

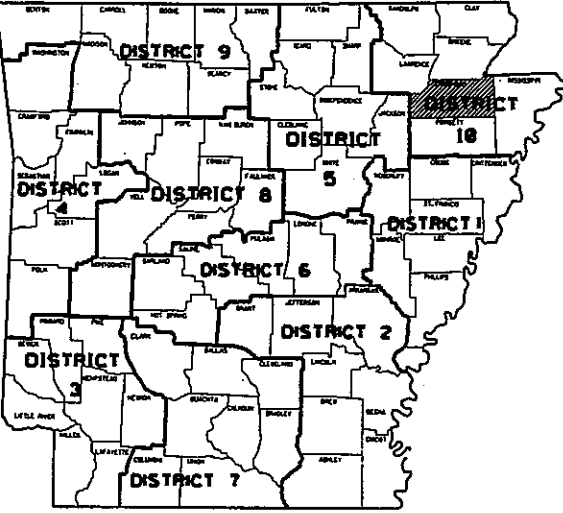
CITY OF JONESBORO CONSTRUCTION PLANS

JONESBORO SRTS I (2008)

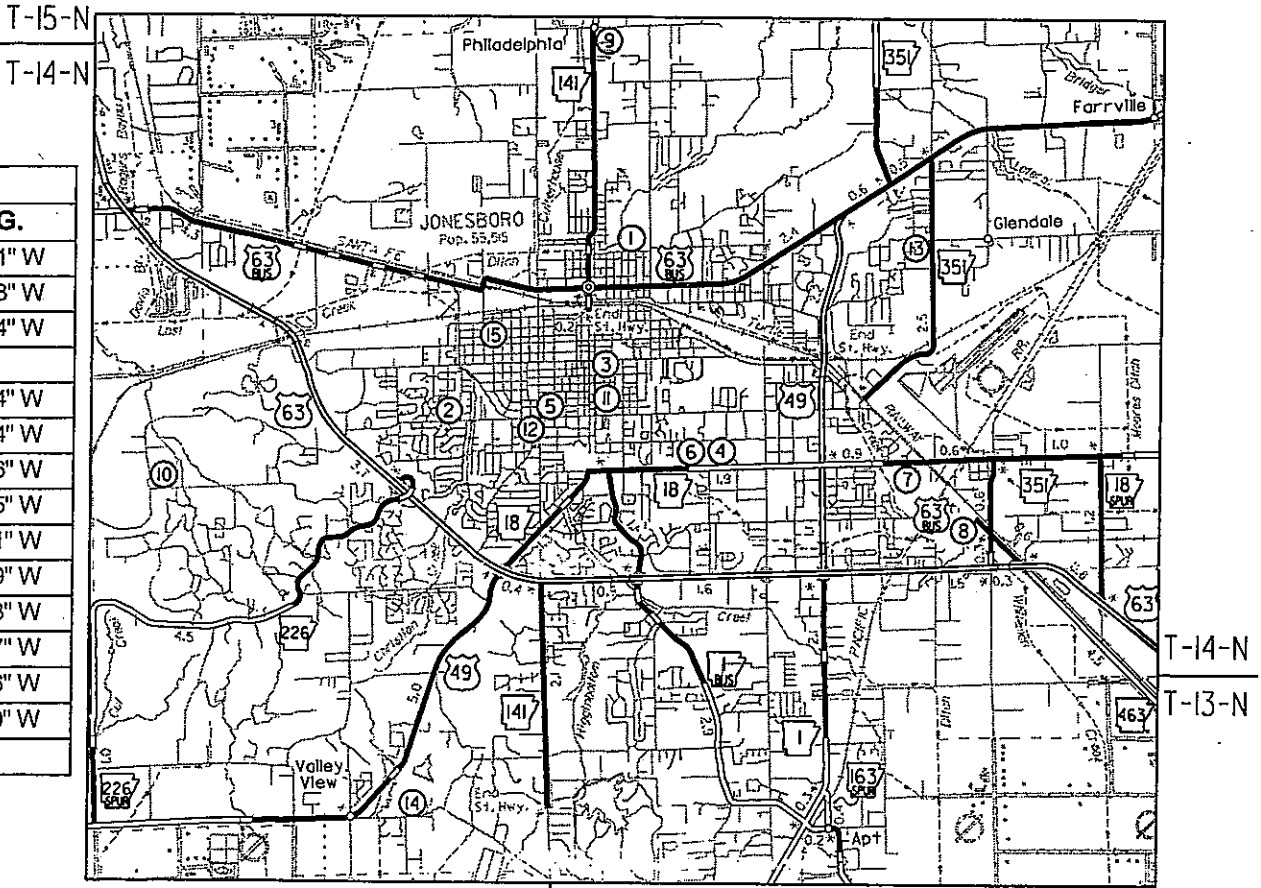
CRAIGHEAD COUNTY

JOB 100681

F.A.P. NO. SRSI - 1402(43)



ARK. HWY. DIST. NO. 10



CONSTRUCTION SITES			
#	LOCATION	LAT.	LONG.
1	ALTERNATIVE SCHOOL	35°50'59" N	90°41'51" W
2	ANNIE CAMP JR. HIGH	35°49'43" N	90°43'43" W
3	BLESSED SACRAMENT	35°50'12" N	90°42'14" W
4	VISUAL PERFORMING ARTS	(NOT INCLUDED)	
5	KINDERGARTEN CENTER	35°49'43" N	90°42'44" W
6	MACARTHUR JR. HIGH	35°49'29" N	90°41'24" W
7	NETTLETON INTERMEDIATE	35°49'58" N	90°39'06" W
8	NETTLETON JR. HIGH	35°48'45" N	90°38'55" W
9	MATH AND SCIENCE	35°52'45" N	90°42'01" W
10	RIDGEFIELD CHRISTIAN	35°49'16" N	90°46'19" W
11	INTERNATIONAL STUDIES	35°49'53" N	90°42'03" W
12	HEALTH AND WELLNESS	35°49'27" N	90°43'07" W
13	UNIVERSITY HEIGHTS	35°50'52" N	90°39'16" W
14	VALLEY VIEW	35°46'40" N	90°44'20" W
15	MICRO SOCIETY	(NOT INCLUDED)	

R3E | R4E
NOT TO SCALE

F Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS I
(2008)

Drawn: AT	Checks: JLB	Mech.:	Elec.:	Project No. AHTD 100681 F&H 09089	1 of 30
Civil:	Projects:	Appr.:	Date: 08-11-09	Scale: NTS	

INDEX OF SHEETS

SHEET NO.	TITLE	DRWG. NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS AND GENERAL NOTES		
3	SUMMARY OF QUANTITIES AND REVISION BOX		
4-20	CONSTRUCTION PLAN SHEETS		
21-24	SPECIAL DETAILS		
25	PAVEMENT MARKING DETAILS	PM-1	11/18/2004
26	STANDARD HIGHWAY SIGNS AND SUPPORT ASSEMBLIES	SHS-1	4/17/2008
27	U-CHANNEL POST ASSEMBLIES	SHS-2	10/9/2003
28	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	4/17/2008
29	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	11/20/2008
30	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	11/20/2008

GENERAL NOTES


- LEVEL DATUM U.S.C. & G. S.
- GRADE LINE DENOTES FINISHED GRADE OF SIDEWALKS WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U.S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATION.
- ALL PARTS OF THIS DESIGN & INSTALLATION SHALL BE IN ACCORDANCE WITH THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARDS AND DETAILS, ADA STANDARDS FOR ACCESSIBLE DESIGN AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITIONS.
- CONTRACTOR SHALL NOTIFY ALL EXISTING UTILITY OWNERS BEFORE BEGINNING WORK ON THIS PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES OR SERVICE LINES CROSSED OR EXPOSED BY CONSTRUCTION OPERATIONS. WHERE EXISTING UTILITIES OR SERVICE LINES ARE CUT, BROKEN OR DAMAGED, THE CONTRACTOR SHALL REPLACE OR REPAIR THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER, AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL PROVIDE FOR THE DIVERSION OF PEDESTRIANS AND VEHICLES DURING THE PROGRESS OF WORK IN A MANNER SATISFACTORY TO THE CONSTRUCTION ENGINEER. NO SEPARATE PAY ITEM.
- THE CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS AND SIDEWALKS ADJACENT TO PROJECT FREE OF MUD AND DEBRIS.
- ALL MODIFICATIONS TO THE CONSTRUCTION PLANS SHALL MEET THE LATEST VERSION OF ADA STANDARDS FOR ACCESSIBLE DESIGN AND BE APPROVED BY THE ENGINEER.
- PERFORM ELECTRICAL WORK IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE NFPA 70 (2002) NATIONAL ELECTRICAL CODE, NFPA 101 (2000) LIFE SAFETY CODE, STATE ELECTRICAL CODE AND LOCAL ELECTRICAL CODE.
- PAVEMENT MARKING SHOWN FOR REFERENCE ONLY. SEE PAVEMENT MARKING PLAN SHEETS.
- AS DETERMINED BY THE ENGINEER, FOUNDATION EMBEDMENT MAY BE DECREASED BY A MAXIMUM OF TWO FEET IF COMPETENT ROCK IS ENCOUNTERED PRIOR TO ACHIEVING PLAN EMBEDMENT AND AT LEAST HALF OF THE REMAINING PLAN EMBEDMENT LENGTH IS KEYED INTO COMPETENT ROCK.
- ALL STEEL POLES SHALL BE DESIGNED TO MEET THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4th EDITION (2001) WITH 2003 AND 2006 INTERIMS.
- ALL BREAKAWAY POLE BASES SHALL MEET NCHRP350 DESIGN AND PERFORMANCE STANDARDS.

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
105-1	CONSTRUCTION CONTROL MARKINGS
105-2	EQUIPMENT AND MATERIAL STORAGE ON BRIDGE STRUCTURES
107-1	WORKER VISIBILITY
108-1	LIQUIDATED DAMAGES
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
715-1	DESIGN AND MATERIAL REQUIREMENTS FOR TRAFFIC SIGNAL PEDESTAL POLES
718-1	REFLECTORIZED PAINT PAVEMENT MARKINGS
723-1	GENERAL REQUIREMENTS FOR SIGNS
JOB 100681	CHANNEL POST SIGN SUPPORT
JOB 100681	DOCUMENTATION OF PAYMENTS MADE TO DISADVANTAGED BUSINESS ENTERPRISES
JOB 100681	FLASHING BEACON ASSEMBLY



 Florence & Hutcheson, Inc. CONSULTING ENGINEERS					
JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS (2009)					
Drawn: AT	Checks: JLB	Mech.: 	Elec.: 	Project No.: AHTD 100681 FH 03089	2 of 30
Civil:	Project:	Appr.: 	Date: 08-11-09	Scale: NTS	

ITEM NUMBER	ITEM	ALTERNATIVE SCHOOL	ANNIE CAMP JR. HIGH	BLESSED SACRAMENT	KINDERGARTEN CENTER	MAC ARTHUR JR. HIGH	NETTLETON INTERMEDIATE	NETTLETON JR. HIGH	MATH AND SCIENCE	RIDGEFIELD CHRISTIAN	INTERNATIONAL STUDIES	HEALTH AND WELLNESS	UNIVERSITY HEIGHTS	VALLEY VIEW	TOTAL	UNIT
601	MOBILIZATION	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	1.00	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	0.08	1.00	LUMP SUM
726	STANDARD SIGN	16	24	16	16	16	24	52	16	16	16	16	24	16	268	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	3	2	2	2	3	3	2	2	2	2	3	2	30	EACH
SS&718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (12")	0	0	0	0	0	0	80	0	0	0	0	0	0	80	LIN. FT.
SS&718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (WORDS)	0	0	0	0	0	0	2	0	0	0	0	0	0	2	EACH
SP&728	CHANNEL POST SIGN SUPPORT (TYPE U-2)	0	0	0	0	0	0	2	0	0	0	0	0	0	2	EACH

NOTE: PROGRAMMING SOFTWARE SHALL BE INCLUDED IN PRICE BID ITEM SP-FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR

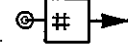


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CONSULTING ENGINEERS

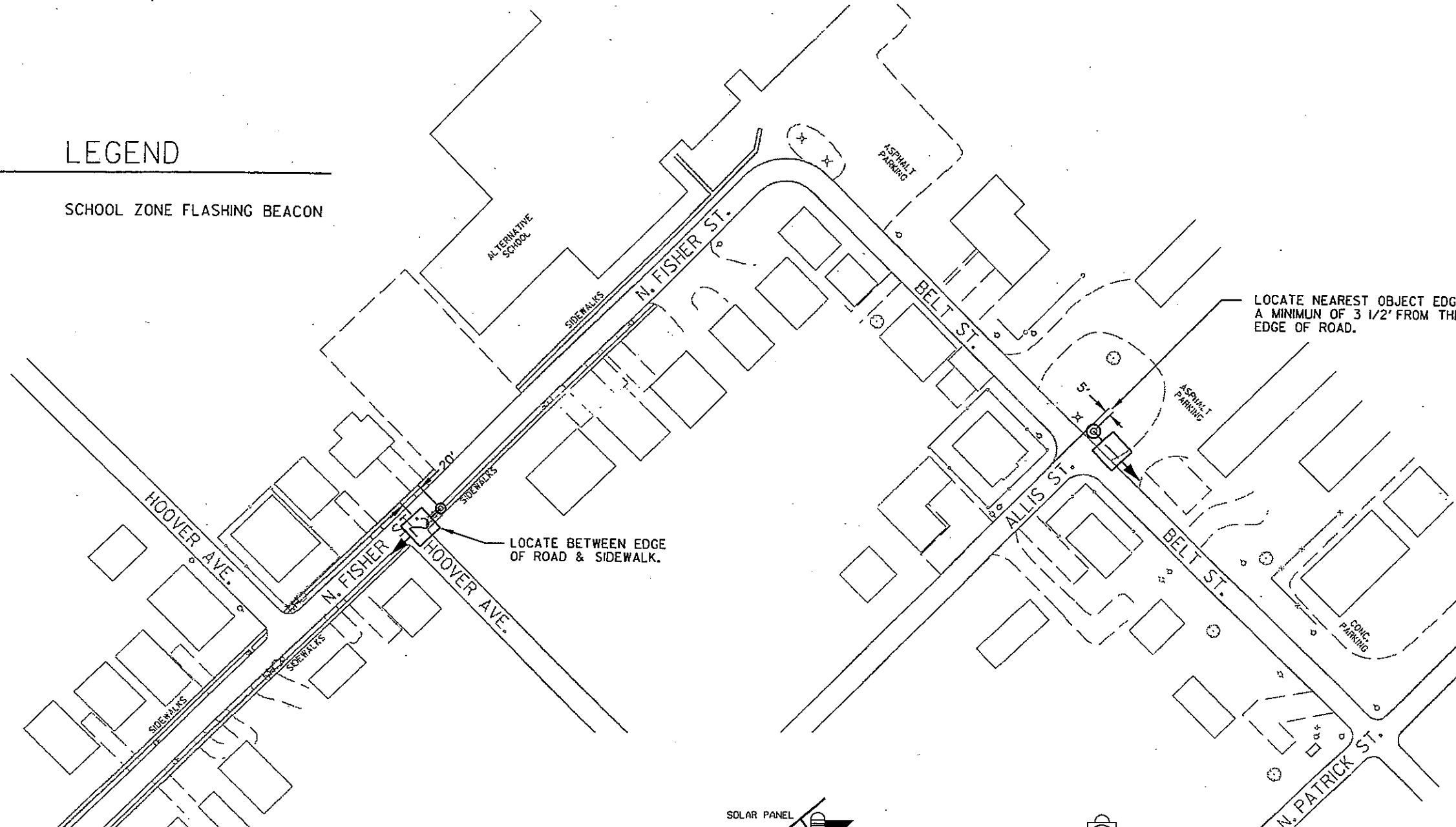
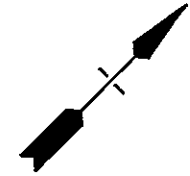
JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS
(2009)

Drawn: AT	Check: JLB	Mech.:	Elec.:	Project No.:	3 of 30
Civil:	Project:	Appr.:	Date:	AHTD 100681 F&H 09089 Scale: NTS	

LEGEND



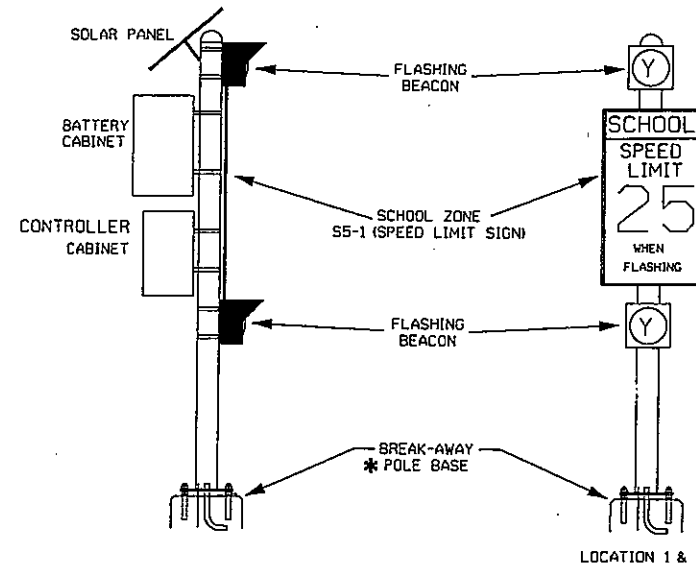
SCHOOL ZONE FLASHING BEACON



NOTES:

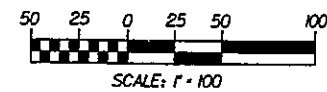
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

QUANTITIES			
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH



SOLAR FLASHERS

* DESIGN SHALL MEET NCHRP 350 STANDARDS

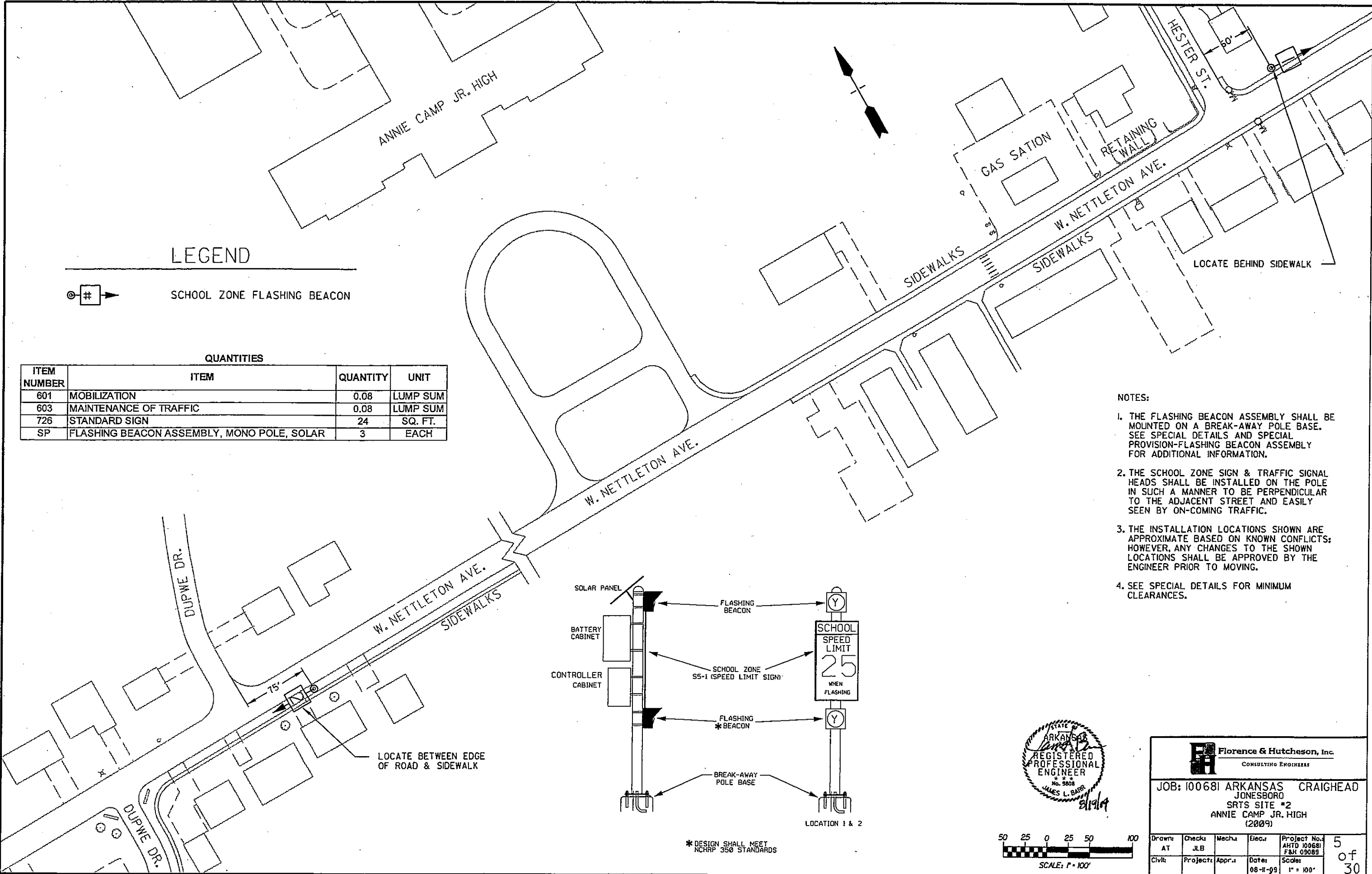


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CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #1 ALTERNATIVE SCHOOL (2009)

Drawn: AT	Checked: JLB	Mech.:	Elec.:	Project No.: AHTD 080329 F&H 0886
Civil:	Project:	Appr.:	Date: 08-11-09	Scale: 1" = 100'

4 of 30

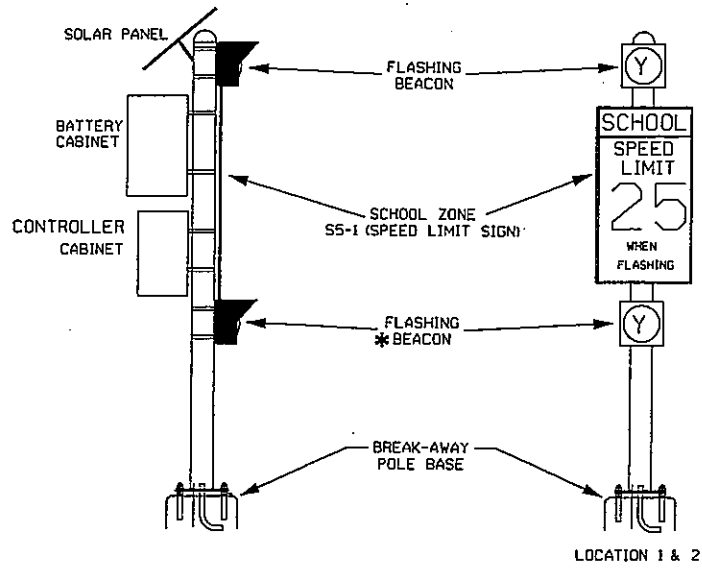


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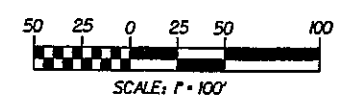
⊙ # → SCHOOL ZONE FLASHING BEACON

QUANTITIES			
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	24	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	3	EACH

- NOTES:
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
 2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
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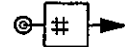


* DESIGN SHALL MEET NCHRP 350 STANDARDS

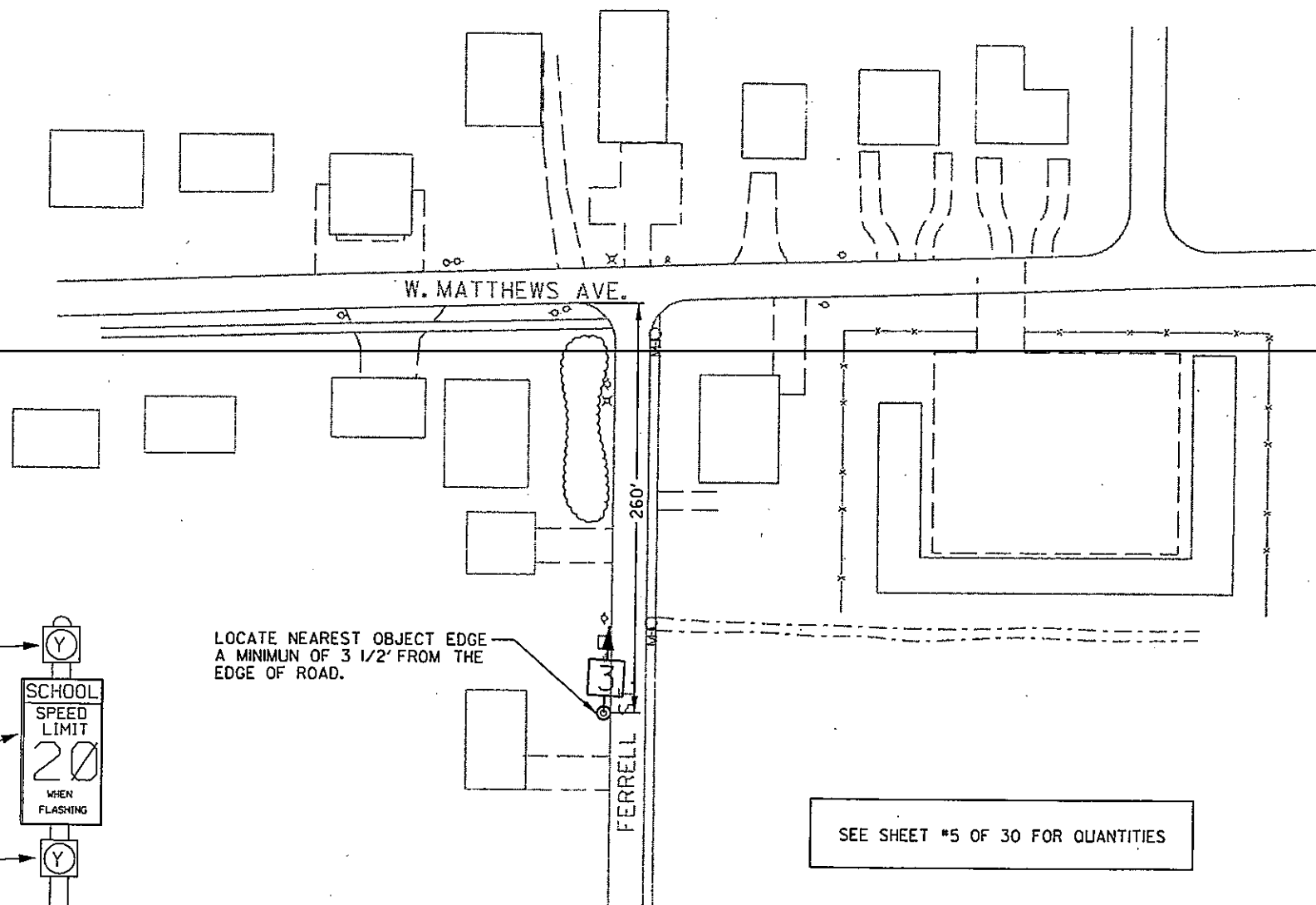


Florence & Hutcheson, Inc. CONSULTING ENGINEERS					
JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #2 ANNIE CAMP JR. HIGH (2009)					
Drawn: AT	Checked: JLB	Mechanical: Appr.:	Electrical: Date:	Project No. AHTD 100681 F&H 09089	5 of 30
Civil:	Project:	Appr.:	Date: 08-11-99	Scale: 1" = 100'	

LEGEND

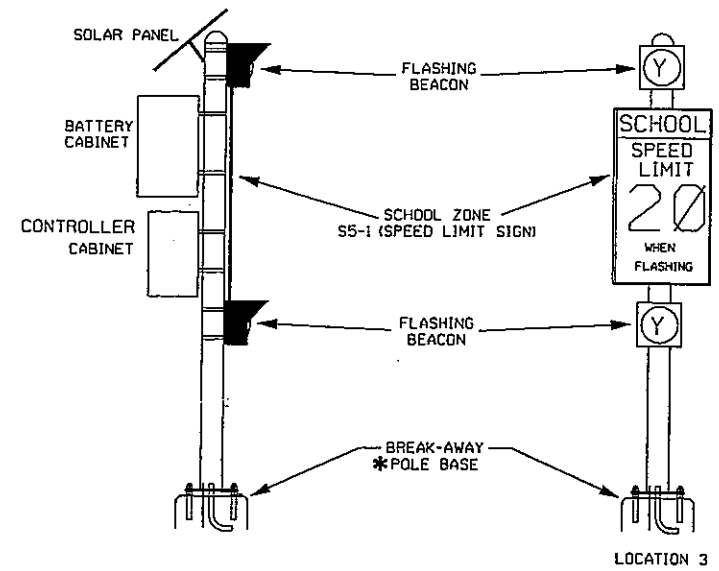


SCHOOL ZONE FLASHING BEACON



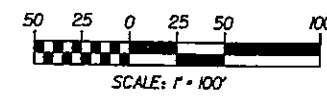
NOTES:

1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
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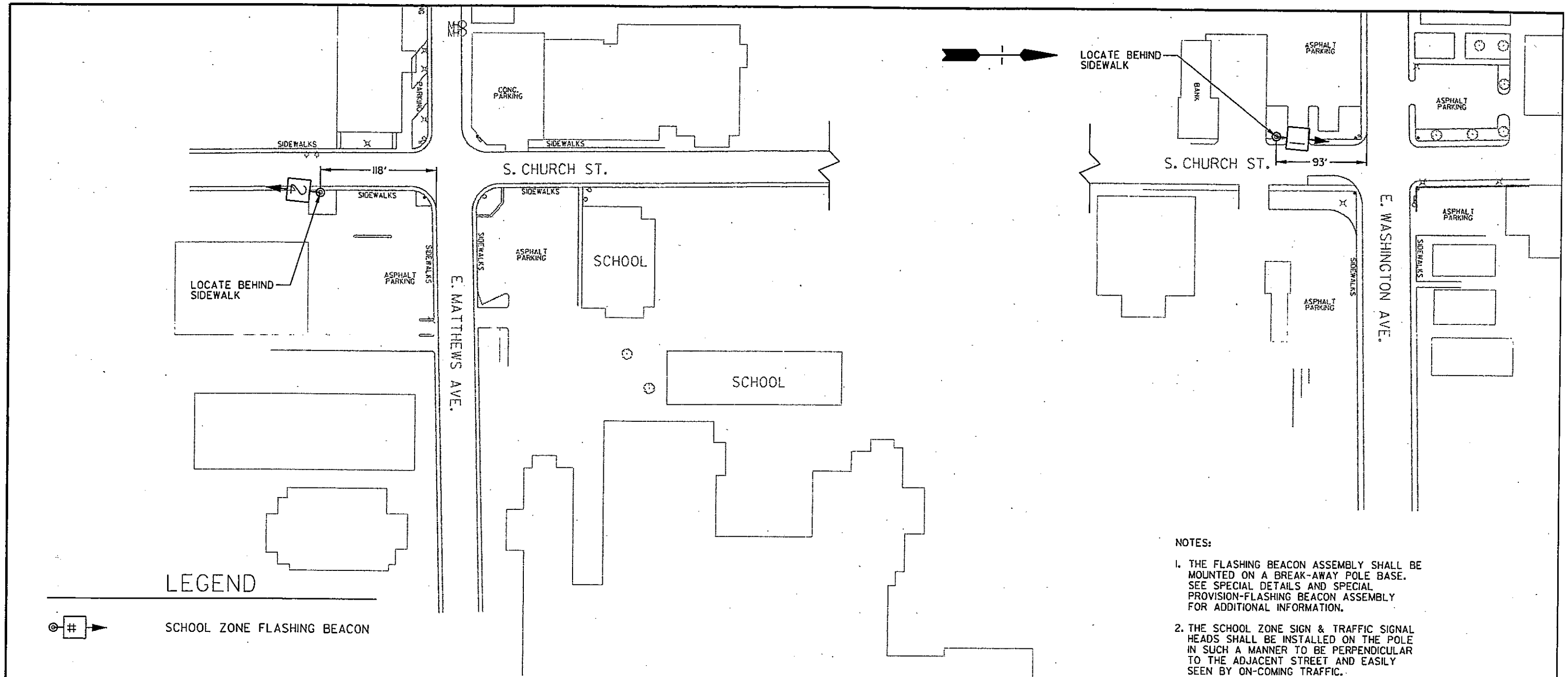


SOLAR FLASHERS
*DESIGN SHALL MEET NCHRP 350 STANDARDS

SEE SHEET #5 OF 30 FOR QUANTITIES

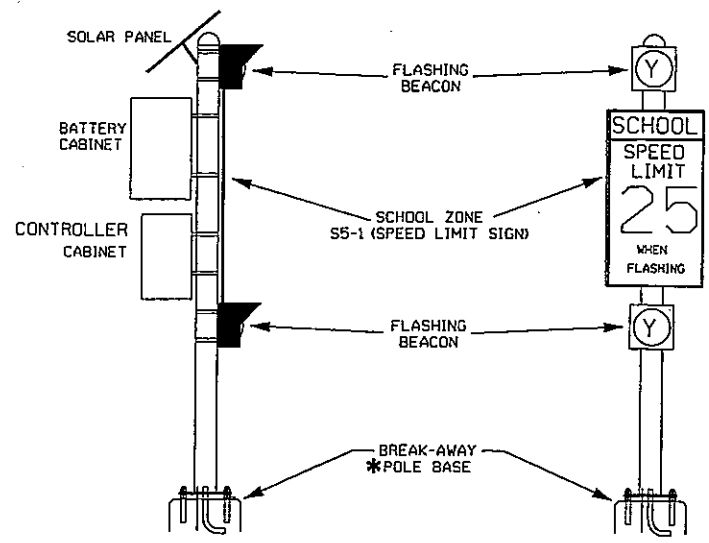


<p>Florence & Hutcheson, Inc. CONSULTING ENGINEERS</p>		<p>JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #2 ANNIE CAMP JR. HIGH (2009)</p>	
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⊙ # → SCHOOL ZONE FLASHING BEACON



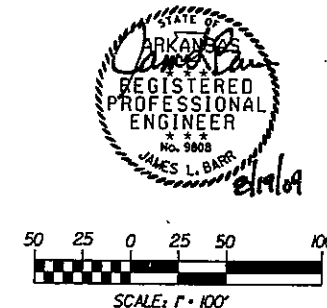
SOLAR FLASHERS

*DESIGN SHALL MEET NCHRP 350 STANDARDS

QUANTITIES			
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH

NOTES:

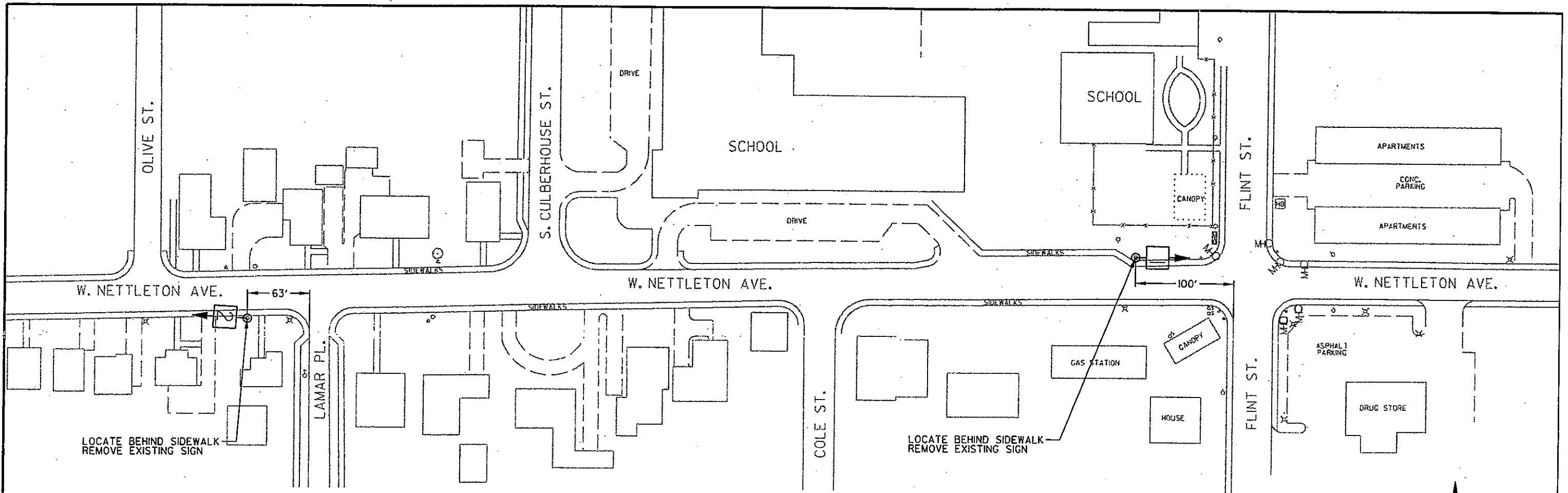
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4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.



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CONSULTING ENGINEERS

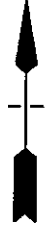
JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #3 BLESSED SACRAMENT (2009)

Drawn: AT	Checked: JLB	Mech: JLB	Elec: JLB	Project No.: AHFD 100681 F&H 09089	7 of 30
Civil: JLB	Project: JLB	Appr: JLB	Date: 08-11-09	Scale: 1" = 100'	



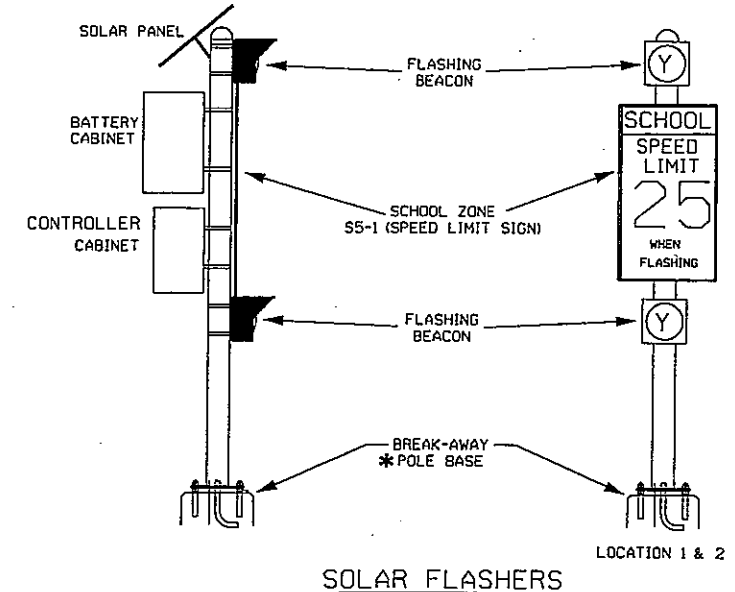
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LEGEND

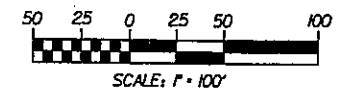
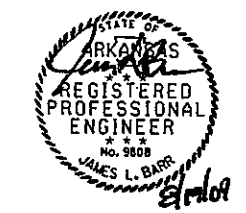
Ⓢ # → SCHOOL ZONE FLASHING BEACON



*DESIGN SHALL MEET NCHRP 350 STANDARDS

QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH



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CONSULTING ENGINEERS

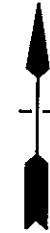
JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #5
KINDERGARTEN CENTER
(2009)

Drawn: AT	Checked: JLB	Mech.:	Elec.:	Project No.:
Civil:	Projects:	Appr.:	Date:	AHTD 100681 F&H 09089
			08-11-09	Scale: 1" = 100'

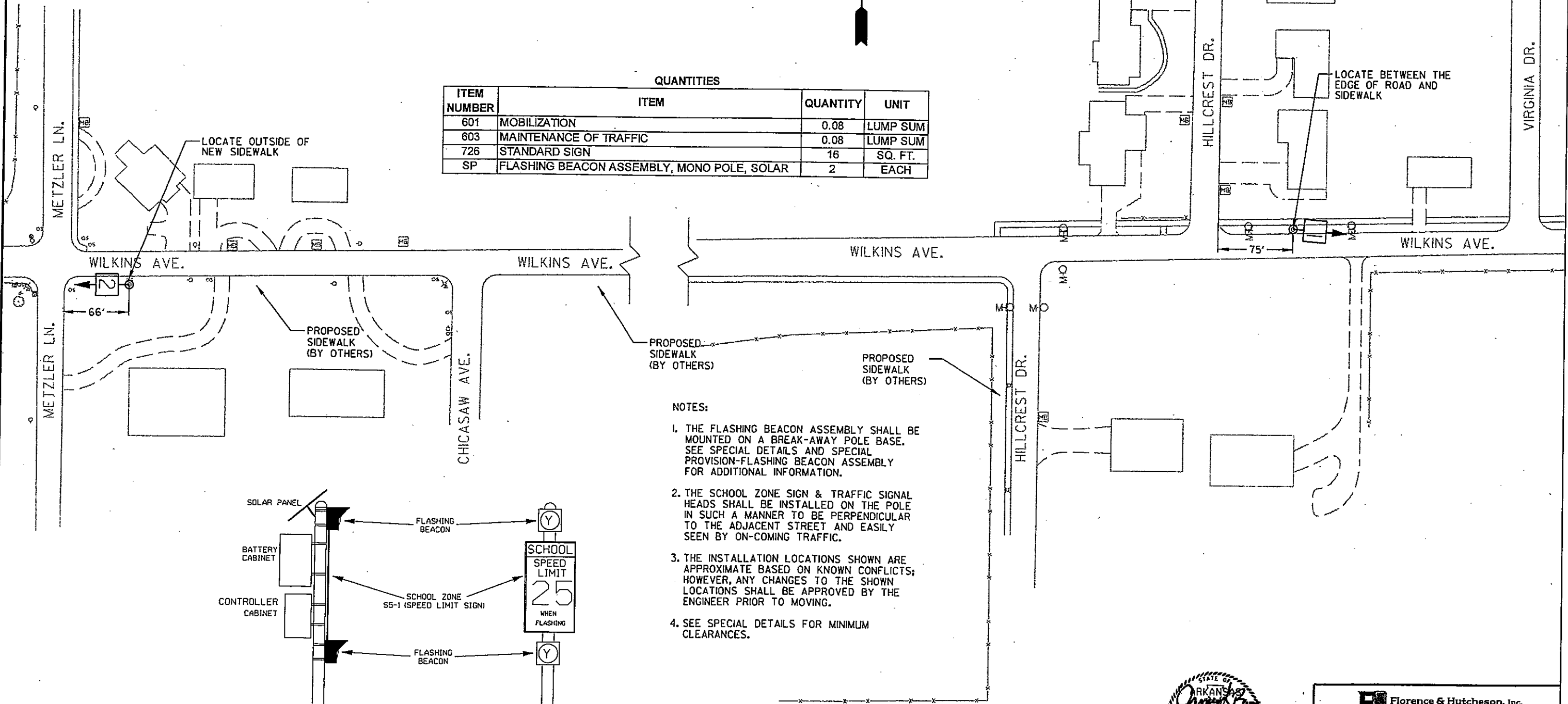
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LEGEND

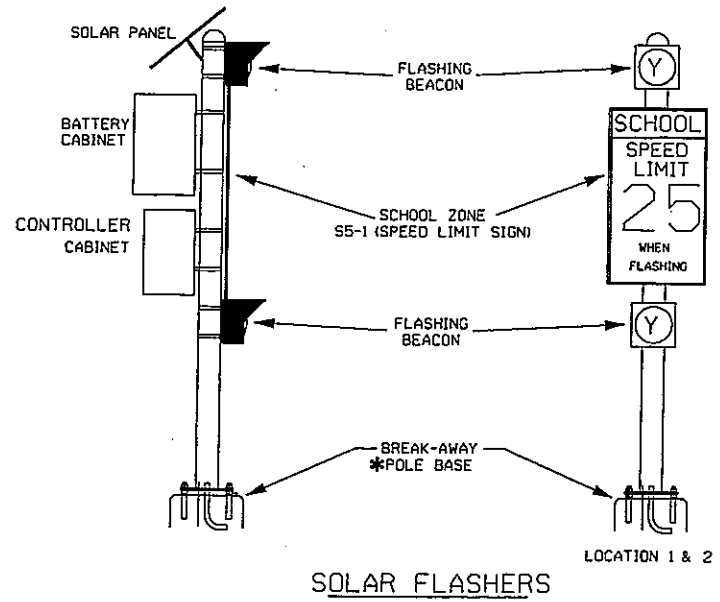
Ⓢ # → SCHOOL ZONE FLASHING BEACON



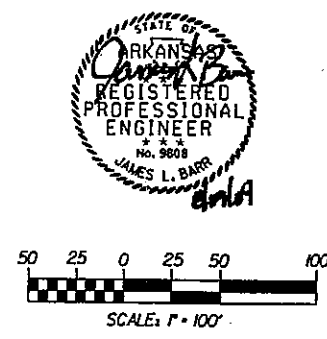
QUANTITIES			
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH



- NOTES:
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
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*DESIGN SHALL MEET NCHRP 350 STANDARDS



Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

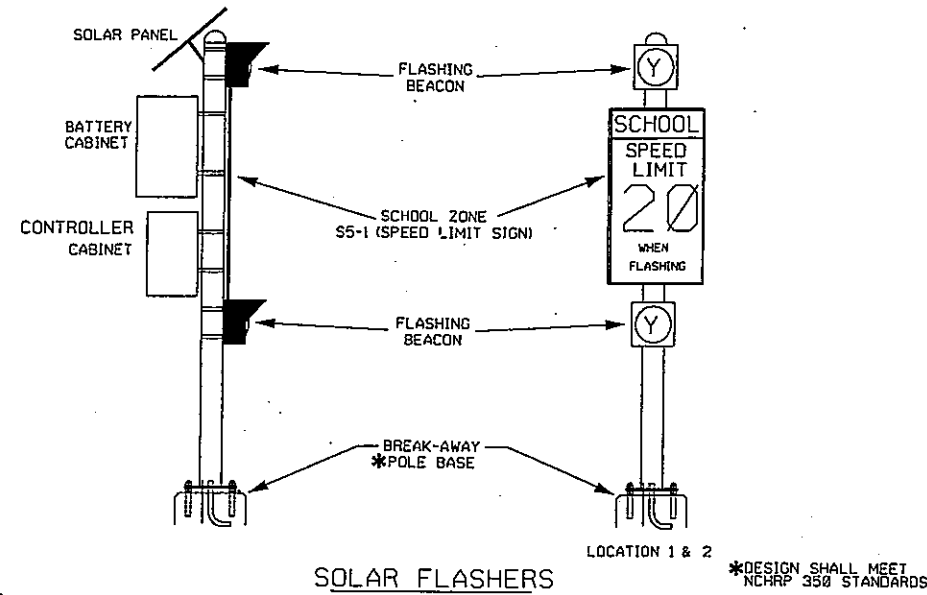
JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #6 MACARTHUR JR. HIGH (2009)

Drawn: AT	Checked: JLB	Mech.: []	Elec.: []	Project No. AHTD 100681 F&H 09089	9 of 30
Civil: []	Projects: []	Appr.: []	Dates: 08-11-09	Scale: 1" = 100'	

QUANTITIES			
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	24	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	3	EACH

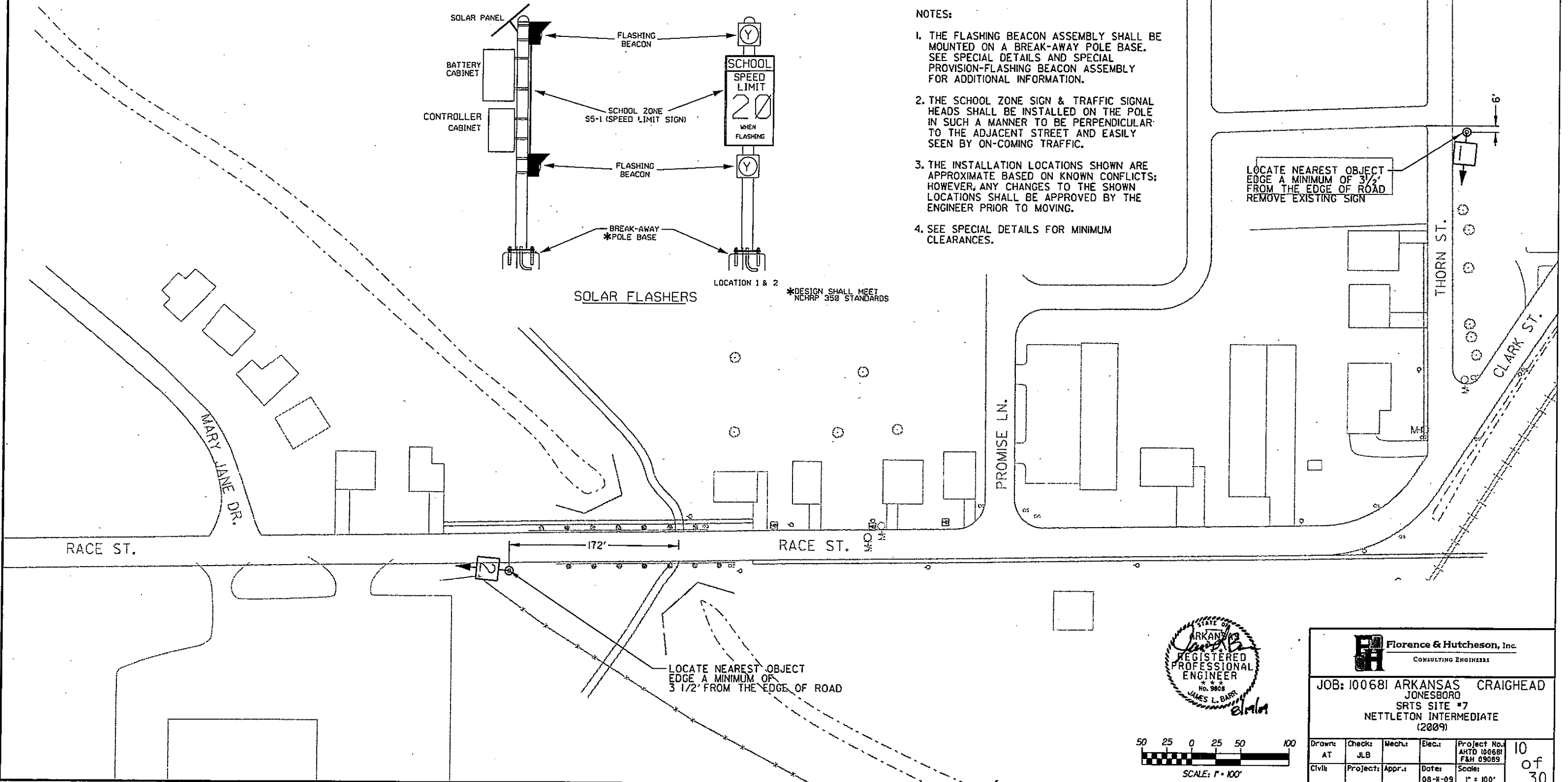
LEGEND

☉ # → SCHOOL ZONE FLASHING BEACON

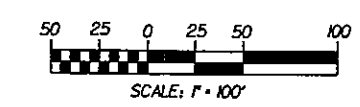


NOTES:

1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

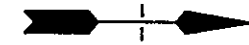


Florence & Hutcheson, Inc. CONSULTING ENGINEERS					
JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #7 NETTLETON INTERMEDIATE (2009)					
Drawn: AT	Check: JLB	Mech.: Appr.:	Elec.: Date: 08-11-09	Project No. AHTD 100681 F&H 09089 Scale: 1" = 100'	10 of 30

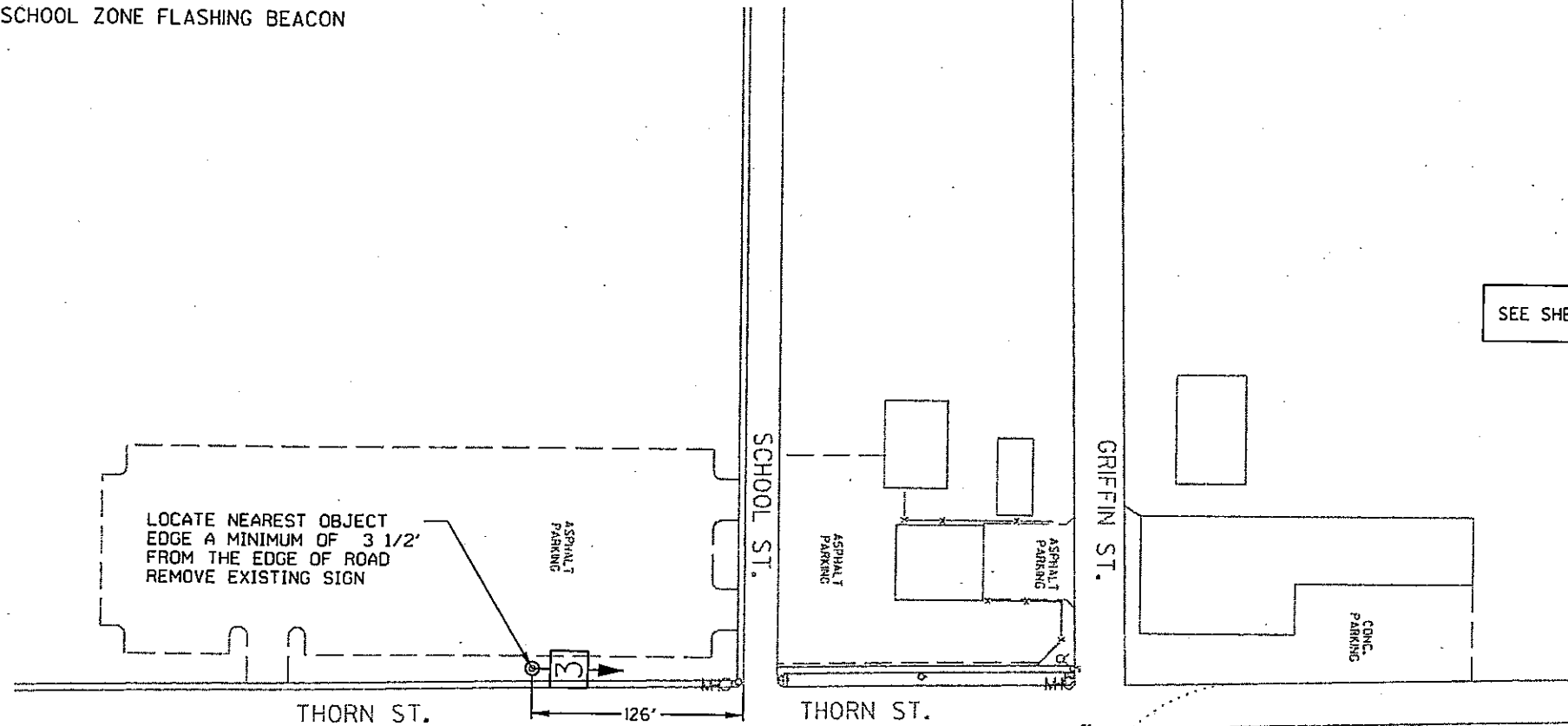


LEGEND

⊕ # → SCHOOL ZONE FLASHING BEACON

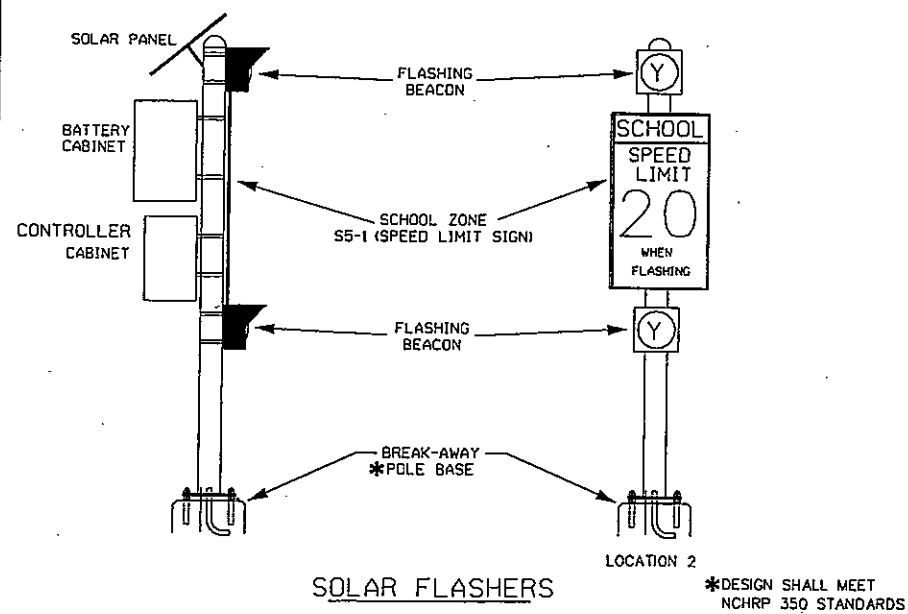


SEE SHEET #10 OF 30 FOR QUANTITIES



NOTES:

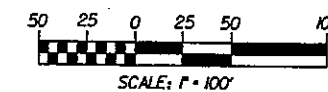
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.



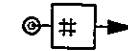
Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO
SRTS SITE #7
NETTLETON INTERMEDIATE (2009)

Drawn: AT	Check: J.L.B.	Mech: Appr.:	Elec: Dates	Project No. AHTD 100681 F&H 09089	Scale: 1" = 100'
					11 of 30



LEGEND



SCHOOL ZONE FLASHING BEACON



NOTES:

1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

LOCATE NEAREST OBJECT
EDGE A MINIMUM OF 3 1/2'
FROM THE EDGE OF ROAD
REMOVE EXISTING SIGN

DUDLEY ST.

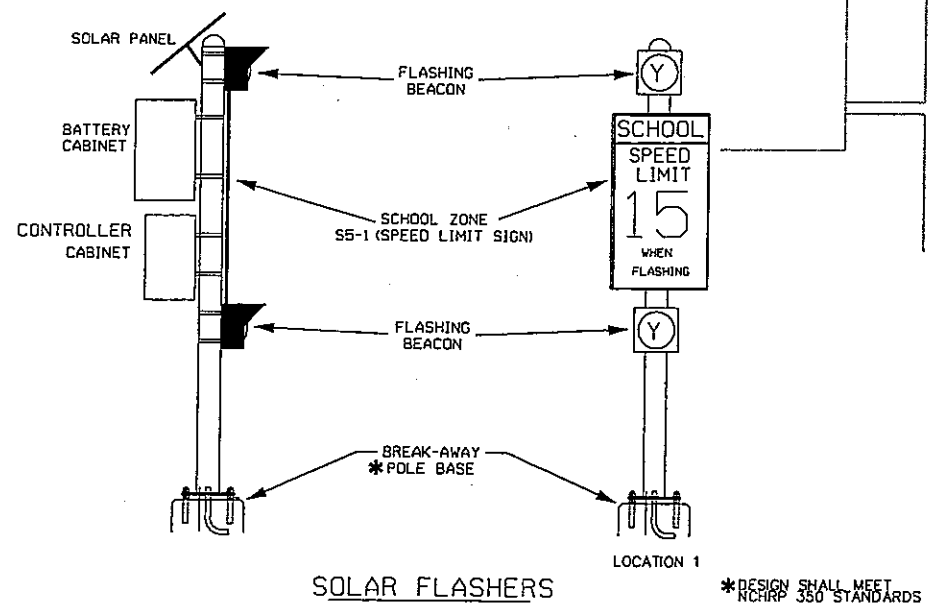
RACE ST.

23'

DONALD ST.

QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	52	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	3	EACH
SS&718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (12")	80	LIN. FT.
SS&718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (WORDS)	2	EACH
SP&729	CHANNEL POST SIGN SUPPORT (TYPE U-2)	2	EACH

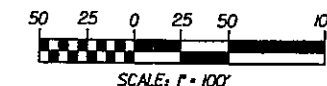


Florence & Hutcherson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #8
NETTLETON JR. HIGH
(2009)

Drawn: AT	Checked: JLB	Mech.:	Elec.:	Project No.:	12 of 30
Civil:	Projects:	Appr.:	Date:	Scale:	

Scale: 1" = 100'



LEGEND

⊙ # → CROSSWALK FLASHING BEACON

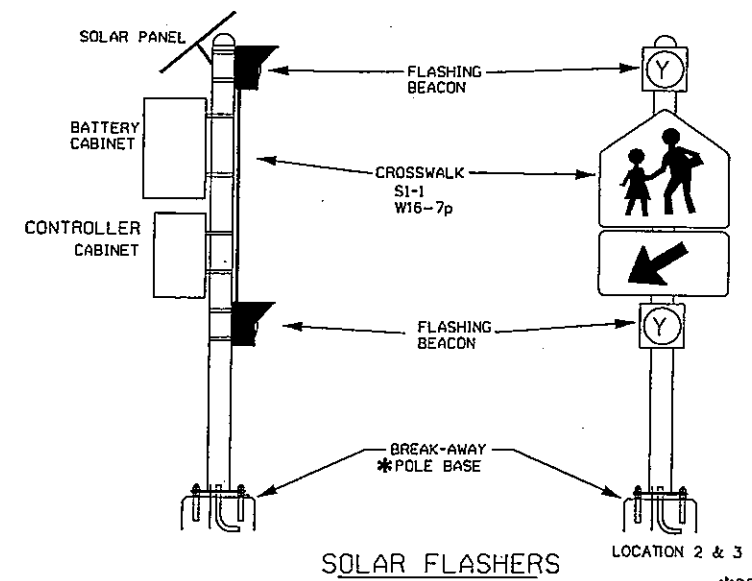
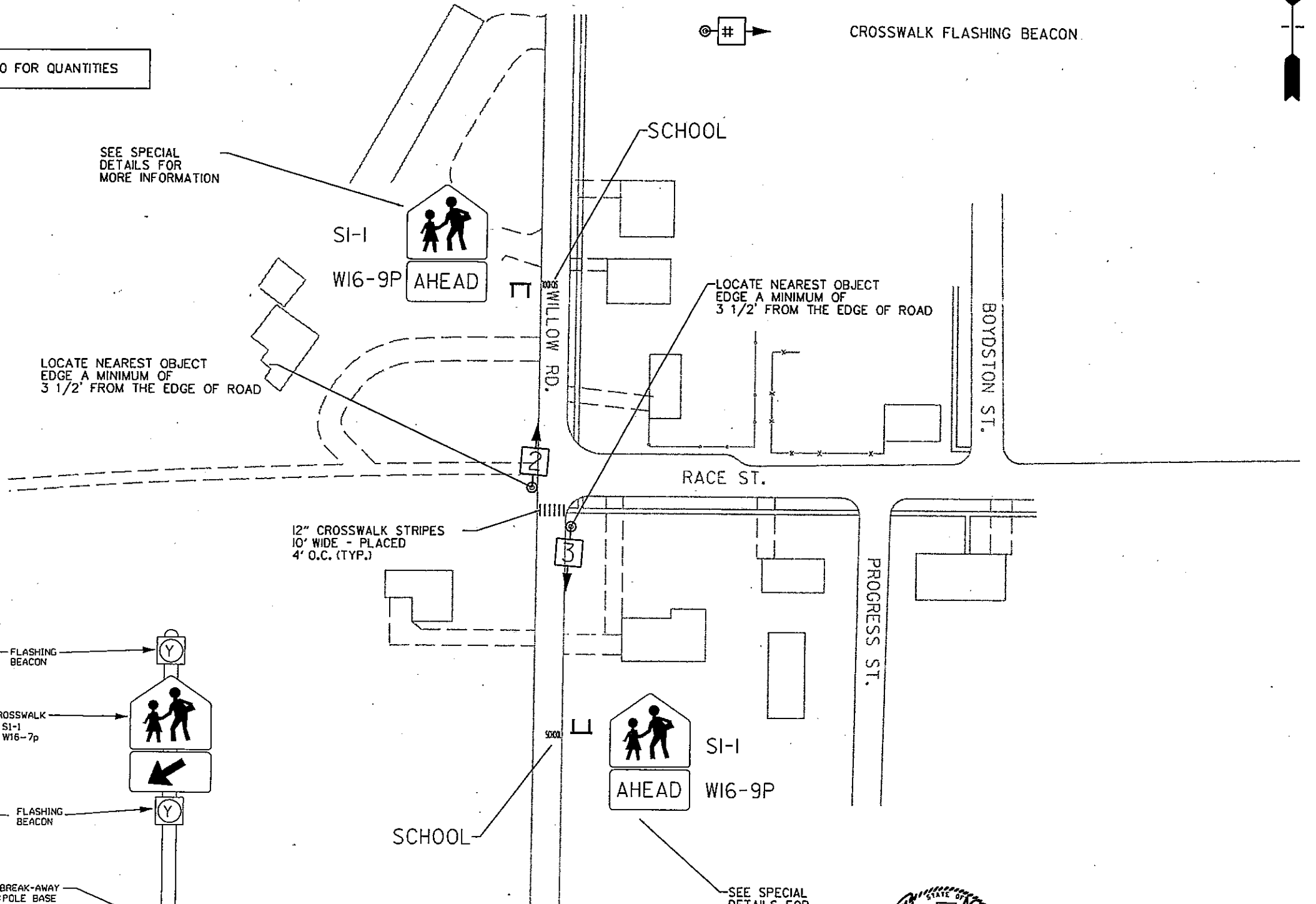


SEE SHEET #12 OF 30 FOR QUANTITIES

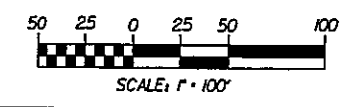
SEE SPECIAL DETAILS FOR MORE INFORMATION

NOTES:

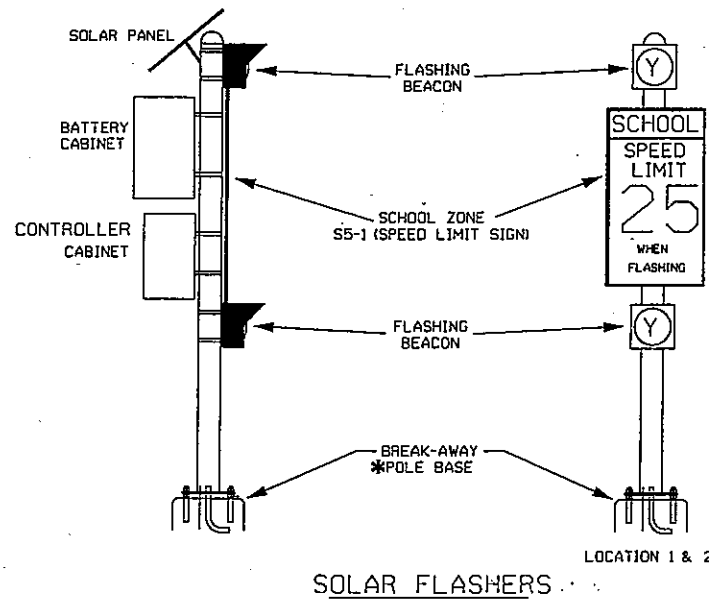
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.



*DESIGN SHALL MEET NCHRP 350 STANDARDS



Florence & Hutcheson, Inc. CONSULTING ENGINEERS		JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #8 NETTLETON JR. HIGH (2009)
Drawn: AT Checked: JLB Project:	Mech: [blank] Elec: [blank] Date: 08-11-09 Scale: 1" = 100'	



NOTES:

1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD.



SOLAR FLASHERS

*DESIGN SHALL MEET NCHRP 350 STANDARDS

E. THOMAS GREEN RD.

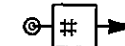
N. CHURCH ST./ST. HWY. 141

DANLEE DR.

MATH AND SCIENCE SCHOOL

LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD. REMOVE EXISTING SIGN.

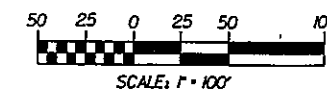
LEGEND



SCHOOL ZONE FLASHING BEACON

QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH

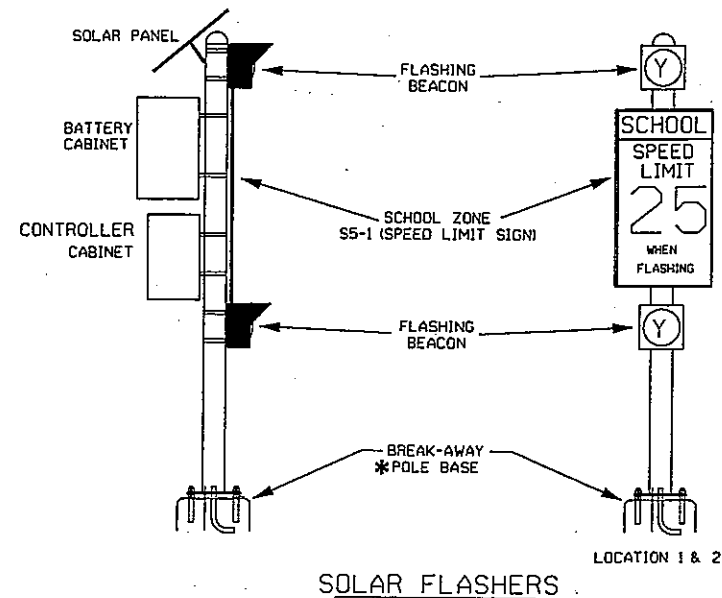


FH Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #9
MATH AND SCIENCE
(2009)

Drawn: AT	Check: JLB	Mech: JLB	Elec: JLB	Project No. AHTD 100681 F&H 09089
Civil:	Project:	Appr.:	Date: 08-11-09	Scale: 1" = 100'

14
of
30

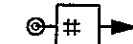


*DESIGN SHALL MEET NCHRP 350 STANDARDS

NOTES:

1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

LEGEND



SCHOOL ZONE FLASHING BEACON

LOCATE NEAREST OBJECT EDGE:
A MINIMUM OF 3 1/2' FROM THE
EDGE OF ROAD. REMOVE
EXISTING SIGN.

LOCATE NEAREST OBJECT EDGE
A MINIMUM OF 3 1/2' FROM THE
EDGE OF ROAD.

STRAWFLOOR DR.

CASEY SPRINGS ROAD

NEW ROAD

VALLEY VIEW DR.

QUANTITIES

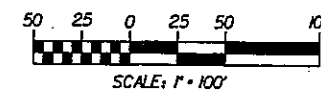
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH



F Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #10
RIDGEFIELD CHRISTIAN
(2009)

Drawn: AT	Check: JLB	Mech.:	Elec.:	Project No.:	15 of 30
Civil:	Project:	Appr.:	Date:	AHTD 100681 F&H 09089 Scale: 08-11-09 1" = 100'	



NOTES:

1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

LEGEND



SCHOOL ZONE FLASHING BEACON

QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH

LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD. REMOVE EXISTING SIGN.

RICHMOND AVE.

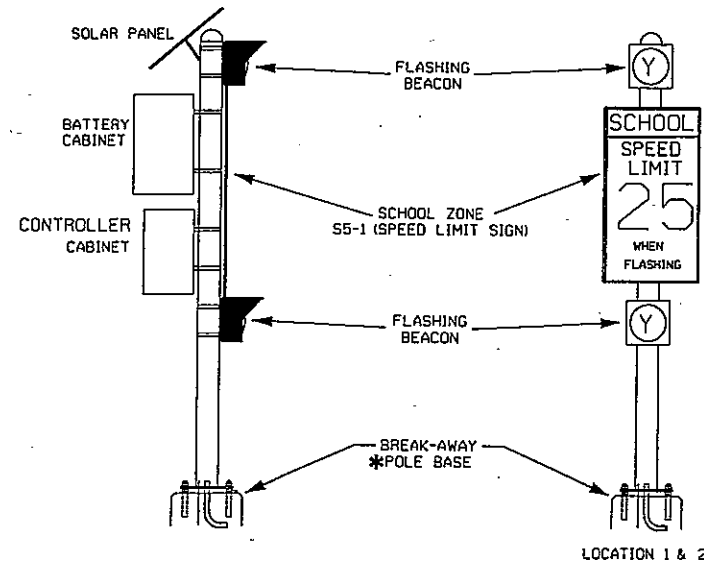
E. OAK ST.

RAINS ST.

RAINS ST.

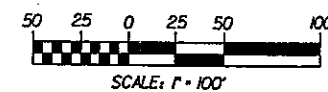
E. OAK ST.

LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD.



SOLAR FLASHERS

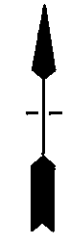
*DESIGN SHALL MEET NCHRP 350 STANDARDS



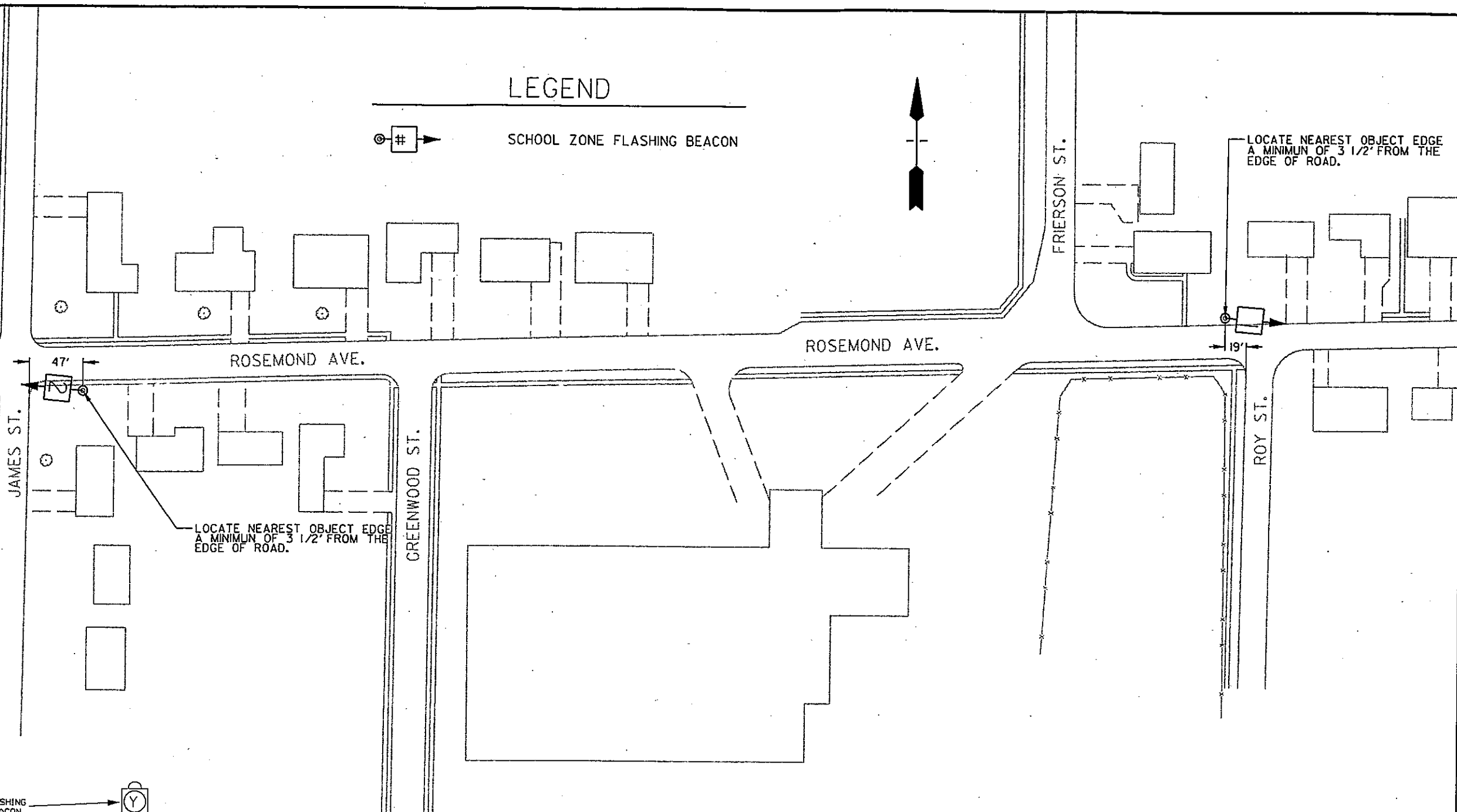
F Florence & Hutcheson, Inc. CONSULTING ENGINEERS				
JOB: 100681 ARKANSAS CRAIGHEAD JONESBORO SRTS SITE #11 INTERNATIONAL STUDIES (2009)				
Drawn: AT	Checks: JLB	Mech.:	Elec.:	Project No. AHTD 100681 F&H 09089
Civil:	Project:	Appr.:	Date: 08-11-09	Scale: 1" = 100'
				16 of 30

LEGEND

⊕ # → SCHOOL ZONE FLASHING BEACON

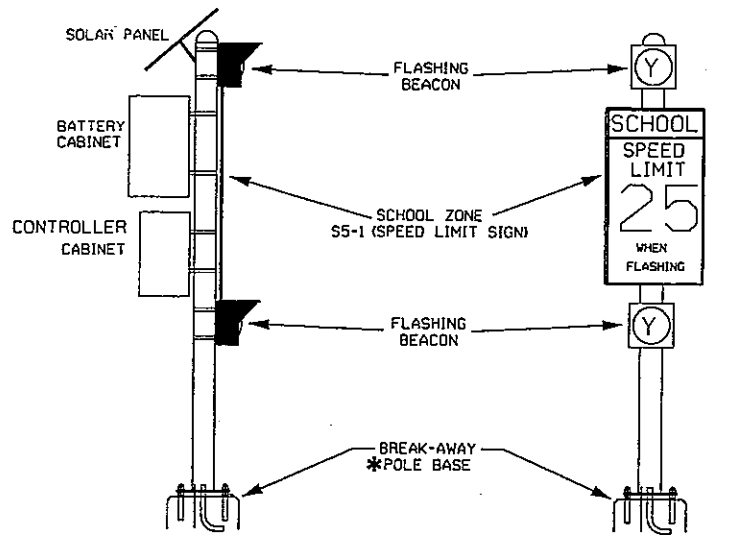


LOCATE NEAREST OBJECT EDGE
A MINIMUM OF 3 1/2' FROM THE
EDGE OF ROAD.



- NOTES:**
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
 2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
 3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
 4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

LOCATE NEAREST OBJECT EDGE
A MINIMUM OF 3 1/2' FROM THE
EDGE OF ROAD.



SOLAR FLASHERS

*DESIGN SHALL MEET
NCHRP 350 STANDARDS

QUANTITIES			
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH

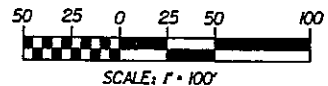


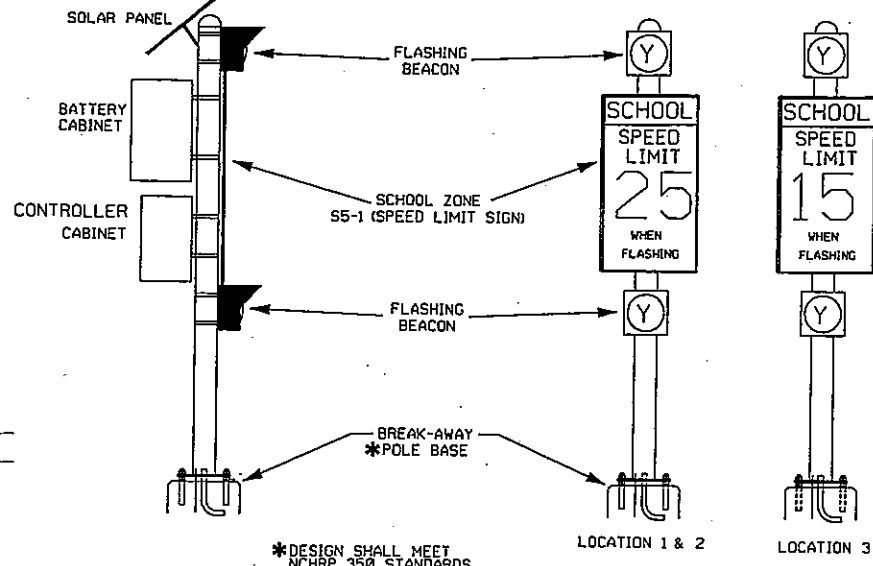
Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #12
HEALTH AND WELLNESS
(2009)

Drawn: AT	Check: JLB	Mech: Appr.:	Elec: Date: 08-11-09	Project No.: AHTD 100681 F&H 09089	17 of 30
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SCALE: 1" = 100'





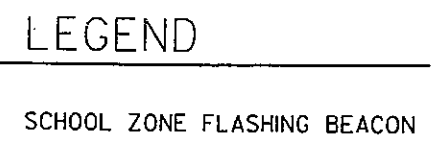
- NOTES:
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
 2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
 3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
 4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.



LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD.

LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD.

LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD. REMOVE EXISTING SIGN.



QUANTITIES

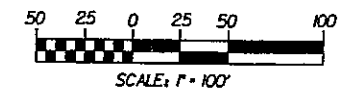
ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	24	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	3	EACH

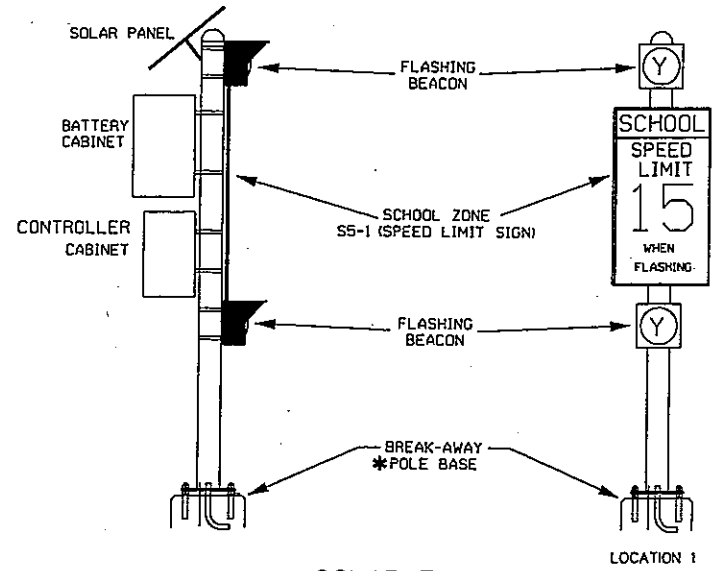


Florence & Hutchason, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #13
UNIVERSITY HEIGHTS
(2009)

Drawn: AT	Checks: JLB	Mechs:	Elec.:	Project No.:	18
Civil:	Project:	Appr.:	Date:	AHTD 100681 F&H 09089	of
			08-11-09	Scale:	30
				1" = 100'	





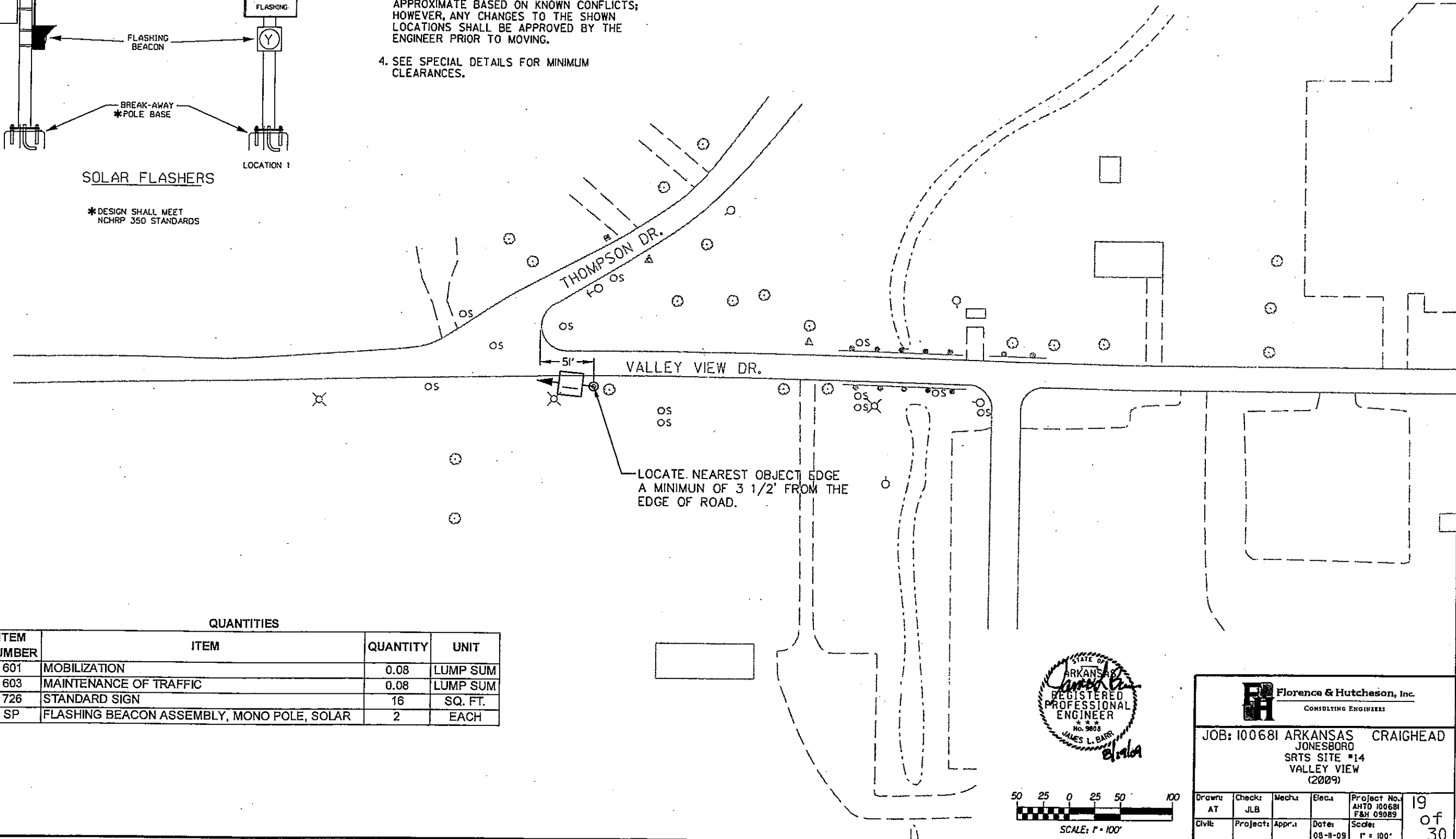
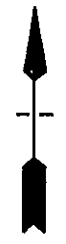
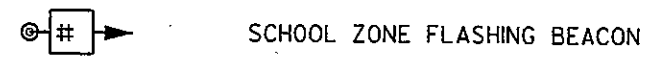
SOLAR FLASHERS

*DESIGN SHALL MEET NCHRP 350 STANDARDS

NOTES:

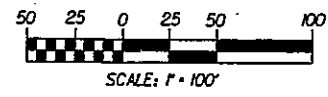
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
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4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.

LEGEND



QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	0.08	LUMP SUM
603	MAINTENANCE OF TRAFFIC	0.08	LUMP SUM
726	STANDARD SIGN	16	SQ. FT.
SP	FLASHING BEACON ASSEMBLY, MONO POLE, SOLAR	2	EACH



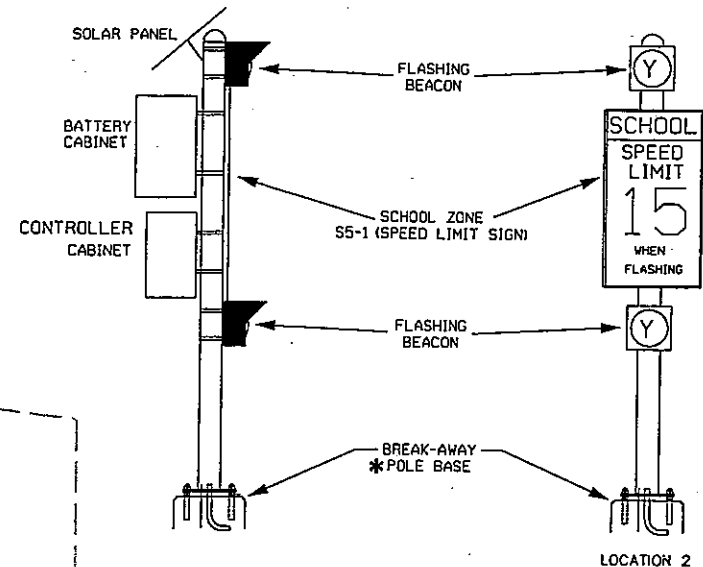
Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #14
VALLEY VIEW
(2009)

Drawn: AT	Check: JLB	Mech: 	Elec: 	Project No. AHTD 100681 F&H 09089	19 of 30
Civil:	Project:	Appr.:	Date: 08-11-09	Scale: 1" = 100'	

LEGEND

⊕ # → SCHOOL ZONE FLASHING BEACON

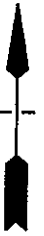


SOLAR FLASHERS

*DESIGN SHALL MEET NCHRP 350 STANDARDS

NOTES:

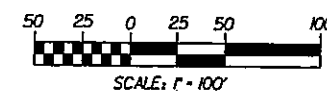
1. THE FLASHING BEACON ASSEMBLY SHALL BE MOUNTED ON A BREAK-AWAY POLE BASE. SEE SPECIAL DETAILS AND SPECIAL PROVISION-FLASHING BEACON ASSEMBLY FOR ADDITIONAL INFORMATION.
2. THE SCHOOL ZONE SIGN & TRAFFIC SIGNAL HEADS SHALL BE INSTALLED ON THE POLE IN SUCH A MANNER TO BE PERPENDICULAR TO THE ADJACENT STREET AND EASILY SEEN BY ON-COMING TRAFFIC.
3. THE INSTALLATION LOCATIONS SHOWN ARE APPROXIMATE BASED ON KNOWN CONFLICTS; HOWEVER, ANY CHANGES TO THE SHOWN LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO MOVING.
4. SEE SPECIAL DETAILS FOR MINIMUM CLEARANCES.



LOCATE NEAREST OBJECT EDGE A MINIMUM OF 3 1/2' FROM THE EDGE OF ROAD.

210'

SEE SHEET #19 OF 30 FOR QUANTITIES



F Florence & Hutcheson, Inc.
CONSULTING ENGINEERS

JOB: 100681 ARKANSAS CRAIGHEAD
JONESBORO
SRTS SITE #14
VALLEY VIEW
(2009)

Drawn: AT	Checked: JLB	Mechs:	Elec.:	Project No.:	20 of 30
Civil:	Project:	Appr.:	Dates:	100681 F&H 09089	
				Scale:	
				08-11-09	1" = 100'

NOTES:

- All construction shall conform to Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 2003, Division 700, Traffic Control Facilities.
- The flashing beacon assembly shall include lightning suppressors, Traffic Signal Cable, two 12" Traffic signal Heads LED (1Sec., 1Way) with yellow lenses, Flashing Beacon Controller and a solid state calendar date time clock with daylight savings time programming and 48 hour power fail protection. Date time clock required only for school zones. Signal heads shall be wired to flash alternately.
- Any fittings, bands, ground rod, fusion weld, or accessories necessary to mount Flashing Beacon Controller shall be considered subsidiary to the Item for Flashing Beacon Controller.
- All couplings, tees, clamps and hardware necessary to mount signal heads shall be considered subsidiary to the Item Flashing Beacon Assembly, Mono Pole, Solar per the Special Provision - Flashing Beacon Assembly. Contractor shall remove signs and re-install them after flashing beacons are installed.
- The flashing beacon controller and solar panel should be located within the highway right away, but as far away from the vehicle travelway as possible to avoid vehicle impacts.

TYPICAL FOUNDATION DETAILS

POLE FOUNDATION MINIMUM DIMENSIONS AND STEEL REINFORCING. ALL REINFORCING STEEL SHALL BE GRADE 40 MIN.

ARM LENGTH	FDN. DIAMETER	DEPTH 'L' *	STEEL		
			VERT.	HORZ.	O/C.
PED	30"	7'-0"	12-#7 (6'-6")	10-#4	8.44'

DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION (2001) WITH 2003 AND 2006 INTERIMS.

DESIGN SHALL MEET NCHRP. 350 STANDARDS.

USE FATIGUE CATEGORY II.

CONSTRUCTION SPECIFICATIONS: ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (2003 EDITION) WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

BASE WIND SPEED: 90 MPH

STEEL MEMBERS CONSIDERED MAIN LOAD CARRYING MEMBERS WITH A THICKNESS GREATER THAN 1/2" SHALL MEET THE LONGITUDINAL CHARPY V-NOTCH TEST SPECIFIED IN SUBSECTION 807.05 OF THE STANDARD SPECIFICATIONS.

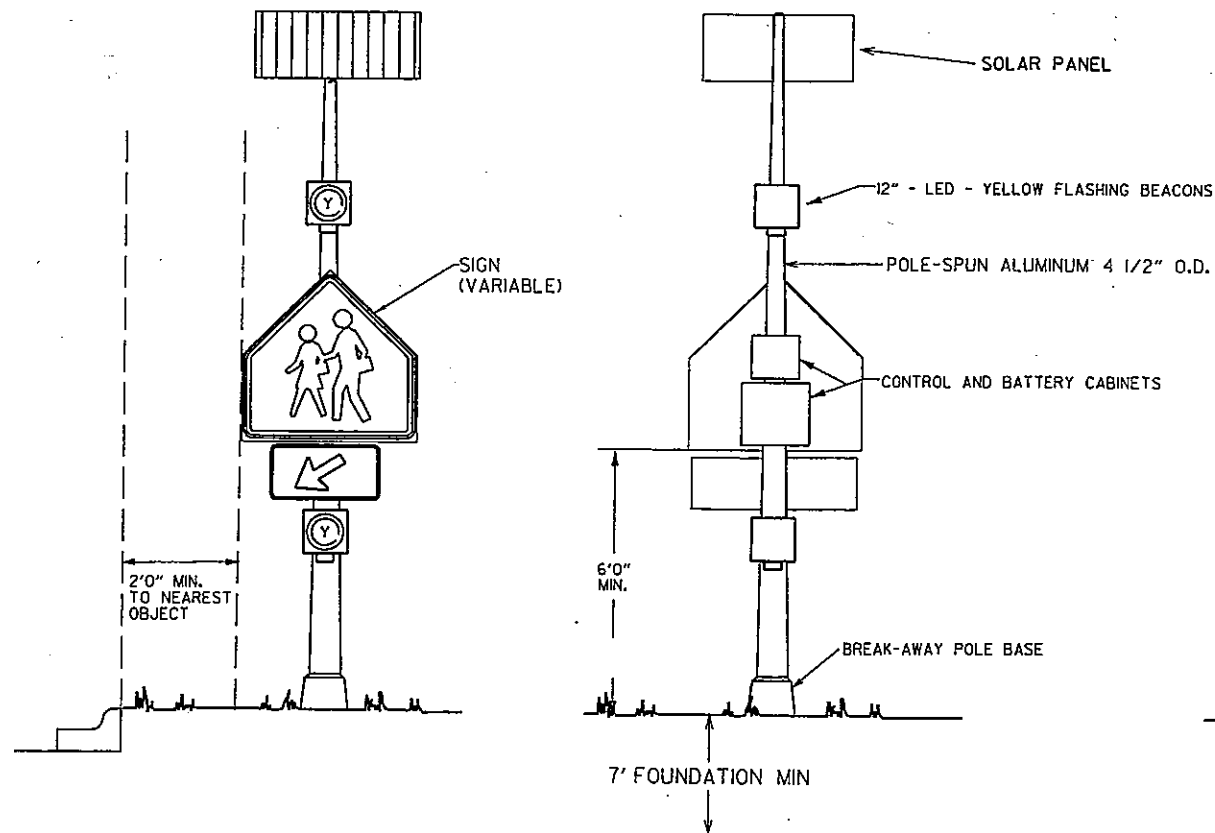
THE GROUND ROD SHALL BE FUSION WELDED TO A 1C/#8 A.W.G. SOLID COPPER GROUND WIRE. ATTACHMENT TO THE PRIMARY GROUND MAY BE BY AN APPROVED CLAMP.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	100681		21	30

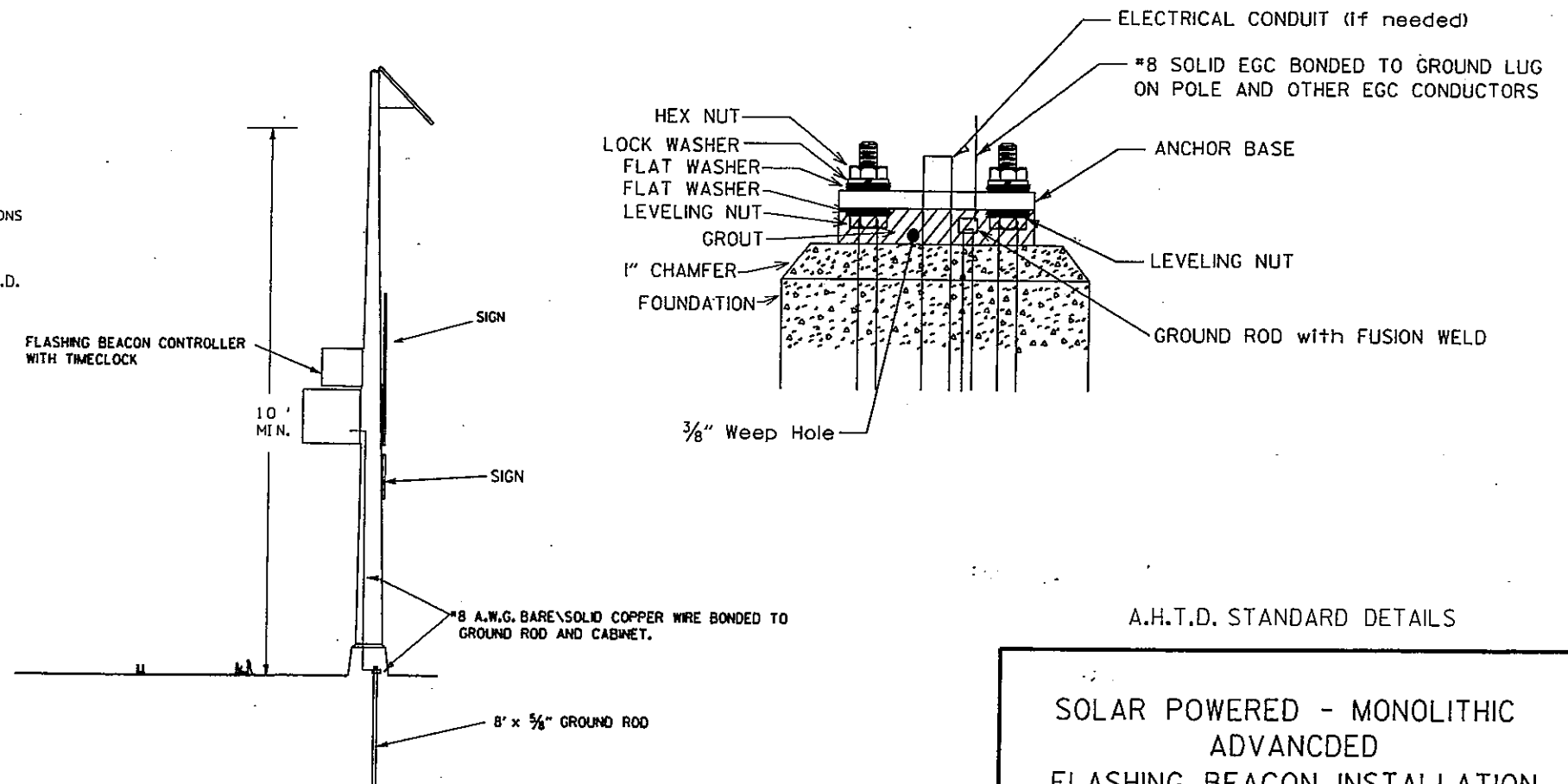
2 FLASHING BEACON DETAILS



DETAIL OF SIGN SUPPORT ASSEMBLY
TYPICAL INSTALLATION



ANCHOR BASE



A.H.T.D. STANDARD DETAILS

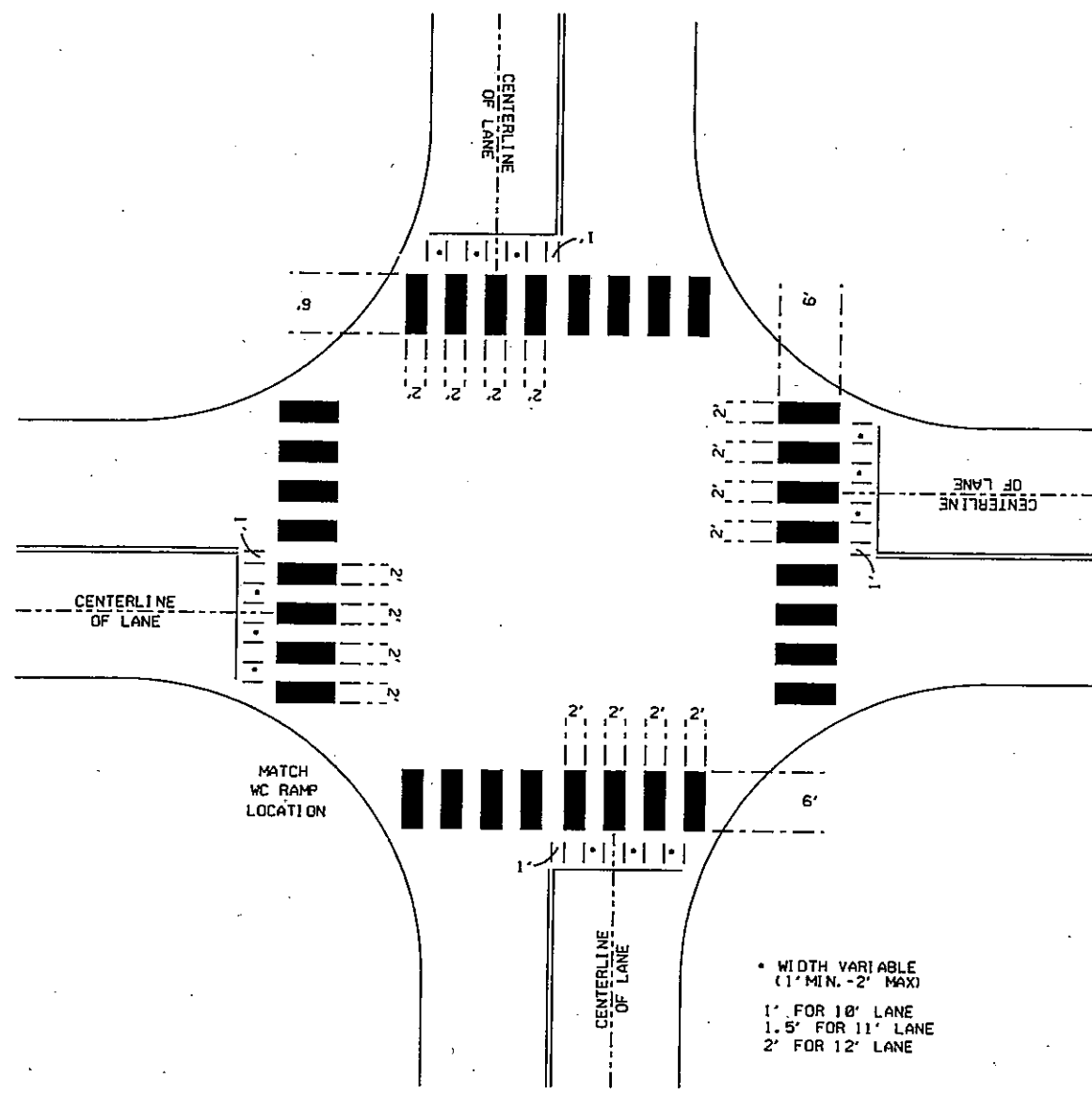
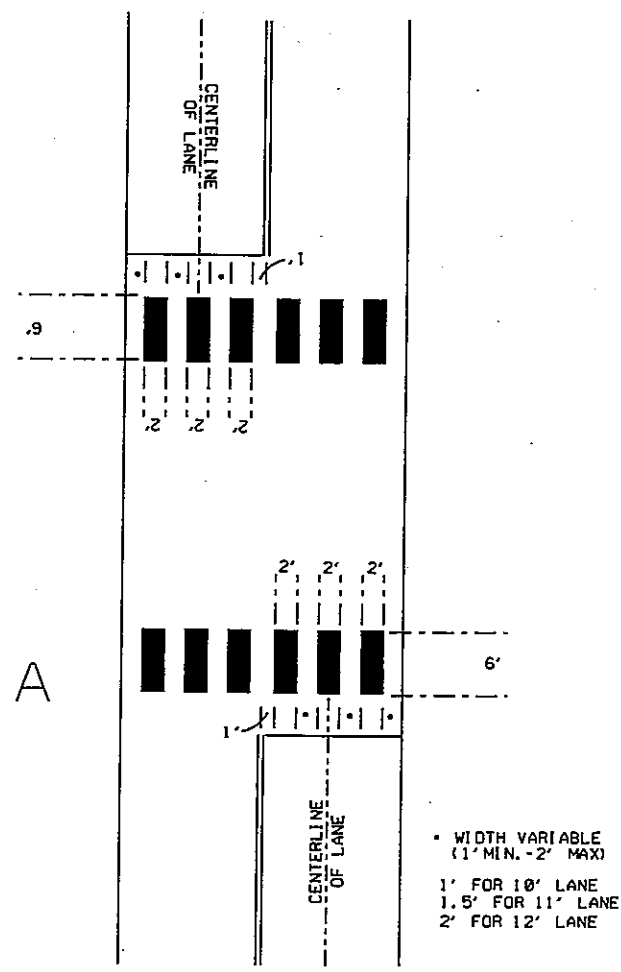
SOLAR POWERED - MONOLITHIC
ADVANCED
FLASHING BEACON INSTALLATION

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	100681		22	30

2 HIGH PERFORMANCE PAVEMENT MARKINGS (CROSSWALK IMPRINT)

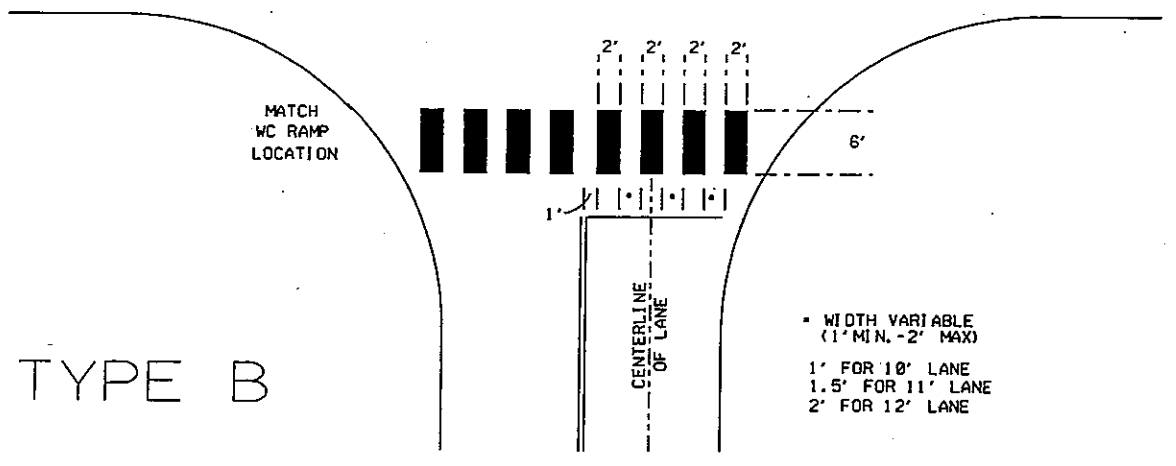


TYPE A



TYPE C

TYPE B

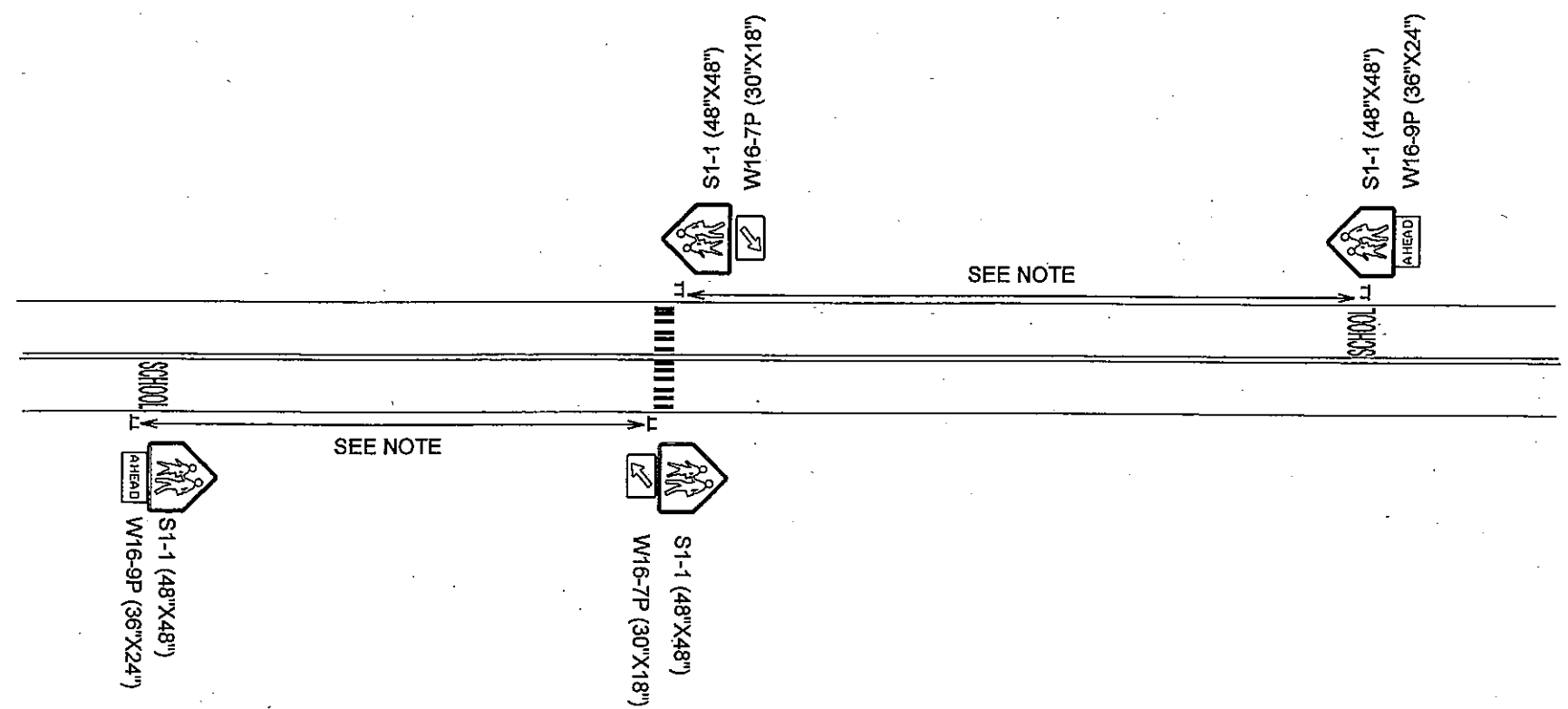


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				6	ARK.				
JOB NO.							100681	23	30

2 SCHOOL ZONE SIGNING WITHOUT SPEED REDUCTION

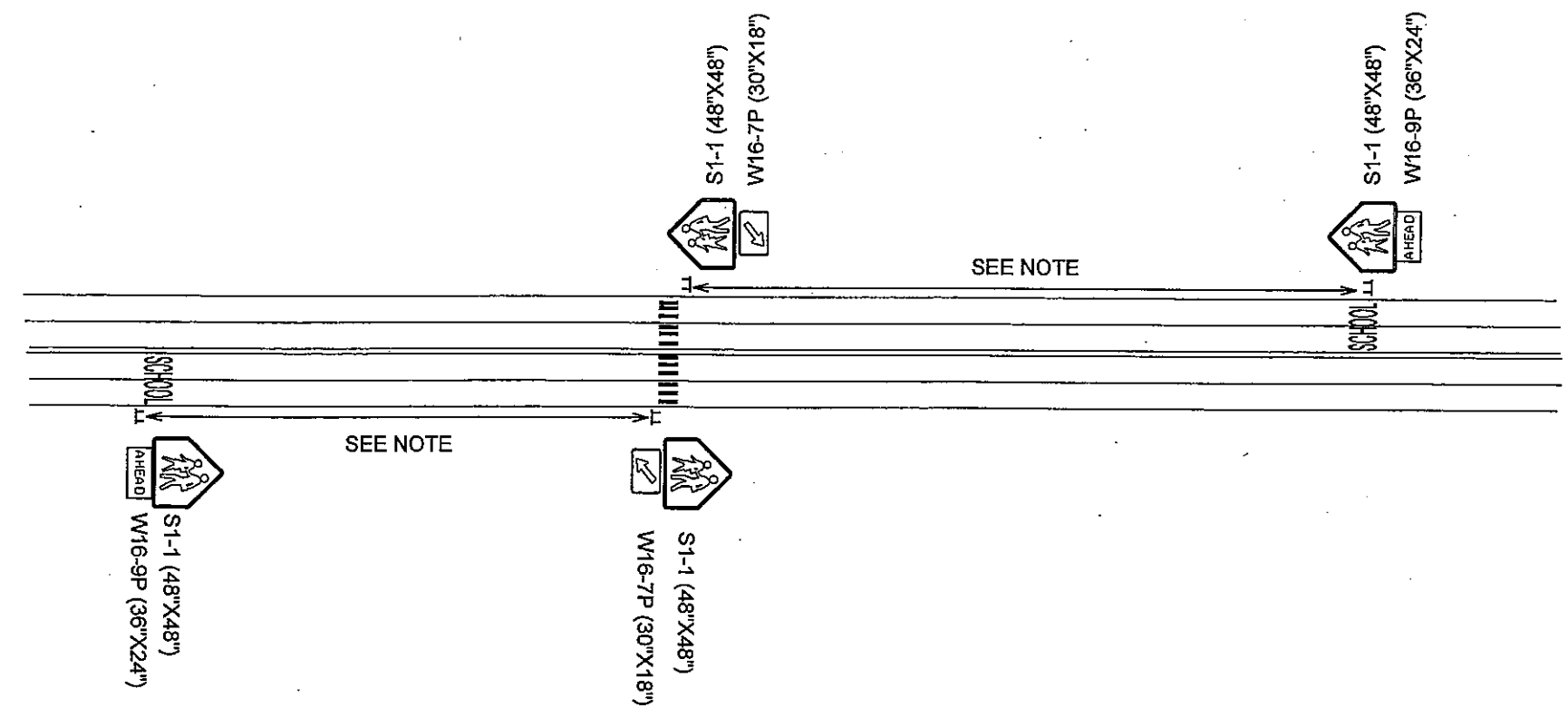


SINGLE LANE INSTALLATION



NOTE:
150 FEET MINIMUM
700 FEET MAXIMUM

TWO-LANE INSTALLATION



NOTE:
150 FEET MINIMUM
700 FEET MAXIMUM

A.H.T.D. STANDARD DETAILS

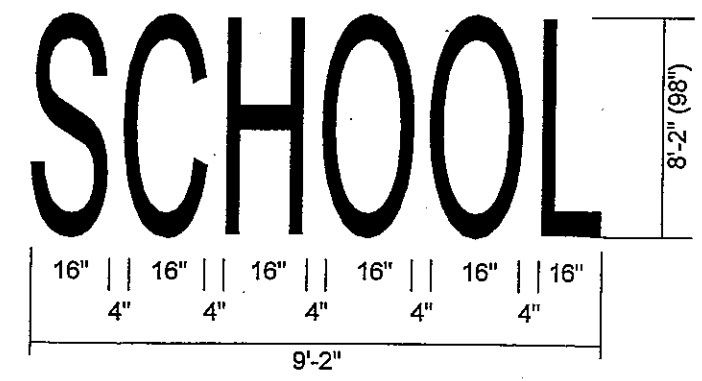
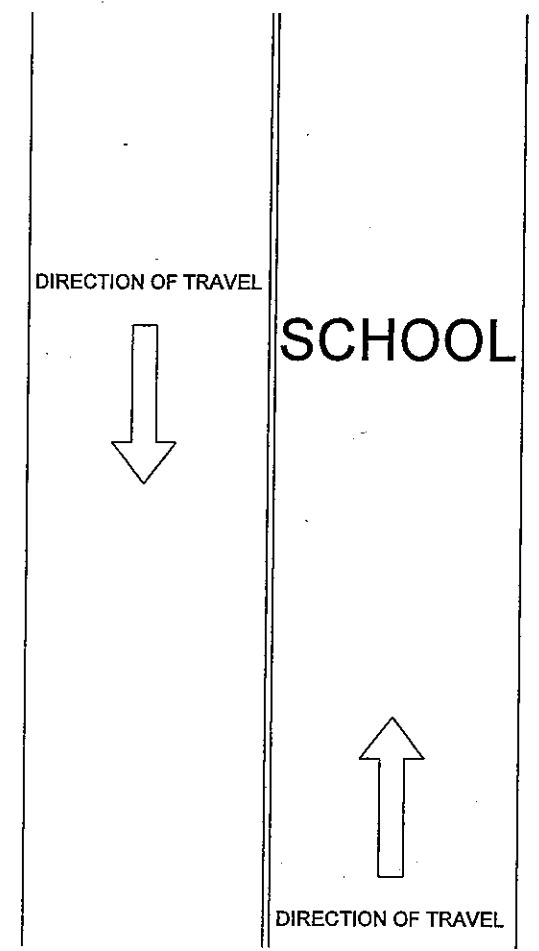
SCHOOL ZONE SIGNING
WITHOUT SPEED REDUCTION

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	100681	24	30	

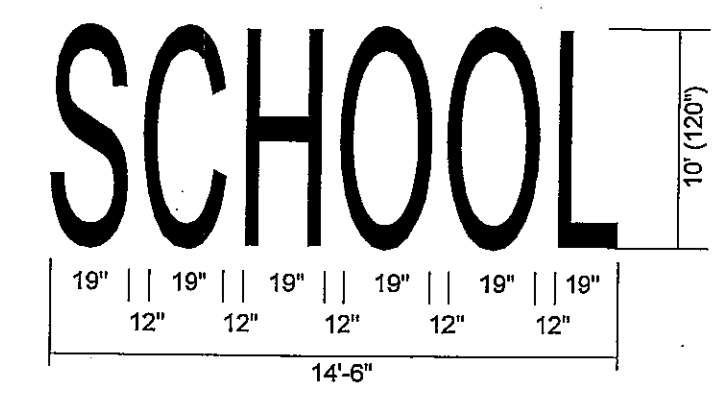
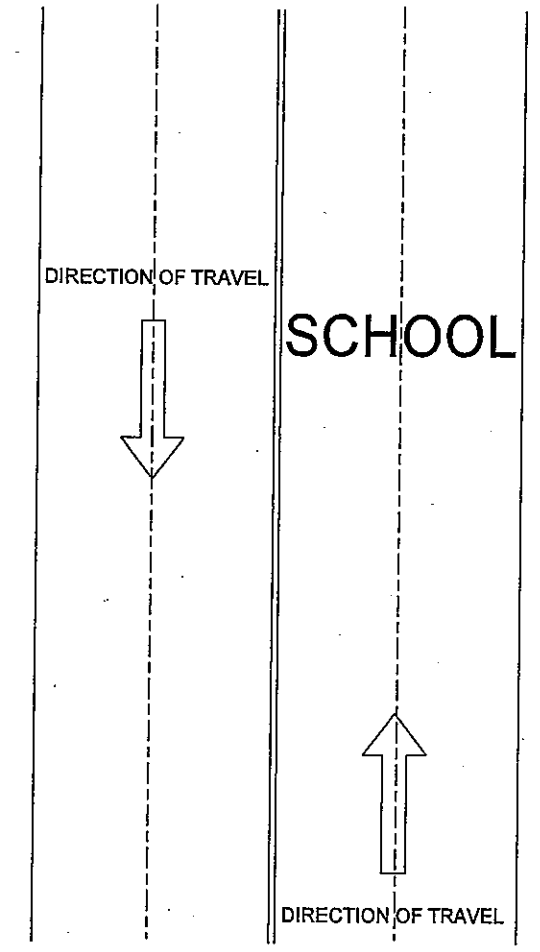
2 HIGH PERFORMANCE PAVEMENT MARKINGS (SCHOOL IMPRINT)



TYPE A
SINGLE LANE

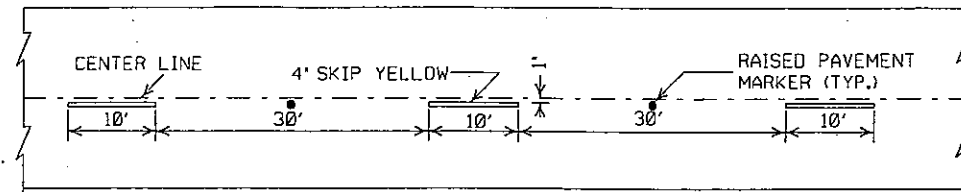


TYPE B
DOUBLE LANE

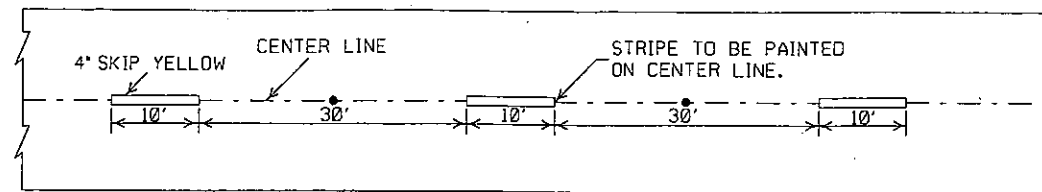


NOTES:

1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.

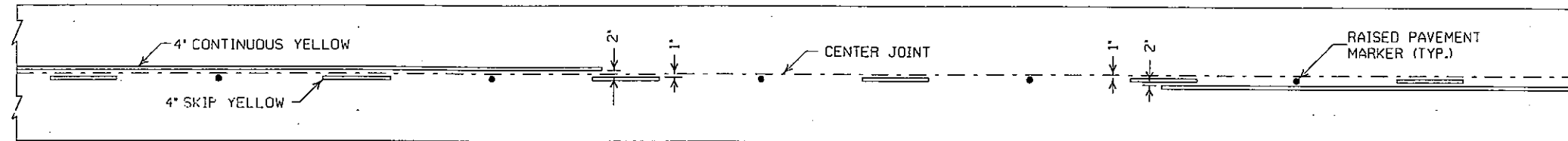


CONCRETE PAVEMENT

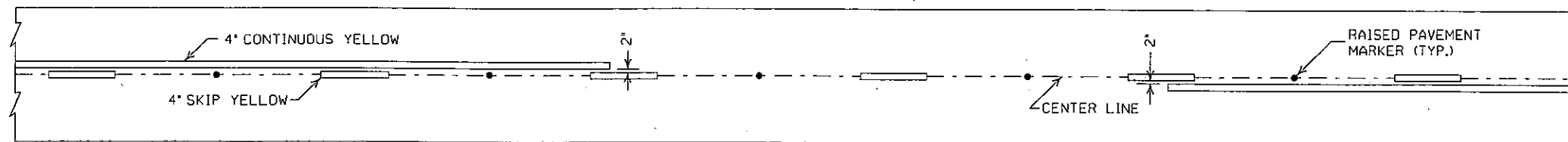


ASPHALT PAVEMENT

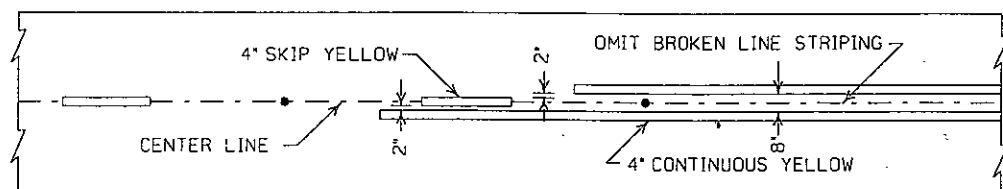
BROKEN LINE STRIPING



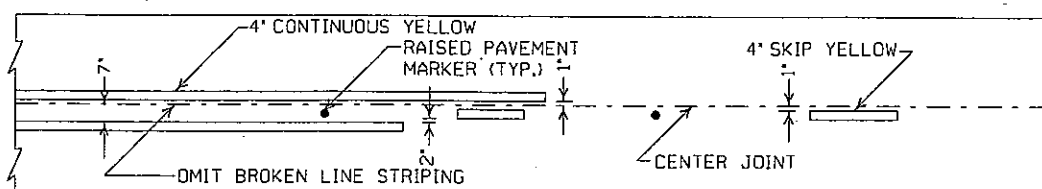
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

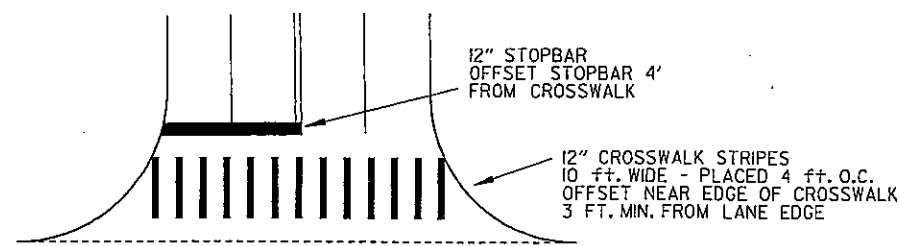


ASPHALT PAVEMENT



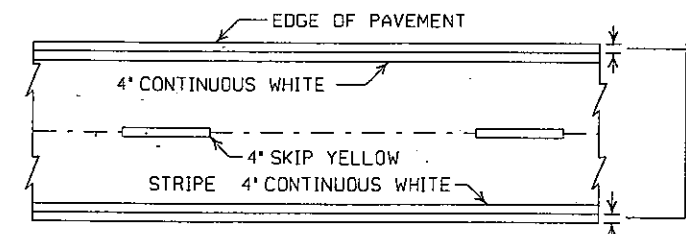
CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

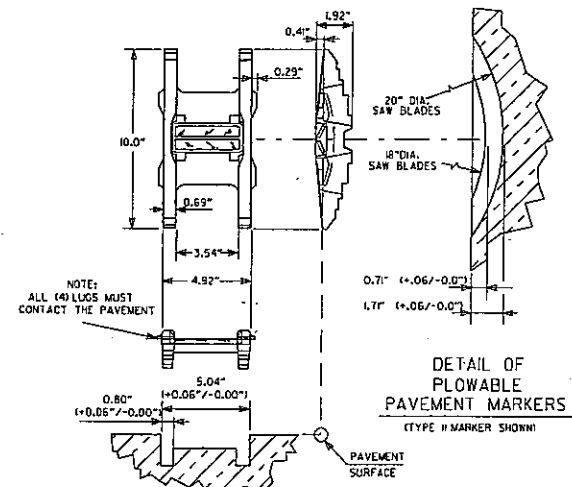


CROSSWALK AND STOPBAR DETAILS

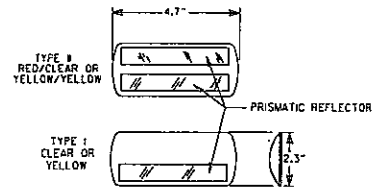
2" FOR ASPHALT OR CONCRETE PAVEMENT
6" FOR BITUMINOUS SURFACE TREATMENT



PAVEMENT EDGE LINE MARKING



DETAIL OF PLOWABLE PAVEMENT MARKERS (TYPE 3 MARKER SHOWN)



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE: THE RED LENS OF THE TYPE 3 R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR PLOWABLE PAVEMENT MARKERS AND RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.


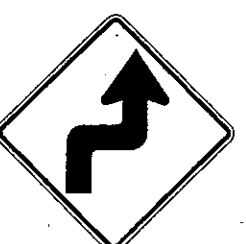
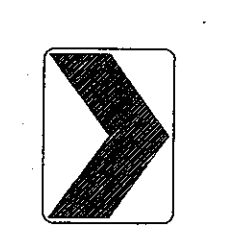

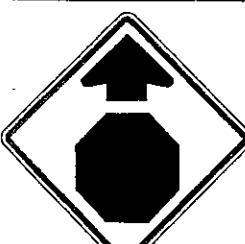
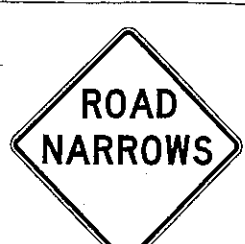
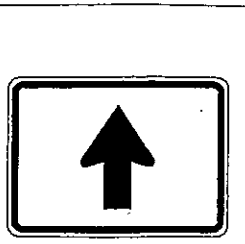
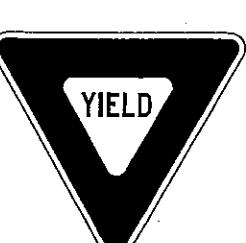
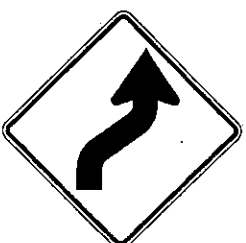
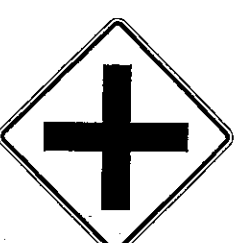


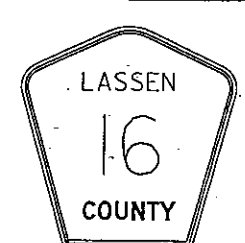
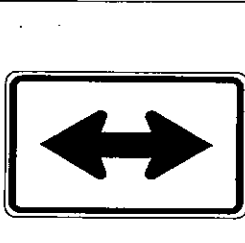
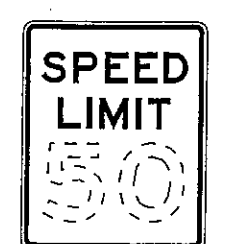



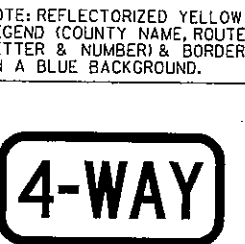
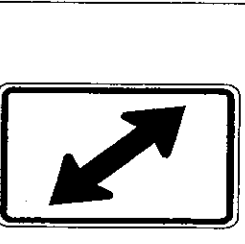
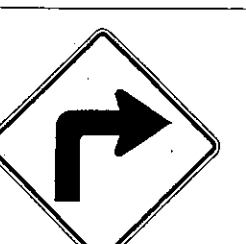
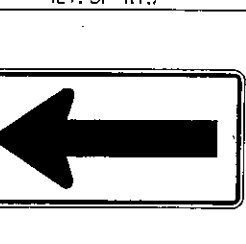
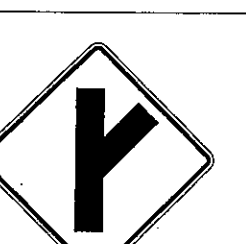

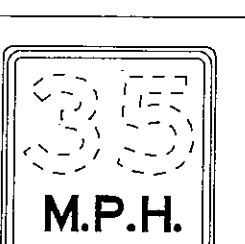
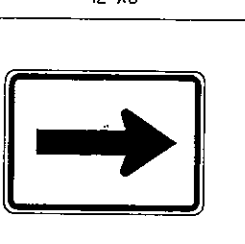
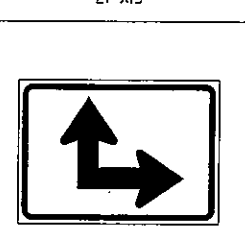
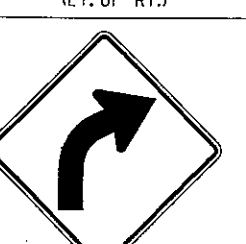
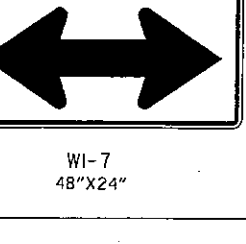
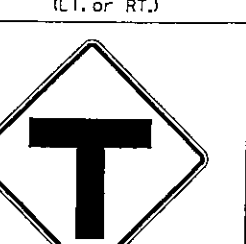

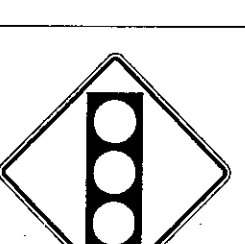
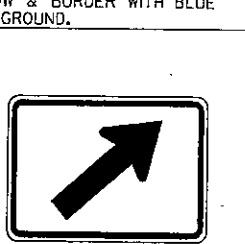
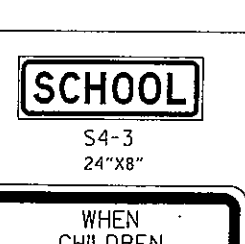

DATE	REVISION	FILMED
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTL.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	J-9-30-80

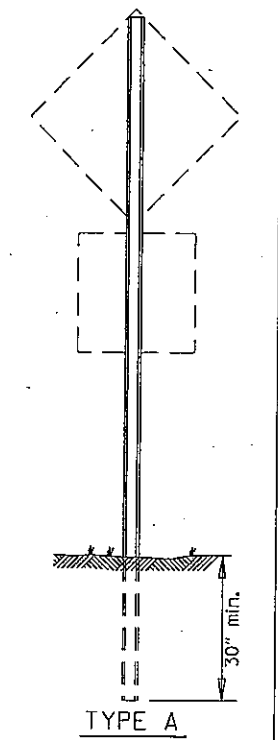
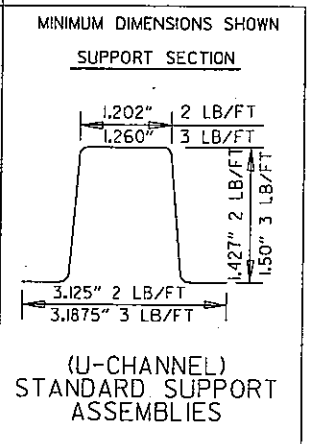
ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

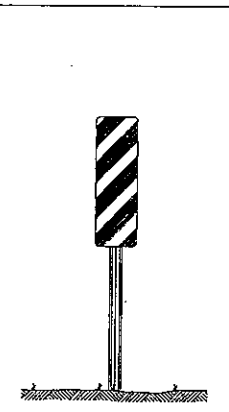
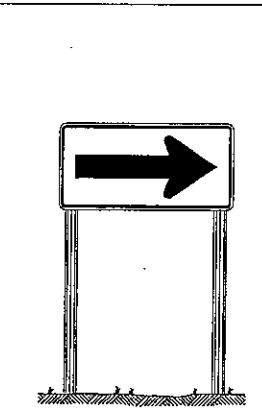
STANDARD DRAWING PM-1

FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
6	ARK.			
JOB NO.				
4 STD. HWY. SIGNS & SUP ASSEMB				

 RI-1 30"x30"	 WI-3 30"x30" (LT. OR RT.)	 WI-8 18"x24"	 W2-5 30"x30"	 W3-1 36"x36"	 W5-1 36"x36"	 M6-3 21"x15"
 RI-2 36"x36"x36"	 WI-4 30"x30" (LT. OR RT.)	 W2-1 30"x30"	 SI-1 36"x36"	 W3-2 36"x36"	 MI-5 24"x24" <small>NOTE: REFLECTORIZED YELLOW LEGEND (COUNTY NAME, ROUTE LETTER & NUMBER) & BORDER ON A BLUE BACKGROUND.</small>	 M6-4 21"x15"
 R2-1 24"x30"	 WI-5 30"x30" (LT. OR RT.)	 W2-2 30"x30"	 W5-2 36"x36"	 W8-3 36"x36"	 RI-3 12"x6" <small>NOTE: REFLECTORIZED YELLOW LEGEND (COUNTY NAME, ROUTE LETTER & NUMBER) & BORDER ON A BLUE BACKGROUND.</small>	 M6-5 21"x15"
 WI-1 30"x30" (LT. OR RT.)	 WI-6 48"x24"	 W2-3 30"x30" (LT. OR RT.)	 W5-3 36"x36"	 WI3-1 18"x18" <small>NOTE: ALL M6 SIGNS TO BE MADE WITH REFLECTORIZED YELLOW ARROW & BORDER WITH BLUE BACKGROUND.</small>	 M6-1 21"x15"	 M6-6 21"x15"
 WI-2 30"x30" (LT. OR RT.)	 WI-7 48"x24"	 W2-4 30"x30"	 W10-1 36" DIAMETER	 W3-3 36"x36"	 M6-2 21"x15"	 S4-3 24"x8"  S4-2 24"x10"



NOTE: LENGTH OF SIGN POSTS SHALL BE DETERMINED SO AS TO PROVIDE FOR MINIMUM VERTICAL CLEARANCES AS CALLED FOR IN THE SPECIFICATIONS PLUS A MINIMUM VERTICAL PENETRATION OF 30" IN THE SOIL.



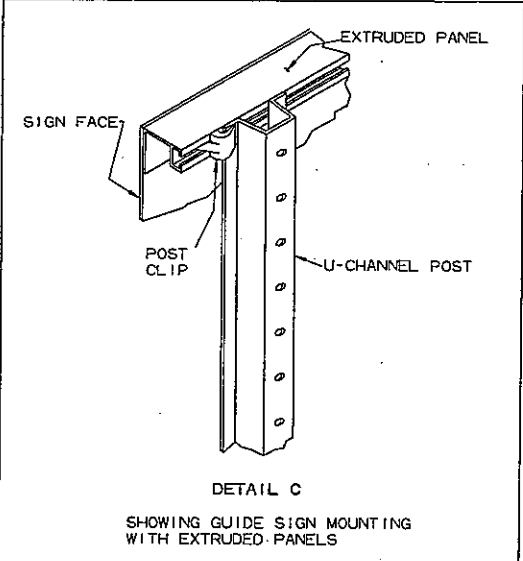
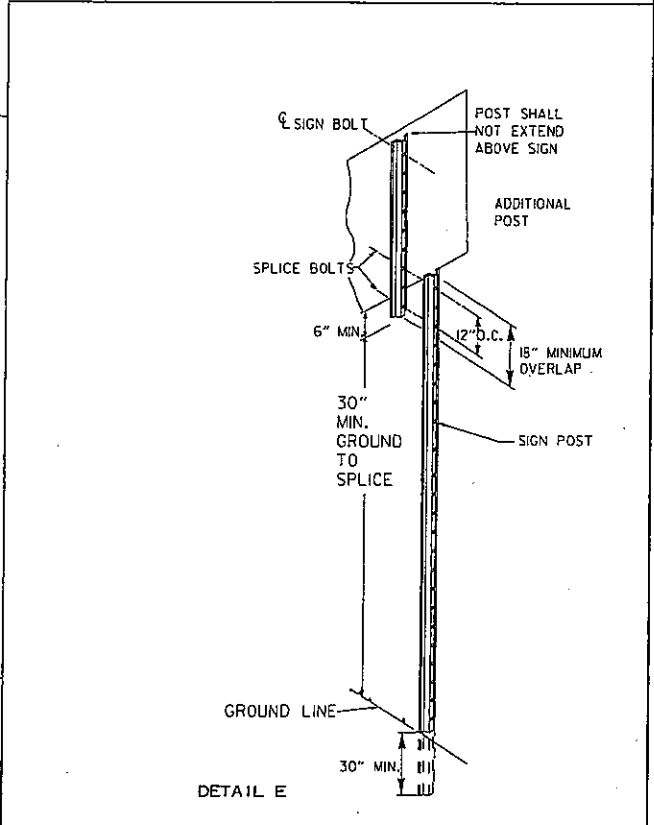
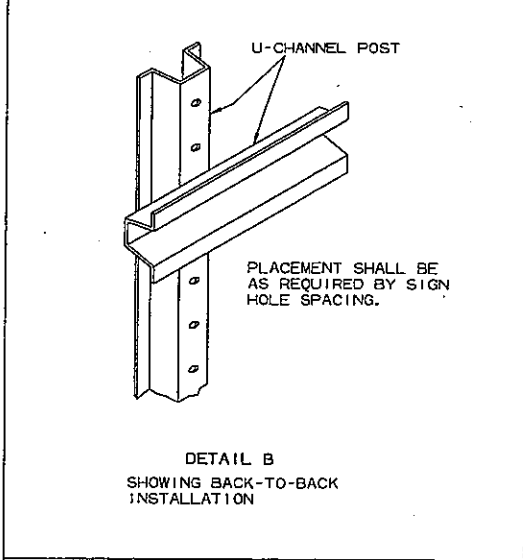
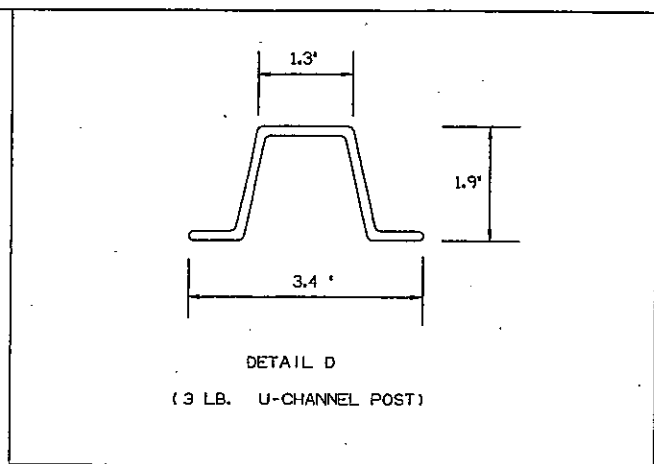
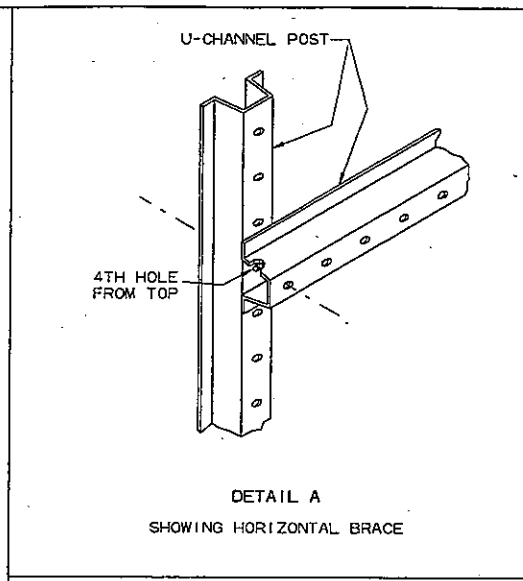
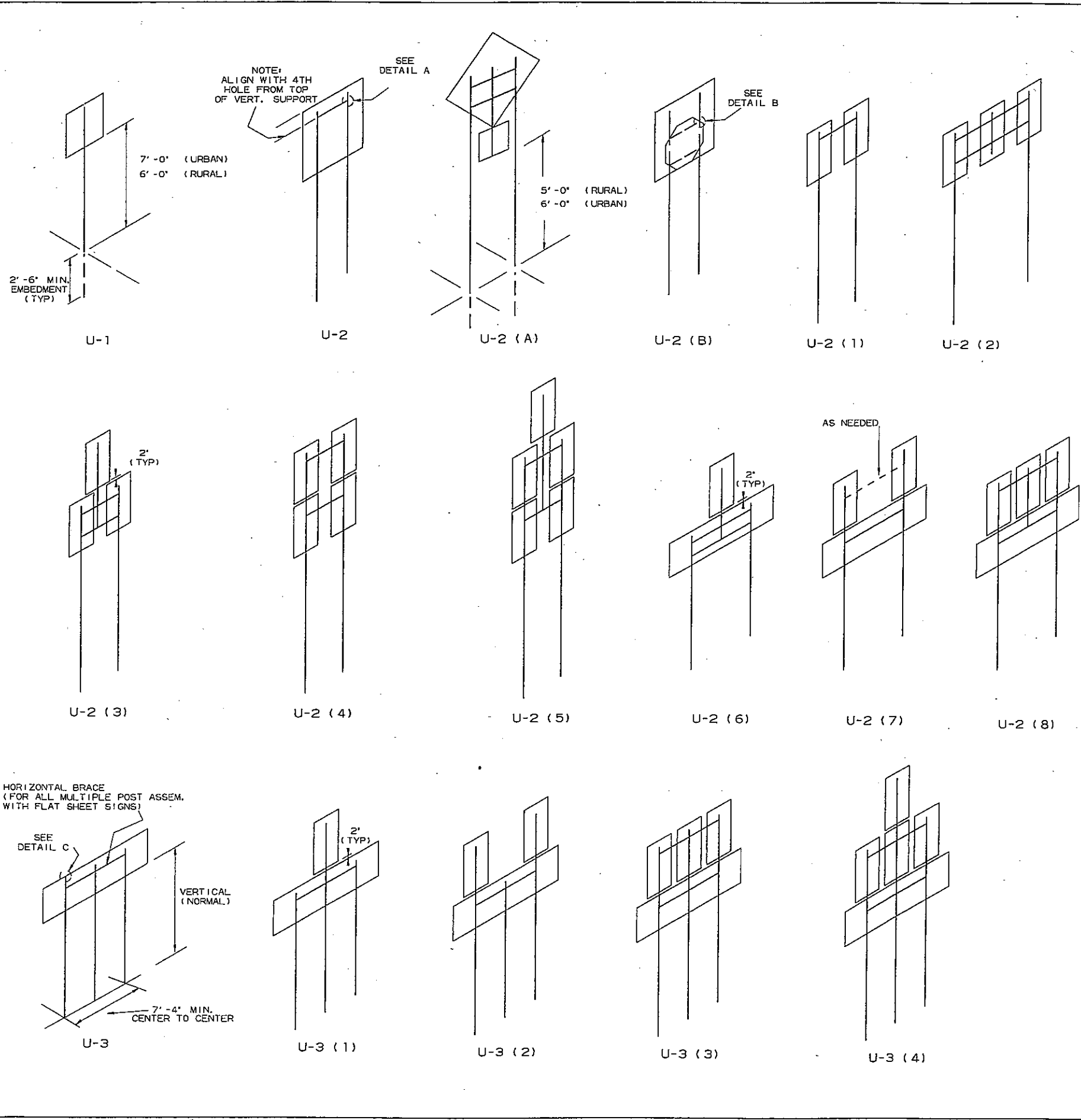
MINIMUM WEIGHT
TYPE A & B = 3 LBS./FT.
TYPE C = 2 LBS./FT.

STANDARD HIGHWAY SIGNS

SUPPORT ASSEMBLIES


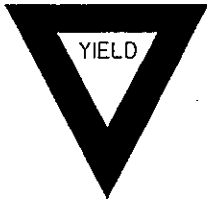
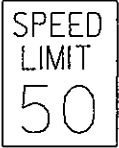






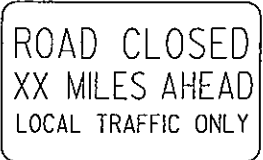
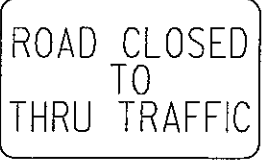

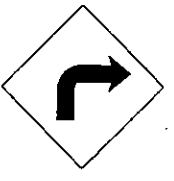





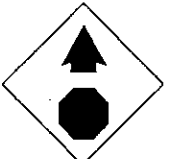

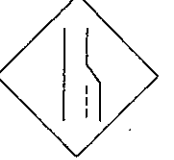



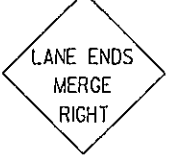


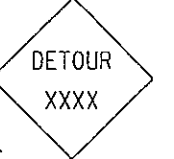





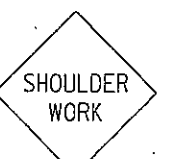





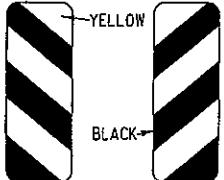



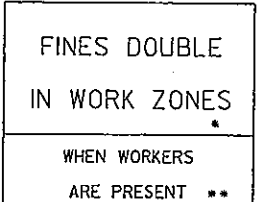
4-17-03	REVISED SIGN DESIGNATION - W3-1 & W3-2	
4-10-03	REVISED W5-2, W8-3, OM-3; ADDED WI-8	
1-5-81	REDRAWN	960-1-15-81
9-5-78	ADDED W4-3	877-9-15-78
9-2-76	POST WT.	823-9-3-76
	STEEL POST WT. FROM 2" - 3"	
5-3-76	ADDED S4-2 & S4-3	504-5-3-76
8-12-74	REV. HT. TYPE "C" ASSEMBLY	500-8-21-74
12-21-72	ADDED M6-2, 3, 4, 5, 6	500-12-21-72
12-1-72	ISSUED	567-12-1-72
DATE	REVISION	DATE FILMED

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD HIGHWAY SIGNS
AND SUPPORT ASSEMBLIES
STANDARD DRAWING SHS-1



NOTES:
 SIGNS AT LEAST 8' IN LENGTH MAY BE INSTALLED ON THREE 3 LB. POST. IN NO CASE SHALL THERE BE MORE THAN TWO 3 LB. POSTS WITHIN A 7' PATH.
 SPLICES NECESSARY TO ATTAIN PROPER MOUNTING HEIGHT SHALL BE AS SHOWN IN DETAIL (E).
 NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. CARRIAGE BOLTS TO MOUNT SIGNS TO POST AND 3/8" DIA. CARRIAGE BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS.
 ALL SIGN POSTS SHALL BE PLUMB.

ARKANSAS STATE HIGHWAY COMMISSION	
U-CHANNEL POST ASSEMBLIES	
STANDARD DRAWING SHS-2	
10-9-03	REMOVED ROUND POST & REVISED SPACING
10-12-95	MOVED UPPER SPLICE
6-8-95	REVISED SPLICE DETAIL
2-2-95	REDRAWN
DATE	REVISION
	FILMED

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>
<p>W20-3</p>  <p>STD. 48"x48"</p>	<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET 186-2 24"</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>
						<p>R56-1</p>  <p>STD. 18"x18"</p>
						<p>R55-1</p>  <p>WHEN WORKERS ARE PRESENT **</p> <p>36"x60"</p> <p>* USE 6" C LETTERS ** USE 4" D LETTERS</p>

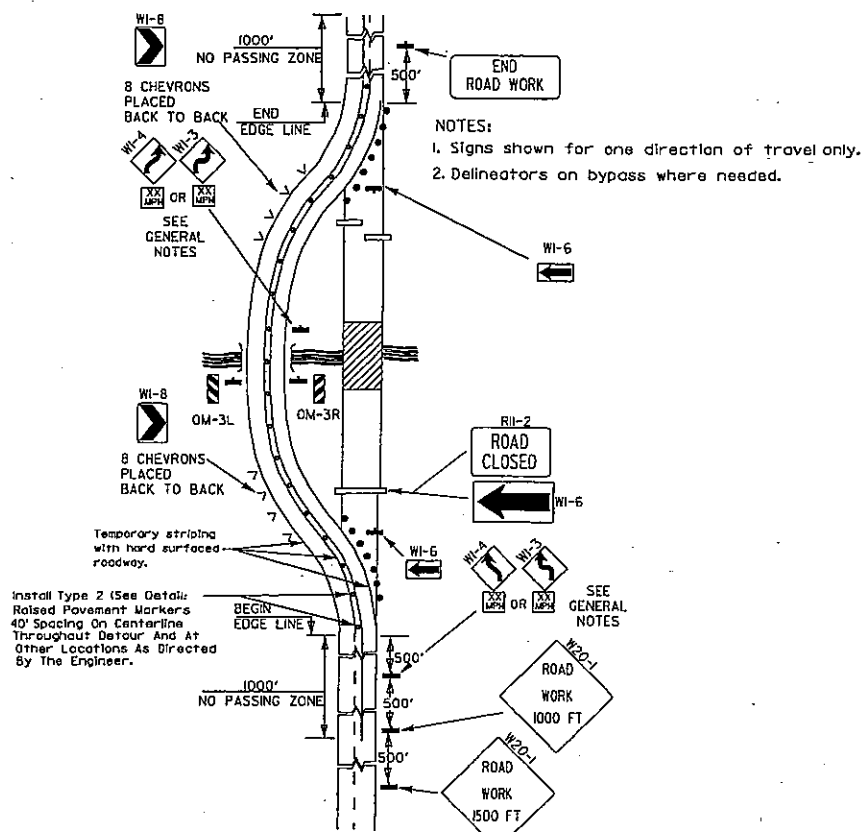
ADVANCE DISTANCES

	(XXXX)	1/2 MILE
500 FT		3/4 MILE
1000 FT		1 MILE
1500 FT		AHEAD

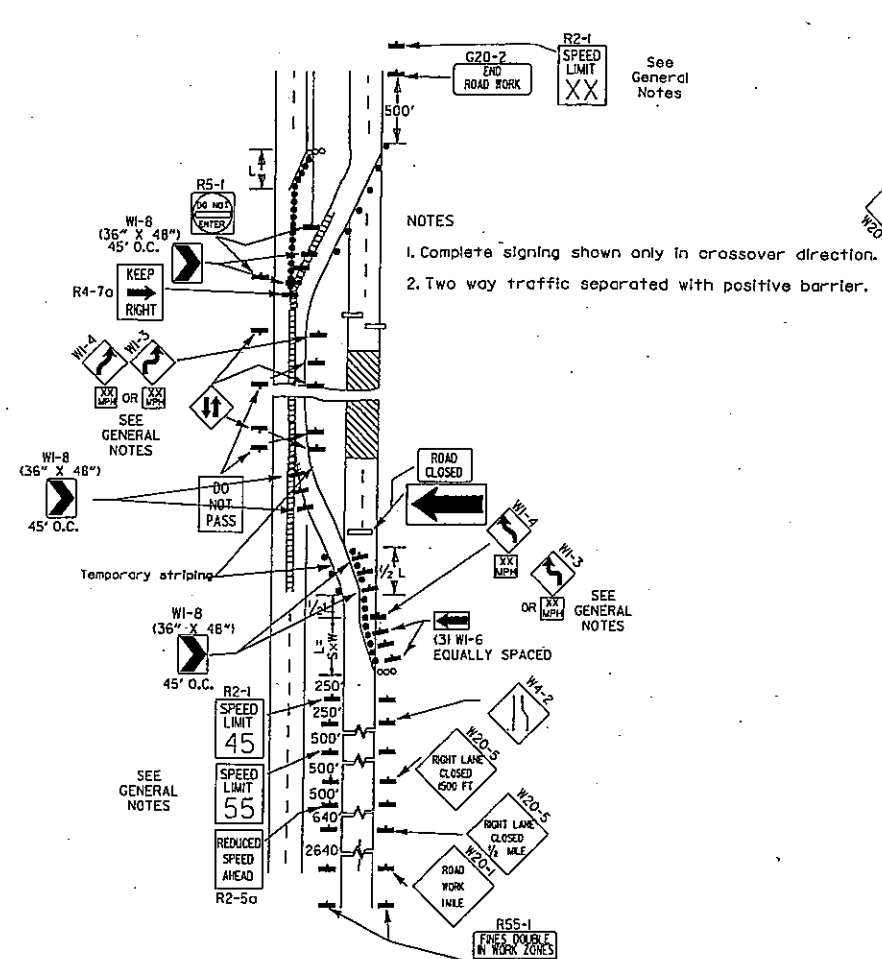
- GENERAL NOTES:
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
 - TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
 - EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACTED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
 - SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 50. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
 - SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD. POSTS, CHANNEL POSTS SHALL BE PAINTED GREEN, WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
 - POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
 - ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
 - FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
 - MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
 - R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

*NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 IS REQUIRED FOR ALL PROJECTS.

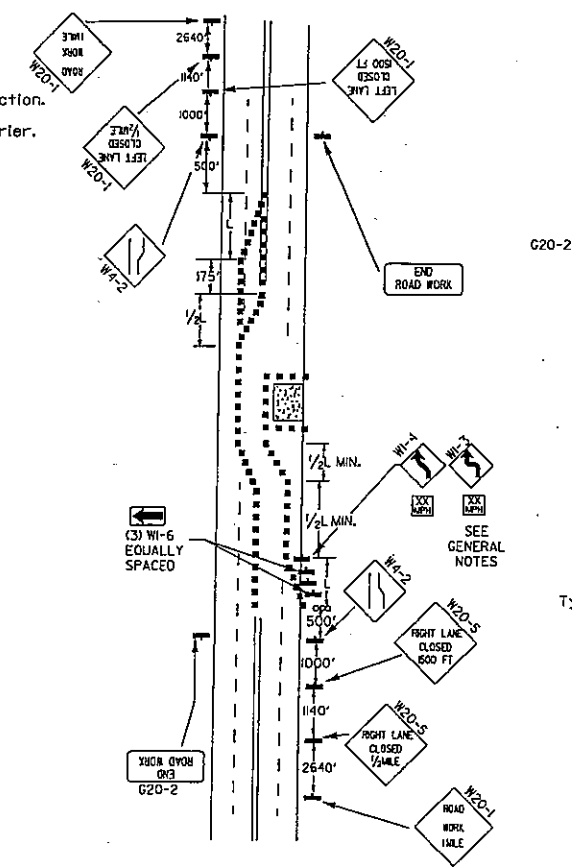
DATE	REVISION	FILED
4-11-08	REVISED SIGN DESIGNATIONS	
1-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
1-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
4-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-16-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
9-15-91	DRAWN AND PLACED IN USE	



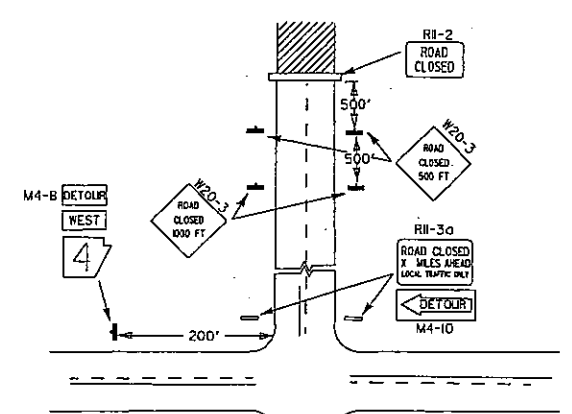
(A) Typical application of traffic control devices on a 2-lane highway where the entire roadway is closed and a bypass detour is provided.



(B) Typical application - 4-lane divided roadway where one roadway is closed.

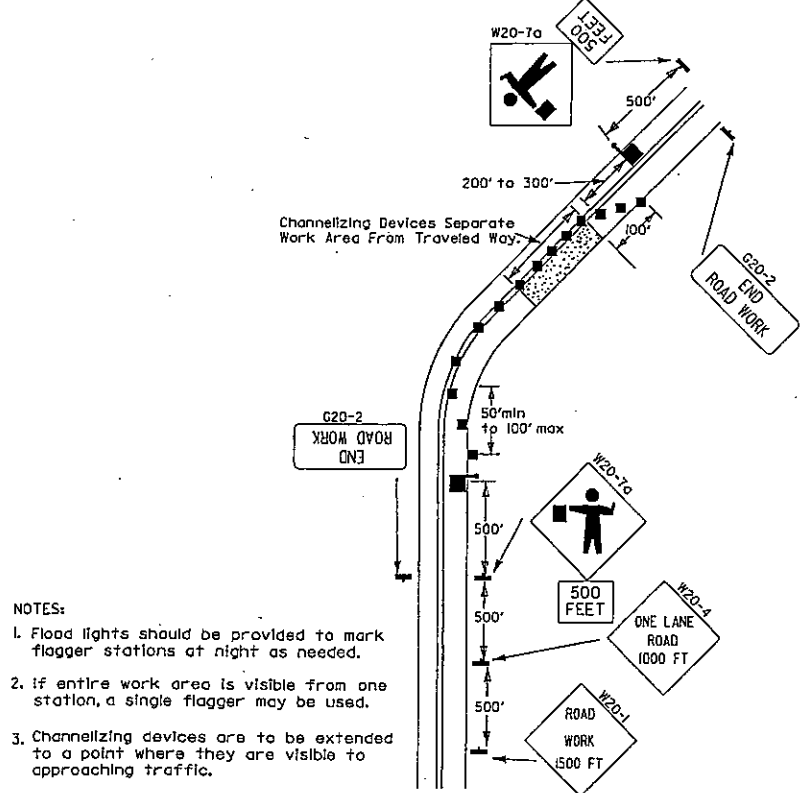


(C) Typical application - 4-lane undivided roadway where half of the roadway is closed.

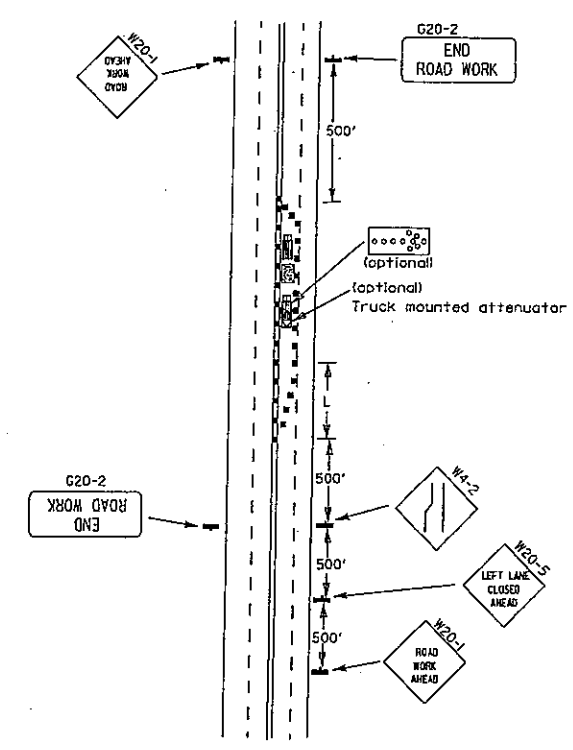


- NOTES:
1. Regulatory traffic control devices to be modified as needed for the duration of the detour.
2. Street names may be used when desirable for directing detoured traffic.

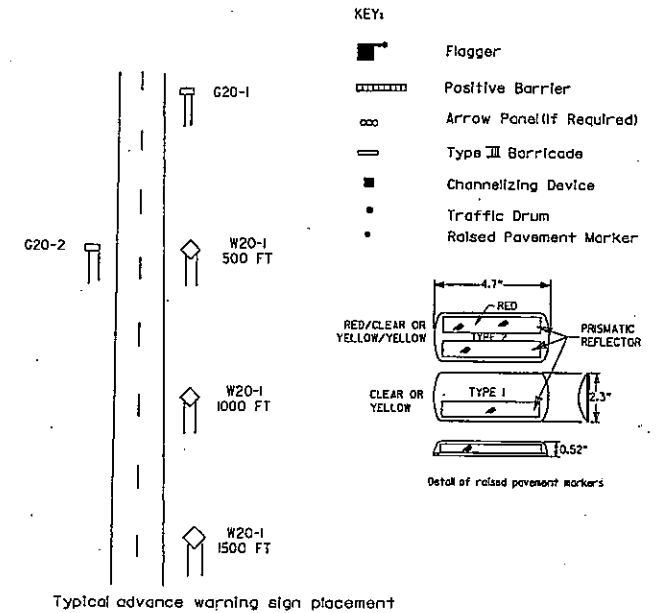
(D) Typical application - roadway closed beyond detour point.



(E) Typical application of traffic control devices on 2-lane highway where one lane is closed and flagging is provided.



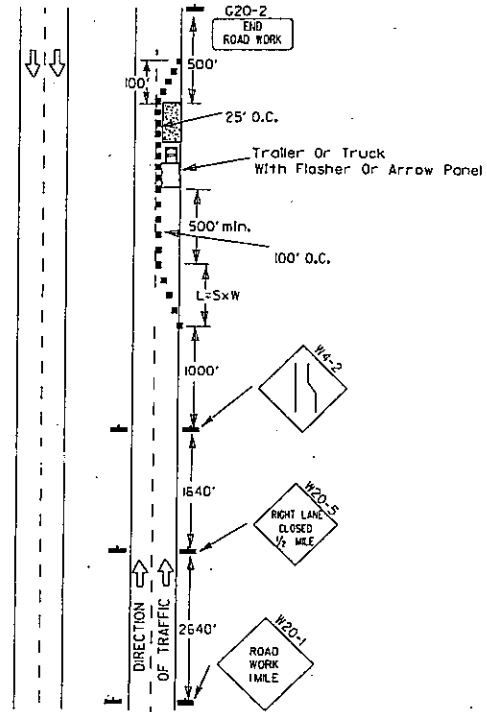
(F) Typical application - 4-lane undivided roadway with inside lane closed.



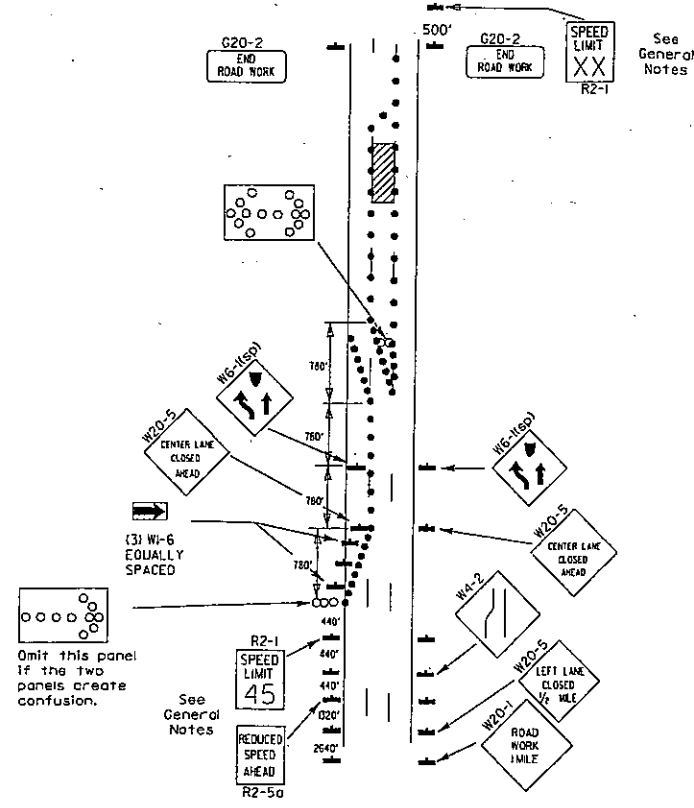
Taper formulae:
 $L = S \times W$ for speeds of 45mph or more.
 $L = \frac{WS^2}{60}$ for speeds of 40mph or less.
 Where:
 L = Minimum length of taper.
 S = Numerical value of posted speed limit prior to work or 85th percentile speed.
 W = Width of offset.

- GENERAL NOTES:
 1. Advisory speed posted on W1-3 or W1-4 curve warning signs to be determined at site. Use W1-4 when speed is greater than 30mph and W1-3 when 30mph or less.
 2. When the existing speed limit is 45mph and the plans require a speed limit of 55mph, the R2-1(K45) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(K45) shall be installed to match original speed limit.
 3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(K45) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(K45) shall be installed to match original speed limit.
 4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit, or as directed by the Engineer.
 5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
 6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
 7. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

DATE	REVISION	FILED
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-95	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

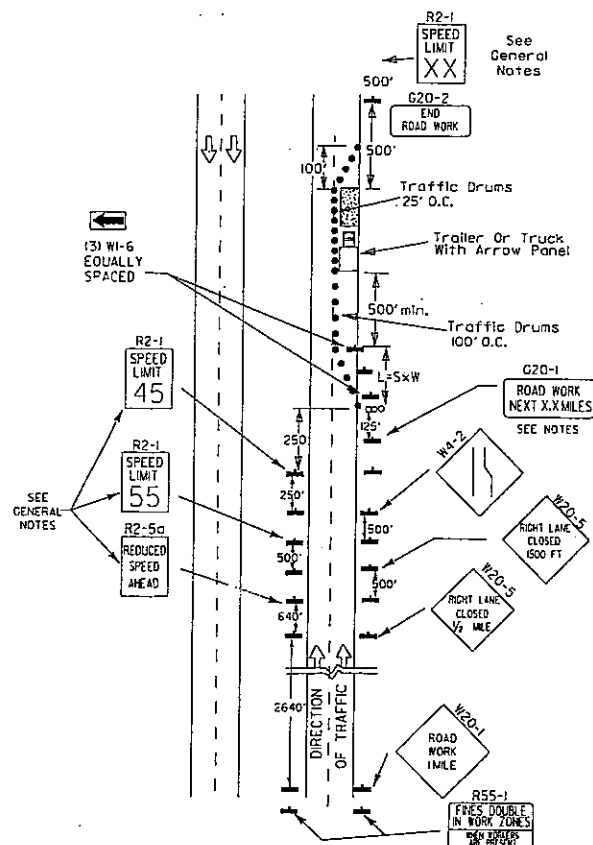


(B) Typical application - 3-lane oneway roadway where center lane is closed.

- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

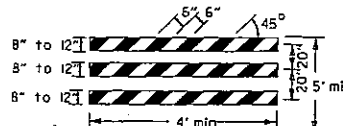
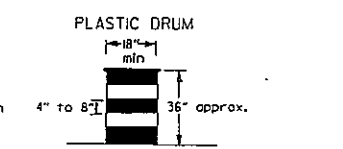
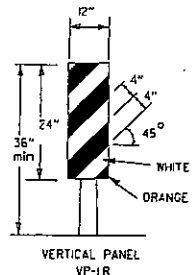
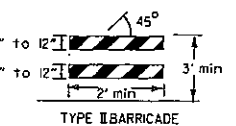
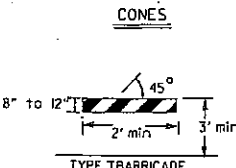
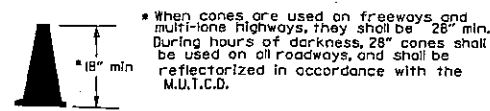
GENERAL NOTES:

1. A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1XX shall be installed to match original speed limit.
3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1XX shall be installed to match original speed limit.
4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
7. The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1(1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
8. Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
9. All plastic drums and cones shall meet the requirements of NCHRP-350.
10. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

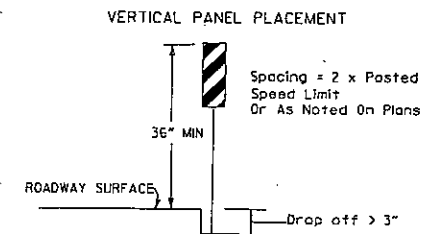


(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

Channelizing devices



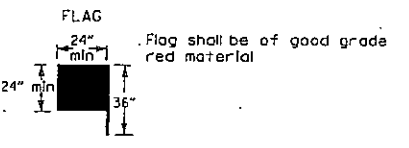
NOTE:
For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



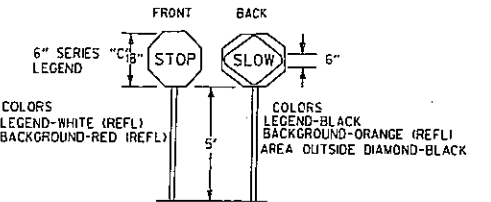
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

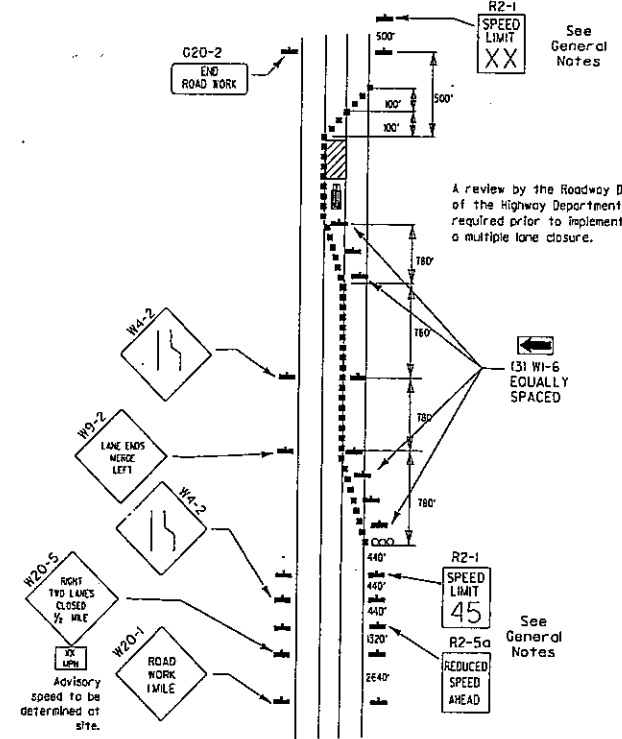
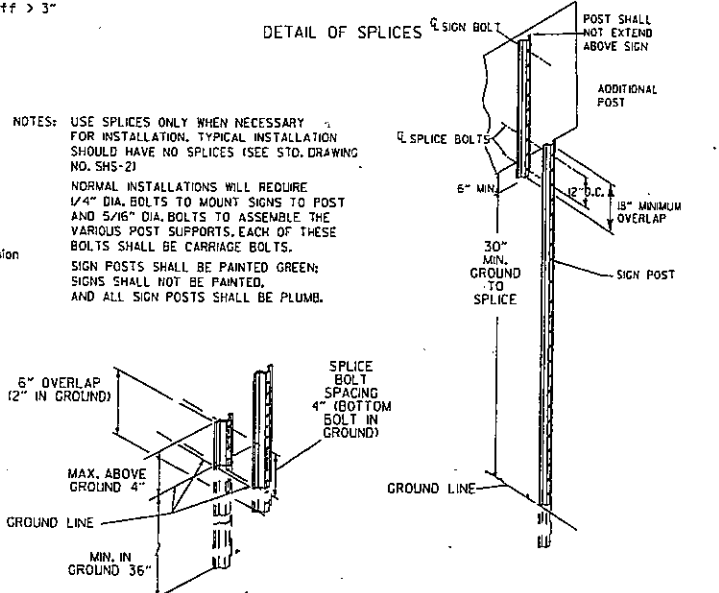
* When shown on the plans concrete barrier will be used.
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



STOP SLOW PADDLE



DETAIL OF SPLICES



(D) Typical application - closing multiple lanes of a multi-lane highway.

DATE	REVISION	FILMED
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SPI) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-9-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	