)</li> </ul>	Q Q'	General Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the property line along that street</li> </ul>	Q*	Side	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residential zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	P	Rear	<ul style="list-style-type: none"> <li>6' (min) only if adjacent to any single-family detached residentially zoned lot;</li> <li>0' for all other adjacencies</li> </ul>	R
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<p>Standards for <b>S</b> and <b>T</b> in the illustrations above are referenced in standards for Driveways, Alleys and Service Access in Subsection 4.1 (c) iii.</p>																
(ii) Single-Family Residential Parking																
<p style="text-align: center;">Alley Pavement</p>	<table border="1"> <tr> <td>Public Open Space Frontage</td> <td> <ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that frontage; or</li> <li>Shall be located behind the principal building along that street frontage</li> </ul> </td> <td>O P</td> </tr> <tr> <td>General Frontage</td> <td> <ul style="list-style-type: none"> <li>Min. 6' behind the property line along that street; or</li> <li>At or behind the building façade line along that frontage</li> </ul> </td> <td>O P</td> </tr> <tr> <td>Side / Rear</td> <td> <ul style="list-style-type: none"> <li>4' min; no parking permitted on driveway, OR</li> <li>20' min; parking permitted on driveway</li> <li>On corner lots where alleys are not available or not feasible, rear garages may be entered from the side street.</li> </ul> </td> <td>Q R</td> </tr> <tr> <td>Alley Fence</td> <td> <ul style="list-style-type: none"> <li>4' min from edge of typical pavement or</li> <li>8' min from edge of typical paving where above ground franchise equipment exists</li> </ul> </td> <td>S T</td> </tr> </table>	Public Open Space Frontage	<ul style="list-style-type: none"> <li>Min. 3' behind the building façade line along that frontage; or</li> <li>Shall be located behind the principal building along that street frontage</li> </ul>	O P	General Frontage	<ul style="list-style-type: none"> <li>Min. 6' behind the property line along that street; or</li> <li>At or behind the building façade line along that frontage</li> </ul>	O P	Side / Rear	<ul style="list-style-type: none"> <li>4' min; no parking permitted on driveway, OR</li> <li>20' min; parking permitted on driveway</li> <li>On corner lots where alleys are not available or not feasible, rear garages may be entered from the side street.</li> </ul>	Q R	Alley Fence	<ul style="list-style-type: none"> <li>4' min from edge of typical pavement or</li> <li>8' min from edge of typical paving where above ground franchise equipment exists</li> </ul>	S T			
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**5.0 Building Design Standards**

**5.1 Purpose and Intent**

The Building Design Standards for the DJDC establish a coherent urban character to encourage an enduring and attractive redevelopment area. Site and/or building plans shall be reviewed by the Director of the Planning and Zoning Department or his or her designee or designee for compliance with the standards in this Subsection. Table 5-1 details which elements of this Subsection are reviewed at Site Plan Review and/or Building Permit Review.

<b>Table 5-1: Review Process for Building Design Standards</b>						
<i>Applicable Subsection</i>	<i>5.2 Building Orientation &amp; Entrances</i>	<i>5.3 Façade Composition</i>	<i>5.4 Commercial Ready Standards</i>	<i>5.5 Shading Requirements</i>	<i>5.6 Building Materials</i>	<i>5.7 Design of Parking Structures</i>
Site Plan Review	•	•	•	•		•
Building Permit Review	•	•	•	•	•	•

The key design principles establish essential goals for the redevelopment within the DJDC area to be consistent with the vision for Downtown Jonesboro to become a vibrant, walkable city center that serves a range of commercial, civic, educational and residential uses that benefit the neighborhood and the region. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scale spaces. The key design principles are:

- a) New and redeveloped buildings and sites shall utilize building and site elements and details to achieve a pedestrian-oriented public realm with appropriate glazing, shading and shelter;
- b) Design compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity;
- c) Strengthen and celebrate the history of Downtown Jonesboro;
- d) Building façades shall include appropriate architectural details and ornament to create variety and interest;
- e) Open space(s) shall be incorporated to provide usable public areas integral to the urban environment; and
- f) Increase the quality, variety, adaptability, and sustainability in Jonesboro’s building stock.

**5.2 Building Orientation and Entrances**

- a) Buildings shall be oriented towards Pedestrian Priority Frontages, where the lot has frontage along Pedestrian Priority Frontages. If a building has no frontage along a Pedestrian Priority Frontage, then it should front a Pedestrian-Friendly Frontage. All other buildings may be oriented towards General Streets or Public Open Spaces.
- b) Primary entrances to buildings shall be located on the street along which the building is oriented (see figure below). At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection. Building entrances shall be provided for all separate ground floor commercial use tenant spaces that are located along Pedestrian Priority or Pedestrian-Friendly Frontage.

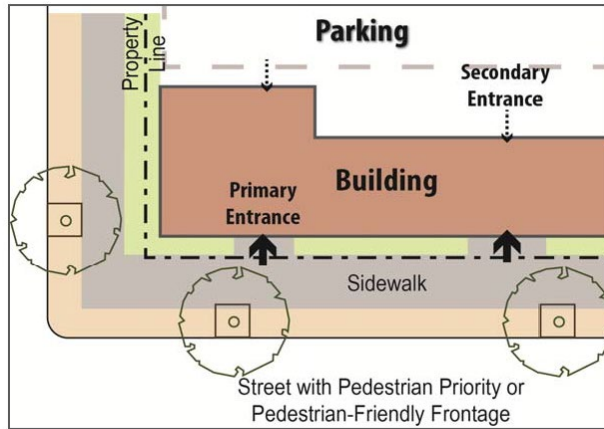


Figure showing required building orientation and location of primary entrances

- c) All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from parking areas or alleys.
- d) Primary Entrance Design: Primary building entrances along Pedestrian Priority and Pedestrian-Friendly Frontages shall consist of at least two of the following design elements so that the main entrance is architecturally prominent and clearly visible from that street, see figures below:
  - i) Architectural details such as arches, friezes, awnings, canopies, arcades, tile work, murals, or moldings; or
  - ii) Integral planters or wing walls that incorporate landscape or seating elements; or
  - iii) Prominent three-dimensional, vertical features, such as belfries, chimneys, clock towers, domes, spires, steeples, towers, or turrets; or
  - iv) A repeating pattern of pilasters projecting from the façade wall by a minimum of eight inches or architectural or decorative columns.

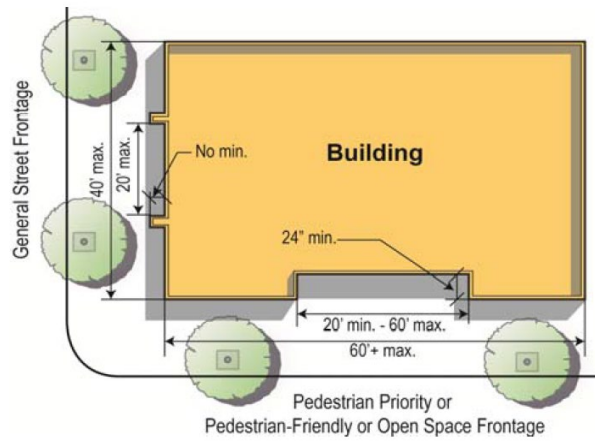


Images showing appropriate transparency required along Pedestrian Priority and Pedestrian-Friendly Frontages

### 5.3 Façade Composition

- a) Commercial, Mixed Use and Multi-Family Use Buildings:
  - i) Façades greater than 60 feet in length along all Pedestrian Priority Frontage, Pedestrian-Friendly Frontages and Public Open Spaces shall meet the following façade articulation standards:

1. Include façade modules so that a portion of the façade steps back or extends forward with a depth of at least 24 inches (see figure below).
  2. The distance from the inside edge of a building projection to the nearest inside edge of an adjacent projection shall not be less than 20 feet and not greater than 60 feet (see figure below).
- ii) All other façades shall be articulated by at least one discernable architectural element every 20 feet. Such architectural elements include, but are limited to:
1. Changes in material, color, and/or texture either horizontally or vertically at intervals not less than 20 feet and not more than 40 feet (see figure below); or
  2. The construction of building entrances, bay windows, display windows, storefronts, arcades, façade relief, panels, balconies, cornices, bases, pilasters or columns.



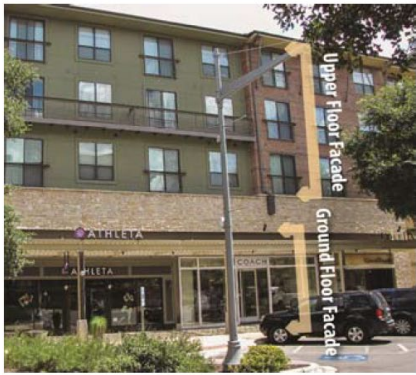
*Illustration showing building articulation requirement*



*Images showing examples of appropriate building articulation*

- iii) Façade Transparency Required:
1. All façades shall meet the minimum requirement for façade glazing (percentage of glass doors and windows) as established in Table 5-2 below. Glazing for ground floor commercial uses shall be transparent enough for the public to view inside.

Table 5-2: Required Minimum Façade Glazing by Façade Frontage Type			
Façade Frontage Type	Pedestrian Priority or Pedestrian-Friendly Frontage	Public Open Space Frontage	General Frontage or Other Façades
<b>Commercial Use or Mixed Use Buildings</b>			
Ground Floor	70% minimum	60% minimum	None
Upper Floor(s)	40% minimum	40% minimum	None
<b>Multi-Family Residential Use Buildings</b>			
Ground Floor	40% minimum	40% minimum	None
Upper Floor(s)	20% minimum	20% minimum	None



*Images showing appropriate transparency required along Pedestrian Priority and Pedestrian-Friendly Frontages*

- b) Single-Family Detached Residential Buildings:
  - i) At least one of the following shall be added along single-family residential building façades to add pedestrian interest along the street:
    1. Porches;
    2. Forecourts;
    3. Courtyards;
    4. Stoops;
    5. Eaves; or
    6. Balconies.

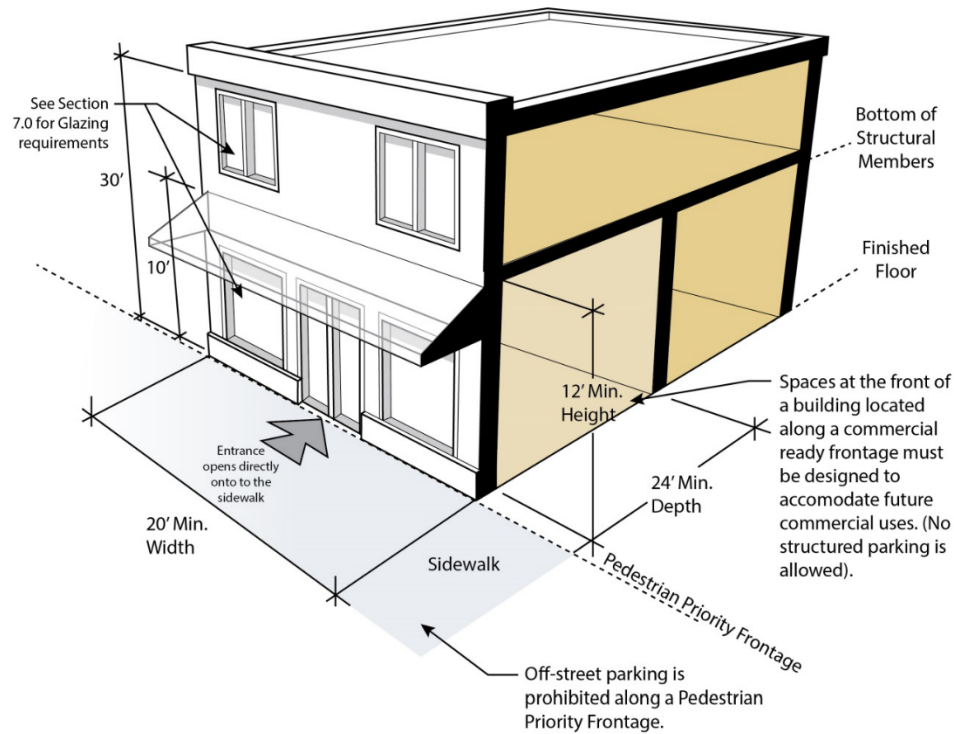
- ii) If a single-family residential use building is setback less than 10 feet from the front property line, the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk.



*Residential buildings with porches, fencing, balconies, and stoops to add interest along the street*

#### **5.4 Commercial-Ready Standards**

- a) Ground floors of all buildings with Pedestrian Priority Frontage designation per the DJDC District Map shall be built to Commercial-Ready standards. In addition, the following standards shall apply (see figure below):
  - i) An entrance that opens directly onto the sidewalk according to Subsection 5.0;
  - ii) A height of not less than 12 feet measured from the entry level finished floor to the bottom of the structural members of the ceiling;
  - iii) Minimum leasable width of 20 feet;
  - iv) A front façade that meets the window glazing requirements in Subsection 5.0; and
  - v) Off street-surface parking shall be prohibited between the sidewalk and the building.



*Illustration showing application of Commercial-Ready Frontage Requirements*

### 5.5 Shading Requirement

Shading of public sidewalks, especially, sidewalks located along Pedestrian Priority and Pedestrian-Friendly Frontages is important to implementing the vision for walkable mixed-use along the Corridor. Shading may be achieved through any combination of canopies, awnings, street trees, and other similar devices. To this end, the following standards shall apply (see figures below):

- a) Shaded sidewalk shall be provided alongside at least 50% of all building façades with Pedestrian Priority Frontage designation per the DJDC District Map.
- b) A shaded sidewalk must meet the following requirements:
  - i) Along a street, a shaded sidewalk shall comply with the applicable sidewalk standards for its designated street type. If not otherwise required, the shaded sidewalk shall provide trees planted no more than 60 feet on center. Exception can be made for the preservation of existing mature trees that provide shading over the sidewalk.
  - ii) Building entrances along Pedestrian Priority and Pedestrian-Friendly Frontages shall be located under a shade device, such as an awning, portico, or other artificial shade structure, as approved by the Director of Planning and Zoning.
  - iii) Shading devices shall adhere to Streetscape clearance standards in Subsection 8 (Streetscaping and Landscaping Standards).



*Images showing examples of shading along sidewalks*

## 5.6 Exterior Building Materials

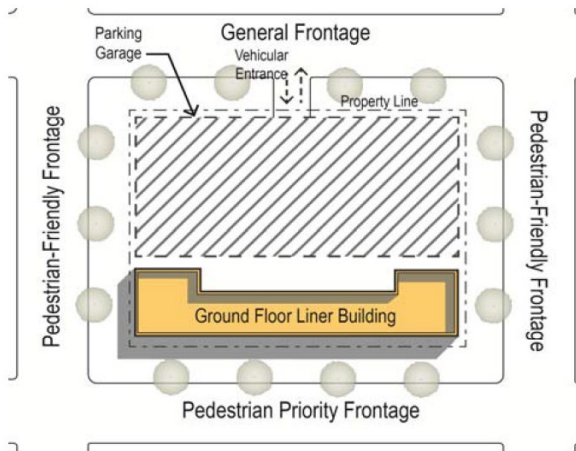
- a) Commercial and Mixed Use Buildings:
  - i) At least 70% of each Primary and Secondary street facing façade (not including alleys) of all new buildings (excluding doors and windows) shall be externally finished with:
    1. Masonry, including brick, stone, marble, granite, cast stone, or other durable material as approved by the Director of the Department of Planning and Zoning or his or her designee.
    2. Architectural metal may be used in the Industrial Arts Zone.
  - ii) Additions to existing buildings, to the extent possible, shall match the existing external finish materials and corresponding proportions of such building.
  - iii) Other materials will be considered as primary building materials on a case-by-case basis and may only be approved by the Director of the Department of Planning and Zoning or his or her designee or designee.
  - iv) No more than 30% of each Primary and Secondary street fronting façades (except alleys) shall use accent materials such as:
    1. Wood;
    2. Split-face concrete block;
    3. Tile;
    4. Stucco (3-step application process);
    5. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty;
    6. Exterior Insulating Finishing System (EIFS) on upper floors only; or
    7. Architectural metal.
- b) Residential Use Buildings: At least 70% of all Primary and Secondary street fronting façades (except alleys) of residential buildings shall be finished with one or more of the following materials. No more than three different materials shall be used on any single façade:



- i) Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty;
- ii) Lap-sided wood; or
- iii) Masonry (brick, stone, cast stone, or other durable material as approved by the Director of the Department of Planning and Zoning or his or her designee or designee).

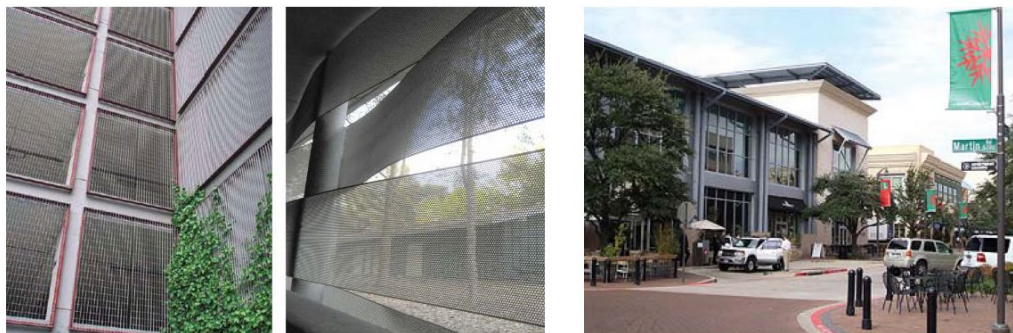
**5.7 Design of Parking Structures**

- a) All ground floors of parking garages located on Pedestrian Priority Frontages shall be built to Commercial Ready standards to a minimum depth of 24 feet.
- b) To the extent possible, the amount of Pedestrian Priority Frontage devoted to a parking structure shall be minimized by placing the shortest dimension(s) of the parking structure along the Pedestrian Priority edge(s), see figure below.



*Allowed Parking Garage Configurations*

- c) Where above ground structured parking is located at the perimeter of a building with Pedestrian Priority Frontage, it shall be screened in such a way that cars on all parking levels are completely screened from view (see figures below). Architectural screening shall be used to articulate the façade, hide parked vehicles, and shield lighting. Parking garage ramps shall not be visible from any Pedestrian Priority Frontages. Ramps shall not be located along the exterior perimeter of the parking structure.



*Illustrative Examples of Parking Garage Screening*

- d) Ground floor façade treatment (building material, windows and architectural detailing) shall be continued to the second floor of a parking structure along all Pedestrian Priority Frontages (see figure below).
- e) When parking structures are located at street intersections, corner emphasizing elements shall be incorporated. These include towers, pedestrian entrances, signage, glazing, etc.
- f) Parking structures and adjacent sidewalks shall be designed so pedestrians and bicyclists are clearly visible to entering and exiting automobiles. These may include sight distance clearance, signage, and other warning signs.



*Images showing appropriate design of Parking Structures*

## **6.0 Street Design Standards**

### **6.1 Purpose and Intent**

The vision for Downtown Jonesboro recognizes the importance of providing adequate public infrastructure, which includes an ultimate street network of multi-modal streets, and other needed infrastructure to serve the vision for redevelopment within the DJDC area.

Street design and connectivity is envisioned to support and balance automobile, bicycle, and walking trips in addition to becoming the "civic" space for development. The civic nature of streets will not only serve private redevelopment, but the community at large and the people who live in and use it. This Subsection establishes Context Sensitive Solutions for street design and connectivity and is intended to complement the City of Jonesboro Complete Streets Plan.

### **6.2 Street Designation Classification**

In order to service multiple modes of transportation within an appropriate redevelopment context, streets within the DJDC area are designated by its classification. The Street Classification designation establishes standards for the actual cross section of the street. The Street Classification includes information on automobile, bicycle, pedestrian, and parking accommodation. It typically addresses the space allocation within the public right-of-way and its emphasis towards different modes of transportation. Table 6-1 identifies the Street Classification designations within the DJDC area and provides cross sections for the different Street Classifications.

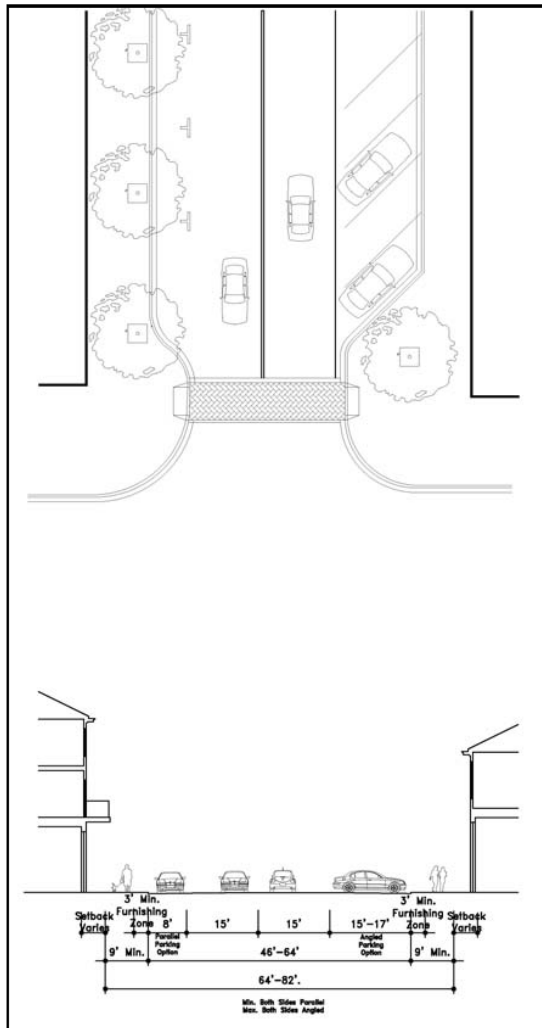
### **6.3 Street Standards**

Table 6-1 and associated cross sections shall establish standards for all elements within the public right-of-way including travel lanes, on street parking, bicycle accommodation, streetscape, street landscape, and sidewalks. The standards in the City's Subdivision Ordinance shall apply to the extent that they do not conflict with the standards in this Subsection and the vision for Downtown Jonesboro.

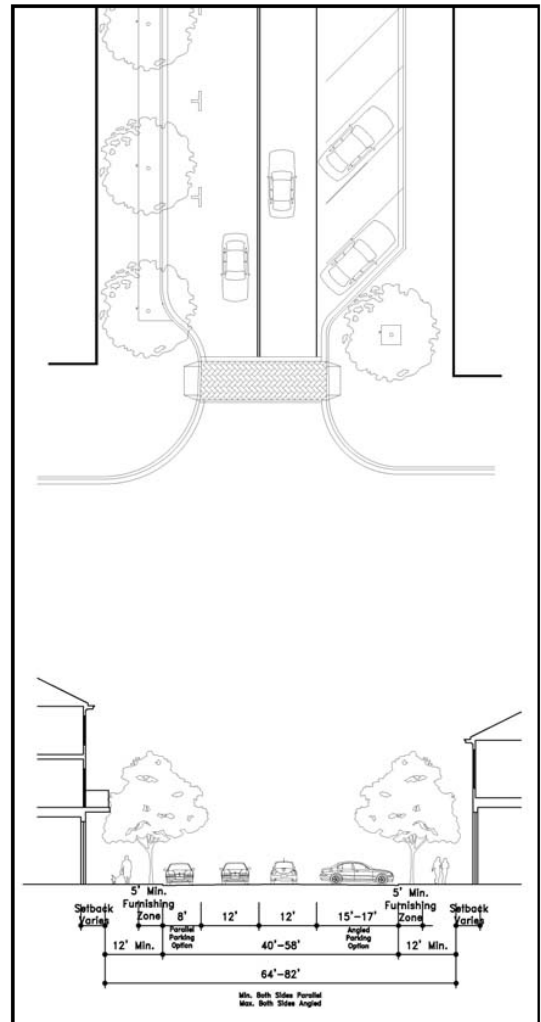
**Table 6-1: Street Classification Table**

Street Classification	Standards			
	Minimum Streetscape Zone	Minimum Furnishing Zone	Street Trees Required?	Recommended Development Frontage
Main Street	12 feet	5 feet	Yes (COM only)	Pedestrian Priority, Pedestrian-Friendly and General
Union Street	12 feet	5 feet	Yes (COM only)	Pedestrian Priority, Pedestrian-Friendly and General
Aggie Street and Cate Street	12 feet	5 feet	Yes (COM Only)	Pedestrian Priority, Pedestrian-Friendly and General
Mixed Use Street Along Public Space	10 feet	3 feet	No	Pedestrian Priority
Mixed Use General Street	9 feet	2 feet	No	Pedestrian-Friendly and General
Neighborhood General Street	10 feet	6 feet	No	General
Alley/Mews	N/A	N/A	N/A	N/A

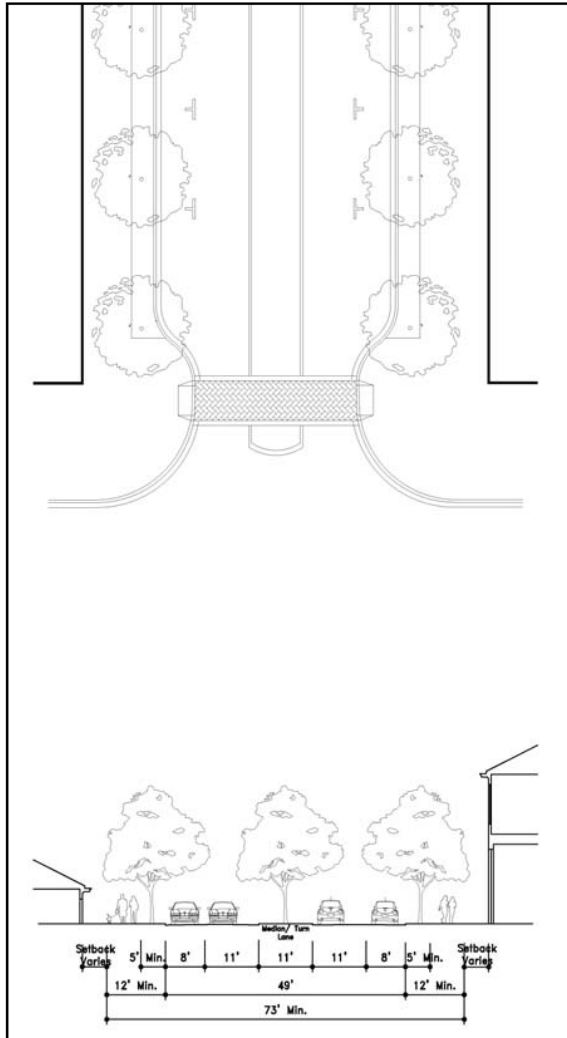
### 3. Downtown Street



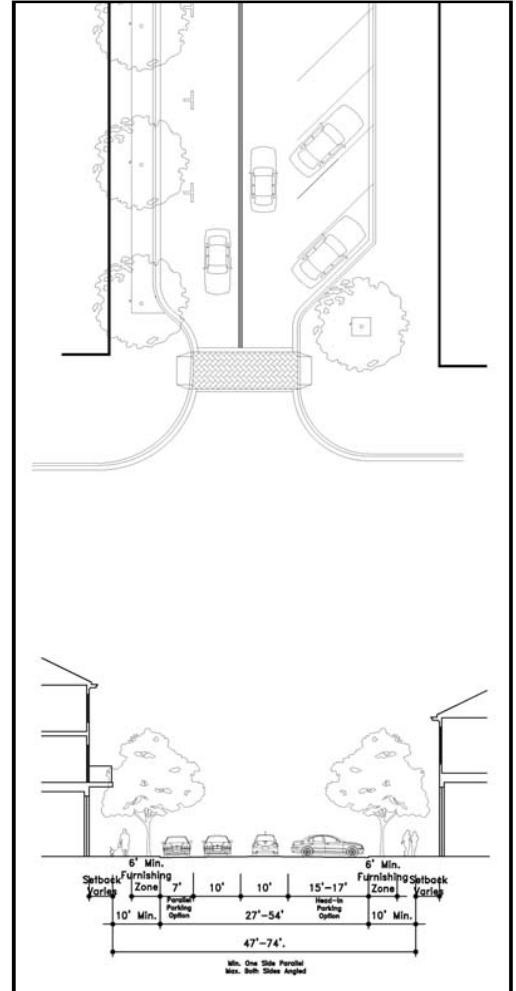
### 4. Mixed Use Street



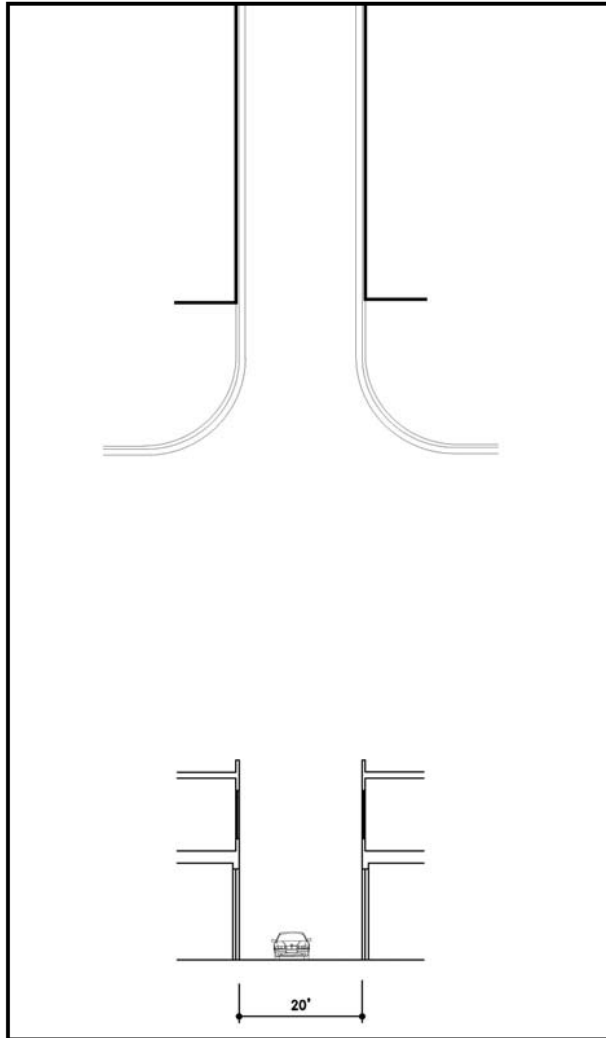
## 5. Connector Street



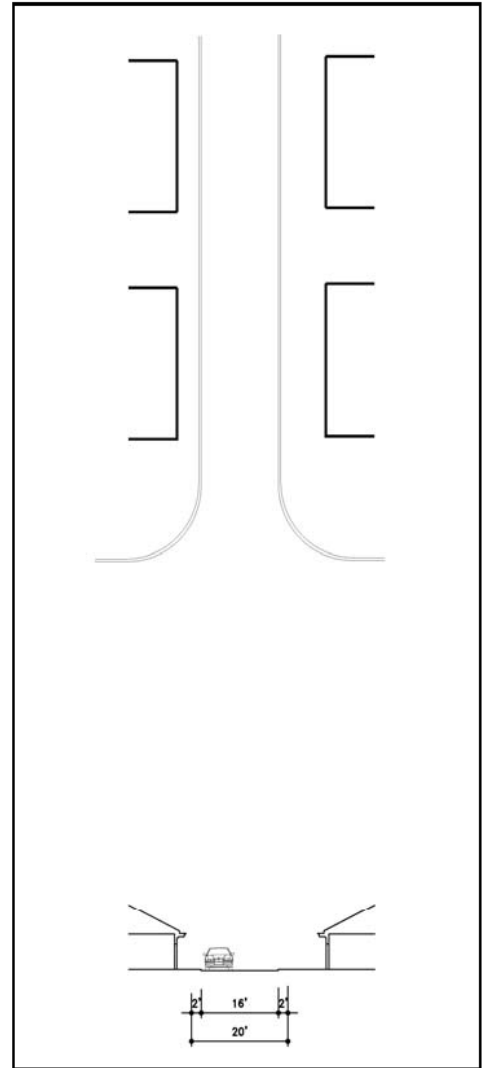
## 6. Neighborhood Street



## 7. Commercial Alley



## 8. Residential Alley



### 7.0 Open Space Standards

#### 7.1 Purpose and Intent

The illustrative vision for Downtown Jonesboro recognizes the importance of providing a network of open spaces with passive and active recreation opportunities. The open space network will be serviced by an interconnected network of sidewalks, trails and paths for pedestrians and bicyclists alike, providing desired amenities and facilities for residents and visitors to Downtown Jonesboro.

Much of the publicly owned and maintained open space already exists in Downtown Jonesboro, with the Rotary Park in the center of Downtown, connectivity to ancillary public open space is essential for the public open space to be activated in conjunction with redevelopment. Ideal connections through sidewalks, trails and paths are identified in the Downtown Jonesboro Implementation Plan. The Open Space Standards

identifies the variety of public open space types that are available and appropriate in Downtown Jonesboro and serve as a means to categorize the various public open space types that will be used in the area.

**7.2 Open Space Classification**

Within the DJDC area, all Open Space shall fall into one of the following classifications:

- a) **Public Open Space:** Open air or unenclosed to semi-unenclosed areas intended for public access and use. These areas range in size and development and serve to complement and connect surrounding land uses and code requirements. Public Open Space may be publicly or privately owned and maintained.
- b) **Private Common Open Space:** Privately owned and maintained outdoor, unenclosed or semi-unenclosed area located on the ground or on a terrace, deck, porch or roof. This open space is designed and accessible for outdoor gathering, recreation and landscaping and is intended for use by the residents, employees, and/or visitors to the development.

**7.3 Open Space Requirements**

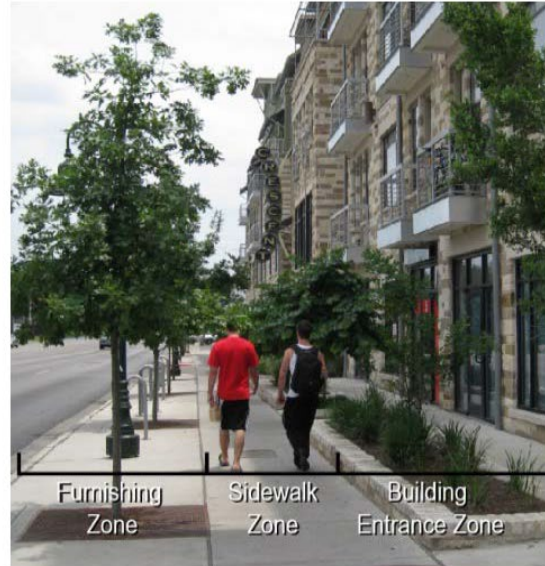
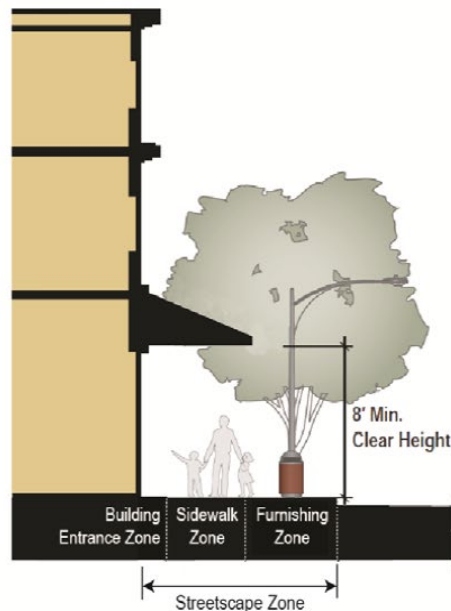
- a) All non-residential development in the Commercial Mixed Use or Core Mixed Use Districts shall provide four square feet of Public Open Space or Private Common Open Space for every 100 square feet of non-residential space or fraction thereof. This standard shall only apply to all site plans two acres in size or larger.
- b) All residential development in the DJDC area shall meet the Private Common Open Space standards established in this Subsection. Table 7-1 establishes the Private Common Open Space requirement based on the proposed intensity of residential development. Residential projects numbering less than eight dwelling units per acre are not required to provide Private Common Open Space.
- c) Any Public Open Space provided in a development shall identify and program the Public Open Space per the palette of open space types in Attachment 4 of this Subsection.

<b>Table 7-1: Private Common Open Space Requirements</b>	
<b>Residential Density (dwelling units per acre)</b>	<b>Private Common Open Space Standard (area of private common open space per dwelling unit)</b>
0 - 8 DU/acre	Not required
8 - 20 DU/acre	Provide a minimum of 160 sf per dwelling unit
20 - 30 DU/acre	Provide a minimum of 120 sf per dwelling unit
30 - 40 DU/acre	Provide a minimum of 80 sf per dwelling unit
More than 40 DU/acre	Provide a minimum of 60 sf per dwelling unit

## 8.0 Streetscape and Landscaping Standards

### 8.1 Streetscape Zone

- a) Streetscapes are required for all development in the DJDC area and shall meet the standards of this Subsection. All streetscape zones are based on the Street Classification per Table 6-1 (Street Classification Table) and the associated cross sections. The minimum streetscape width may vary depending on available right-of-way, but in all instances the minimum sidewalk zone width requirement shall apply. Variations in the streetscape width may be approved by the Director of the Department of Planning and Zoning or his or her designee or designee with appeals of the planner's decision heard by the Board of Zoning Adjustments. The Streetscape Zone shall consist of the following, see figures below:
- Furnishing Zone:** The furnishing zone is intended for the placement of function street items. These items include street trees, street furniture, lighting, waste receptacles, fire equipment, signage, vending boxes, bus shelters, bicycle racks, public utility equipment and other elements that may disrupt pedestrian activity. In residential areas, the furnishing zone may be fully landscaped, whereas in mixed use or commercial areas, the furnishing zone may be fully paved. These items may be placed in a manner that does not obstruct pedestrian access or motorist visibility.
  - Sidewalk Zone:** The sidewalk zone shall be hardscape and shall be located adjacent to the furnishing zone. The sidewalk zone shall comply with ADA and local accessibility standards and shall be unobstructed by any permanent or non-permanent element for the required minimum width and a minimum height of eight feet. Accessibility is required to connect sidewalk zones on adjacent sites.
  - Building Entrance Zone:** The building entrance zone is an area adjacent to the building frontage along the sidewalk zone. The building entrance zone shall be a minimum of two feet in width.



*Illustrations delineating the Streetscape Zone elements*

### 8.2 Street Trees and Streetscape

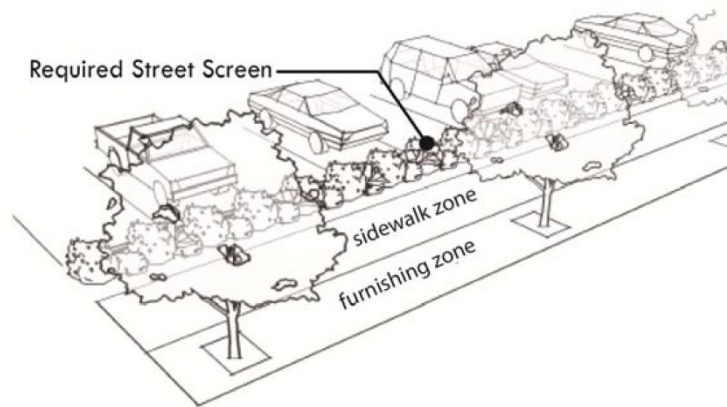
- a) Street trees shall be required within the Furnishing Zone.



- b) Street trees shall be centered within the Furnishing Zone and be planted a minimum of two feet from the face of curb.
- c) Spacing shall be an average of 40 to 50 feet on center (measured per block face) but may not exceed 60 feet on center along all streets.
- d) Each tree shall be planted in a planting area or tree well no less than 25 square feet.
- e) The planting area for trees shall be covered with either landscape, groundcover or tree grates.
- f) Street trees planted in a public right-of-way or platted easement in which utilities are located must meet the applicable requirement under Section 117-326.

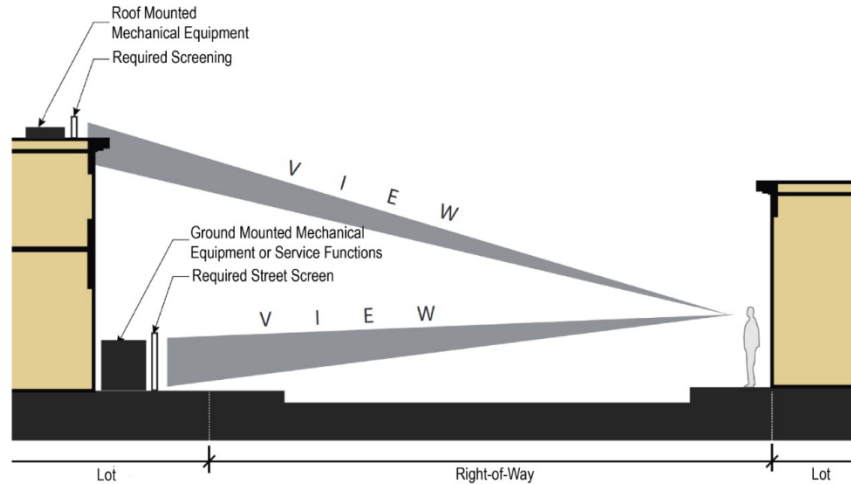
### 8.3 Screening Standards

- a) Street Screen Requirement:
  - i) Along all Pedestrian Priority and Pedestrian-Friendly Frontages with surface parking within the BTZ shall be defined by a Street Screen. The street screen shall be a minimum of three feet high and not exceed four feet in height (see figure below).



*Illustration showing required Street Screen along all frontages without a Building within the BTZ*

- ii) Along all streets (except alleys) service areas shall be screened in such a manner that the service area shall not be visible to a person standing on the property line on the far side of the adjoining street (see figure below).
- iii) All roof mounted mechanical equipment (except solar panels) shall be screened from view of a person standing on the property line on the far side of the adjoining street (see figure below).



*Illustration showing required screening of roof and ground mounted equipment*

- iv) Required Street Screens and equipment screening shall be built from one of the following:
  1. The same building material as the principal structure on the lot; or
  2. A vegetative screen composed of shrubs planted to be opaque at maturity; or
  3. A combination of two below:
    - a. The required Street Screen shall be located at the minimum setback line along the corresponding frontage.
    - b. Street Screens cannot block any required sight triangles along a cross street or driveway.
    - c. Street Screens may include breaks to provide pedestrian access from any surface parking or service area to the public sidewalk.
  4. The screening material used for roof mounted equipment shall be the same as the primary exterior building material used.
  5. Street trees planted in a public right-of-way or platted easement in which utilities are located must meet the applicable requirement under Section 117-326.

#### **8.4 Street Lighting**

Pedestrian scaled lighting shall be required along all streets within the DJDC area. The following standards shall apply for pedestrian scale lighting:

- a) Pedestrian oriented lighting shall be no taller than 16 feet.
- b) Street lights along Pedestrian Priority, Pedestrian-Friendly and Public Open Space Frontages shall be placed at an average of 40 to 50 feet on center, placed on average between every street tree. Street lights shall be placed a minimum of two feet behind the face of curb, preferably lined up with street trees.
- c) Street lights along General Frontages shall be placed at an average of 40 to 80 feet on center placed an average between every other street tree. Street lights shall be placed a minimum of two feet behind the face of curb, preferably lined up with street trees.
- d) The light standard selected shall be compatible with the design of the street and buildings, but shall be consistent throughout the DJDC area to maintain efficiencies in maintenance.

### 8.5 Street Furniture

- a) Street Furniture shall be placed within the Furnishing Zone or within the front setback area only. Benches and bike racks shall be placed in alignment with light poles and street trees.
- b) All street furniture shall be located within the Furnishing Zone in such a manner that the Sidewalk Zone not be blocked or obstructed.
- c) Materials selected for street furniture shall be of durable quality and require minimal maintenance.

### 8.6 Utilities

- a) All new utility lines within private property shall be underground where feasible. The Director of the Department of Planning and Zoning or his or her designee or designee may approve above ground utilities if there are restrictions to underground installation.
- b) Existing overhead utility lines shall be relocated to alleys where feasible.
- c) Where locations on private property must be used for switchgear and transformer pads, such locations shall be either along General Frontages or at the side or rear of the property and screened in accordance with Subsection 8.3.

### 8.7 Parking Lot Landscaping

Where feasible, the use of permeable materials shall be allowed if approved by the Director of the Department of Planning and Zoning or his or her designee or designee.

### 9.0 Definitions

Many terms used in this section have been defined in other sections of the Jonesboro City Code. If the definition of a term contained in this section conflicts with the definition of the same term contained in a separate section of the Jonesboro City Code, the definition contained in this section shall apply.



*Image of an accessory unit over a garage*

#### ***Accessory Unit***

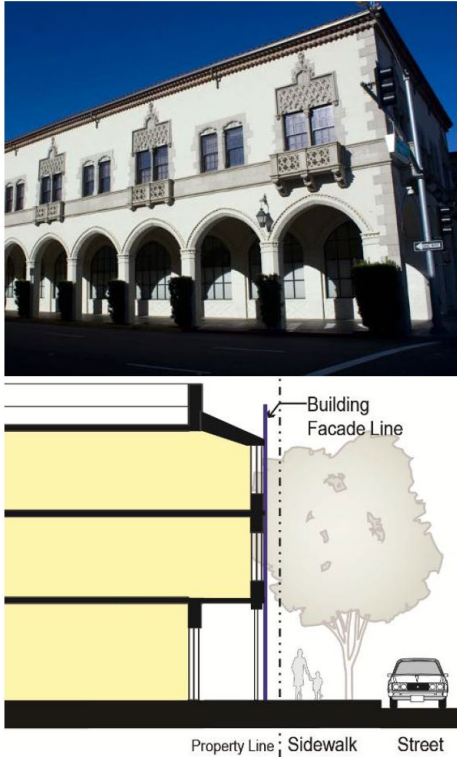
A Secondary/Garage Apartment Building Type is an accessory structure typically located at the rear of a lot. This structure typically provides either a small accessory residential unit or home office space that may be above a garage or at ground level. This Building Type is important for providing affordable housing opportunities within Downtown Jonesboro.

#### ***Adjacent Single Family Detached Residentially Zoned Lots***

Lots that share a common property line for at least 15-feet and that are zoned Residential.

### ***Allowable Adjustment***

Means a requested adjustment allowed in the standards per the Allowable Adjustments provision of Subsection 2.3 (Administration and Development Phasing). The Director of the Department of Planning and Zoning or his or her designee or designee shall have the authority to administratively approve a request for an allowable adjustment.



### ***Images of arcade frontages***

#### ***Arcade or Colonnade***

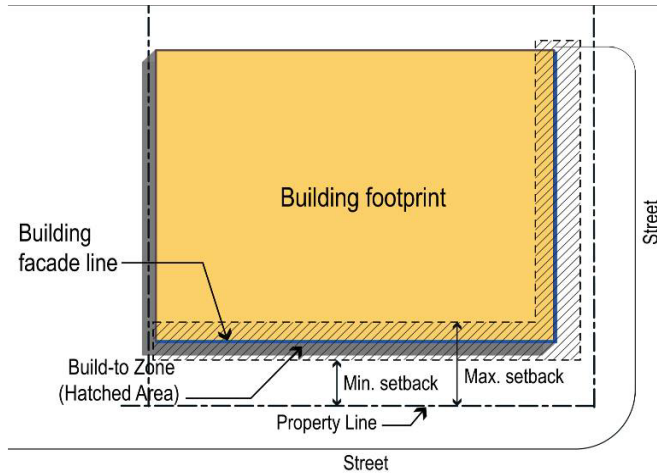
A portion of the main façade of the building that is at or near the property line and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and may be one or two stories. The ground floor area within the arcade may be conditioned or non-conditioned space.

#### ***Build-to Zone (BTZ)***

The area between the minimum and maximum front setbacks from the property line. The principal building façade line shall be located within this area.

#### ***Building Form and Site Development Standards***

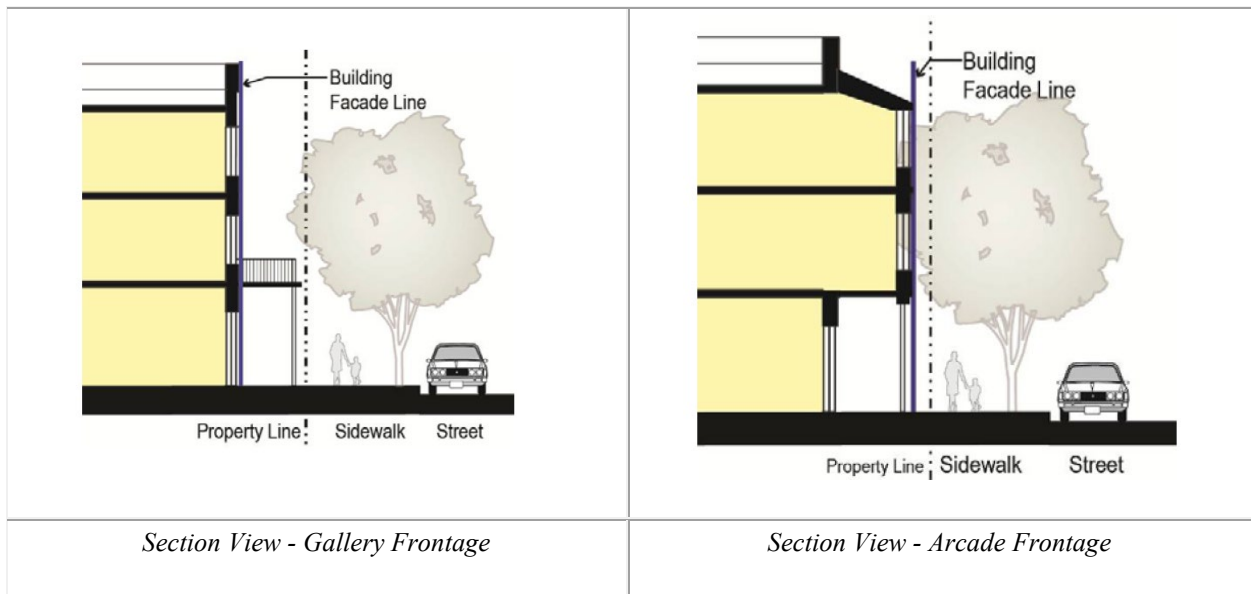
Standards established for each DJDC District including but not limited to building placement, building height, parking, service access, and other functional design standards.

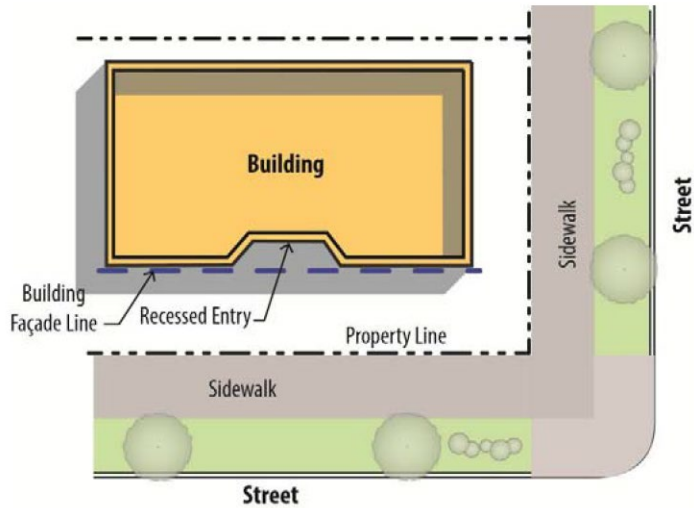


*Illustration indicating the location of the build-to zone relative to the minimum and maximum setbacks and the building façade line*

***Building Façade Line***

The location of the vertical plane of a building along a street frontage.

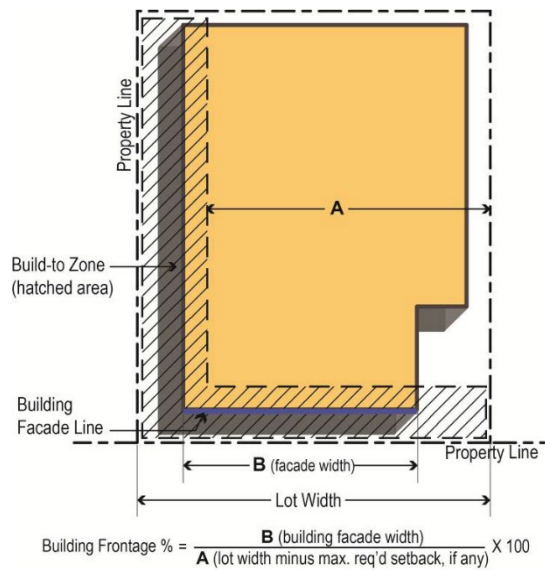




*Plan View - Building Façade Line Illustrations*

**Building Frontage**

The percentage of a building's façade line that is required to be located within the Build-to Zone (BTZ) as a proportion of the lot's width along the fronting public street. Required driveways, stairs to access entrances, parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall count towards the required building frontage.



*Image showing Building Frontage calculation*

**Building Step-Back**

Building step-back is the setting back of the building façade line away from the street at a specific floor or height.

### ***City Water & Light***

City Water & Light means City Water & Light Plant of the City of Jonesboro, a consolidated municipal improvement district.

### ***Commercial Use or Mixed-Use Building***

Means a building in which at least the ground floor of the building is built to Commercial Ready standards and any of the floors are occupied by non-residential or residential uses.

### ***Commercial Ready***

Means a ground floor space constructed with appropriate building orientation, entrance and window treatment and floor-to-floor height in order to accommodate ground floor retail/commercial uses (including but not limited to commercial, retail, restaurant, entertainment, and lobbies for civic, hotel, or multi-family uses). Standards for Commercial Ready frontage are in Subsection 5.4. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Commercial Ready space, the space must comply with all building and construction codes for commercial uses. The intent of Commercial Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.



*Image of a cottage court*

### ***Complete Street***

Means a street that not only accommodates various modes of transportation such as automobiles, transit, bikes, and pedestrians, but also establishes a design context that is conducive for redevelopment along the street.

### ***Cottage Court***

The Cottage Court building type consists of a series of small, detached structures, providing multiple units arranged to define a shared court that is typically perpendicular to the street. The shared court takes the place of a private rear yard and becomes an important community-enhancing element of this type. This type is appropriately scaled to fit as transitions between single-family neighborhoods and commercial corridors. It enables appropriately-scaled, well-designed infill residential and is important for providing a broad choice of housing types and promoting walkability.



*Image of a cottage home*

***Cottage***

The Cottage building type consists of small, one-story single-family detached residential units that are built close to the street and each other. They are typically sideyard/zero lot line homes with a small yard or patio space. They may also be called patio homes. This type of building enables appropriate infill residential within existing neighborhoods and is important for providing a broad choice of housing types and promoting walkability.

***DJDC District***

Means an area within the DJDC that is intended to preserve and/or create an urban form that is distinct from other areas within the DJDC Area. DJDC Districts are identified on the District Map.



*Image of a duplex home type*

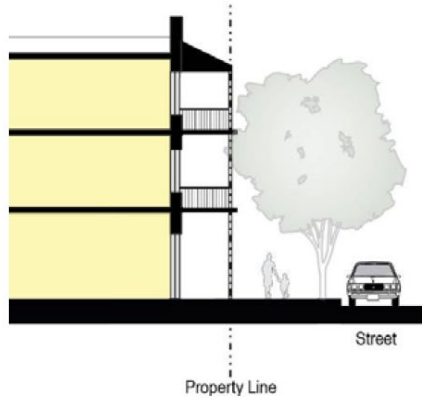
***Duplex***

The Duplex is a small- to medium-sized structure that consists of two side-by-side or stacked dwelling units, both facing the street, and within a single building massing. This type has the appearance of a medium to large single-family home and is appropriately scaled to fit within primarily single-family neighborhoods or medium-density neighborhoods. It enables appropriately-scaled, well-designed residential infill and is important for providing a broad choice of housing types and promoting walkability.



***Façade Area***

Means the surface area of a building's elevation (including all floors) not counting minor indentations fronting a particular street. Ground floor façade area is the surface area of a building's ground floor elevation not counting minor indentations fronting a particular street. Upper floor façade area is the surface area of a building's upper floor elevations not counting minor indentations fronting a particular street.



***Images of Galleries***

***Gallery***

Is a roofed promenade or canopy, especially one extending along the wall of a building and supported by arches or columns on the outer side. The gallery space is an unenclosed (non-conditioned) space and may be two or more stories tall.

***J-Swing Garage***

A home with a driveway that loads from the primary street and swings into the side of the home to enter the garage. Two of these homes side-by-side, should share a driveway or a curb-cut to driveways and have garages facing each other.



*Images of a home with a J-Swing Garage from the front and side of home*



*Image of a live-work building type*

***Live-Work Unit***

Means a dwelling unit that is also used for work purposes, provided that the "work" component is restricted to the uses of professional office, artist's workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The "live" component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by the Jonesboro City Code in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

***Multi-Unit Home***

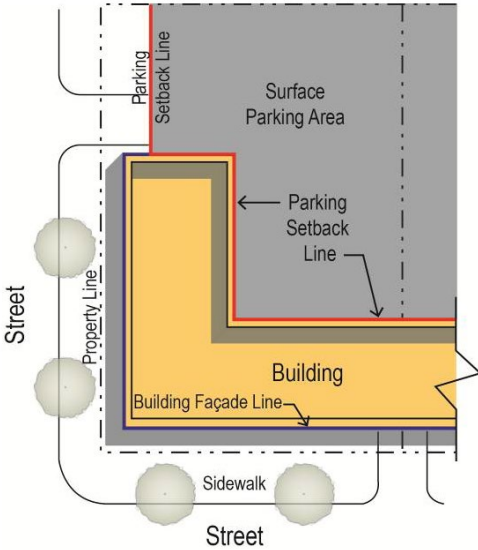
A multi-unit building (2 to 4 units) that is designed to appear as a large home from the exterior, but functions as a multi-unit building on the interior. Multi-Unit Homes have one main front door for the building, but may also have side and rear entries. Parking is accessed from an alley or a driveway to the rear of the lot. Parking does not face a public right-of-way.



*Images of Multi-Unit Homes*

***Numerical Standard***

Means any standard that has a numerical limit (minimums and maximums) or value as established within both the text and graphic standards of this Section.



*Illustration of a parking setback line*

***Open Space***

Publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, playgrounds, squares, etc., provided to meet the standards in Subsection 7.0 of this Section. Open space may be privately or publicly owned and/or maintained.

***Patio Homes***

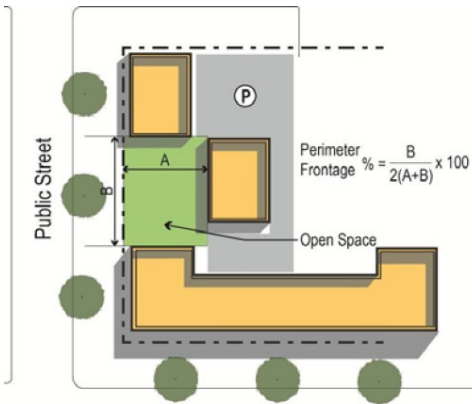
Single-family detached or attached (duplex) homes of no more than two attached units, each located on a small lot that has at least some private yard space, generally in the back and/or side yards, but also possibly a small front yard. These types of homes may also be called "Villa" or "Zero Lot Line" homes if the homes are located on one of the side property lines.

***Parking Setback Line***

Means the distance that any surface parking lot is to be set back from either the principal building façade line or property line along any street frontage (depending on the specific standard in the DJDC District). Surface parking may be located anywhere behind the parking setback line on the property.

***Perimeter Frontage***

Means the measurement of a proportion of public street frontage of the total exterior perimeter of a park or open space area.



*Illustration showing how Perimeter Frontage is calculated for Open Space along a Public Street*

### ***Primary Entrance***

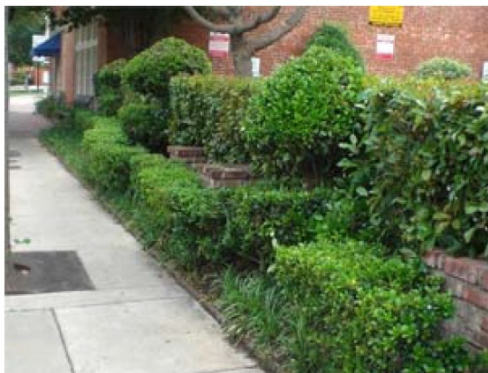
The public entrance located along the front of a building facing a street or sidewalk and provides access from the public sidewalk to the building. It is different from a secondary entrance which may be located at the side or rear of a building providing private controlled access into the building from a sidewalk, parking or service area.

### ***Residential Use Building***

A building that is built to accommodate only residential uses on all floors of the building, such as a detached single-family home, attached single-family home (i.e. townhome), two-family home (i.e. duplex), mansion home (3 - 4 units), multi-family building (5 or more units; under single ownership or under multiple owners within a condominium regime).

### ***Service-Related Uses***

Means all uses that support the principal use on the lot including parking access, garbage/trash collection, utility meters and equipment, loading/unloading areas, and similar uses.



*Image of a combination masonry and living street screen*

### ***Street Frontage Designation***

As identified on the DJDC District Map, existing and recommended streets in the DJDC area are designated as Pedestrian Priority, Pedestrian-Friendly, or General Streets. Each frontage designation establishes a certain development context in order to improve walkability and pedestrian orientation within the DJDC Area.

### ***Street Screen***

A freestanding wall, living fence or combination fence built along the frontage line or in line with the building façade along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.

### ***Street Wall***

Indicates the creation of a "wall" or a sense of enclosure along the street with buildings placed immediately adjacent to the street/sidewalk. A street wall has a "void" if there is a surface parking lot or service area adjacent to the sidewalk/street.



*Image of townhomes*

### ***Townhouse/Rowhouse***

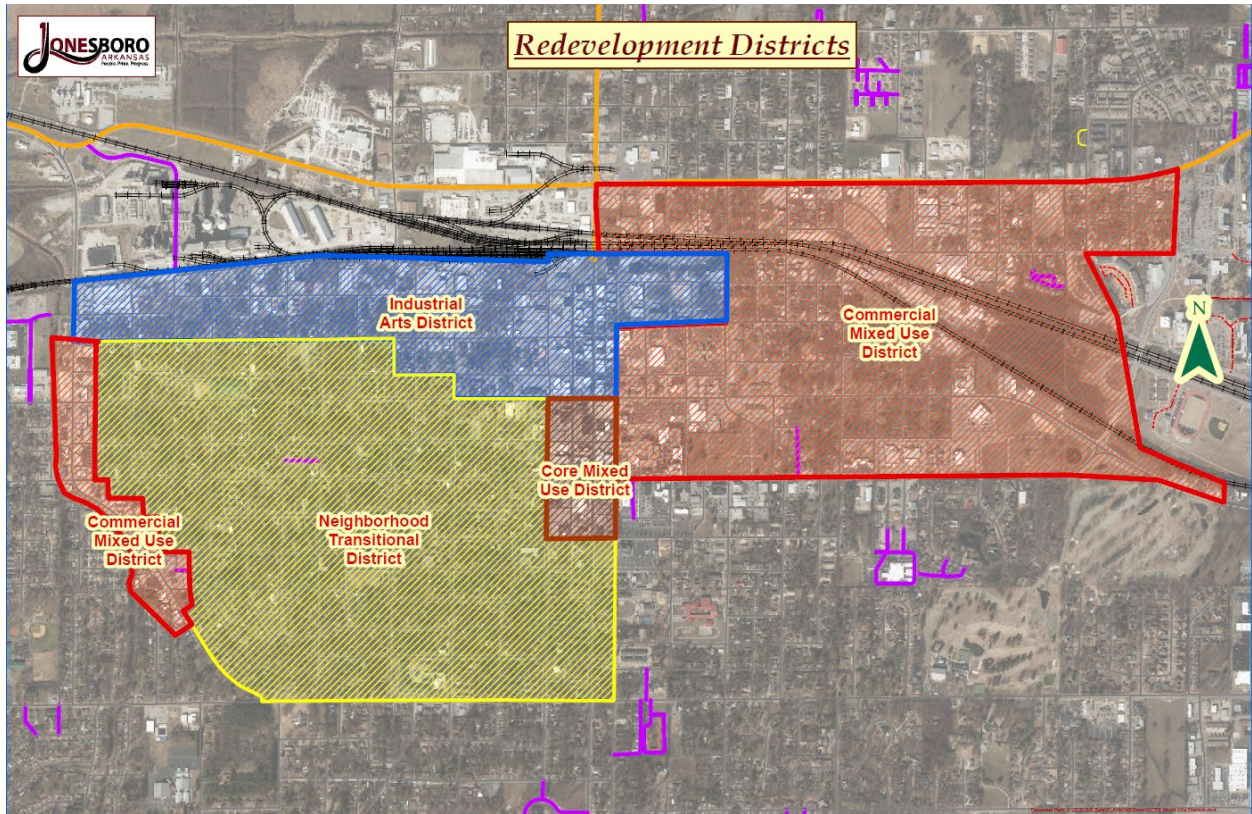
The Townhouse or Rowhouse is a small- to medium-sized typically attached structure that consists of 2 to 8 houses placed side-by-side. This type of residential building is typically located in a location that transitions from a primarily single-family neighborhood into a commercial corridor. This type of building enables appropriately-scaled, well-designed residential infill and is important for providing a broad choice of housing types and promoting walkability.

## **ATTACHMENTS**

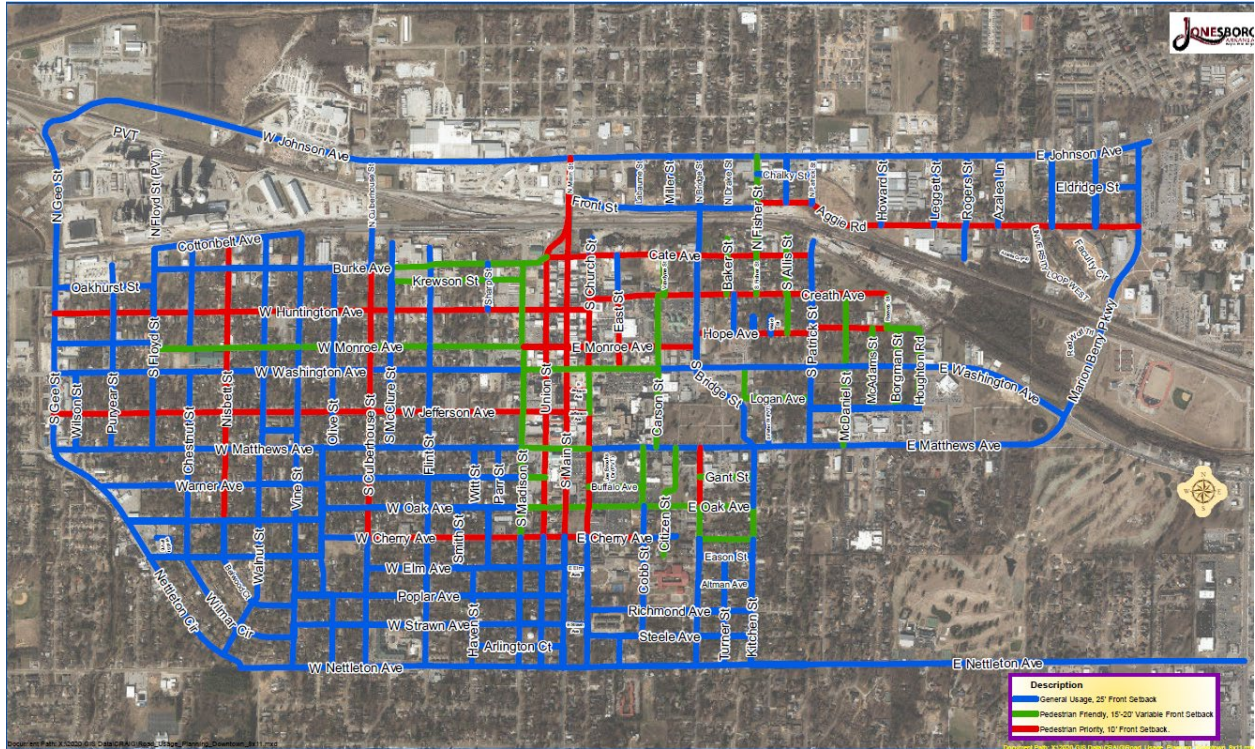
### **Table of Attachments**

- 1: DJDC District Map
- 2: Downtown Initiative Master Plan
- 3: Illustrations of Changes to Non-Complying Structures
- 4: Open Space Palette

Attachment 1: DJDC District Map



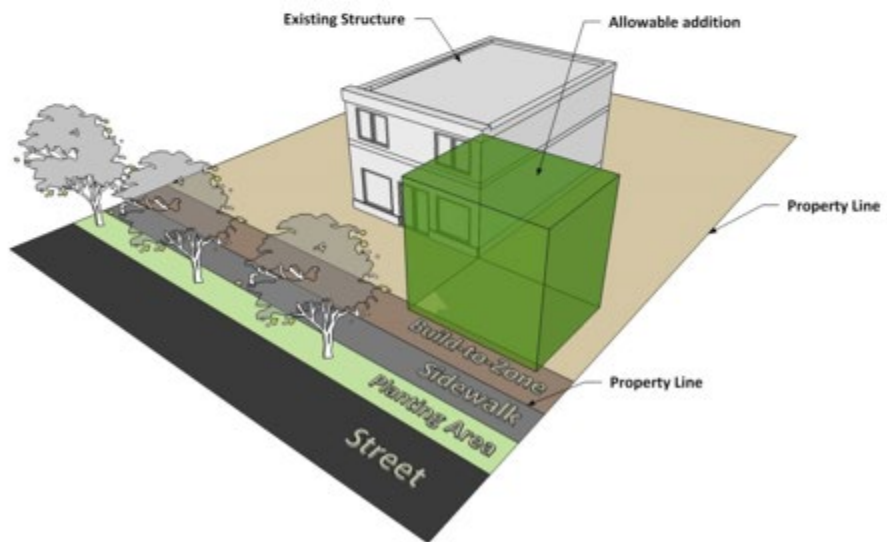
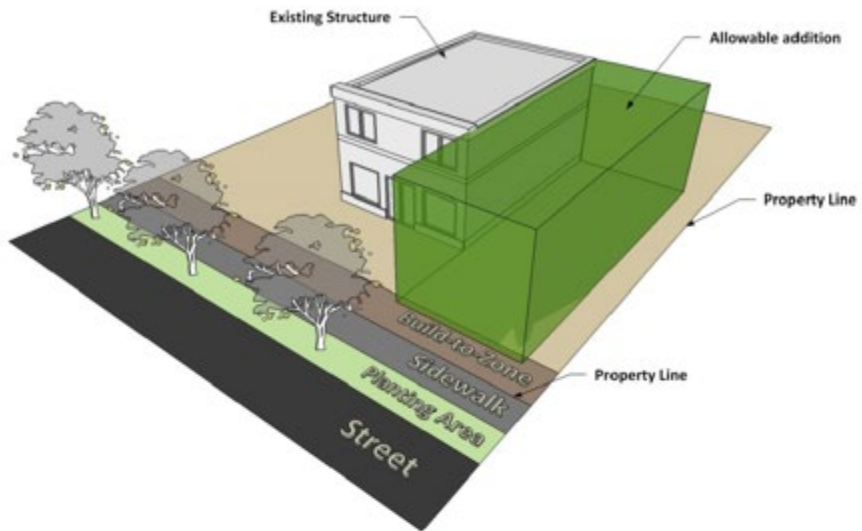
**Attachment 2: Downtown Initiative Master Plan**



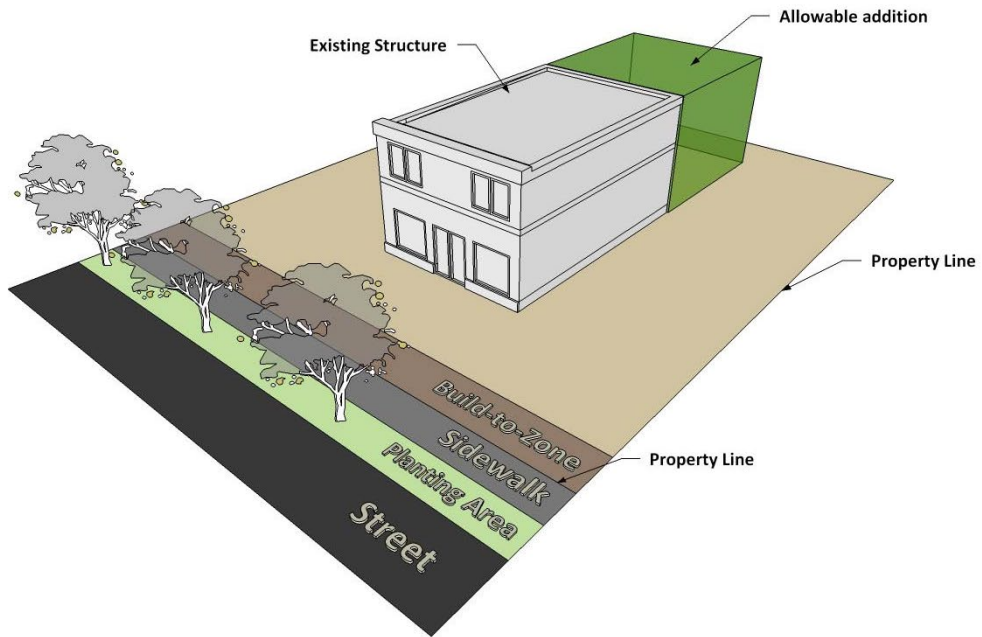
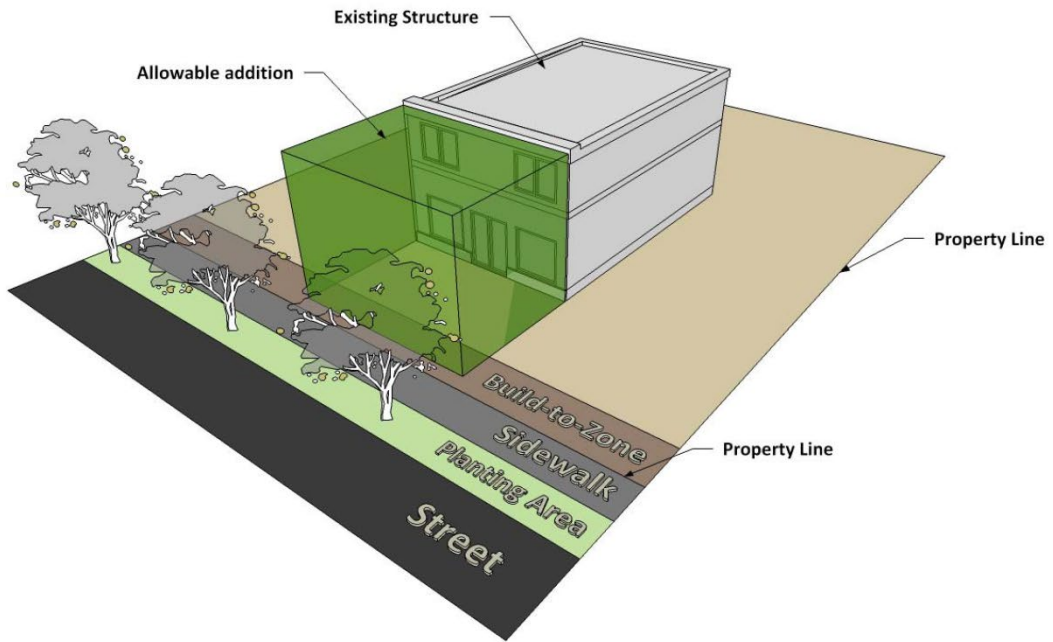
### Attachment 3: Illustrations of Changes to Non-Complying Structures

The following illustrations shall provide guidance to the property owners on the allowed and prohibited modifications to existing non complying structures and sites within the DJDC district.

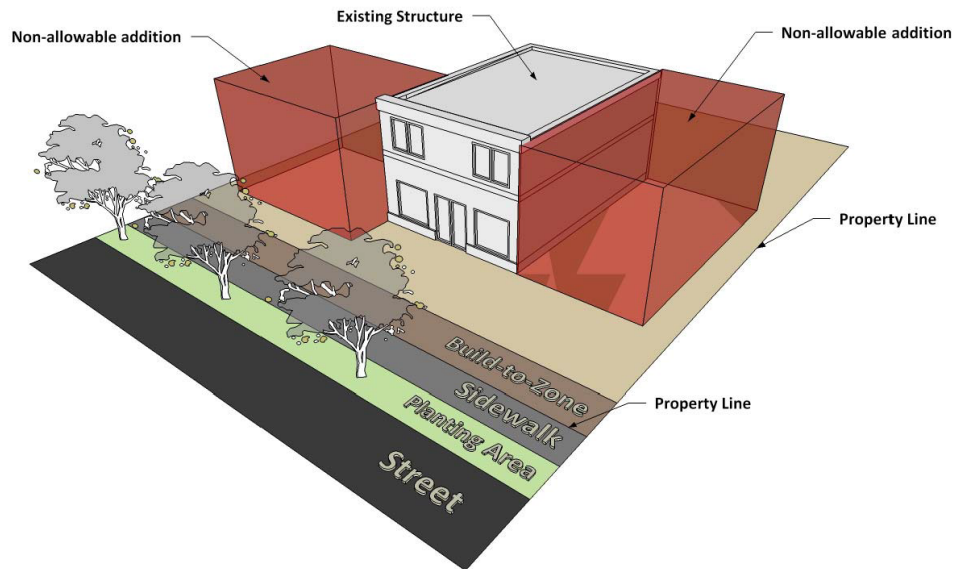
- a) Allowable Additions: The following illustrations show potential allowable additions to non-complying structures and sites. Additions shall meet the build-to-zone standards of the character district they are located.







b) Non-Allowable Additions: The following illustration shows potential non-allowable additions to non-complying structures and sites. This is due to the additions not complying with the build-to-zone standards of the DJDC zone.



## Attachment 4: Open Space Palette

The Open Space Palette and standards contained herein shall be used as guidelines to fulfill the required and recommended Public Open Space and Private Common Open Space requirements of Subsection 7 of this Section. The following Subsections provide a description of these Open Space types including the design context and criteria consistent with the vision for Downtown Jonesboro

### 1. Public Open Space Standards

#### a) Pocket Park Standards



Pocket Parks are small scale public urban open spaces intended to provide recreational opportunities where (publicly accessible/park) space is limited. Typically, pocket parks may be placed in any DJDC Zone. They are often located between buildings and developments; on single vacant lots; and on small irregular pieces of land. Low maintenance landscaping and facilities is recommended in order to support multiple pocket parks in a park system.

Development may include pavilions, picnic tables, small performance stage, seating areas, gathering areas, family play areas, gazebos, small game areas, small community gardens, dog parks, and interactive art. Shade and lighting is desired.

#### Typical Characteristics

##### General Character

Small urban open space responding to specific user groups and space available.

Range of character can be for intense use or aesthetic enjoyment. Low maintenance is essential.

##### Location and Size

0.25 – 1.99 acres

Within walking distance of either a few blocks or up to a ¼ mile of residences

##### Typical Uses

Varies per user group

b) **Green Standards**



A Green is a public urban open space available for civic purposes, commercial activity, unstructured recreation and other passive uses. Greens shall primarily be naturally landscaped with many shaded places to sit. Open lawn areas shall encourage civic gathering. Appropriate paths, civic elements, fountains or open shelters may be included and shall be formally placed within the Green.

A Green shall be adjacent to a public right of way and be spatially defined by buildings which shall front onto and activate this space.

**Typical Characteristics**

**General Character**

- Open space
- Spatially defined by street and building frontages and landscaping
- Lawns, trees and shrubs naturally disposed
- Open shelters and paths formally disposed

**Location and Size**

- 0.25 – 4 acres
- Minimum width – 25'
- Minimum pervious cover – 80%
- Minimum perimeter frontage on public right of way – 50%

**Typical Uses**

- Unstructured recreation
- Casual seating
- Commercial and civic uses
- No organized sports

c) **Square Standards**



A square is a public urban open space available for civic purposes, commercial activity, unstructured recreation and other passive uses. The square should have an urban, formal character and be defined by the surrounding building frontages and adjacent tree-lined streets. All buildings adjacent to the square shall front onto the square. Adjacent streets shall be lined with appropriately scaled trees that help to define the square.

The landscape shall consist of lawns, trees, and shrubs planted in formal patterns and furnished with paths and benches. Shaded areas for seating should be provided. A civic element or small structure such as an open shelter, pergola, or fountain may be provided within the square.

**Typical Characteristics**

**General Character**

Formal open space

Spatially defined by buildings and tree-lined streets.

Open shelters, paths, lawns, and trees formally arranged

Walkways and plantings at all edges

Abundant seating opportunities

**Location and Size**

0.25 – 4 acres

Minimum width – 25'

Minimum pervious cover – 60%

Minimum perimeter frontage on public right of way – 60%

Located at important intersections

**Typical Uses**

Unstructured and passive recreation – no organized sports.

Community gathering

Occasional commercial and civic uses

d) **Plaza Standards**



A plaza is a public urban open space that offers abundant opportunities for civic gathering. Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces.

The landscape should have a balance of hardscape and planting. Various types of seating should be provided from planter seat walls, to steps, to benches, to tables, and chairs. Trees should be provided for shade. They should be formally arranged and of appropriate scale. Plazas typically should be located at the intersection of important streets. A minimum of one public street frontage shall be required for plazas.

**Typical Characteristics**

**General Character**

Formal open space

A balance of hardscape and planting

Trees important for shade

Spatially defined by building frontages

**Location and Size**

0.1 – 1 acre

Minimum width – 30'

Minimum pervious cover – 20%

Minimum perimeter frontage on public right of way – 25%

Located at important intersections, at vista termini, or at entrances to public/civic buildings

**Typical Uses**

Commercial and civic uses

Formal and casual seating

Tables and chairs for outdoor dining

Retail and food kiosks

e) **Pocket Plaza Standards**



A pocket plaza is a small scale public urban open space that serves as an impromptu gathering place for civic, social, and commercial purposes. The pocket plaza is designed as a well-defined area of refuge separate from the public sidewalk. It is frequently located in a building supplemental zone next to the streetscape.

These areas contain a lesser amount of pervious surface than other open space types. Outdoor dining with café tables and chairs, water features, public art and other shaded amenities are appropriate uses.

**Typical Characteristics**

**General Character**

Formal open space for gathering

Defined seating areas

Refuge from the public sidewalk

Spatially defined by the building configuration

**Location and Size**

Min. 300 s.f.

Min. width – 15' / Max. width 20'

Minimum pervious cover – 10 %

Minimum perimeter frontage on public right of way – 25%

Located at important intersections, at vista termini, or at entrances to public/civic buildings

**Typical Uses**

Civic and commercial uses

Formal and casual seating

f) **Pedestrian Passage (Paseo) Standards**



Pedestrian passages or paseos are linear public urban open spaces that connect one street to another at through-block locations. Pedestrian passages create intimate linkages through buildings at designated locations. These wide pathways provide direct pedestrian access to residential or other commercial addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm.

Pedestrian passages should consist of a hardscape pathway with pervious pavers activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants. Shade is required for the success of the paseo.

This Public Open Space type is most appropriate for the reinvention of the existing alleys within Downtown Jonesboro

**Typical Characteristics**

**General Character**

Hardscape pathway with pervious pavers

Defined by building frontages

Frequent side entries and frontages

Shade Required

Minimal planting and potted plants

Maintain the character of surrounding buildings

**Standards**

Min. Width      15 feet

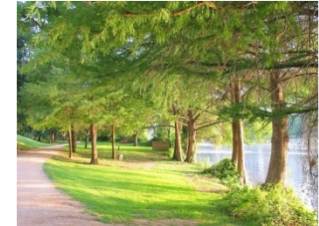
**Typical Uses**

Pedestrian connection and access

Casual seating



g) **Multi-Use Trail Standards**



A multi-use trail is a linear public urban open space that accommodates two or more users on the same, undivided trail. Trail users could include pedestrians, bicyclists, skaters, etc. A trail frequently provides an important place for active recreation and creates a connection to regional paths and biking trails.

Trails within greenways or neighborhood parks shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape.

Pedestrian amenities add to recreational opportunities, particularly in an urban setting. These include drinking fountains, scenic view posts, fitness stations, and directional signs, and may be spread along the trail or grouped in a trailhead area.

Trails shall align with any current City of Jonesboro Bicycle and Pedestrian Connectivity Plan identified as a section of that plan.

**Typical Characteristics**

**General Character**

**Multi-use trail in Neighborhood Park:**

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Formally disposed pedestrian furniture, landscaping and lighting

**Multi-use trail along roads:**

Paved trail with frequent gathering spaces and regular landscaping.

**Standards**

Min. Width                      10 feet

**Typical Uses**

Active and passive recreation

Casual seating

**h) Family-friendly Play Area Standards**



Family-friendly play areas are areas within urban open spaces that are conducive to the recreational needs of families with children. Family-friendly play areas range in style from urban pocket parks within mixed use developments to playscapes within neighborhood parks.

These play areas should serve as quiet, safe places -- protected from the street and typically located where children do not have to cross major streets to access. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided for ease of supervision.

Playscape equipment and design must be reviewed and approved by the City prior to installation. The need for fencing depends on the surrounding environment.

A larger playground may be incorporated into a neighborhood park, whereas a more intimate family oriented design may be incorporated into a pocket park.

**Typical Characteristics**

**General Character**

- Focused toward family-friendly needs
- Fencing depends on surroundings
- Open shelter
- Shade and seating provided
- Play structure, interactive art or fountains

**Standards**

- |           |            |
|-----------|------------|
| Min. Size | 300 sq.ft. |
| Max. Size | N/A        |
- As described by open space type in which playground is located
  - Protected from traffic
  - No service or mechanical equipment

**Typical Uses**

- Active and passive recreation
- Unstructured recreation
- Casual seating

## 2. Private Common Open Space Standards

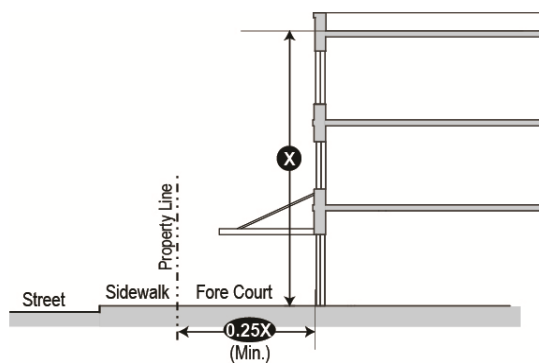
### a) Forecourt Standards



Forecourt is a small scale private common open space surrounded on at least two sides by buildings. A forecourt is typically a building entry providing a transition space from the sidewalk to the building. The character serves as a visual announcement of the building to visitors with additional amenities such as signage, water features, seating, planting, etc.

Forecourts should be laid out proportionate to building height with a 1:4 (min.) ratio (see figure below). In order to offset the impact of taller buildings, the detail of the forecourt level should seek to bring down the relative scale of the space with shade elements, trees, etc.

The hardscape should primarily accommodate circulation. Seating and shade are important for visitors. Trees and plantings are critical to create a minimum of 30% pervious cover and offset the effect of the urban heat island.



#### Typical Characteristics

##### General Character

- Small scale private common open space
- Defined by buildings on at least 2 sides with connection to public sidewalk
- Size of court should be proportionate to building height
- Hardscape should accommodate entry circulation
- Trees and plants are critical
- Enhance the character of surrounding buildings

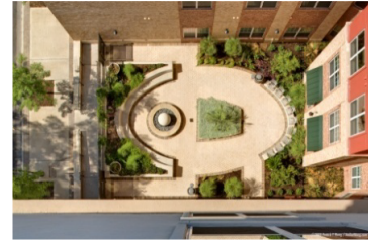
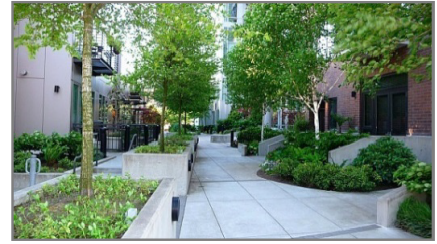
##### Standards

- Min. Width 25'
- Minimum Size Depth: Based on building height ratio; Width: min. of 50% of the building's frontage along that street
- Minimum pervious cover – 30%

##### Typical Uses

- Building Entry Circulation
- Visual building announcement

**b) Courtyard Standards**



Courtyards are small scale private common open spaces surrounded on at least three sides by buildings with a pedestrian connection to a public sidewalk. Courtyards maintain the character and style of the surrounding buildings.

Courtyards should be laid out proportionate to building height between 1:1 and 2:1 ratio. In order to offset the impact of taller buildings, the detail of the courtyard level should seek to bring down the relative scale of the space with shade elements, trees, etc. Transition areas should be set up between the building face and the center of the court.

The hardscape should accommodate circulation, gathering, seating, and shade. Trees and plantings are critical to create a minimum of 30% pervious cover and offset the effect of the urban heat island.

**Typical Characteristics**

**General Character**

Small scale private common open space

Defined by buildings on at least 3 sides with connection to public sidewalk

Size of court should be proportionate to building height

Hardscape should accommodate circulation, gathering, and seating.

Trees and plants are critical

Maintain the character of surrounding buildings

**Standards**

Min. Width 25'

Minimum Area 650 s.f.

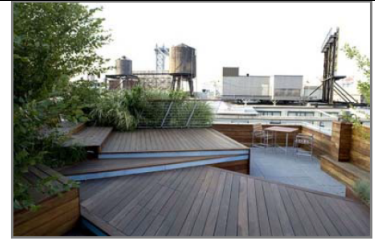
Minimum pervious cover – 30%

**Typical Uses**

Gathering

Casual seating

**c) Roof Terrace Standards**



A Roof Terrace is a private common open space serving as a gathering space for tenants and residents that might not be at grade.

Up to 50% of the required private common open space may be located on a roof if at least 30 % of the roof terrace is designed as a Vegetated or Green Roof. A Vegetated or Green roof is defined as an assembly or system over occupied space that supports an area of planted beds, built up on a waterproofed surface.

Private common open space on a roof must be screened from the view of the adjacent property. The hardscape should accommodate circulation, gathering, seating, and shade.

**Typical Characteristics**

**General Character**

Small scale private common open space on roof top

Screened from view of adjacent property

Vegetated portion critical

Hardscape should accommodate gathering, seating, shade

Provides common open space that might not be available at grade

**Standards**

Min. Area                      50% of the roof top

Planted area – 30% min.

**Typical Uses**

Gathering for tenants and residents

Green Roof