



City of Jonesboro

Municipal Center
300 S. Church Street
Jonesboro, AR 72401

Meeting Agenda Metropolitan Area Planning Commission

Tuesday, August 12, 2025

5:30 PM

Municipal Center, 300 S. Church

1. Call to order

2. Roll Call

3. Approval of minutes

[MIN-25:068](#) MINUTES FOR THE MAPC MEETING ON 07-22-25

Attachments: [7.22.25 MAPC Minutes.pdf](#)

4. Miscellaneous Items

[COM-25:032](#) Sidewalk Wavier at 903 Martin Luther King Jr. Blvd.

Attachments: [Sidewalk Letter.pdf](#)

[COM-25:033](#) Presentation Discussing a Comprehensive Growth Plan and Street Master Plan by representatives from DPZ and CTA

Attachments: [Comprehensive Master Growth Plan.pdf](#)

5. Preliminary Subdivisions

[PP-25-06](#) Preliminary Plat Sarah's Crossing Phase IV PLN-8538

Attachments: [CONSTRUCTION DETAILS-DT-1 Erosion.pdf](#)
[CONSTRUCTION DETAILS-DT-2 Water.pdf](#)
[CONSTRUCTION DETAILS-DT-3 Sewer.pdf](#)
[CONSTRUCTION DETAILS-DT-4 Street.pdf](#)
[Sarah Crossing P4 7 11 2025-C-1.pdf](#)
[Sarah Crossing P4 7 11 2025-E-1.pdf](#)
[Sarah Crossing P4 7 11 2025-GR-1.pdf](#)
[Sarah Crossing P4 7 11 2025-OV-1.pdf](#)
[Sarah Crossing P4 7 11 2025-OV-2.pdf](#)
[Sarah Crossing P4 7 11 2025-ST-1.pdf](#)
[Sarah Crossing P4 7 11 2025-SW-1.pdf](#)
[Sarah Crossing P4 7 11 2025-SW-2.pdf](#)
[Sarah Crossing P4 7 11 2025-UT-1.pdf](#)
[Sarah Crossing P4 7 11 2025-WT-1.pdf](#)
[Sarah Crossing P4 8 01 2025-GR-1.pdf](#)
[Sarah Crossing P4 8 01 2025-ST-1.pdf](#)
[SARAH CROSSINGPHASE 4 PRELIM Addresses.pdf](#)

6. Final Subdivisions

7. Conditional Use

8. Rezoning

RZ-25-13 Rezoning RZ 25-13 2106 Bridger Rd. Rezone approximately 6.79 acres

Attachments: [Application Form.pdf](#)
[COJ Rezoning Property Owner Notification.pdf](#)
[Concept Plan.pdf](#)
[Cover Letter.pdf](#)
[FA Rezoning Property Owner Notification Letter.pdf](#)
[Rezoning Justification.pdf](#)

RZ-25-14 Rezone 7 acres +/- from PD-M Planned Development Multi Family to PD-M Commercial

Attachments: [Block B Rezoning.pdf](#)
[Block K Rezoning.pdf](#)
[Rezoning Application.pdf](#)
[Southern Hills PD Amendment Pattern Book R.pdf](#)

9. Staff Comments

10. Adjournment



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Text File

File Number: MIN-25:068

Agenda Date:

Version: 1

Status: To Be Introduced

In Control: Metropolitan Area Planning Commission

File Type: Minutes

MINUTES FOR THE MAPC MEETING ON 07-22-25



City of Jonesboro

Municipal Center
300 S. Church Street
Jonesboro, AR 72401

Meeting Minutes Metropolitan Area Planning Commission

Tuesday, July 22, 2025

5:30 PM

Municipal Center, 300 S. Church

1. Call to order

2. Roll Call

Present 8 - Lonnie Roberts Jr.; Jimmy Cooper; Kevin Bailey; Monroe Pointer; Stephanie Nelson; Jeff Steiling; Jim Little and Dennis Zolper

3. Approval of Minutes

[MIN-25:063](#)

Minutes: July 8, 2025 MAPC

Attachments: [7.8.25 MAPC Minutes](#)

A motion was made by Jimmy Cooper, seconded by Dennis Zolper, that this matter be Approved . The motion PASSED with the following vote.

Aye: 8 - Lonnie Roberts Jr.; Jimmy Cooper; Kevin Bailey; Monroe Pointer; Stephanie Nelson; Jeff Steiling; Jim Little and Dennis Zolper

4. Miscellaneous Items

[COM-25:026](#)

Sidewalk In Lieu: 1625 West Parker Road

Ozark Civil Engineering Inc., on behalf of Reliance Health Facility, is requesting approval to pay a sidewalk in lieu payment of \$76,851.36 for 664 square yards along West Parker Road. The current rate is \$115.74 per square yard.

Attachments: [Reliance Health Facility-sidewalk waiver](#)
[Site](#)

Lonnie Roberts (Chair): This was tabled at our last meeting anyone want to make a motion to un-table this?

(Commission): I make a motion

(Commission): Second

Lonnie Roberts: Alright, all those in favor of un-tabling press your aye button.

A motion was made by Dennis Zolper, seconded by Jeff Steiling, that the matter be un-tabled the motion PASSED with an unanimous 'aye' vote.

Lonnie Roberts (Chair): The reason I had to un-table this is because they would like to withdraw, and I had to un-table it so we could take action on that. So, that item has been withdrawn. They're going to put in the sidewalk in.

Withdrawn

[COM-25:028](#)

Sidewalk In Lieu: 907, 911, 915, & 919 Dr. Martin Luther King Jr. Drive

Woodrow Byrd, on behalf of Blue Byrd Rentals, LLC, is requesting approval to pay a sidewalk in lieu payment of \$3,478.20 for 50.5 square yards along Dr. Martin Luther King Jr. Drive. The rate for this project is \$68.78 per square yard.

Attachments: [Blue Byrd Rentals Letter](#)
 [C2 - SITE PLAN](#)

Lonnie Roberts (Chair): Do we have the proponent for this item? Please state your name for the record so we know who you are.

John Moore (Proponent): John Moore.

Lonnie Roberts: Okay, commissioners have any questions for the applicant?

City staff do you have any comments?

Derrel Smith (City Planner): No, sir.

Lonnie Roberts: I'll take a motion.

Paul Ford (Commission): Before we move on can I ask why the withdrawn sidewalk project was a hundred and fifteen dollars a square yard and this one was \$68 a square yard. What's the reasoning for that difference?

Derrel Smith (City Planner): The reason is, the one on Martin Luther King Drive is an actual highway project that their building now and that was their cost to put sidewalk in. So, we used that on that actual job instead of taking the overall average that we normally do.

Lonnie Roberts (Chair): Yeah the first one was just an estimate.

Paul Ford: That first one would be the average?

Derrel Smith: That's the average, yes.

Michael Morris (City Engineer): That first one is the ARdot weighted average for the annual bids. This was an actual bid for Martin Luther King Drive.

Derrel Smith: It was specific to that project.

Lonnie Roberts: So, you're in lieu of fee was approved.

John Moore: Thank you.

A motion was made by Dennis Zolper, seconded by Jimmy Cooper, that this matter be Approved . The motion PASSED with the following vote.

Aye: 7 - Jimmy Cooper;Kevin Bailey;Monroe Pointer;Stephanie Nelson;Jeff Steiling;Jim Little and Dennis Zolper

[COM-25:029](#)

Site Plan: Elmhurst Drive Storage Facility

Davidson Engineering is requesting a MAPC site plan approval for a storage complex located off Elmhurst Drive. The proposed development is 118,592 sq. ft. and located in a C-3, general commercial district. Conditional use approval was granted for this site on February 11th, 2025.

Attachments: [23-104 LSD cover letter](#)
 [Site Plan](#)
 [Full Set](#)

Lonnie Roberts (Chair): Do we have the proponent for this item?

Bear Davidson (Proponent): Yes, sir. Bear Davidson here on behalf of the applicant.

Lonnie Roberts: Okay, and City Planner do you have any staff comments on

this one?

Derrel Smith (City Planner): We have started the review process on this and so, what they've submitted does meet the ordinance as far as the site plan and civil approvals and everything.

Lonnie Roberts: Okay, so the reason we're seeing this is because it's greater than 75,000 square feet. So, I know we had some discussion yesterday regarding I guess, the architectural design of the building. Would anybody like to make comments on that commissioners? I'll open up for questions or comments.

Kevin Bailey (Commission): I would just like to state I went back and asked Monica to pull the meeting minutes from when we did the conditional use and we specifically asked questions of is this going to look like a big box store? And we're not going to have 40 to 50 overhead doors in it and you were going to internally load it, the project, and then by chance you submitted the elevations with your site plan review and it's exactly what we said, we didn't want.

Bear Davidson: We did submit revised drawings today Mr. Bailey. I agree, I looked at the meeting minutes as well from both the conditional use appearances. The time it was tabled and the time that it was approved. We talked a lot about the frontage having a retail look but I did, see as well where we talked about the north side that was facing that public right of way, not having overhead doors. So, I had a visit with the architect late last night after the meeting yesterday and we submitted today revised renderings where there are no overhead doors on that east or north facing exterior walls which are the walls that face the public rights of way. Also, I think, Monica did you get the photo realistic rendering I submitted as well. This is from the northeast corner. So, the two public roadways are on the east and north. What you don't see in this rendering though along that north property line, additionally, there are proposed non-color sweet gums, 48 sky pencil hollies, 3 red maples, and 15 boxwoods. It's extremely heavily landscaped between our internal drive and the public right of way. So, on top of the fact that we've eliminated those overhead doors facing north, we've also heavily landscaped between the public right of way and our structure. I think we've addressed those comments. I agree with those comments and think we've addressed them.

Kevin Bailey: I would like to say that my comments in that last meeting that my comments about the big box store look and the not that many door and then the masonry and glass, wasn't specific enough, which probably led you guys not giving us exactly what I felt like I was voting for. But I did vote for it. So, I can't vote against what you have already here. But I don't feel like the overall concept of the building is what I thought I was voting for in the conditional use previously. Does that make sense? I was looking for a project that didn't look like a mini storage. It'd look like a big box retail that was loaded from the inside or from the ends with minimal exterior doors and it would look like a block building with some glass entrances in it. And I need to apologize to y'all if my question wasn't clear enough in the, but I did vote for it. So, there's nothing I can do about that now. But this is not what I felt like I was looking for in the conditional use.

Bear Davidson: Thank you for your comments. Our intent was to address those concerns. This is largely similar in appearance to the Benton Arkansas facility that we worked on about 18 months ago. Been by it several times since then. Sometime we have to work on projects that we're not super pumped about the way they look. That's not one of them, driving by, it's a sharp looking facility. Well landscaped, it has a retail look. This will be largely the same. If it's not what you had in mind at conditional use, I apologize. The goal has been to

put forward what was requested.

Kevin Bailey: I appreciate that, I feel like I've let you guys down with it, by not clarifying my comments back then.

Bear Davidson: I hope if this gets built that you'll feel the same way when it's built driving by like we do in Benton that it's a project to be proud of. It looks sharp and it's well kept so.

Kevin Bailey: That's all I had Lonnie.

Jeff Steiling (Commission): I had a question about the site planning and the parking.

Bear Davidson: Yes?

Jeff Steiling: There are a lot of parking spaces around the building right on the building they appear on the site plan. And I was wondering, some of those look like they would be blocking some of the exits that are coming out of the building. I'm wondering what you do to protect the building when you park that close? Do you have bollards out there, how do you keep cars from hitting the metal panels you have on the side of that building?

Bear Davidson: We do generally have an ample amount of bollards at corners especially exposed corners and in at doorways. Storage is short of a unique business, if people come and go but not in droves, we have way more parking spaces shown than we would actually need. We're just trying to make sure we meet the city's ratio requirements. If there were 10 customers at the side at one time, that would be a whole lot and so, we do want folks to have the ability to pull up to the many number of external doors along the boundaries to unload items and be close to their internal climate controlled units and we want them to do so in an organized manner but if you looked at our site plan, you'd think, good grief why is there so many parking spaces? There's not a need for that. One we want to meet the city's requirements and two to just try and keep people parking in an organized manner along those entry points. Does that answer your question?

Jeff Steiling: It does, it doesn't change my concern but it does answer my question. I guess my concern is that there's no curb, there's no sidewalk, there's no nothing there to kind of protect the building from the vehicles. Both parking in those parallel parking spaces or backing up. And we're already a little concerned about how the building looks. And if it has big dents in it, six months down the road after it's built, it's going to look even worse.

Bear Davidson: A few more things to add, there is curb and gutter along the entire perimeter just to clarify-

Jeff Steilings: Along the outside perimeter not the inside around the building right?

Bear Davidson: Yes sir, that's right and we do have a raised six-inch sidewalk at the front where you would go in if you were to inquire about leasing a space. The reason that there's no curb and gutter around the interior or raised sidewalk is intentional. A lot of times people are carrying a heavy couch or using dollies to roll, so that flush entry is important for access as well. But the offset, the risk is yeah, that someone could potentially bump into the building. So, we do have to use bollards.

Jeff Steilings: I guess my thought would have been that you would come out those doors and have kind of what we think of as a handicap ramp down and that you would have some curb and gutter around the rest of that to protect the building. But that's not what's purposed and I'm guessing Derrel that's not required. But I am concerned about what that building looks like, 6 months or a year because people are going to understand those spots are not all going to be filled at the same time, but people are going to park as close as they can to the door to load and unload their vehicles and that means they are going to be

parking as close to the building as they can get. And I'm just a little concerned about that.

Lonnie Roberts: Commissioners have any other questions?

Paul Ford (Commission): I don't know if I have a question but my comment is, I didn't vote for this approval of this project in the first place and I still have significant reservations about it and then to, comeback with a site plan that is inconsistent with the discussions at the February meeting and only changes in the middle of the night after somebody's raised it at the pre-meeting and that photograph is not technically a part of your submission in your application for approval. All of which just adds to my initial angst to this project.

Carol Duncan (City Attorney): I will say that I think an amendment can be made to include that photograph as a replacement photograph.

Bear Davidson: I'll be glad for that to honest.

Carol Duncan: To show that, that's what they're requesting for the site plan approval. If that's helpful in anyway.

Bear Davidson: Thank you.

Dennis Zolper (Commission): Zolper, I make a motion to approve the project, subject to a new photograph being attached to the application.

Carol Duncan: Can we specify that shows no overhead doors on the public right of way? I think that's essentially the change right?

Commission: That's right.

Lonnie Roberts: Motion on the floor, do I hear a second?

Jimmy Cooper (Commission): Cooper, second.

A motion was made by Dennis Zolper, seconded by Jimmy Cooper, that the matter be amended, and the motion was DENIED with the following vote:

Aye (2): Dennis Zolper, Kevin Bailey

Nay (6): Jeff Steiling, Jim Little, Jimmy Cooper, Monroe Pointer, Paul Ford, Stephanie Nelson

Absent (0):

Lonnie Roberts (Chair): Okay, so this site plan has been denied.

Carol Duncan (City Attorney): I thought that vote was on the amendment that was just proposed.

Lonnie Roberts: Was that just for the amendment or did you think you were voting for the site plan?

Unable to transcribe

Carol Duncan: Okay, I took it wrong I thought it was on just the amendment.

Dennis Zolper (Commission): I thought it was too.

Carol Duncan: That was your motion maker.

Lonnie Roberts: So, how do we proceed?

Derrel Smith (City Planner): You have to vote on the amendment first and then vote on the site plan.

Carol Duncan: That's what I understood. Under Robert's Rules of Order, I assumed an amendment was made and you would vote yes or no on the amendment. And then, you would cast a vote on the site plan.

Unable to Transcribe

Carol Duncan: I was saying you can amend the site plan before you vote on it.

Lonnie Roberts: Do you want to call for a revote?

Paul Ford (Commission): What I thought Mr. Zolper's motion was, was to approve the site plan with the amendment of the photograph. So, I thought it was one motion by Mr. Zolper which received the second. So, it to me, there was a motion, the second, and a negative vote. We can't go back and change

the motion.

Carol Duncan: Well, he's saying he thought his motion was to amend the site plan. Which under Robert's Rules of Order would be correct. You would amend the site plan, and then vote on the site plan.

Paul Ford: The minute's will reflect what Mr. Zolper said initially.

Dennis Zolper: Why don't we just make a second motion to approve the existing plan as submitted tonight without an amendment.

Carol Duncan: I think that's what people voted on essentially.

Dennis Zolper: Seems like it was.

A motion was made by Dennis Zolper, seconded by Jim Little, that this matter be Approved . The motion FAILED with the following vote.

Aye: 1 - Dennis Zolper

Nay: 6 - Jimmy Cooper;Kevin Bailey;Monroe Pointer;Stephanie Nelson;Jeff Steiling and Jim Little

5. Preliminary Subdivisions

6. Final Subdivisions

7. Conditional Use

8. Rezoning

9. Staff Comments

10. Adjournment



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Text File

File Number: COM-25:032

Agenda Date: 8/12/2025

Version: 1

Status: To Be Introduced

In Control: Metropolitan Area Planning Commission

File Type: Other
Communications

Sidewalk Wavier at 903 Martin Luther King Jr. Blvd.

Associated Engineering, LLC on behalf of Eric Burch, 870Rentals37, LLC, is requesting approval to pay a sidewalk in lieu payment of \$4,608.26 for 67 s.y. along Marting Luther King Jr. Blvd. which is part of the ArDOT widening project Job #100657.



July 24, 2025

Mr. Derrel Smith
Planning Director
City of Jonesboro
300 South Church Street
Jonesboro, AR 72401

Re: Sidewalk Waiver– 870Rentals37, LLC
903 Martin Luther King, Jr., Blvd.
Jonesboro, Arkansas

Dear Mr. Smith,

On behalf of Eric Burch, 870Rentals37, LLC, we are requesting a waiver to the Sidewalk Ordinance for the following reason based on Number 4 in the Exceptions listed in the Ordinance:

Sec 117-330.b: (4) Other unusual circumstances make the sidewalk installation requirement unreasonable or inappropriate.

The Arkansas Department of Transportation Job No. 100657 will install sidewalks along Martin Luther King, Jr. Blvd. (MLK, Jr.) across this property. The project will consist of widening MLK, Jr., Blvd. and installing sidewalks and curbs. It is unreasonable to install sidewalks now when ArDOT will also install sidewalks as part of their project.

The total frontage for the project is 99.88 l.f. (67 s.y.), resulting in a payment of \$ 4,608.26, using the current bid price of \$ 68.78/s.y.

Should you have any questions or require additional information, please contact me.

Respectfully submitted,


John M. Easley, PE, PLS
Project Engineer



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Text File

File Number: COM-25:033

Agenda Date: 8/12/2025

Version: 2

Status: To Be Introduced

In Control: Metropolitan Area Planning Commission

File Type: Other
Communications

Presentation Discussing a Comprehensive Growth Plan and Street Master Plan by
representatives from DPZ and CTA

DEVELOPMENT OF A FULL COMPREHENSIVE PLAN FOR

JONESBORO



Crafton Tull

URBAN3



Planning, Urban Design,
Coding, and Community
Engagement Lead



Crafton Tull

Local Partner – Transportation,
Infrastructure,
Sustainability, GIS, Community
Engagement Support

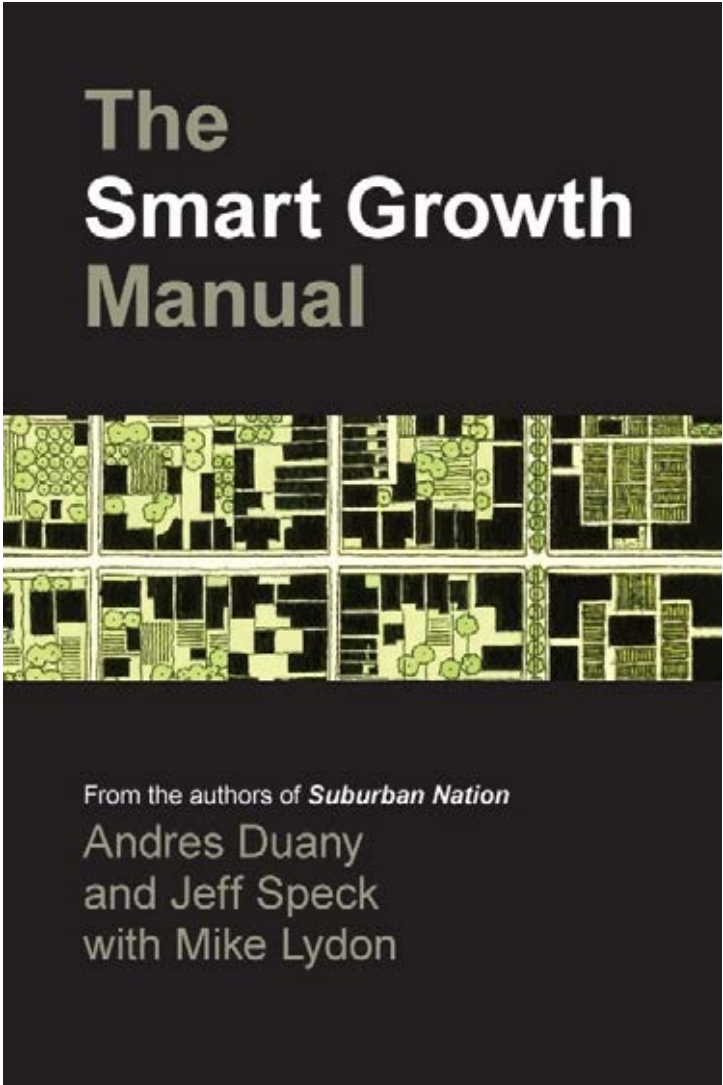
URBAN³

Economic Analyses
and Strategies



DPZ
CODESIGN

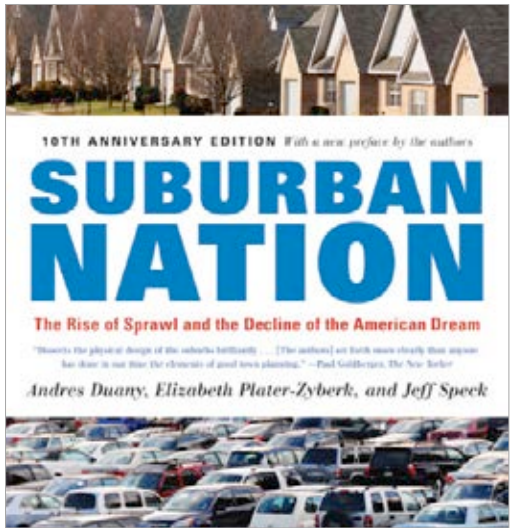
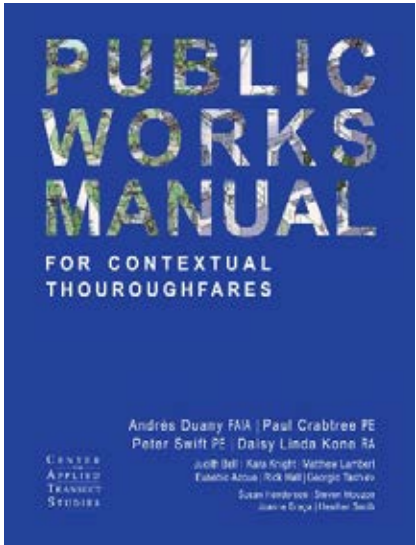
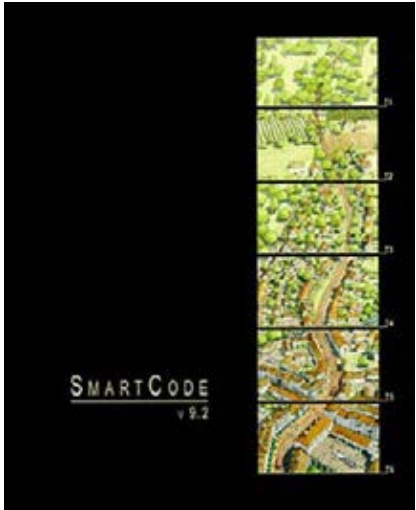
Over 45 Years of Making
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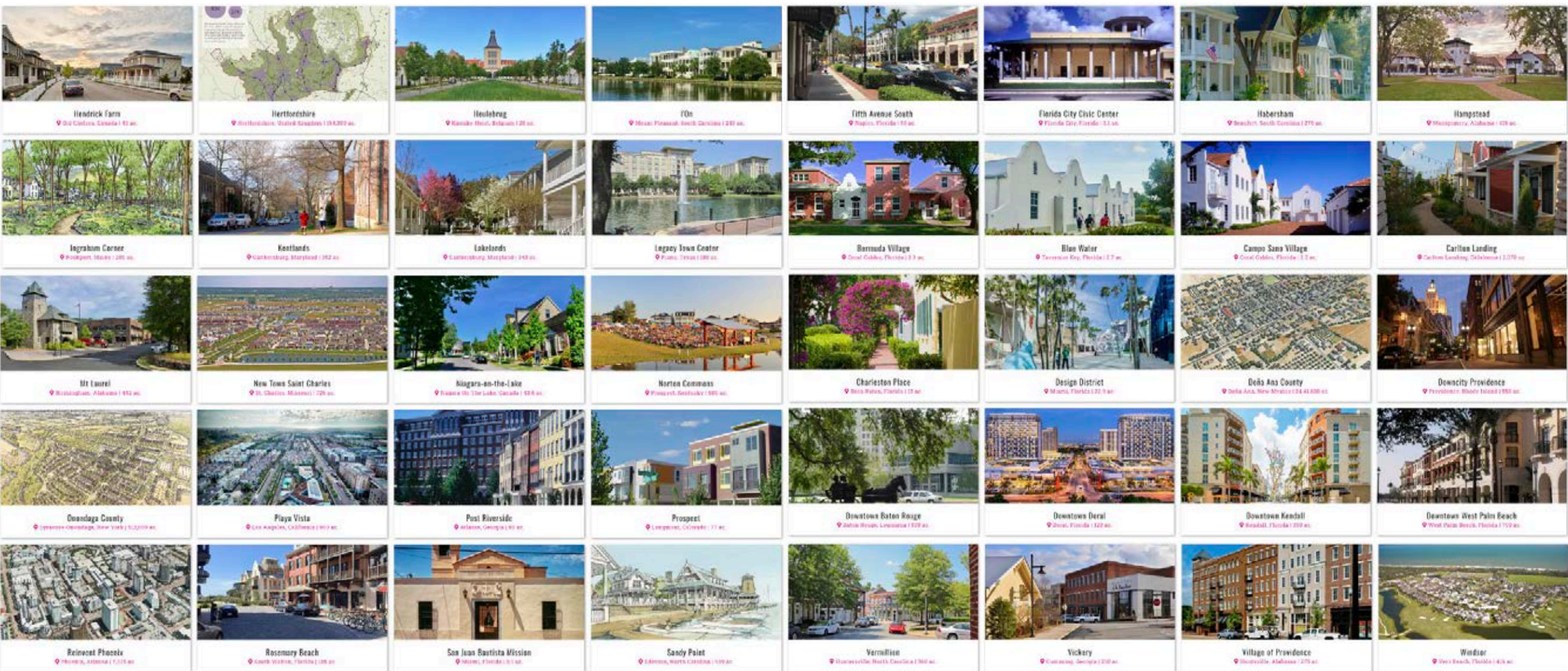
LIVABLE COMMUNITIES
FOR AGING POPULATIONS

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FROM VISIONARY FRAMEWORKS TO BUILT RESULTS

Comprehensive Plans

Aligning land use, infrastructure, housing, and economic development with a community’s long-term goals

Master Plans

Plans for downtowns, corridors, neighborhoods, and districts—ready to guide capital investment and private development

Zoning and Development Codes

Codes including SmartCode, form-based, and hybrid tools—to translate vision into predictable outcomes

Public Engagement Strategies

Engagement that fosters trust, dialogue, and buy-in across diverse stakeholders

Implementation Frameworks

Frameworks to prioritize actions, coordinate partners, and enable accountability over time

Project	Location
Winnipeg Infill Housing Code	Winnipeg, Canada
Northwest Arkansas Regional Growth Strategy	Northwest Arkansas, AR
Walton County Comprehensive Plan EAR	Walton County, FL
Plan Bentonville	Bentonville, AR
Orange County, FL Code and Comprehensive Plan	Orange County, FL
Rogers Future Land Use Map and Form-Based Code	Rogers, AR
Dale Summit Area Plan and Code	Township of College, PA
Downtown Vero Beach Master Plan	Vero Beach, FL
Royal Oak Master Plan (Comprehensive Plan)	Royal Oak, MI
OLF-8 Master Plan	Escambia County, FL
41-10 Mixed Use Area Master Plan	Naples, FL
Village of Key Biscayne Vision Plan	Key Biscayne, FL
Erie Town Center Master Plan and Form-Based Code	Erie, CO
NBV100 Master Plan and Form-Based Code	North Bay Village, FL
Three Corners Master Plan	Vero Beach, FL
Bonita Springs Downtown Code Analysis and Form-Based Code	Bonita Springs, FL
Birmingham Master Plan (Comprehensive Plan)	Birmingham, MI
Coral Gables Zoning Code Update	Coral Gables, FL
Bonita Beach Road Corridor Analysis/Report	Bonita Springs, FL
Lower Merion Twp. Code Analysis/Revisions	Lower Merion, PA
Pensacola CRA Form-Based Code	Pensacola, FL
Downtown Pontiac Master Plan	Pontiac, MI
Downtown Kirkwood Code Analysis and Form-Based Code	Kirkwood, MO
Downtown Bethel TOD Code	Bethel, CT
Derby’s Blueprint for Progress Masterplan	Derby, CT
West Haven TOD Vision and Form-Based Code	West Haven, CT
Charleston, SC BAR Process Analysis/Revisions	Charleston, SC

A selection of our municipal projects from the last 10 years

Who We Are

OFFICES

ARKANSAS

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- Jonesboro
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- Conway
- Fayetteville
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- Hot Springs
- Russellville



PLANNING SERVICES

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- Stormwater Planning
- Corridor Planning + Design
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ACTIVE TRANSPORTATION



MUNICIPAL ENGINEERING



ARCHITECTURE



TRANSPORTATION ENGINEERING



DEVELOPMENT ENGINEERING



LANDSCAPE ARCHITECTURE



ENVIRONMENTAL SERVICES

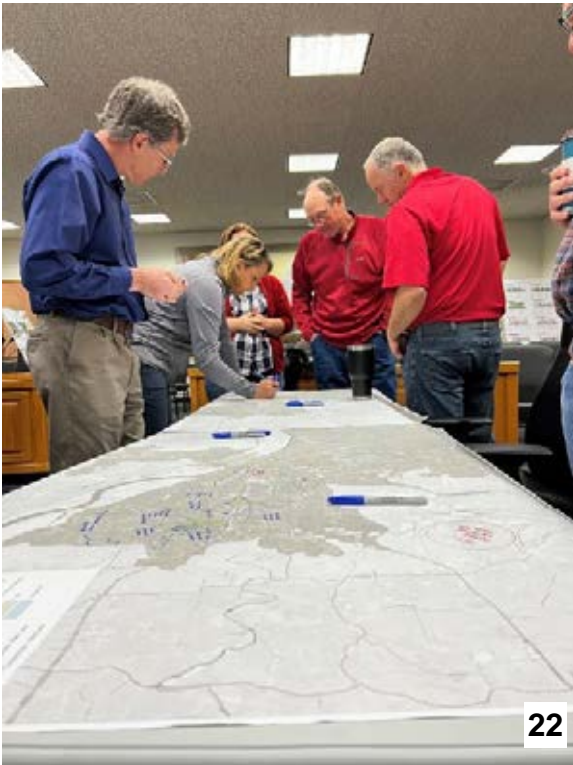
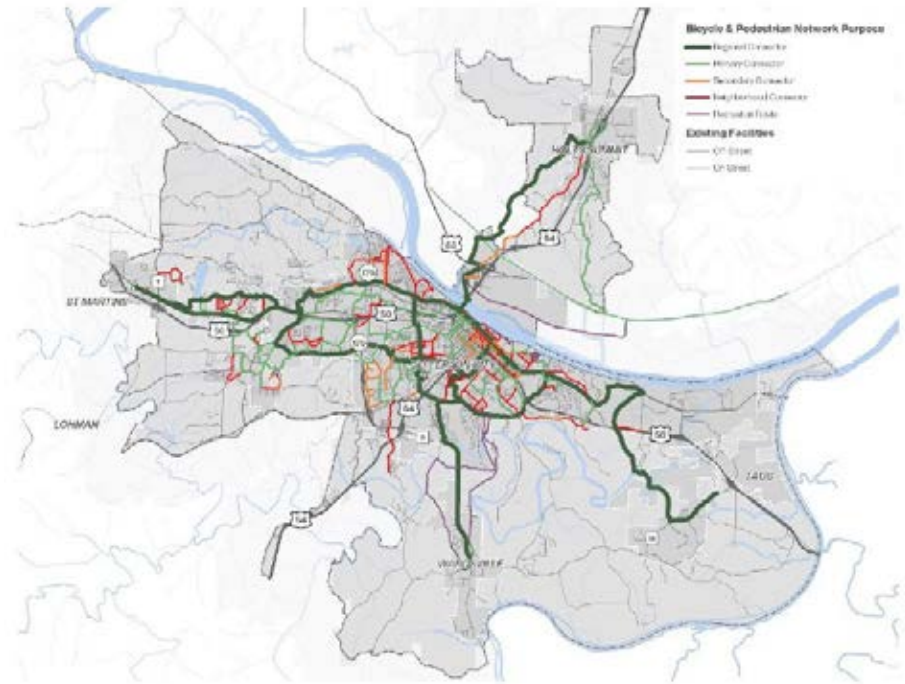
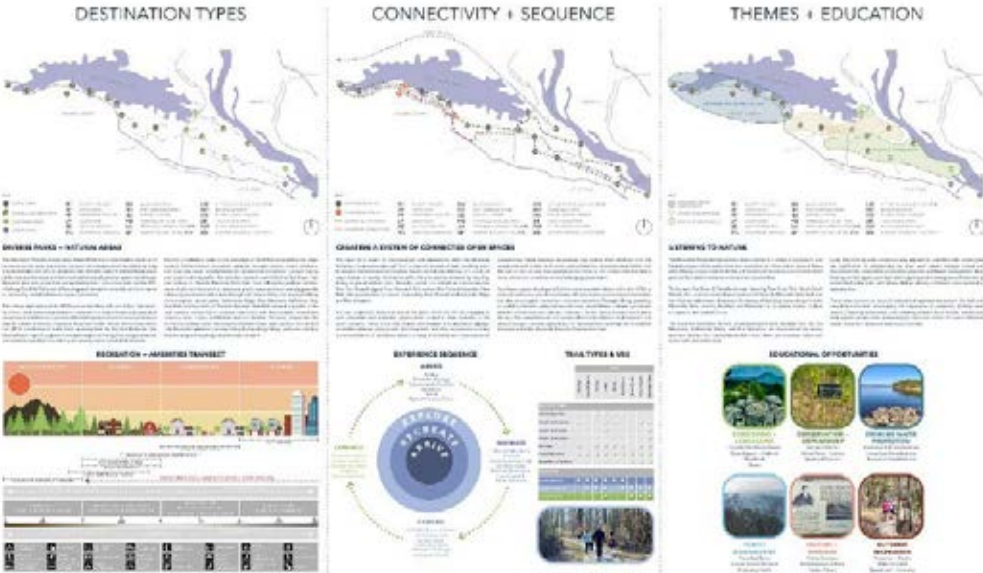


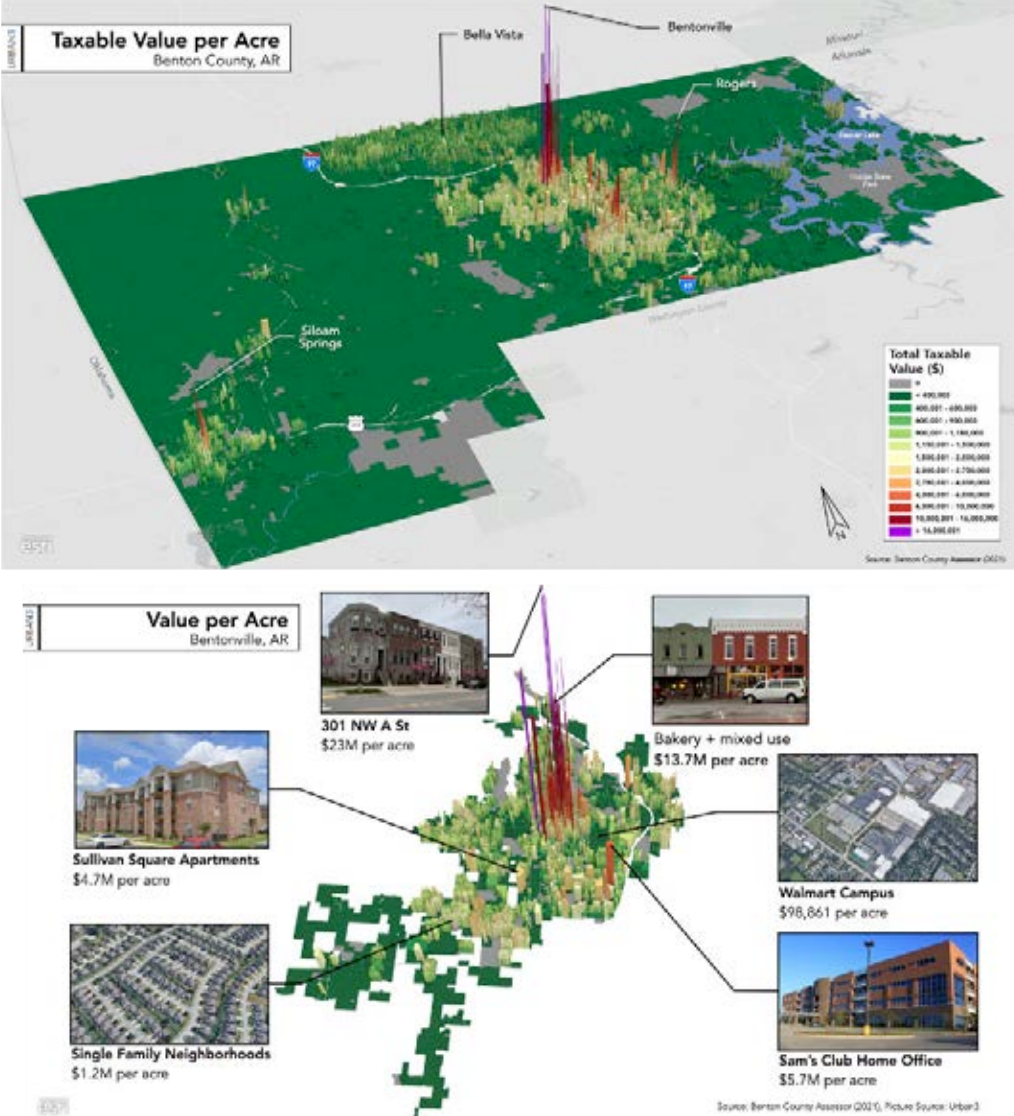
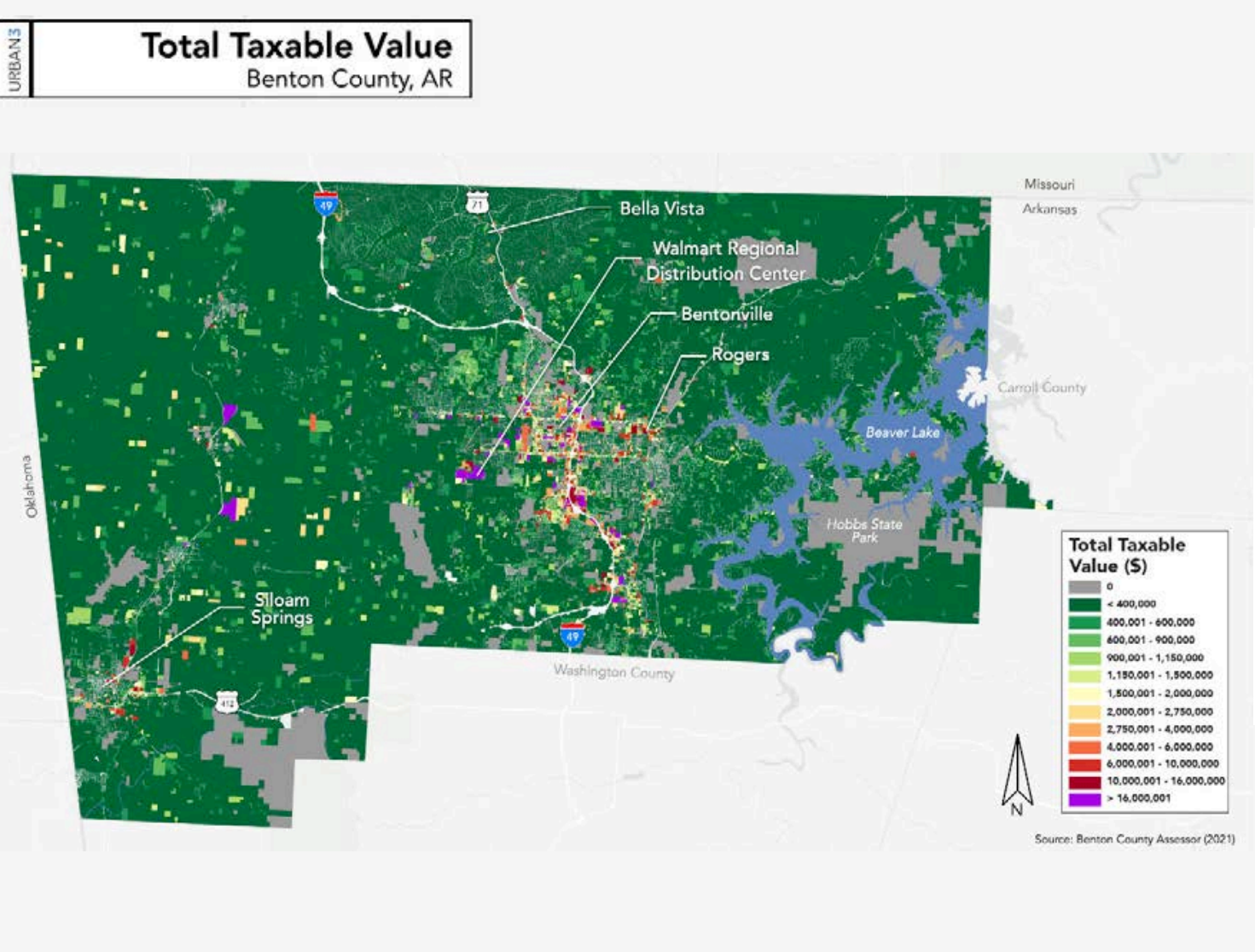
EDUCATION









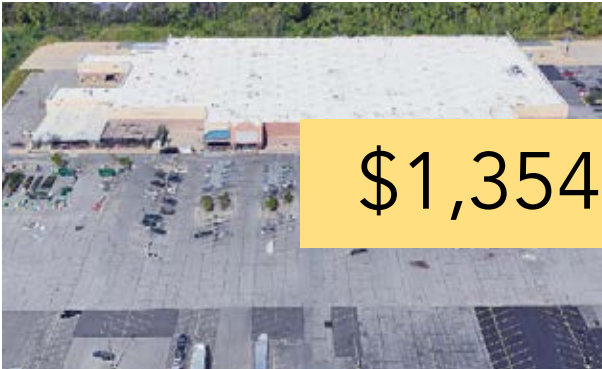
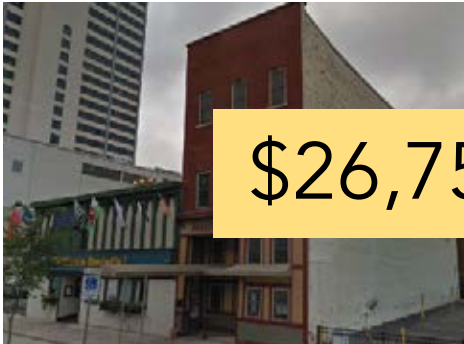

MAUMELLE PINNACLES

OUTDOOR RECREATION VISION MASTER PLAN

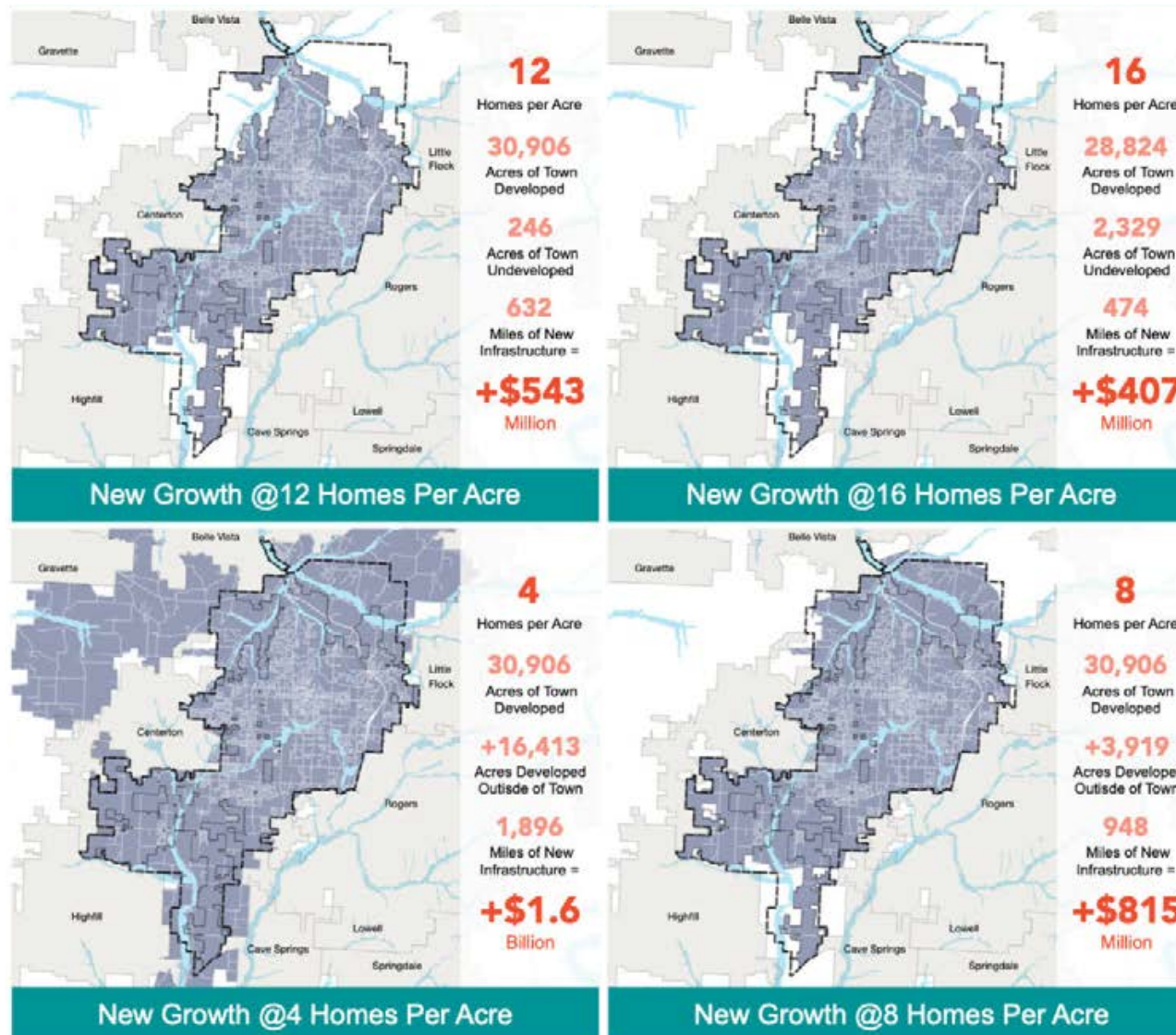




Return-on taxable land and municipal investments

	LOW DENSITY	MEDIUM DENSITY	HIGH DENSITY
RESIDENTIAL	 <p>-\$2,010</p>	 <p>\$1,613</p>	 <p>\$39,369</p>
MIXED-USE	 <p>\$21,663</p>	 <p>\$40,630</p>	 <p>\$142,762</p>
COMMERCIAL	 <p>\$1,354</p>	 <p>\$26,754</p>	 <p>\$155,229</p>

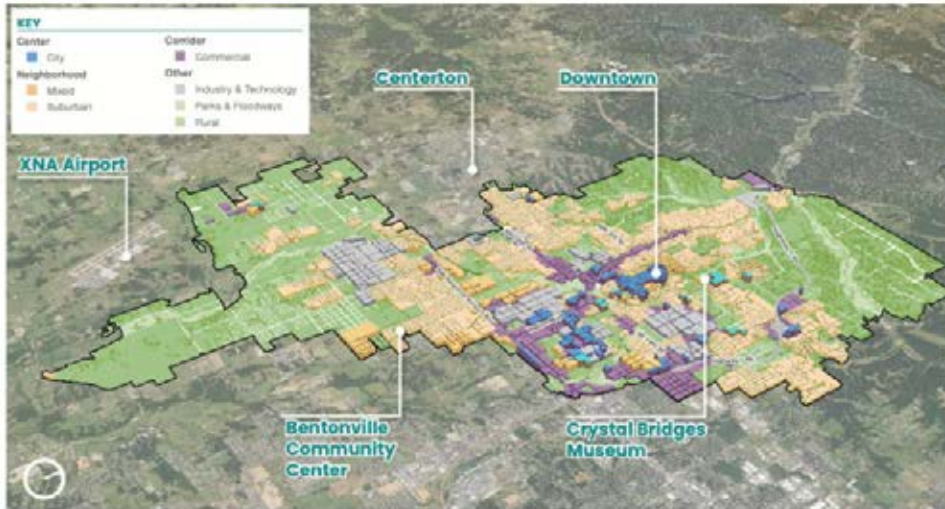
THE COST OF GROWTH



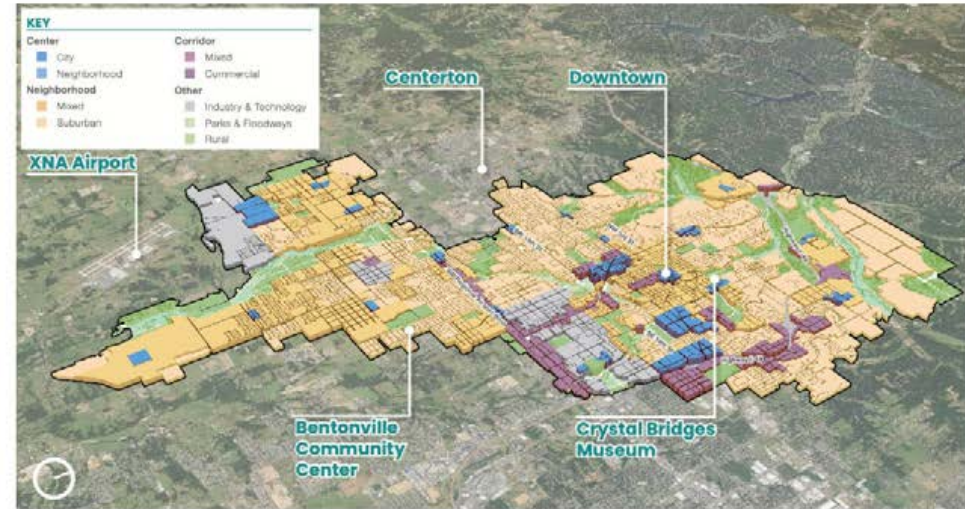
HOW IT COMES TOGETHER: Bentonville Example

GROWTH SCENARIO PLANS

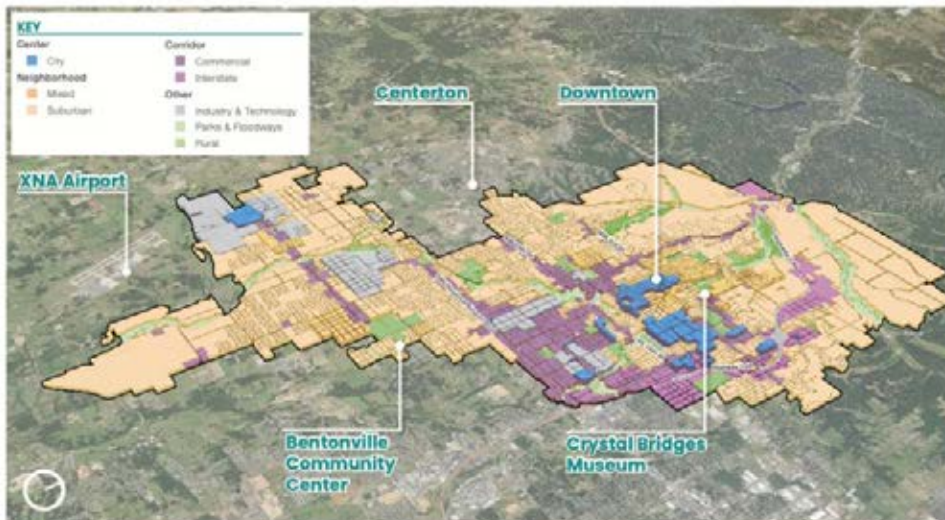
Existing Land Use



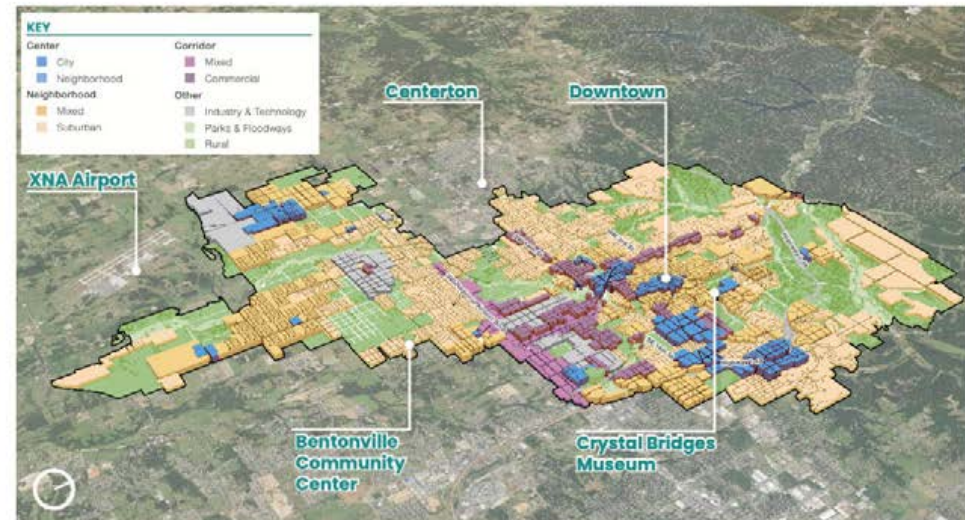
Balanced Growth



Business As Usual

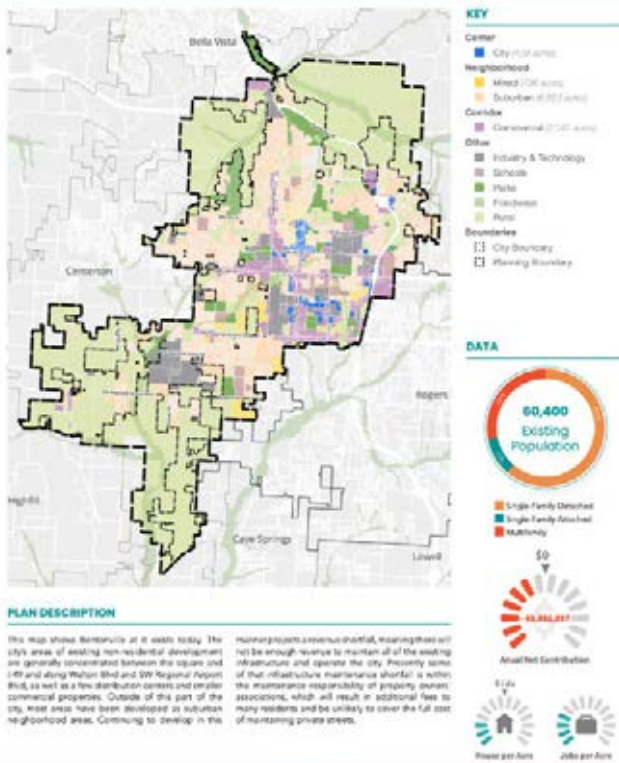


Compact Growth

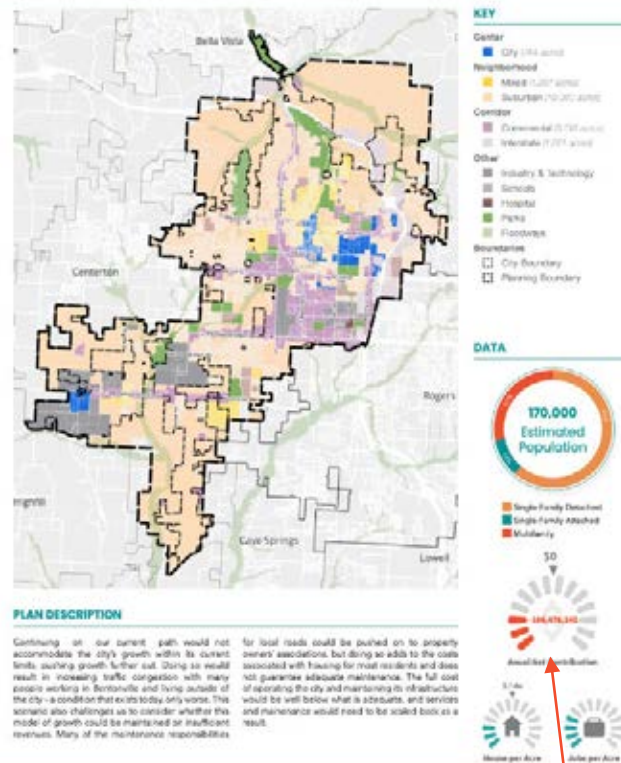


GROWTH SCENARIO PLANS

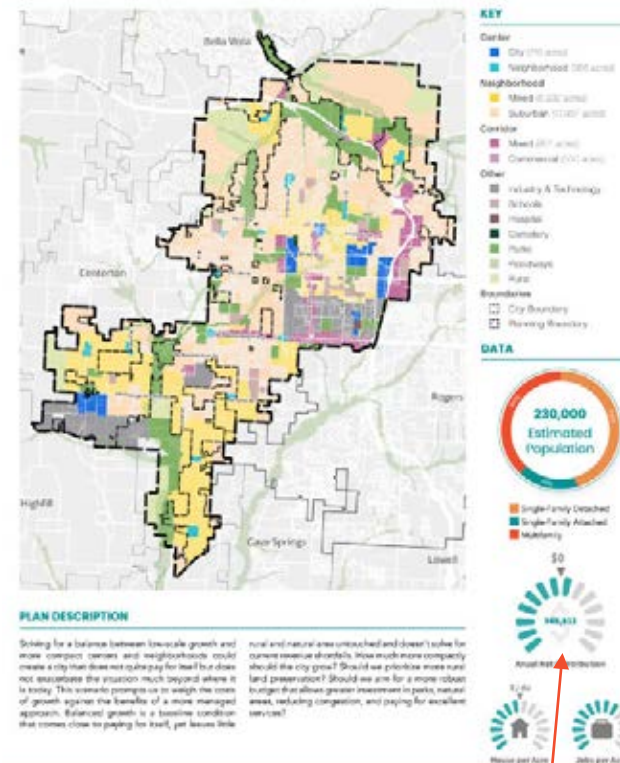
Existing Land Use



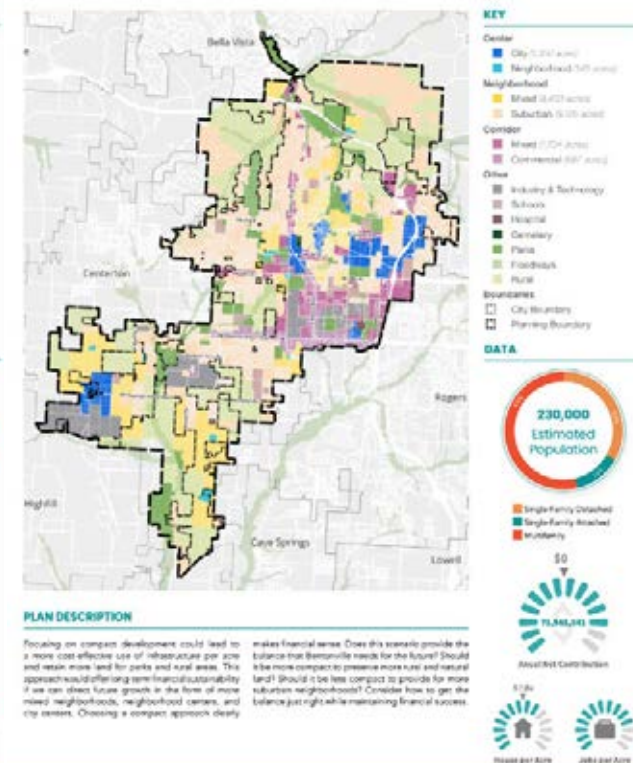
Business As Usual



Balanced Growth



Compact Growth



Fiscal Balance Sheet

MUNICIPAL BUDGET

Managing Growth for Fiscal Sustainability

Operating Bentonville in 2024 costs approximately \$16.824 per developed acre, a figure that will rise with continued growth. The \$209 million budget funds critical services like public safety, infrastructure, parks, and utilities. However, existing revenue streams—sales taxes, property taxes, utility fees, and impact fees—are insufficient to address current and future demands, such as:

- Expanding water, sewer, and power capacity.
- Alleviating traffic congestion and supporting active transportation.
- Addressing regional stormwater challenges.
- Funding additional parks and community amenities.

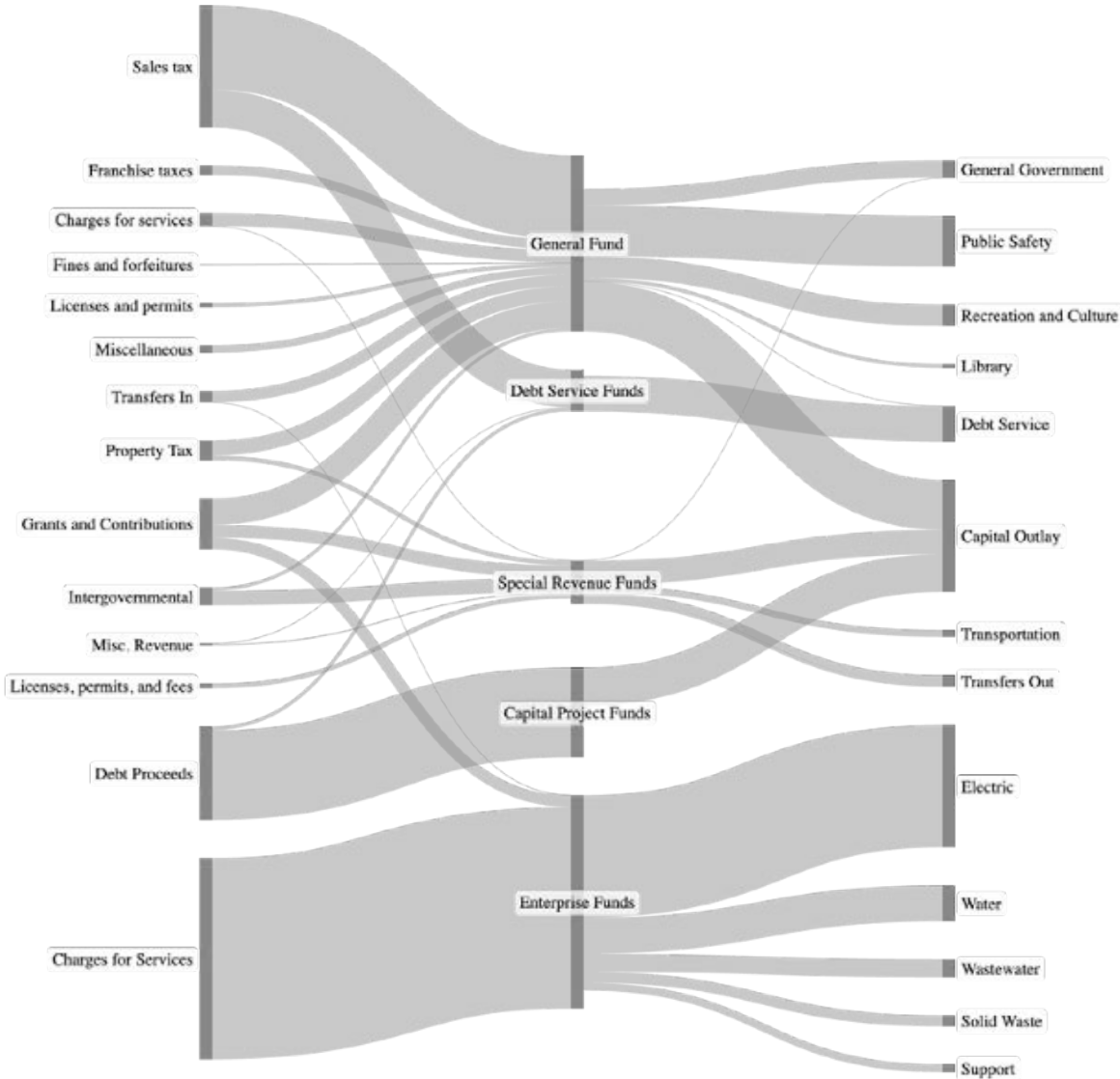
Bentonville's long-term fiscal sustainability hinges on managing growth and land use efficiently to support services without overburdening the budget. Historically, subsidizing low density residential growth with infrastructure investments—roads, sewers, parks, and schools—has stretched resources thin. These patterns assume residential developments cannot pay their way and rely on sales tax contributions. In contrast, townhomes, mixed-use, multi-family housing, and commercial developments typically contribute more

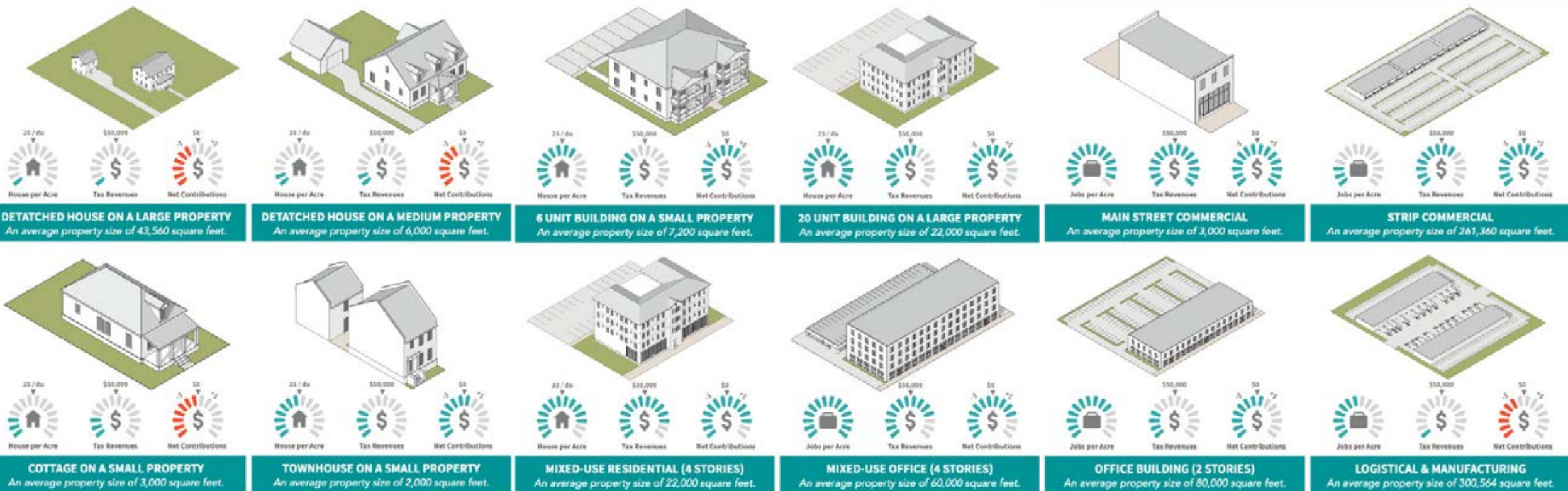
than they consume in resources, offering a path to bridge funding gaps.

As the city prepares for substantial population growth, the current trajectory of subsidized low-density expansion is unsustainable. To ensure a fiscally resilient future, Bentonville must adopt strategies to balance land uses, support self-sustaining growth, and generate surplus revenue to fund unmet needs.



Source: City of Bentonville





Running Bentonville costs \$16,824 per acre, per year

Who is footing the bill?

Housing Supply and Demand



Home Ownership Rate

Today, Bentonville has about the same number of owners as renters, many of whom are renting single family homes. Many of those renters would like to rent an apartment, but there are not enough available. And some of the home owners would prefer to own a townhouse, but there are not enough available. Aligning the demand for housing with the supply, and adjusting new construction to achieve balance can help correct some causes of increased cost, where there are both people who would like a smaller place to live at a lower cost but can't find it, and they are instead occupying a larger home, reducing the number that are available to their ideal demographic.



Existing Supply of Homes



Annual New Home Potential



Oversupplied
& **NOT** footing the bill?



Undersupplied
& **IS** paying a fair share

Supply and Demand for Homes by Type and Ownership

Are we allowing the market to operate?

NEIGHBORHOOD CENTER



Anticipated building height: 3-5 stories

Neighborhood Centers serve as the physical glue between neighborhoods — providing a human-scaled destination where surrounding neighbors can walk or bike, gather in fellowship, dine out, or pick up basic daily needs. Small, local businesses typically provide a variety of entry-level jobs, a sense of increasing independence for area youth, and an opportunity to support more home-grown businesses. They usually also include nearby townhomes, small apartment buildings, and small homes transitioning in scale and activity to less concentrated surrounding neighborhoods. Neighborhood Centers contribute sales tax and concentrated property tax to the city's budget, balancing nearby neighborhoods.



WALKABLE NEIGHBORHOOD

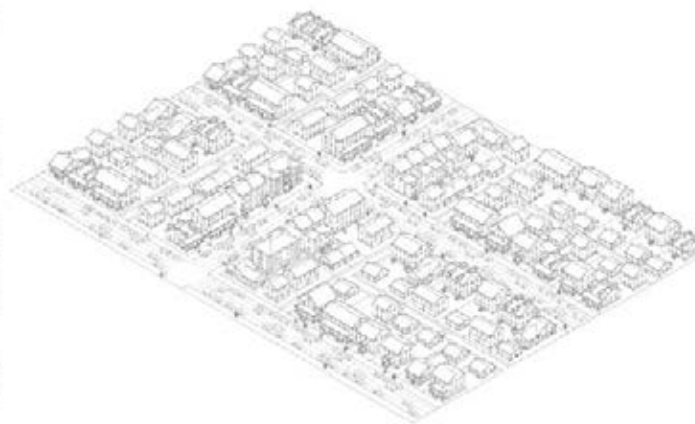


Anticipated building height: 3 stories

Walkable Neighborhoods combine compatibility-scaled single family homes, townhomes, and apartment buildings in a neighborhood setting.

They provide a high degree of housing diversity, allowing for more people of different circumstances to share the same neighborhood. They also support some small neighborhood shops and are often near Neighborhood Centers and parks, making it easy to walk and bike for frequent trips.

Financially, mixed neighborhoods don't fully cover their costs, but by providing housing in a compact footprint, they reduce the amount of land needed for growth.



Growth of these land uses **PAYS A SURPLUS**

Growth of these land uses **IS SUBSIDIZED**

TRADITIONAL NEIGHBORHOOD

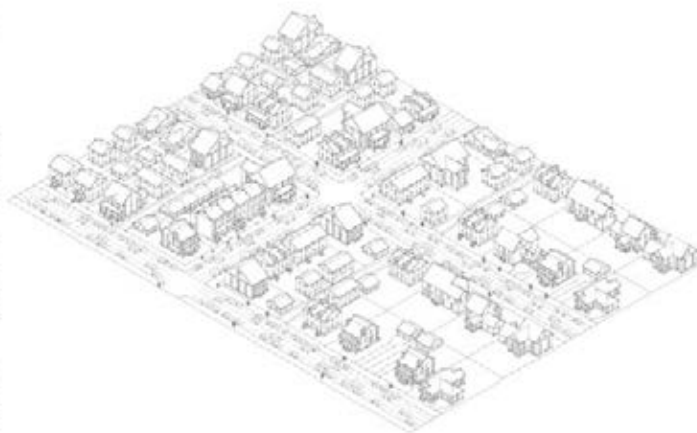


Anticipated building height: 2.5-3 stories

Traditional Neighborhoods serve as a bridge between denser, more mixed-use Walkable Neighborhoods and more dispersed and automobile-oriented Suburban Neighborhoods. As such, they merge features of both.

Characterized by gridded, walkable blocks of single family homes, townhomes, and small-scaled apartment buildings, they provide diverse housing opportunities while easing the way towards areas of single family homes only.

Lots are formally structured but grow incrementally larger as you move outward, maintaining the viability of walking and biking for many trips while contributing to a more relaxed residential character.



SUBURBAN NEIGHBORHOOD



Anticipated building height: 2.5-3 stories

Suburban Neighborhoods, often referred to as subdivisions, consist mainly of single family homes on comparatively larger lots, some including accessory dwellings within the main house volume or in a detached building. They are generally quiet areas more removed from most commercial amenities. Family-oriented, often with large concentrations of children, suburban neighborhoods are often preferred by growing families. Because houses are on larger properties, buildings are more spread out and car trips are required for most needs.

Suburban Neighborhoods don't cover their own costs, but they provide housing needed for a large segment of the community, including families raising the next generation of Bentonville residents.



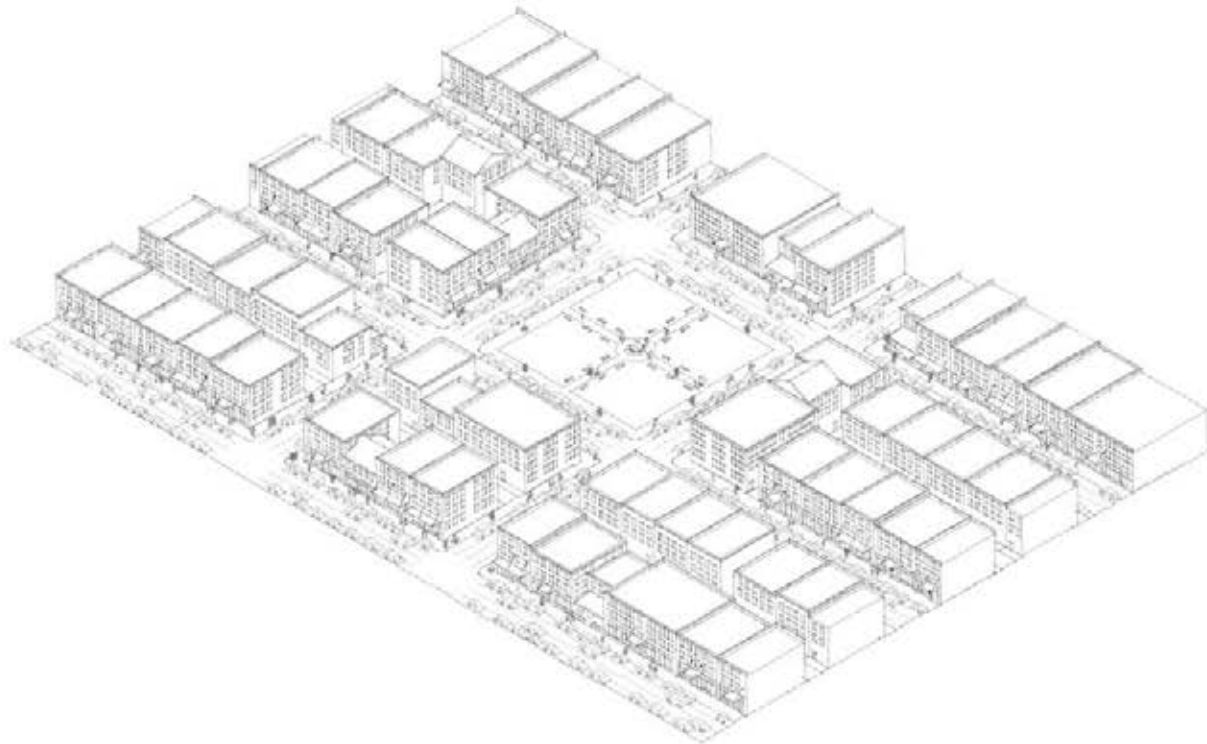
Investing in Downtown **PAYS DIVIDENDS**

CITY CENTER



Anticipated building height: 4-6 stories

City Centers are places like the Downtown Bentonville Square and its surrounding blocks, consisting of jobs, entertainment, convenience retail, daily services, and housing at a variety of scales — from single story shops to office and residential buildings. This combination of types and uses, from sidewalk lining buildings to civic institutions like churches and city hall, to apartments, condos, and townhomes, makes for the city's most active, compact and walkable space. In the future there may be more than one City Center providing convenient access to additional amenities, particularly in the southwest, and reducing the growth pressure around the city's beloved heart. With a high concentration of commercial along with housing, City Centers contribute substantially to the city's budget.



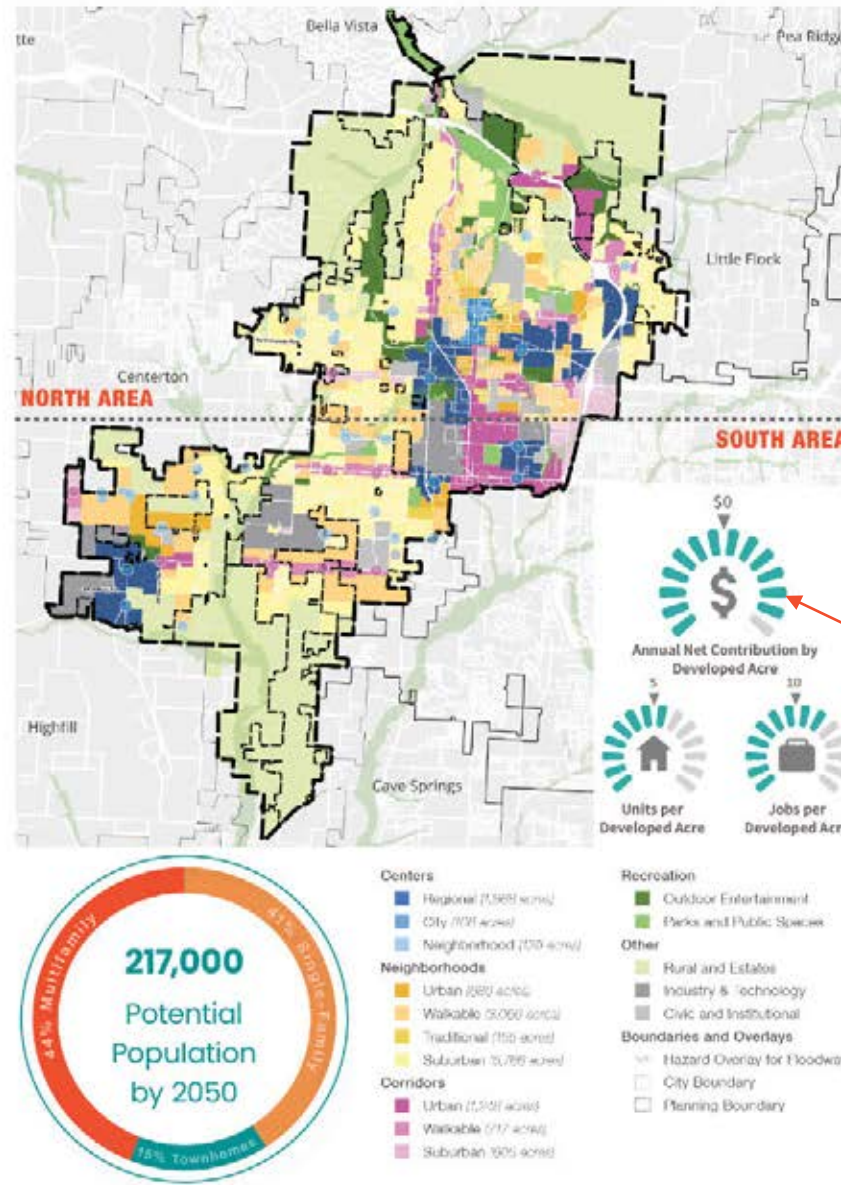
Strategic and Balanced Growth

To ease pressure on downtown and encourage walking and biking, the plan establishes multiple activity hubs, including a major future employment center near Northwest Arkansas National Airport. These Regional Centers will boost the city's economic base with spaces for cultural venues, entertainment, diverse jobs, and higher-density housing in areas away from the airports. Within residential zones, smaller Neighborhood Centers provide convenient retail and gathering spots, fostering a network of community-focused amenities within easy walking, biking, and driving distance for most residents.

Strategically planned infill and compact growth in new areas aim to enhance Bentonville's fiscal health by minimizing infrastructure costs and creating a budget surplus to reinvest in city services, infrastructure, and recreation. With projected growth nearing 220,000 residents, the Future Land Use Plan addresses fiscal sustainability by balancing growth density and housing diversity.

Balanced growth, as outlined in the plan, supports a sustainable fiscal outlook, reduces downtown congestion, promotes alternative travel options, and diversifies housing to meet demand. The Community Plan targets an average new growth of about 13 units per acre, blended between new, high-density places like Regional Centers, medium-density places like Urban Corridors, and low-density places like Walkable Neighborhoods, while discouraging growth in Suburban and Traditional Neighborhoods and Rural areas. Each Place Type contributes to the city's economy, housing, and character, underscoring the need for comprehensive, balanced growth and a diversity of environments for residents, providing a high quality of life and accessibility to housing, jobs, and mobility for future generations.

Preserving rural character remains a key priority. The plan encourages relatively higher-density development within the city's core and hubs, reducing the need for outward expansion. While the plan can accommodate population growth beyond current projections, compact, fiscally responsible growth ensures Bentonville retains its rural surroundings and strong fiscal outlook, meeting population demands while staying adaptable to future challenges.

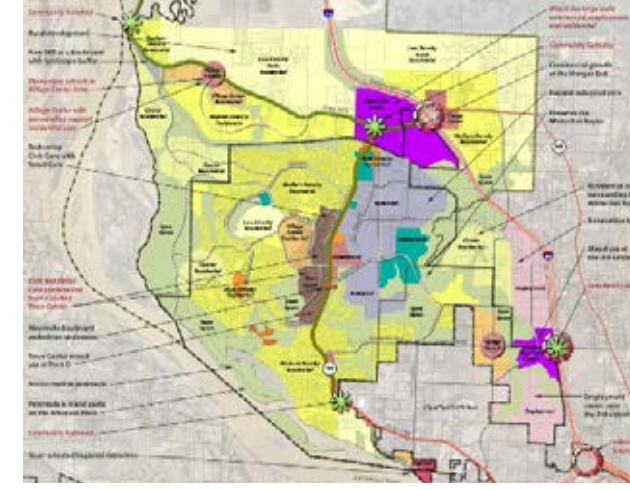
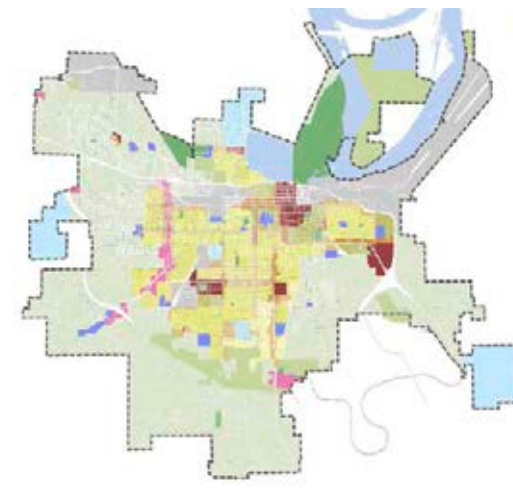
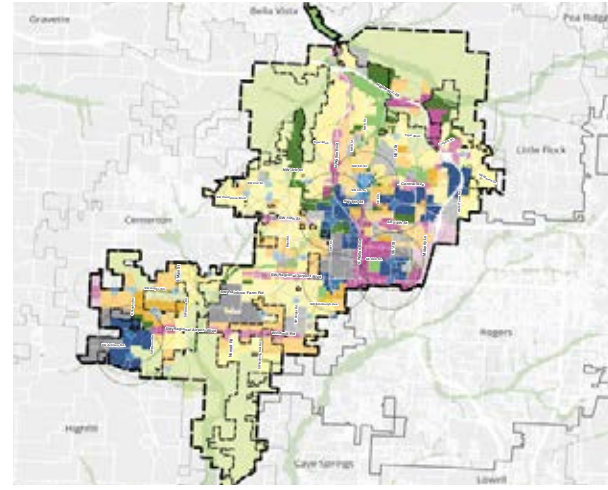


Strategic growth is
SOUND FISCAL POLICY

Pays for:

- City services including Police & fire
- Schools
- Parks & festivals
- Streets
- Utilities

HOW IT COMES TOGETHER: Bentonville Example



Miami 21, FL

Plan Bentonville, AR

Pine Bluff Comprehensive
Plan, AR

Maumelle Strategic Plan, AR

Work Plan Phases

Project website, interviews, focus groups, survey, and mapping

Background analysis and initial input summarized

Charrette process: input + proposals

Charrette report-out and plan direction evaluated

Plan draft creation and reviewed, survey & web

Plan finalization, comment period, final revisions and adoption process



Phase 0: Ongoing
Engagement

Community Engagement:

Phase 0: outreach, brand, messaging, website, communications, social media

Phase 1: 1-on-1 interviews, steering committee, ward meetings, public survey

Phase 2: on-site visioning charrette, pop-ups, public meetings, public survey

Phase 3: planning commission and city council meeting support

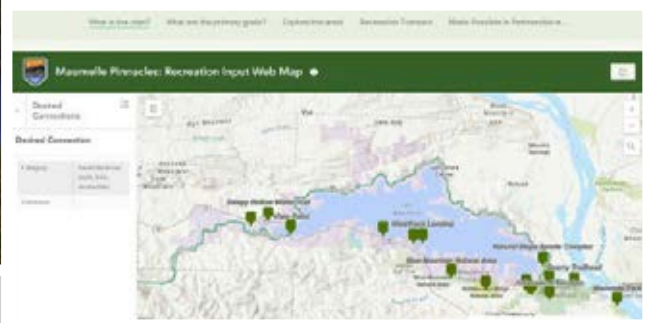

Community Engagement: open houses, pop-ups, walk audits



Online Community Engagement: information, surveys, mapping

Outdoor Recreation Vision Master Plan: Maumelle Pinnacles


The Maumelle Pinnacles Recreation Master Plan is the bold vision for conservation, recreation, and adventure in Central Arkansas




Springdale Trails Feasibility Study

City of Springdale • Crafton Tull
December 3, 2024

Project Description Active Transportation Facility... Project Interactive Web Map Study Area Scoring




cityofsearcy We had a great night planning for Searcy's future as we reviewed the work Crafton Tull has been doing and received even more feedback from community members! Thank you to everyone who came out for the Active Transportation Public Meeting.



Jefferson City Parks Accessibility & Inclusion Plan


Help us make Jefferson City's parks more accessible for everyone!

Jefferson City • Crafton Tull
February 15, 2025




Ellis Porter Park, Pool, and Amphitheater

When accessed for the 2018 Master Plan, Ellis Porter Park had an ADA access within the pool area, over the amphitheater. The amphitheater has since been updated with access ramps to the stage and seating area, and the pool now has an ADA-compliant lift.



City of Searcy
Mar 7




armoneyandpolitics.com
Searcy to Move Forward With \$93M Transformation Plan

43 4 comments 5 shares

SHOWING: 961 of 961 Responses

PAGE: 3 of 13

5. What do you think are the biggest barriers for biking and walking in El Dorado? Select all that



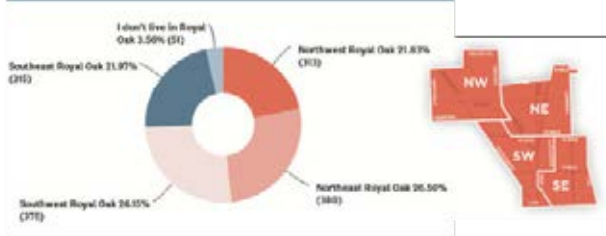
Skipped: 48 Answered: 913

- Speeding vehicles and traffic
- Lack of sidewalks and trails
- Poor sidewalk and infrastructure conditions
- Sidewalks do not lead where I need to go
- Steep hills make traveling difficult for me

Online Community Engagement: information, surveys, mapping

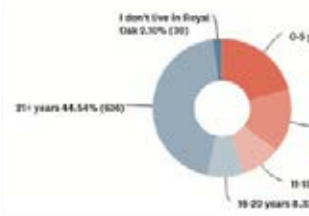
Survey #1 Analysis Survey Overview

Question 2 - Which district of Royal Oak do you live in?



- By age, the distribution is relatively equal across districts.
- By quadrant, respondents are equally divided throughout the city, with slightly more from the Northeast and Southwest.
- By tenure, respondents are roughly equally divided throughout the city.

Question 3 - How long have you lived or worked in Royal Oak?

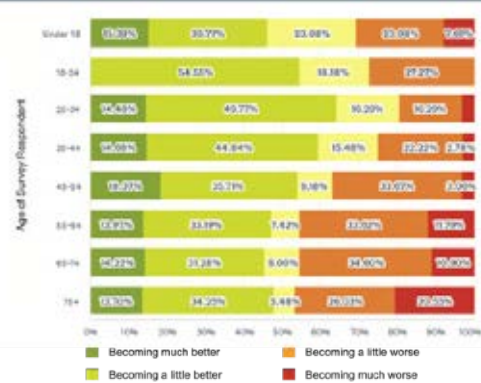


- By age, most respondents between 18 and 34 have lived in the city less than 10 years, but also shows a significant amount of residents at the 5 and 10 year mark.
- By quadrant, about 20% of respondents have lived in the city less than 10 years.

© 2022 DPZ CoDesign | Royal Oak | Draft 07/13/23

Survey #1 Analysis Survey Overview

Question 40 - Overall, Royal Oak is:



Survey #1 Analysis Getting Around

Question 29 - Do you feel you have safe and convenient access to libraries, schools, and other community facilities?



- By age, about 95% of respondents report safe and convenient access or are neutral.
- By quadrant, nearly all respondents report safe and convenient access, with a noticeable drop in satisfaction in the Northwest.
- By tenure, newer residents report slightly less convenience than other brackets at 10%.

HOME PARTICIPATE RSVP OCCUPANTS RESOURCES

Participate

The Royal Oak Master Plan process is underpinned by a robust public engagement program to capture residents', property owners', and other invested stakeholders' opinions, preferences, and visions for the community's future. This page provides details on current and upcoming opportunities to provide input into the process – your participation is critical to the Master Plan's success.

[MAP IDEAS](#)[COMMENT](#)[SIGN UP](#)

Participation Opportunities

Summer 2023
Drop-In Workshops

Drop-in workshops will provide an opportunity to meet with planning team members to discuss theme outcomes and preliminary Master Plan directions and recommendations. Specific date and location information will be posted here when available.

Past Engagements

Feb 3 – Apr 16, 2023
Survey 1

Survey 1 was run between February and mid-April, 2023, to gauge broad resident sentiment across a number of subject areas. This information-gathering survey has been briefly discussed with the Planning Commission while in progress. Survey results are being processed and will be shared on this website when completed.

Future surveys will pay particular attention to targeted topics that arise during the prior survey(s) and in-person engagement activities – focused on testing specific concepts in the draft Master Plan.

HOME PARTICIPATE PROCESS HISTORY DOCUMENTS COMMENT

A Citywide Master Plan for 2040

39

Targeted Stakeholders: meetings, webinars, workshops



Leadership

Advisory

Technical

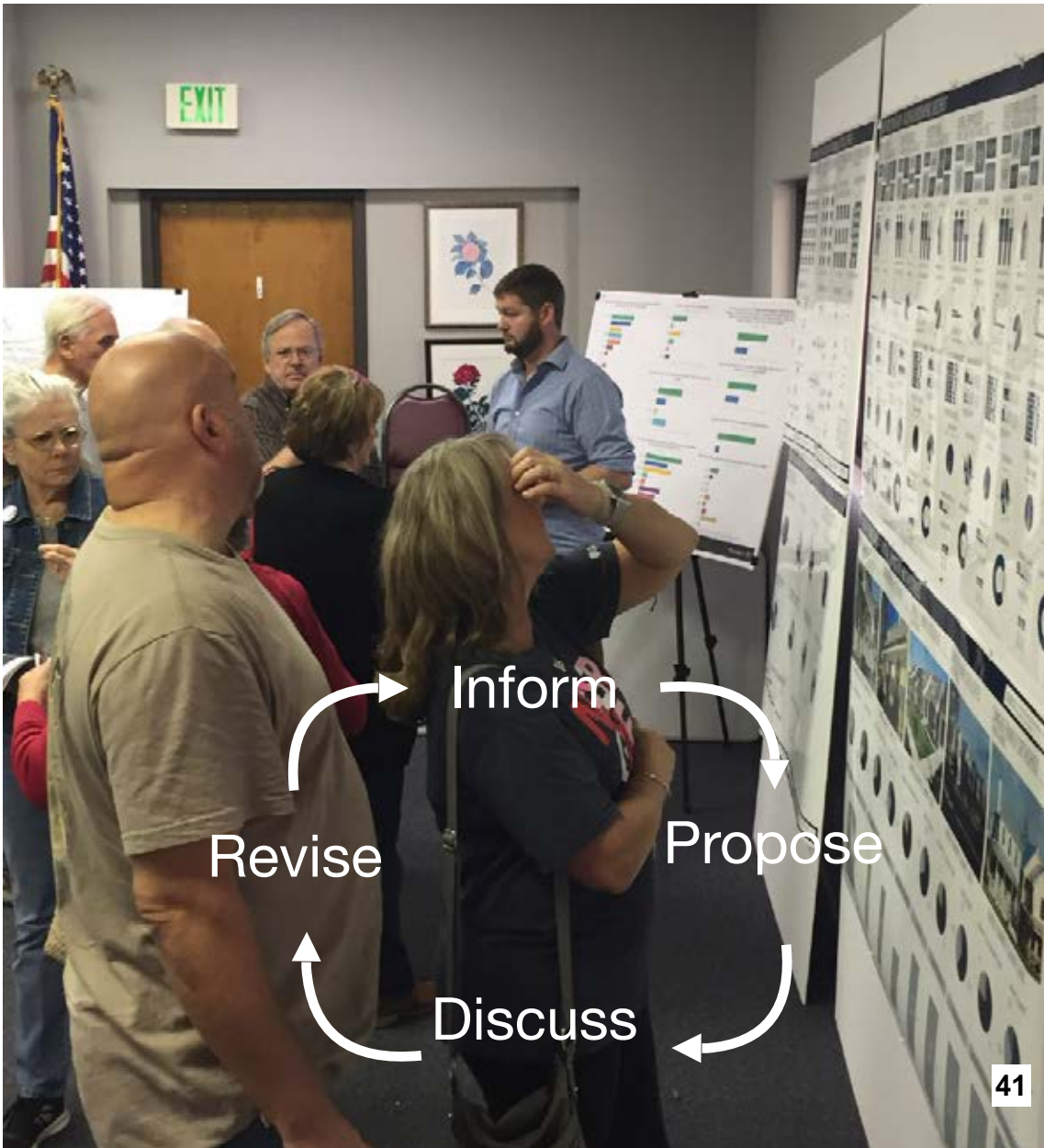
Charrette: on-site meetings, open houses, production, and feedback

What makes a street pleasant to walk along Downtown?		
Tree - shade	Stable, slow traffic	Pedestrian protection from vehicles & noise, quality of materials, sidewalks, & landscaping
Attractive landscaping	Interesting window displays	Less traffic noise & slower
Shade trees and more trees on the street	Most office uses on 1st floor	
	Attractive sidewalks	Make sure there is a physically separate area for pedestrians from the vehicle traffic. Could use bollards or other safety objects

What makes a street pleasant to walk along your neighborhood?		
Who is across the street?	WALK DOWN LAUREN ST. as a pilot example.	Continuity & community character
Need a more thoughtfully designed street	Continuity of the street	Lighting, street furniture, street trees, etc.
the tree canopy!	Continuity of the street	We need places to really enjoy the street

Safety		
Safe - no cars parked	Safe spaces where people can walk	Strong, well-lit sidewalks
Easy	Greater safety on the roads	Safe - no cars parked
More safety	Clear sidewalks	

Walkability & Bicyclability		
Walkability	Walk/Bicyclability	Walkability + Bicyclability
Safe	Walk/Bicyclability	Safe
Safe	Walk/Bicyclability	Safe
Safe	Walk/Bicyclability	Safe



Work Plan Phases

Project website, interviews, focus groups, survey, and mapping

Background analysis and initial input summarized



} 1

Charrette process: input + proposals

Charrette report-out and plan direction evaluated

Plan draft creation and reviewed, survey & web



} 2

Plan finalization, comment period, final revisions and adoption process



— 3

Phase 0: Ongoing
Engagement

Phase 1

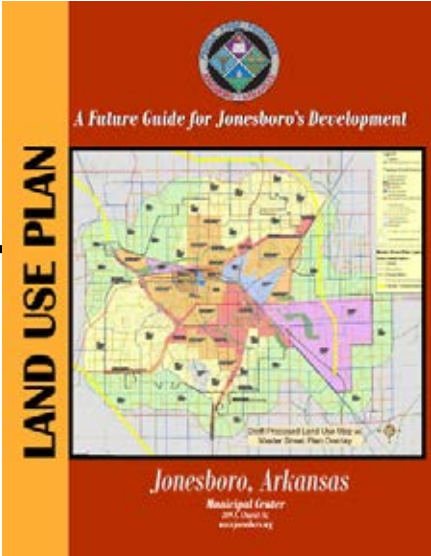
Inventory, Analysis, &
Outreach

Diagnostics: review, coordination, & evaluation of current plans

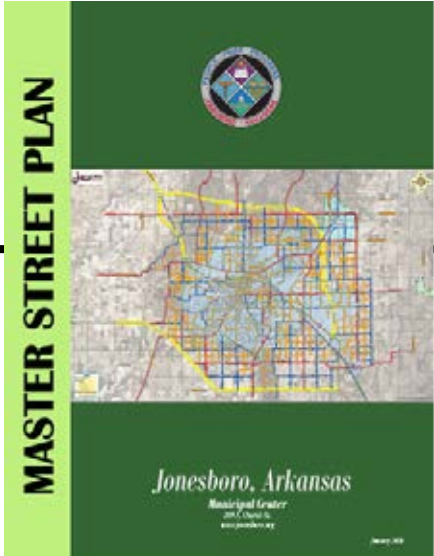
2018



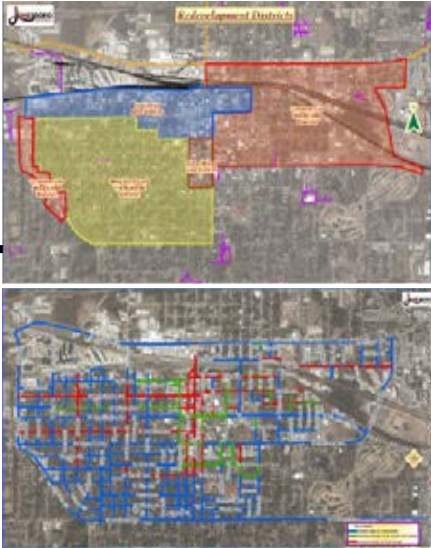
2018



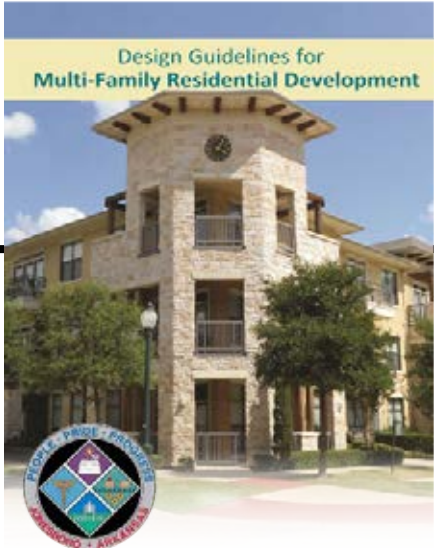
2020



2020



2021



Vision & Goals,
Evaluation,
Coordination,
Shared Direction

Current



Diagnostics: study & evaluation of the built environment and land use

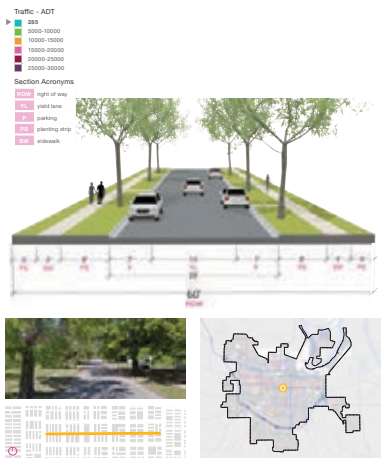
Pedestrian Frontage Assessment

Downtown

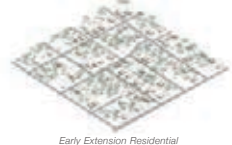


Existing Locals

West 18th Avenue (near South Ash Street)



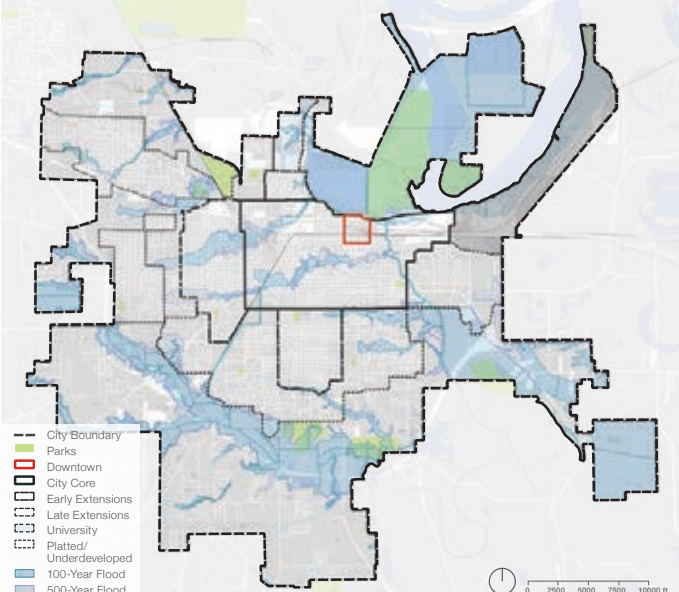
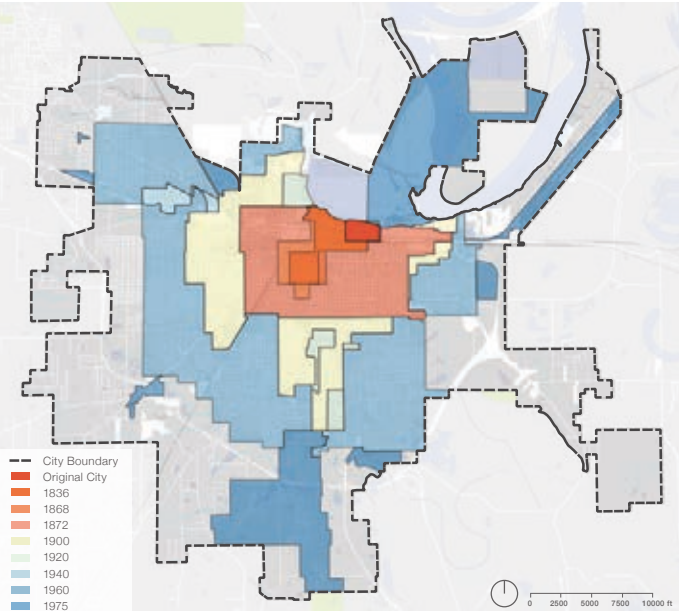
City Core Residential



Early Extension Residential



Late Extension Residential



ACCESS TO COMMUNITY ASSETS

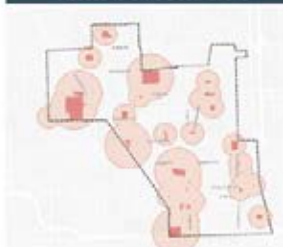
Community Access

This series of maps identifies the locations of key community assets and how near they are to where people live. They address the question of whether people can easily walk to amenities and services. Maps with circular highlighted areas indicate where people are likely to walk to access an asset. The 1/4 mile circle depicts a walk that would take approximately 5 minutes to reach the asset, and the 1/2 mile circle depicts a 10 minute walk. Rather than focusing on the assets within the circles, these maps are intended to identify the areas that lack access, which may be addressed through the master plan.

Access to Parks



Access to Civic Destinations



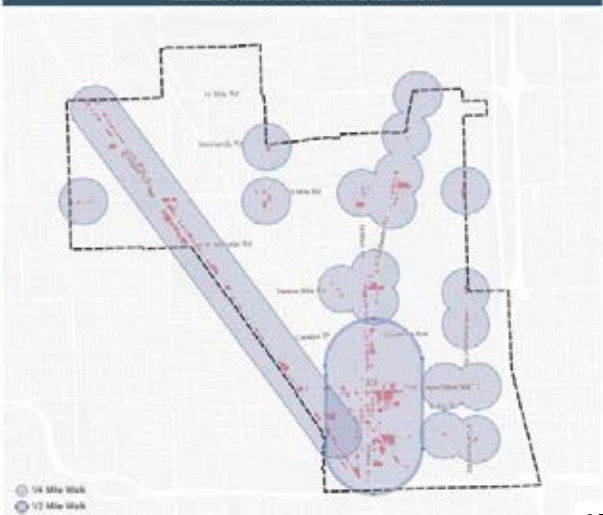
Civic Institutions



Schools



Access to Retail and Service Destinations



Diagnostics: demographics & projections

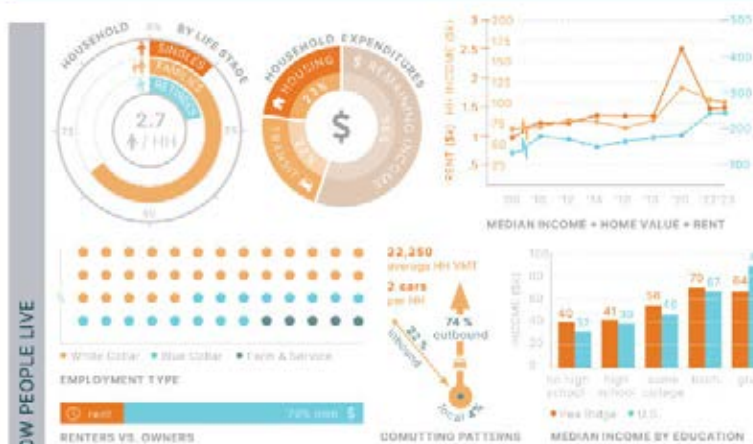
Pea Ridge

Land Area: 7.6 square miles

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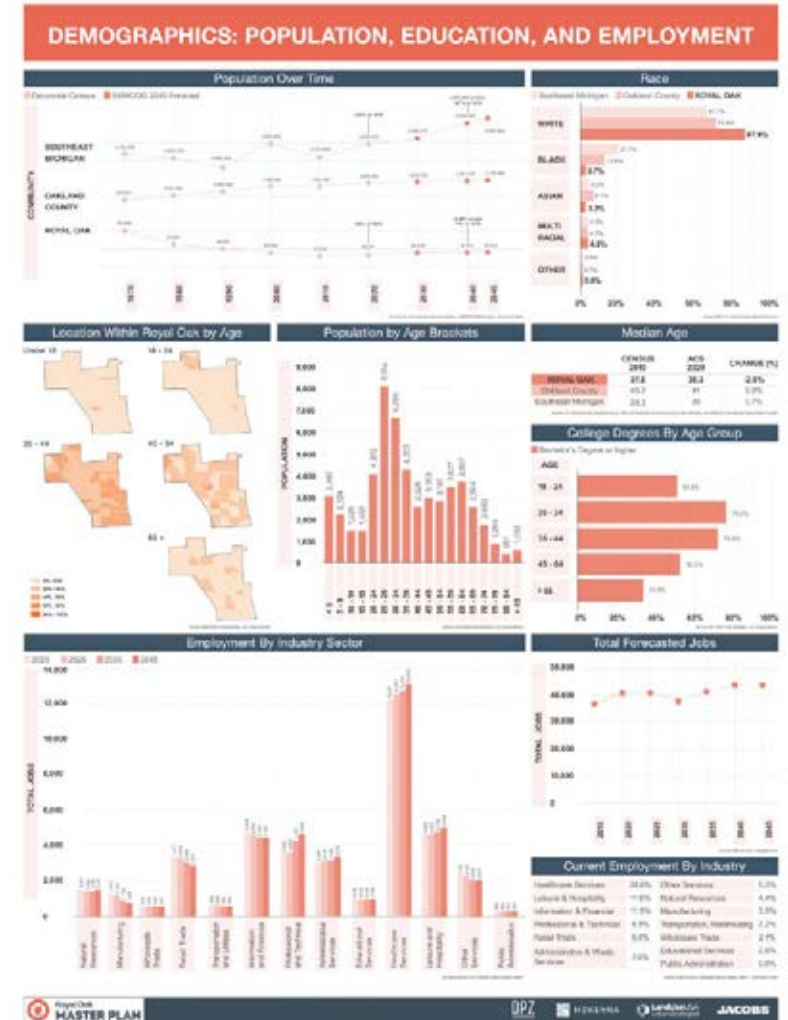
A snapshot of population growth, diversity, and the changing makeup of the city.



Economic insights into local opportunity and cost of living.



How housing stock, density, and development patterns shape the built environment.



Diagnostics: land value productivity

URBAN3

Value per Acre
Bentonville, AR

Source: Benton County Assessor
(2021), Picture Source: Urban3



301 NW A St
\$23M per acre



Bakery + mixed use
\$13.7M per acre



Sullivan Square Apartments
\$4.7M per acre



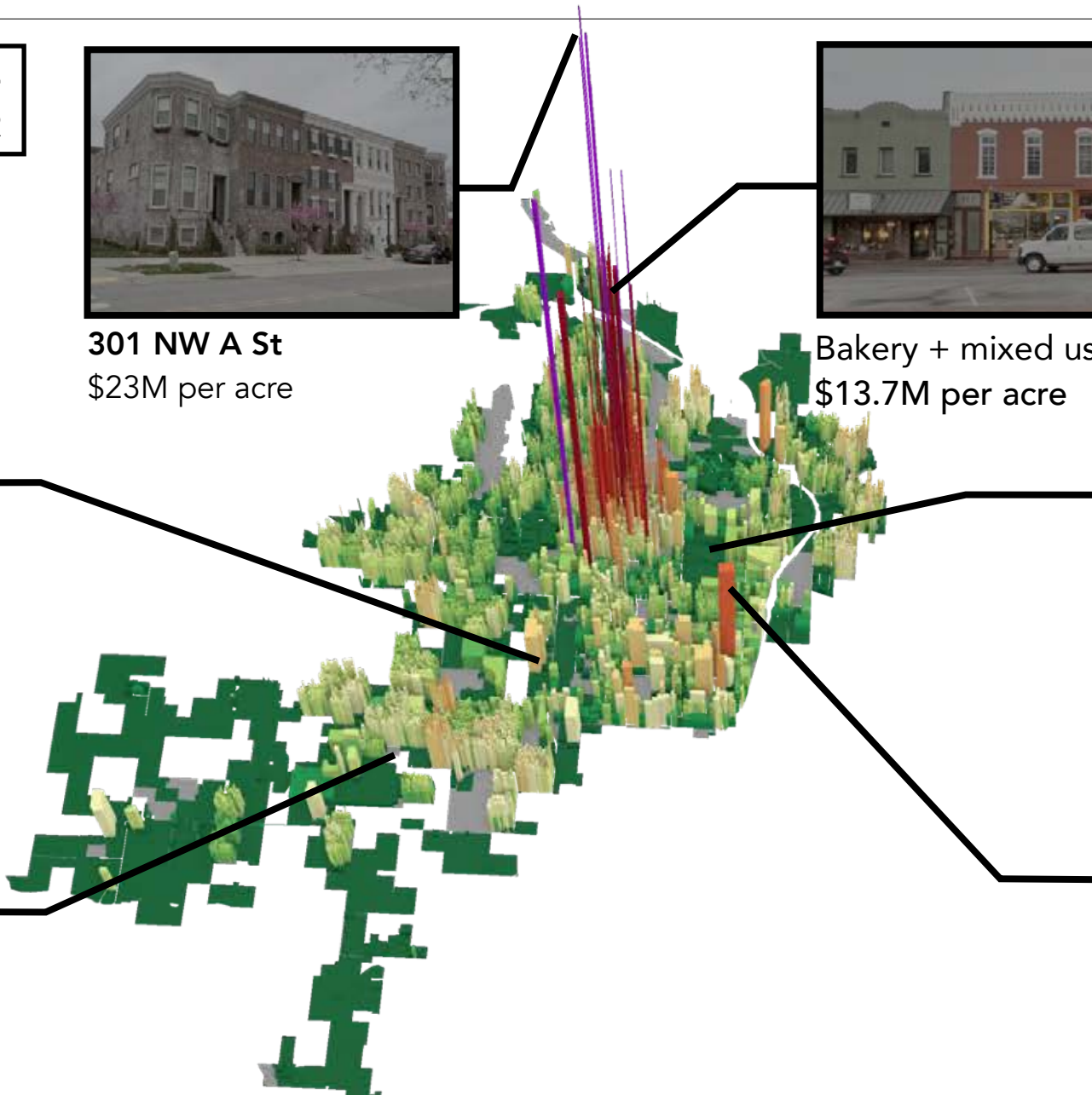
Walmart Campus
\$98,861 per acre










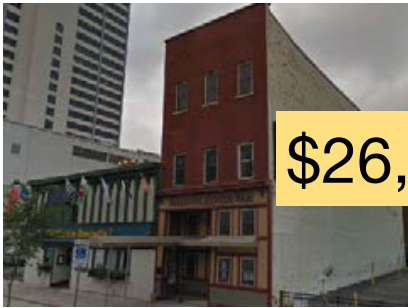
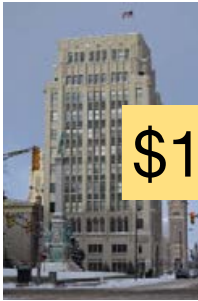
Single Family Neighborhoods
\$1.2M per acre



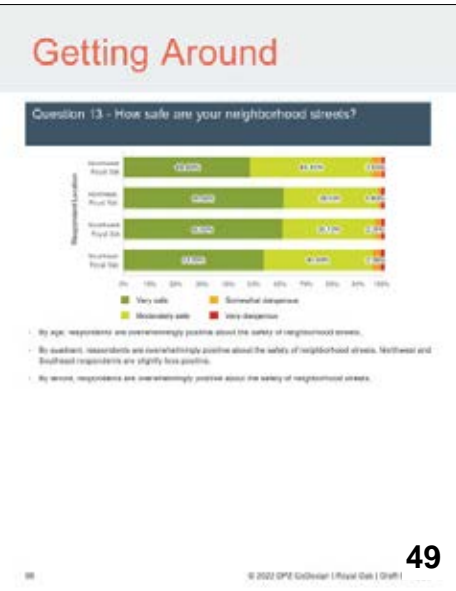
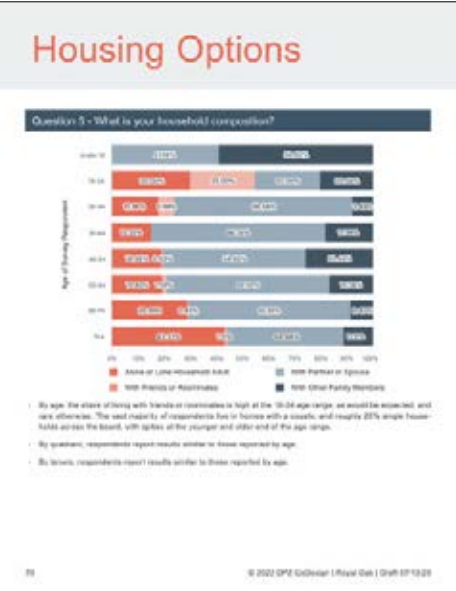
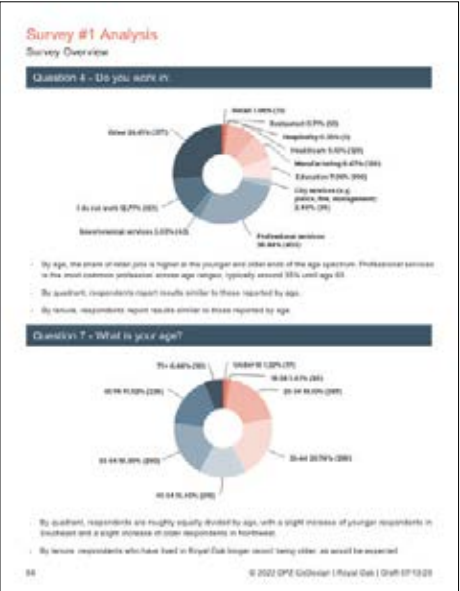
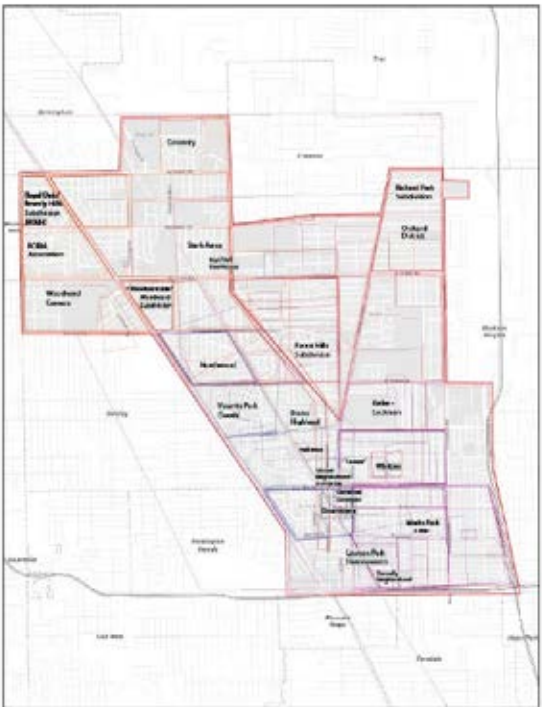
Sam's Club Home Office
\$5.7M per acre



Diagnostics: land value productivity

	LOW DENSITY	MEDIUM DENSITY	HIGH DENSITY
RESIDENTIAL	 <p>-\$2,010</p>	 <p>\$1,613</p>	 <p>\$39,369</p>
MIXED-USE	 <p>\$21,663</p>	 <p>\$40,630</p>	 <p>\$142,762</p>
COMMERCIAL	 <p>\$1,354</p>	 <p>\$26,754</p>	 <p>\$155,229</p>

Engagement: 1-on-1s, ward meetings, public presentation, survey



Public Engagement Overview

Neighborhood Meeting: NW Quadrant

Northwest Quadrant Neighborhood Meeting

Key Themes and Takeaways

Topic	Detail
What do you value about Royal Oak?	<ul style="list-style-type: none">Multigenerational NeighborhoodsRoyal Oak is "livable" navigated by everyoneWalkability (e.g., to Major from 14 and Crooks neighborhood, to Woodward)Lower density housing (single-family and parks - more neighborhood than downtown)Natural areas, small neighborhood parks
Challenges	<ul style="list-style-type: none">People don't feel safe in parking structures, desire camerasAll areas of the city aren't senior friendly, especially with regard to parking accessibilityPrivacy concerns with the height of multifamily structures
Opportunities and Ideas: Neighborhoods	<ul style="list-style-type: none">Protect existing neighborhoods and preserve their characterLimit size of new buildings, when tearing down existing homesProhibit overnight neighborhood parking (2am-6am)Encourage neighborhood friendly housingEncourage grocery stores and walkable necessities (closer to 13 and Crooks)Avoid "cookie cutter" housingADU might be ok as a strategy for aging in place, but with many restrictionsLike to see a range of housing options - costs from low to highKeep small existing homes that cost less than new big homes
Opportunities and Ideas: Transportation	<ul style="list-style-type: none">Provide smaller, more frequent buses - make places accessible with bike + busEncourage bike routes on side streetsProvide better senior transportation

Public Engagement Overview

Neighborhood Meeting: NE Quadrant

Northeast Quadrant Neighborhood Meeting

Key Themes and Takeaways

Topic	Detail
What do you value about Royal Oak?	<ul style="list-style-type: none">Sense of communityTree-lined streets, tree canopy, tree planting programAvailability of parksDiversity of people, diversity of agesQuality of schoolsMoved to live where there's a "Main St."WalkabilityNight lifeCity services (especially Police and Fire)Farmers MarketAccess to multiple downtownsAccess to schoolsInvestment in aging in place
Challenges	<ul style="list-style-type: none">Flooding (perception is it's related to construction of larger homes)Park maintenanceParking structures don't feel safeParking accessibility downtownAffordabilityIncreased height in smaller-scale neighborhoodsLarge houses on small lotsVacancy on WoodwardPlane and scale of change and new development - "too much, too fast"Maintaining a "beautiful" park systemNew development perceived to be putting increased challenges on infrastructureSpending on Crooks (12 Mile - 13 Mile)

Public Engagement Overview

Neighborhood Meeting: SW Quadrant

Southwest Quadrant Neighborhood Meeting

Key Themes and Takeaways

Topic	Detail
What do you value about Royal Oak?	<ul style="list-style-type: none">Cultural preservationTree canopyRegional location and connectivity (proximity to downtown, Woodland, L-686, I-75)Quiet neighborhoodsSense of community, trust among neighborsThe park systemDiversity in residentsDiversity of housingCharacter of single-family homesRD Public LibraryLocal retailWalkabilityBikeabilityCity's focus on sustainabilityCommunity engagement/involvementFarmers marketSchoolsAccess to transit (Amtrak, bus availability at 11 Mile and Woodward)Access to downtown - restaurants/grocery shopsNeighborhood schools - more schools are now closed
Challenges	<ul style="list-style-type: none">Parks (especially newly-developed parks)Current housing diversityRegional locationUnique housing characteristics in established neighborhoodsSafetyCitizen Police Academy, residential academyWalkabilityFarmers MarketSense of communityAccess to retail and services - "Most of what I need is in Royal Oak"Royal Oak feels "community-minded"Local retailWalkabilityBikeabilityParks of south side feel under-invested (e.g., east of Campbell)Parking in downtownTraffic flow and back-up lights on 11 MileOff-street parking accessibilityShort-term rentalsCommercial vacancy (e.g., 4th street and 11 Mile)Cut-through traffic / speed in residential neighborhoodsScale of development - high-rise encroachment into neighborhoodsLack of residential parkingAllow for more townhomesMake sure south and residential in former industrial areas becomes a complete residential neighborhood
Opportunities and Ideas: Neighborhoods	

Public Engagement Overview

Neighborhood Meeting: SE Quadrant

Southwest Quadrant Neighborhood Meeting

Key Themes and Takeaways

Topic	Detail
What do you value about Royal Oak?	<ul style="list-style-type: none">Parks (especially newly-developed parks)Current housing diversityRegional locationUnique housing characteristics in established neighborhoodsSafetyCitizen Police Academy, residential academyWalkabilityFarmers MarketSense of communityAccess to retail and services - "Most of what I need is in Royal Oak"Royal Oak feels "community-minded"Local retailWalkabilityBikeabilityParks of south side feel under-invested (e.g., east of Campbell)Parking in downtownTraffic flow and back-up lights on 11 MileOff-street parking accessibilityShort-term rentalsCommercial vacancy (e.g., 4th street and 11 Mile)Cut-through traffic / speed in residential neighborhoodsScale of development - high-rise encroachment into neighborhoodsLack of residential parkingAllow for more townhomesMake sure south and residential in former industrial areas becomes a complete residential neighborhood
Challenges	
Opportunities and Ideas: Neighborhoods	

Charrette Planning: focus areas and top community goals & concerns



Work Plan Phases

Project website, interviews, focus groups, survey, and mapping

Background analysis and initial input summarized



Charrette process: input + proposals

Charrette report-out and plan direction evaluated

Plan draft creation and reviewed, survey & web



Plan finalization, comment period, final revisions and adoption process



Phase 0: Ongoing
Engagement

Phase 2

Engagement & Plan Preparation

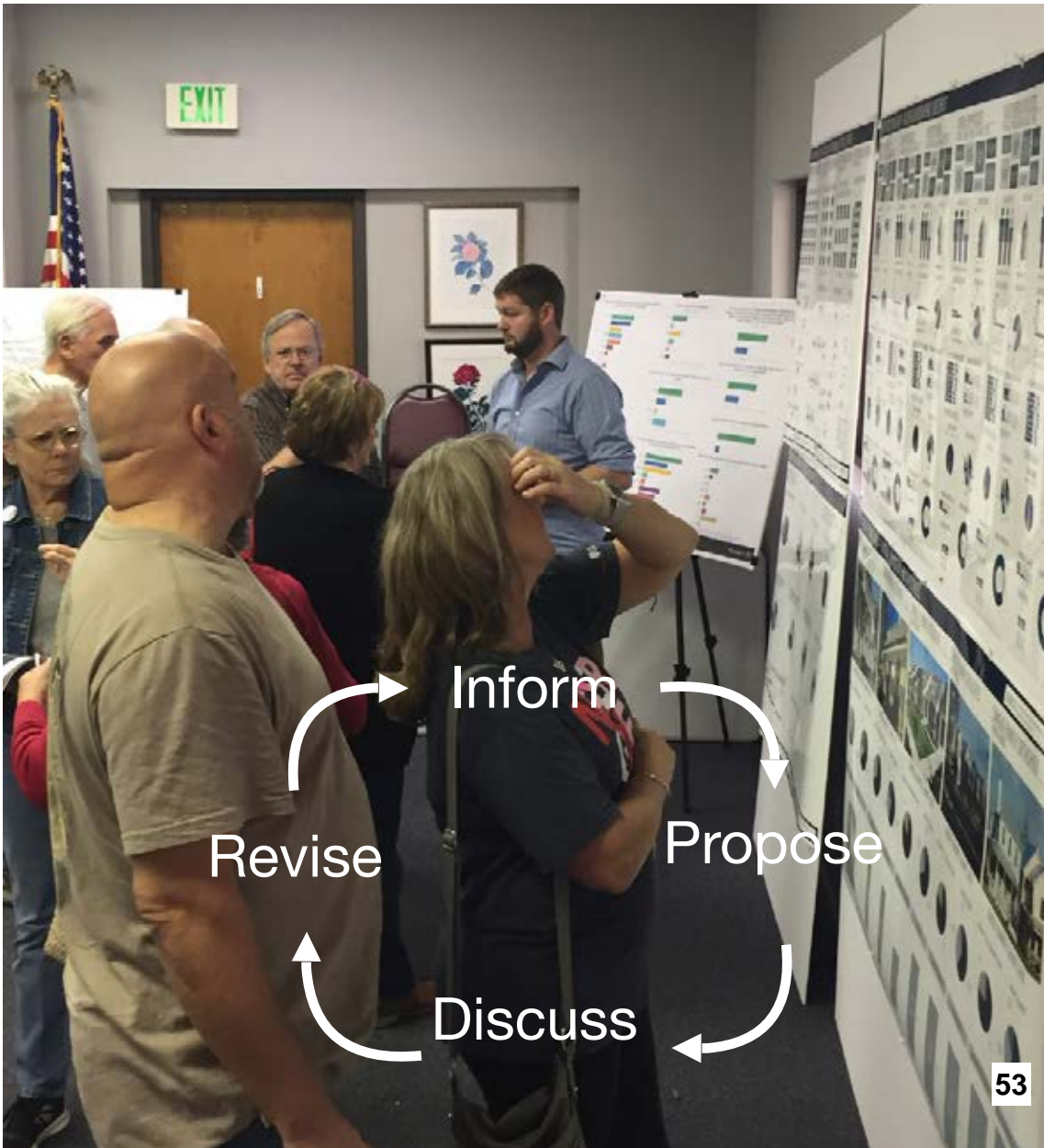
Charrette Process: 7-days of meetings, open houses, & ideation

What makes a street pleasant to walk along downtown?		
Tree - shade	Stable, slow traffic	PEDESTRIAN PROTECTION FROM VEHICLES & HIGH QUALITY LANDSCAPE, UNIFORMITY & BUREAUCRACY
ATCOP - walking	interesting window displays	Less traffic noise & slower
interesting window displays	not office uses on 1st floor	
Shades and trees are great on high	Attractive sidewalks	Make them more of the physically separate from roadways from the urban safety. Could use decorative or urban safety subject

What makes a street pleasant to walk along your neighborhood?		
Who is going to be there?	WALK DOWN LAUREN ST. as a pilot example.	CONTINUITY & COMMUNITY CHANGES
need a small change	CONCRETE sidewalk	Lighting (streetlights) - streetlights should be more frequent
the tree canopy!	sidewalk	we need more to make people of enjoying street

Safety		
SPR - streetlights	Safe spaces where they don't walk	Strong and clear boundaries
Canopy	Greater Safety on the roads	Stronger and clear boundaries
SPR - streetlights	Clear and safe	Stronger and clear boundaries

Walkability & Bicyclability		
Walkability	Walk/Bicyclability	Walkability + Bicyclability
SPR - streetlights	Walk/Bicyclability	Walkability + Bicyclability
SPR - streetlights	Walk/Bicyclability	Walkability + Bicyclability
SPR - streetlights	Walk/Bicyclability	Walkability + Bicyclability



Charrette Process: concept production & public testing



Charrette Process: focus on special project areas & city-wide



Figure 5.35 - Proposed Area Plan Building Types



Figure 19. Haynes Square reconfiguration.



Figure 20. Creating Haynes Square at Haynes St., Old Woodward, and big Woodward.

Preliminary Direction: comp plan strategy review, survey, drop-ins



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CODESIGN

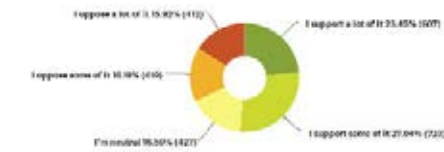
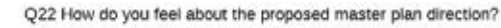
Royal Oak Master Plan Direction

08/31/23



<h3>Place Types / Future and Use Categories</h3>	<h3>Neighborhood Residential</h3>
<p>Future land use categories describe the general character of different portions of Royal Oak should exhibit in the future. These are not zoning, rather they describe the broader characteristics of a collection of properties and as well as public spaces such as streets, parks, and schools, that will be used to create places of different types. The terminology used in this section is temporary and will be adjusted, including more descriptive names.</p>	<p>This place type is characterized by small-to-medium single-family lots with detached buildings set back from the sidewalk, and infrequent, historic duplex and small multi-family buildings, with significant tree canopy including street trees and trees on private properties.</p>
<h3>Neighborhood Edge</h3>	<h3>Neighborhood Edge</h3>
	<p>This place type is characterized by a medium scale</p>

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Q13 At Campbell and 12 Mile, should land use be changed to allow mixed-uses and housing in addition to the uses that are there today?



Downtown Infill Plan



Districts

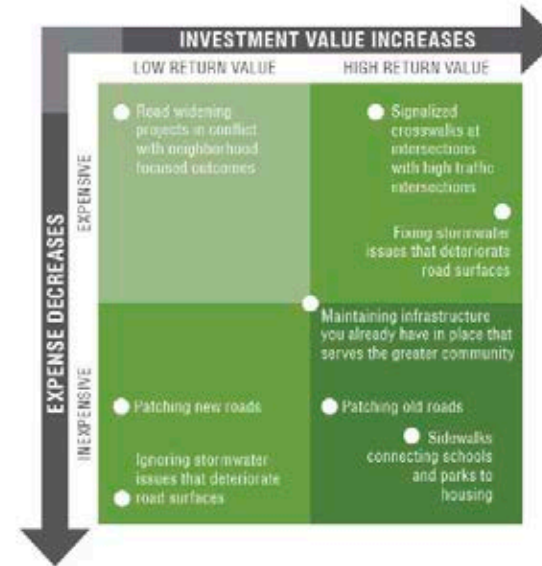
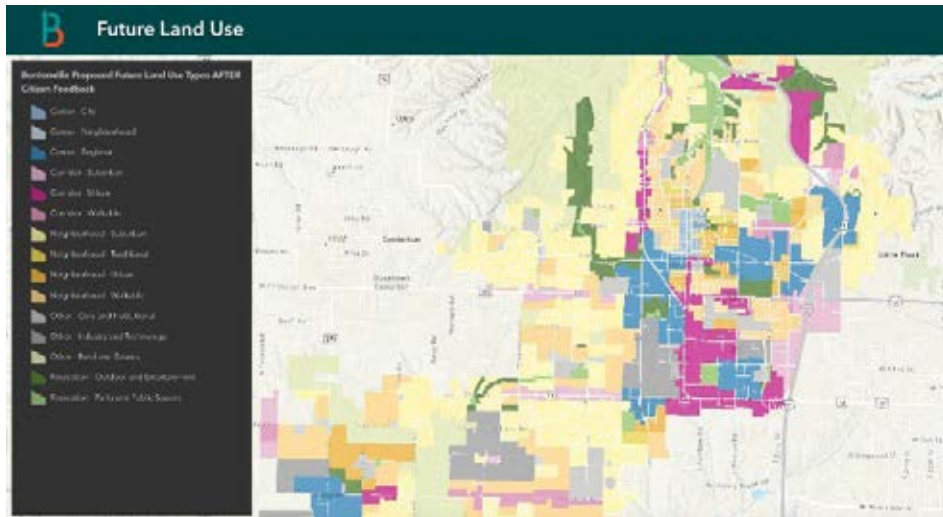


Parking Distribution

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Master Plan Direction
Activity Centers

Plan Production: comp plan production, technical & steering meetings



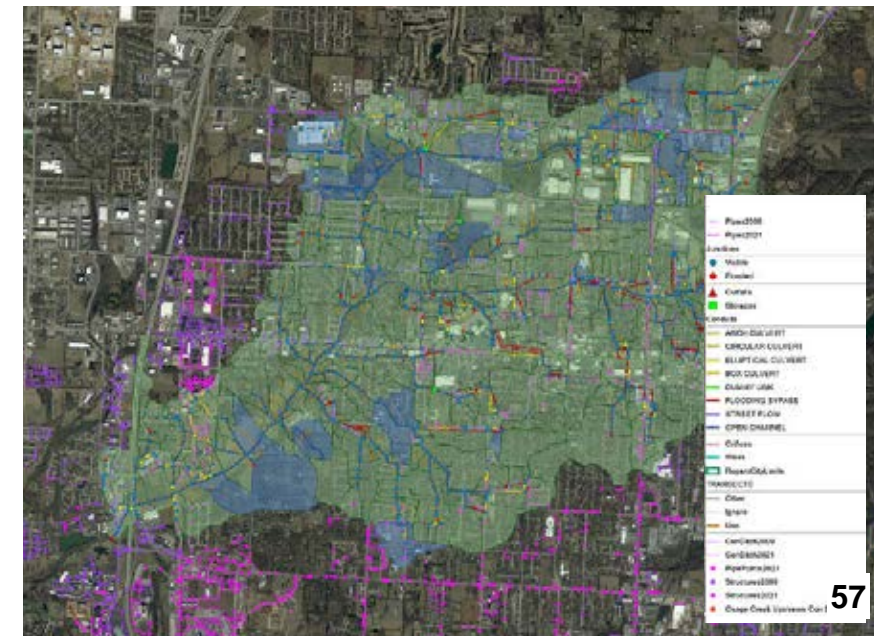
The Downtown City Center is an active and vibrant, relatively high-intensity mixed-use area which is active from the early morning and late into the evening. It includes entertainment, outdoor gathering areas, shopping, dining, residential, and employment uses lining small, regular blocks. Buildings face windows and entrances towards the street and contribute to a welcoming pedestrian environment. The small blocks contribute to the walkable nature of the area. The scale and character of the area continues the traditions of Downtown Rogers.



The Midtown City Center is an active and vibrant, relatively high-intensity mixed-use area, modeled on Downtown Rogers and intended to provide convenient access to goods, services, jobs, and housing by redeveloping existing low intensity commercial uses along Walnut. Redevelopment will require new streets and civic spaces, and will add a tight network of pedestrian and bicycle paths, bringing activity and vibrancy to an area that is car dominated today.



Neighborhood Nodes are small scale and low intensity destinations for neighborhood services, retail, and civic institutions. They occur at the intersection of larger roadways and extend only a few properties from the intersection. Neighborhood Nodes are much smaller in scale than Main Streets, and are relatively low in activity. They provide convenience yet retain the low scale and intensity found in surrounding neighborhood fabric.



Plan Refinement: review, refinement, and implementation strategy

IMPLEMENTATION TABLES

ELEMENT 3: ECONOMIC DEVELOPMENT

Element 3. Economic Development				
Item	Observation Group	Recommendation	Responsibility	Timeline
3.1.1	Recruiting Developers & Investors	Prioritize the key development concepts for implementation.	POA CEO	2018
3.1.2	Recruiting Developers & Investors	Develop an internal marketing package including master plan renderings, market analysis and prospective pro forma analysis.	POA Communications Manager	2019
3.1.3	Recruiting Developers & Investors	Identify prospective incentives for discussion, including regulatory incentives, infrastructure capacity, and use of assembled properties.	POA CEO	2018
3.1.4	Recruiting Developers & Investors	Identify the target experienced, financially secure master developers and investors most likely to develop a product that meets the community's needs.	POA Director of Placemaking & Development	2018
3.1.5	Recruiting Developers & Investors	Draft a Request for Developers that includes the marketing package and other information pertinent to the prospective targets.	POA CEO	2020
3.1.6	Recruiting Developers & Investors	Establish an approach for negotiating with the selected developer/investor.	POA CEO	2019
3.2.1	Revitalization of Neighborhood Centers	Focus on re-establishing, upgrading and maintaining key recreational amenities within the neighborhood centers	POA Director of Recreation	Ongoing
3.2.2	Revitalization of Neighborhood Centers	Determine the market potentials for retail concepts and uses outlined in the master plan.	POA Director of Placemaking & Development	2018
3.2.3	Revitalization of Neighborhood Centers	Examine opportunities for leveraging retail redevelopment and/or tenancing such as through developer/operator recruitment.	POA Director of Placemaking & Development	2019
3.2.4	Revitalization of Neighborhood Centers	Improve signage from DeSoto to retail and service neighborhood centers. Improve curb appeal and beautifications of retail and service establishments.	POA Director of Placemaking & Development	2019
3.3.1	Attracting Businesses	Establish a town center, which provides a mixed-use, walkable environment with a variety of housing, commercial, recreational and community uses.	POA Board	2020-2025
3.3.2	Attracting Businesses	Establish a town center with up to 150,000 square feet of retail space at full buildout	POA Board	2020-2025

C. Summary of Key Actions

Key Actions

Type	Title	Description	Where
New Plans	17. Mixed-use Districts Branding Plan	Create a Mixed-use Districts Branding Plan, in coordination with the Birmingham Shopping District, to brand the City's multiple mixed-use districts.	1 & 4
Policy	18. Art Murals Policy	Implement an art-mural program for large blank wall surfaces in key locations. Coordinate with the Mixed-use Districts Branding Plan.	Ch. 4
New Plans	19. Green Infrastructure	Create a Green Infrastructure Plan to address stormwater run-off and areas experiencing regular flooding.	Ch. 5
New Plans	20. Sustainability Action Plan	Create a Sustainability Action Plan. The following goals are provided greater detail within chapter 5. <ul style="list-style-type: none">- Reduce environmental impacts of municipal operations.- Incentivize green building, renewable energy, and green landscaping.- Expand recycling and composting.- Implement green stormwater practices in streets and parks.- Support Rouge River Natural Area Improvements.- Implement other sustainability focused recommendations of this plan.- Create a Hazard Mitigation Plan addressing the City's future climate.- Increase inter-governmental cooperation around sustainability initiatives.- Study the best path towards encouraging or requiring businesses reduce plastic and Styrofoam use.	Ch. 5
New Plans	21. Rouge River Restoration Plan	Develop a plan to improve and maintain the Rouge River natural area. The following goals are provided greater detail within chapter 5. <ul style="list-style-type: none">- Inventory and analyze the Rouge corridor's wildlife, ecology, natural systems, and pollution sources.- Stabilize riverbanks, remove invasive species, reintroduce native ground covers, wildflowers, under-story, and canopy tree species.- Mitigate potential pollution or chemical sources, including the existing Springdale snow storage dumping area.- Work with area organizations and agencies to oversee, build support, and raise funding for the park's enhancements.- Work with other area and regional organizations and communities to improve the quality of the Rouge River watershed.	Ch. 5
New Plans	22. Rouge River Trails and Access Master Plan	Develop and implement a trails and access master plan to improve the Rouge River trails and trail heads. <ul style="list-style-type: none">- Install pedestrian linkages to the park's surrounding neighborhoods and commercial districts, including to Quanton Road.- Secure easements to expand the park area and improve its walkability, for complete ecological restoration, and universal accessibility.- Coordinate with Bloomfield and Beverly Hills to expand trail connections.- Install an environmentally sensitive, hard-surfaced pathway for pedestrians and cyclists along the Rouge River.- Expand the extent of the trail system, crossing the river at more locations to access large portions of the natural area currently cut off.- Install bridges, ramps, and other enhancements to enable access by all ages and abilities.	Ch. 5

Introduction

Summary of Actions

1.3. Summary of Actions

The following list summarizes the actions recommended throughout the Master Plan, grouped by the primary means in which they will be accomplished. A number of actions are related to goals and strategies in the Sustainability and Climate Action Plan (S-CAP) or Aging in Place Plan (AIP-P), indicated with reference to those plans. Reference is also provided to the overarching goals of this Master Plan, whether addressing a single goal or multiple.

1. Adopt the Future Land Use Map

Adopt the Future Land Use Map with a focus on preserving the scale and character of existing neighborhoods while identifying appropriate areas for "Missing Middle" and medium-to-high density housing. Promote diverse housing options, including multi-family units, along major corridors and neighborhood hubs, supporting schools and neighborhood businesses.

Goals: A, B, D, E, G
Actions: 3.1.0, 3.2.0.1, 3.2.0.2, 3.3.0, 3.4.1, 4.3.5
S-CAP: 6.4.1
AIP-P: 4.3

2. Increase Public Education and Outreach Associated with Master Plan Goals and Sustainability Actions

Promote public education and outreach to support community and master plan goals. Focus on topics such as energy efficiency, sustainable landscaping, water conservation, and small-scale development aligned with updated regulations. Use these efforts to engage residents and encourage actions that enhance sustainability and community well-being.

Goals: B
Actions: 2.2.1, 2.2.3, 2.2.6, 2.4.7, 2.6.4, 2.7.2, 3.1.4
S-CAP: 1.1.4, 1.2.2, 1.2.7, 5.1.2, 5.2.3
AIP-P: 3.2, 3.3

3. Adopt a Unified Development Ordinance to Streamline and Improve Zoning

Adopt a Unified Development Ordinance to streamline zoning and align policies with the Future Land Use Map. Promote sustainability, housing diversity, and community development by enabling townhomes, duplexes, multi-family units, and accessory dwelling units (ADUs). Encourage mixed-use, pedestrian-friendly developments that respect neighborhood character, and integrate stormwater, energy efficiency, and climate-adaptive practices. Simplify permits for aligned projects, incentivize affordable housing, and support infill and redevelopment in key areas. Ensure zoning supports infrastructure improvements and advances master plan goals for growth and preservation.

Goals: A, B, C, D, E, F, G
Actions: 2.2.2, 2.2.5, 2.3.1, 2.3.2, 2.3.3, 2.4.10, 2.4.12, 2.5.8, 2.6.1, 2.6.5, 3.1.1, 3.1.2, 3.1.3, 3.2.1, 3.2.2, 3.3.1, 3.3.2, 3.3.4, 3.4.2, 4.1.1, 4.2.1, 4.2.2, 5.2.3, 5.3.2, 5.3.3, 5.3.4, 5.4.1, 5.5.3, 6.5.5, 6.6.1, 6.6.2, 6.6.6.0, 6.6.6.1, 6.6.6.2, 6.6.6.3
S-CAP: 3.2.3, 4.4.5, 4.4.6, 5.2.1, 5.1.3
AIP-P: 1.2, 4.3

4. Create a Downtown Plan

Develop a Downtown Master Plan to enhance vibrancy and support new housing and business growth. Create active ground-floor spaces between Main and Washington, forming a continuous loop, and establish distinct district identities through branding and streetscape improvements. Focus on infrastructure upgrades, including crosswalks, seating, bicycle parking, trees, and lighting. Encourage the redevelopment of public parking lots to include new housing and retail, such as a downtown market, to foster community and economic success. Explore reconfiguring Main Street to better support pedestrians, replacing aging parking structures, and adding a new transit center and public plazas.

Goals: A, B, C, D, E, F, G
Actions: 5.2.1, 5.2.1.1, 5.2.1.2, 5.2.1.3, 5.2.1.4, 5.2.1.5, 5.2.1.6, 5.2.1.7, 5.2.2, 5.2.4
AIP-P: 1.2, 2.2

Plan Refinement: review, refinement, and implementation strategy

4.0 PRIORITIZED PROGRAMS OF PROJECTS

PRIORITIZATION LISTS

4.3 Complete prioritized project list

Each project in the comprehensive list was scored and ranked based on the cumulative weighted score. This process identifies the highest scoring projects, but neglects the distribution of equitable infrastructure investments across

different project types and locations. The complete list has been separated into lists for specific project types to manage the implementation of infrastructure improvements.

4.3 Complete prioritized project list

Rank	Project Title	Description
1	Bridge - S Dix @ Gas Creek	Replace 7-bridge existing bridge
2	Bridge - S Dix @ Gas Creek	Replace 7-bridge existing bridge
3	Gas Creek Valley Park	Installation of curbs
4	Pecan Street Sidewalks	Widen, add curb & gutter and sidewalks, and install streetlights
5	South Road Widening, Ph. 1	Widen, add curb & gutter and sidewalks from Main St. to Governor St.
6	Phonetic Road Improvements	Widen, add curb & gutter and sidewalks
7	Race Avenue, Lakeside Drive, and Willow D. Mills Avenue Intersection	Installation of a roundabout to replace existing intersection of Race Ave., Lake Shore Dr., Willow Mills Ave., and Parkway D.
8	Race Avenue Improvements	Widen, add curb & gutter and sidewalks from Race Ave. to Beebe Capps
9	River Oaks area Drainage	Install storm sewers and/or culverts along Pine Oak Dr. and Golf View Dr.
10	South Road Widening Ph. 2	Widen, add curb & gutter and sidewalks from Hwy. 242 to Hwy. 13
11	W. Phoenix and Beebe Capps Intersection	Major intersection redesign
12	Bedford Avenue Improvements	Widen, add curb & gutter and sidewalks from Beebe Capps to University St.
13	Chillico Avenue Improvements	Redesign and streetlight installation on Chillico Ave.
14	Main Street Improvements	Widen Main St. from Beebe Capps to Moore Ave.
15	Cloverdale Drainage III	Rebuild and enlarge ditch that runs along east side of Cloverdale Addition and Park Avenue. Ditch has exposed gas pipelines
16	Race Avenue & Davis Drive Intersection	Intersection improvements and traffic signal installation
17	W. Lincoln Avenue	Widen, add curb & gutter and sidewalks
18	Seary Sports Complex Drainage	Address flooding at Sports Complex and at event center
19	Moore Avenue Improvements, Ph. 1	Widen, add curb & gutter, install sidewalks from Maple St. to Turner St.
20	S Dix Street Improvements	Road improvements on S Dix St. from Lincoln Ave. to W. Woodluff Ave. with curbside at Beebe Capps
21	Country Club East Improvements	Widen, add curb & gutter and sidewalks
22	Skyline Drive	Redesign along Skyline Dr. add curb & gutter with potential for space to S Dix St. intersection if pedestrian crossing added

TRANSPORTATION INFRASTRUCTURE PROJECTS WITH CITY COMMITMENTS

CITY PROJECT #	PROJECT TITLE	DESCRIPTION	2018 COST ESTIMATE	CUMULATIVE COST	COMMENTS
400-66-85014	Walnut Street Improvements (A-DOT Job 030330)	Widen to 6 lanes from 4th Street to Dowdland Road (city contribution)	\$13,000,000	\$13,000,000	City share of A-DOT GRIP project, two "Top 10 Intersection"
	Walnut Street Interchange (A-DOT Job SB0923)	Single Point Urban Interchange at I-49 (city contribution)	\$2,000,000	\$15,000,000	City share of A-DOT ISP project, one "Top 10 Accident Intersection"
400-66-85042	28th Place South Phase I	North of Pleasant Grove Road to Bellevue Road	\$4,300,000	\$19,300,000	City share of Federal Aid project, cost split from city
400-66-85043	JS Hunt Drive (28th Place North Phase I)	Pauline Whitaker Pkwy to Bellevue Road	\$6,300,000	\$25,600,000	City share of Federal Aid project, cost split from city
	Water Service in Family Amended Areas	Reimbursement to RWU for providing water services in new city areas as required by the annexation ordinance	\$560,000	\$26,160,000	Cost estimate from RWU
			\$26,160,000		



Rendering of Proposed Walnut Street / Dixieland Road Intersection Streetscape

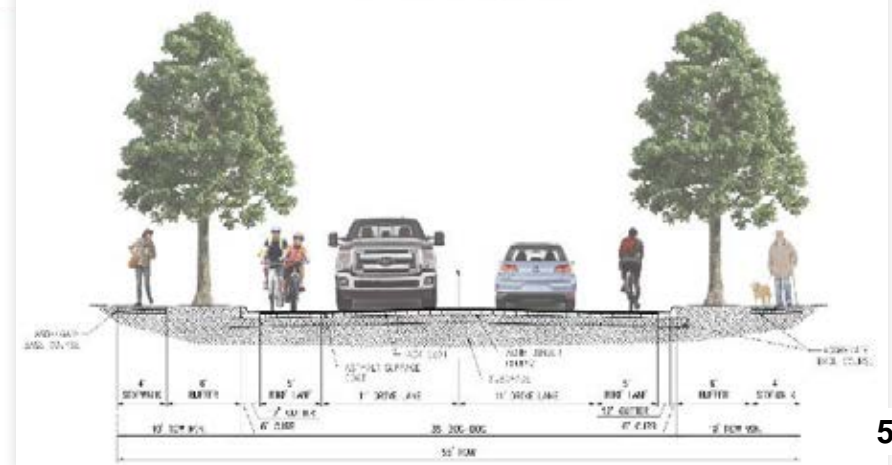
4.0 PRIORITIZED PROGRAMS OF PROJECTS

PRIORITIZATION LISTS

Criteria Performance Evaluation of Projects										Weight
	1	2	3	4	5	6	7	8	9	
Safety	Unstable	Minor Dev	Consistent	Good	Excellent	Excellent	Excellent	Excellent	Excellent	
5	3	4	2	5	2	4	3	5	90	
5	3	4	2	5	2	4	3	5	90	
5	5	3	2	5	1	5	1	5	85	
5	3	4	2	4	2	4	3	5	87	
4	3	4	3	3	4	4	3	5	88	
3	3	5	3	5	4	4	3	5	89	
5	1	3	5	2	4	5	2	5	83	
3	3	4	4	5	4	3	3	5	82	
5	5	1	1	5	1	4	3	5	81	
4	3	4	3	1	4	4	3	5	80	
3	1	3	4	3	3	4	2	5	70	
3	3	4	4	2	4	3	3	5	73	
5	1	4	1	5	1	5	2	5	73	
4	2	3	4	1	5	5	2	5	73	
5	5	1	1	4	2	4	1	5	73	

SECTION IV DESIGN STANDARDS

Local Urban Multi-Modal



Work Plan Phases

Project website, interviews, focus groups, survey, and mapping

Background analysis and initial input summarized

Charrette process: input + proposals

Charrette report-out and plan direction evaluated

Plan draft creation and reviewed, survey & web

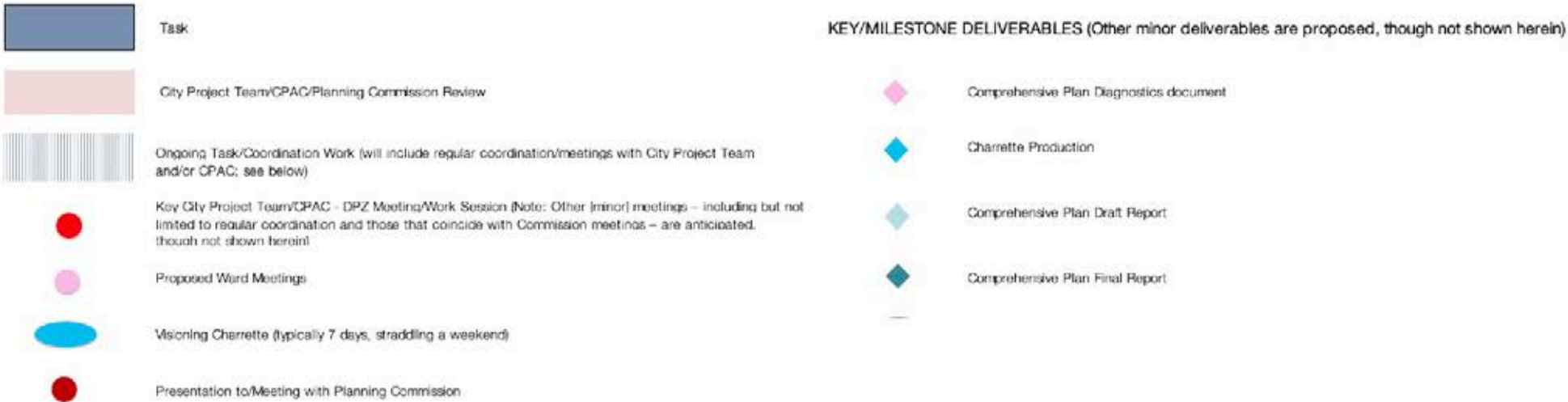
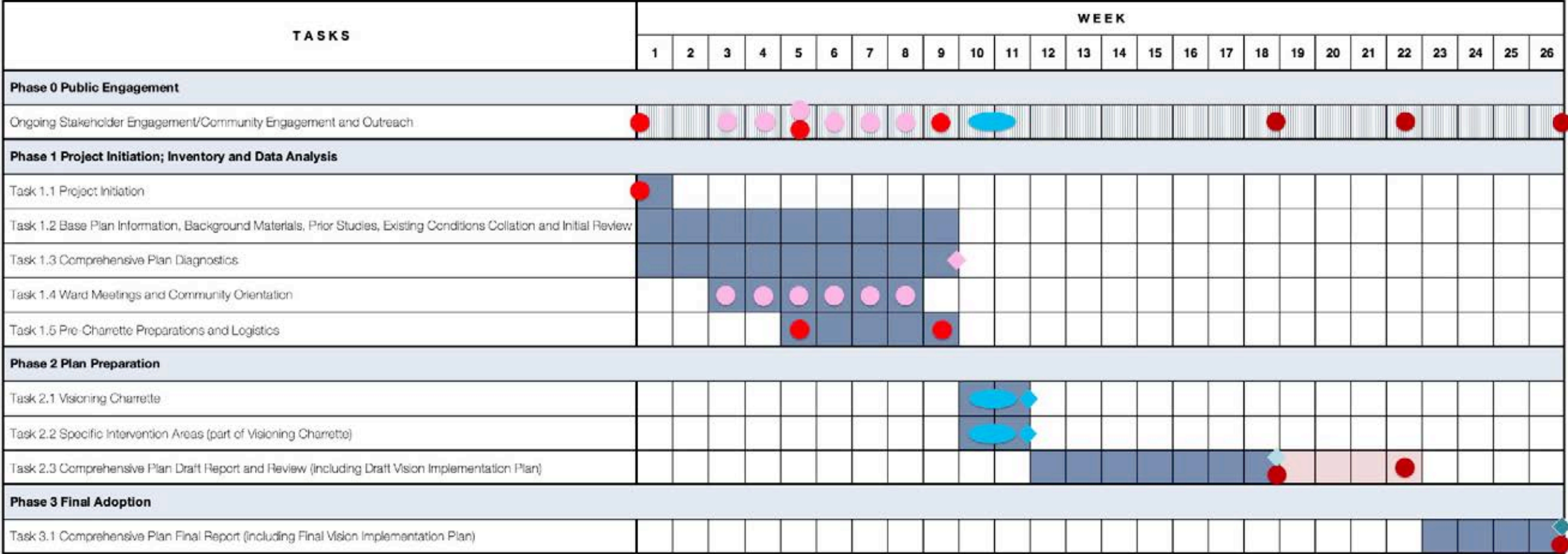
Plan finalization, comment period, final revisions and adoption process



Phase 0: Ongoing
Engagement

Phase 3

Final Draft & Adoption



Why Jonesboro Needs a Comprehensive Plan



From Planning to Doing: Organizing Implementation

As a Strategic Compass

- Directs growth and development where it's most needed
- Helps prioritize infrastructure upgrades, mobility enhancements, and public space investments

As an Implementation Toolkit

- Short- and long-term recommendations
- Responsible parties and phasing
- Metrics for success and adaptability over time

Jonesboro's New Comprehensive Plan

As an Integrative Framework

- Connects vision to implementation across zoning, design standards, and policy updates
- Unifies city staff, elected officials, developers, and the community around common goals

As a Living Community Agreement

- Built through deep public engagement
- Designed for regular updates and continued relevance through leadership transitions

DEVELOPMENT OF A FULL COMPREHENSIVE PLAN FOR

JONESBORO



URBAN3



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Text File

File Number: PP-25-06

Agenda Date:

Version: 2

Status: To Be Introduced

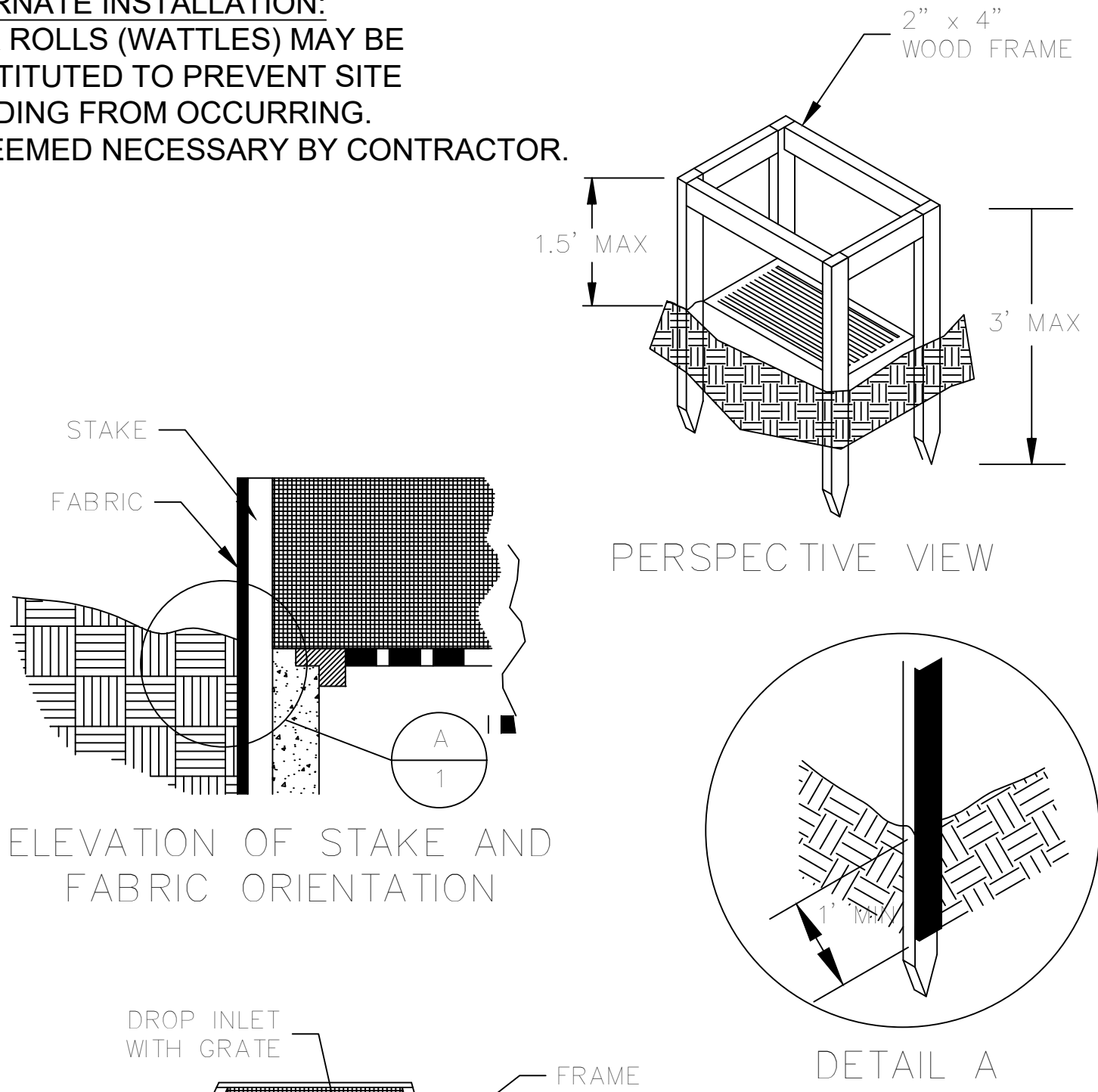
In Control: Metropolitan Area Planning Commission

File Type: Subdivisions

Preliminary Plat Sarah's Crossing Phase IV PLN-8538

Mark Morris is platting 51 lots on 15.29 acres of Phase IV of Sarah's Crossing Phase IV in the R-1 Medium Density Single Family District. Property is located North of Southwest Drive.

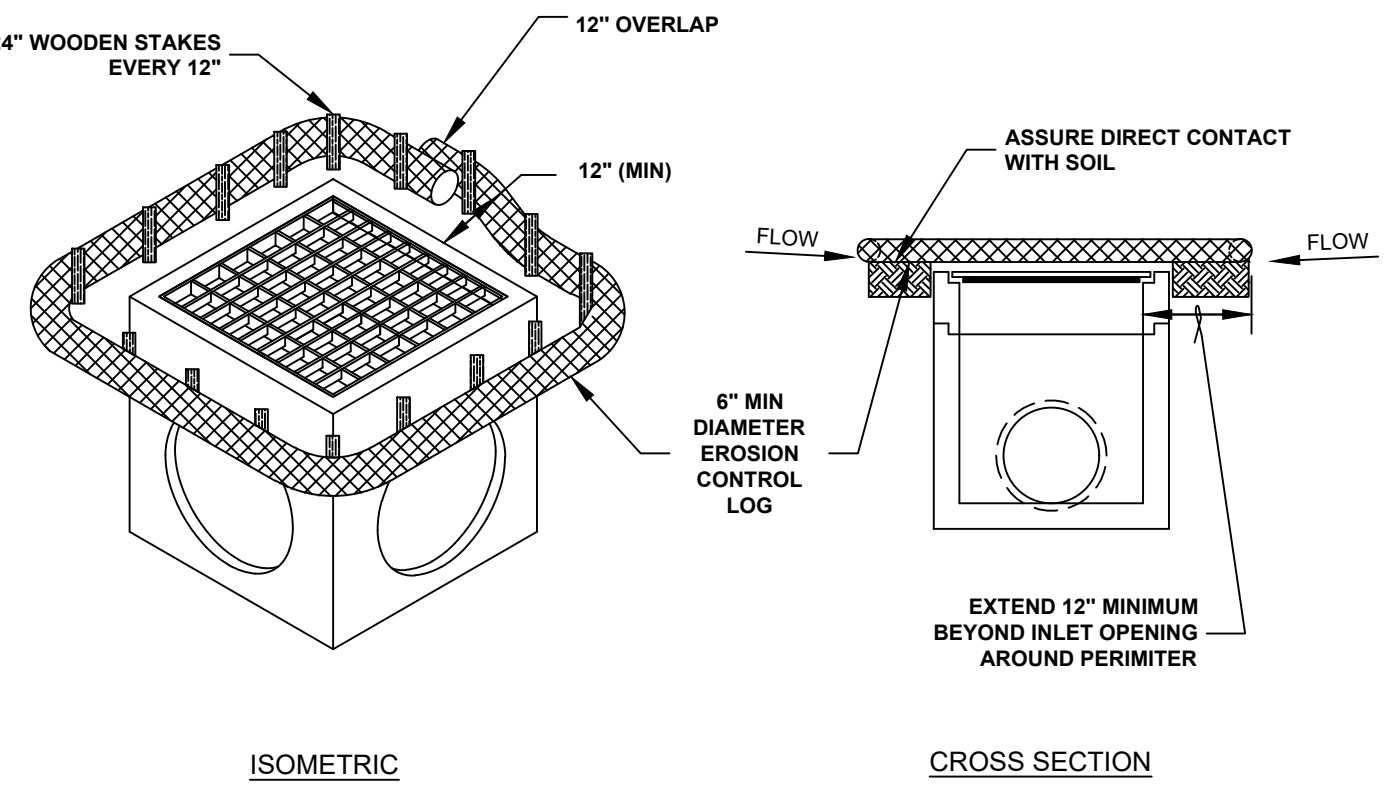
ALTERNATE INSTALLATION:
FIBER ROLLS (WATTLES) MAY BE
SUBSTITUTED TO PREVENT SITE
FLOODING FROM OCCURRING.
AS DEEMED NECESSARY BY CONTRACTOR.



PERSPECTIVE VIEW

FILTER FABRIC INLET PROTECTION

NOT TO SCALE

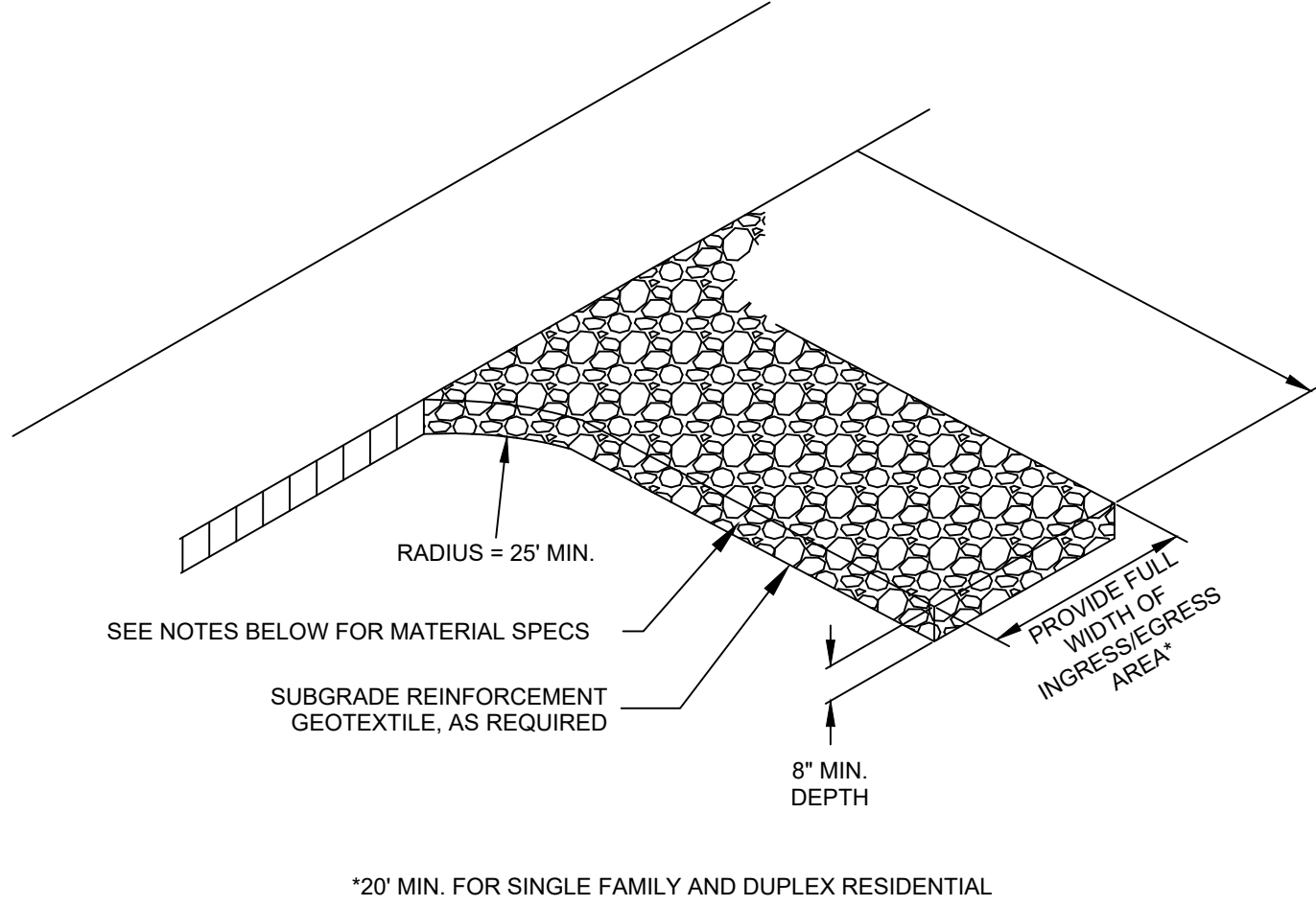


NOTES:

1. EROSION CONTROL LOG CONTAINMENT MESH SHALL BE 100% BIODEGRADABLE, PHOTODEGRADABLE OR RECYCLABLE; AND FILL MATERIAL SHALL CONSIST OF MULCH, ASPEN EXCELSIOR FIBERS, CHIPPED SITE VEGETATION, COCONUT FIBERS, 100% RECYCLABLE FIBERS, OR ANY OTHER ACCEPTABLE MATERIAL EXCLUDING STRAW AND HAY.
2. DAILY INSPECTION SHALL BE MADE BY THE CONTRACTOR AND SILT ACCUMULATION MUST BE REMOVED WHEN DEPTH REACHES 50% CAPACITY.
3. CONTRACTOR SHALL MONITOR THE PERFORMANCE OF INLET PROTECTION DURING EACH RAINFALL EVENT AND IMMEDIATELY CLEAN THE INLET PROTECTION IF EXCESSIVE PONDING OCCURS.
4. INLET PROTECTIONS SHALL BE REMOVED AS SOON AS THE SOURCE OF SEDIMENT IS STABILIZED.

FIBER ROLL (WATTLE) INLET PROTECTION

NOT TO SCALE

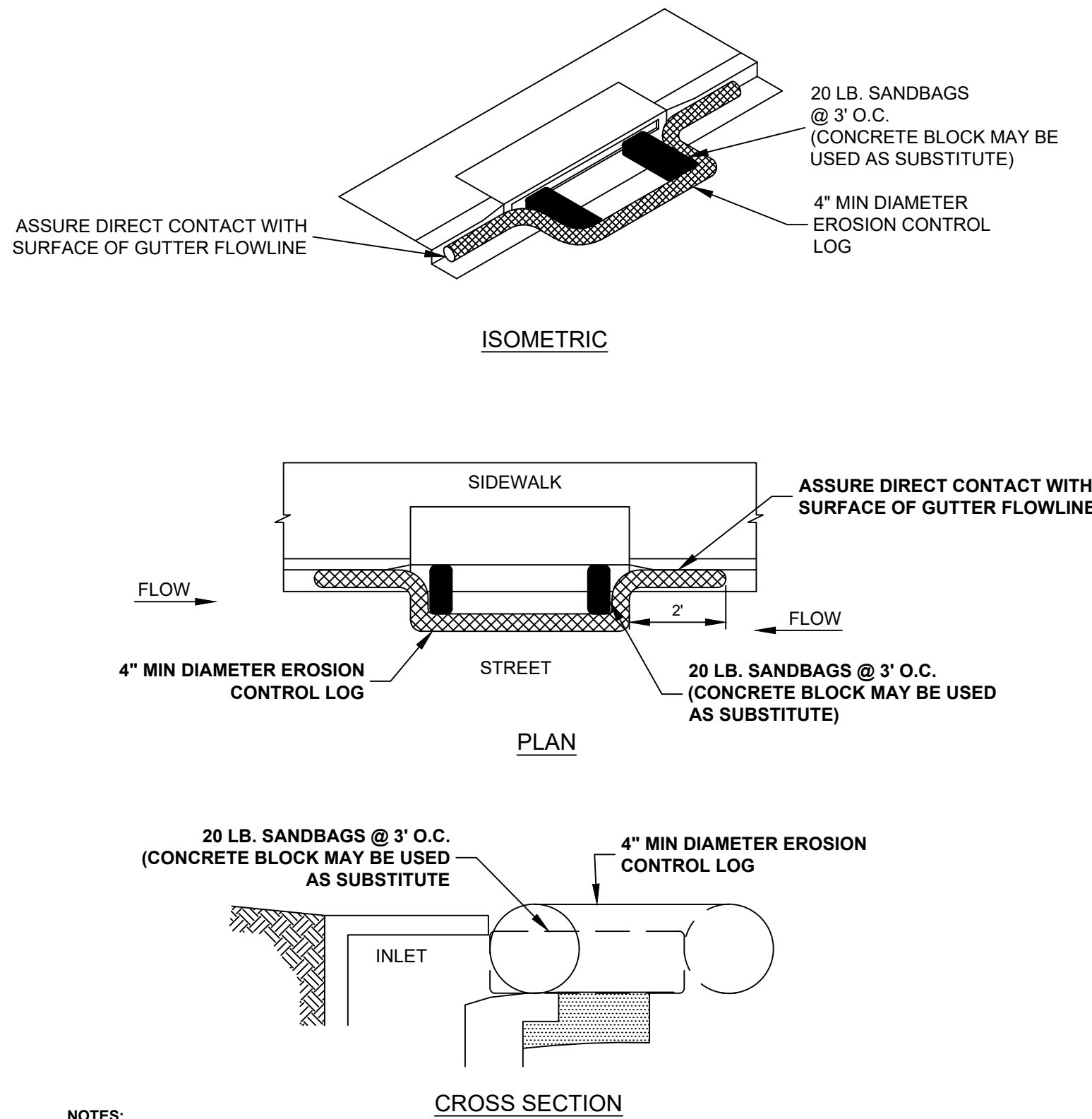


NOTES:

1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT.
2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.
4. WHERE RUNOFF CONTAINING SEDIMENT-LADEN WATER IS LEAVING THE SITE VIA THE CONSTRUCTION ENTRANCE, OTHER MEASURES SHALL BE IMPLEMENTED TO DIVERT RUNOFF THROUGH AN APPROVED FILTERING SYSTEM.
5. DIMENSIONS:
SINGLE FAMILY AND DUPLEX
20' LONG BY 20' WIDE, 3-6" DEEP OF 3/4" MINUS CLEAN ROCK.
COMMERCIAL
50' LONG BY 20' WIDE, 3-6" DEEP CLEAN ROCK. GOVERNING AUTHORITY MAY REQUIRE GEOTEXTILE FABRIC TO PREVENT SUB-SOIL PUMPING.

CONSTRUCTION ENTRANCE

NOT TO SCALE

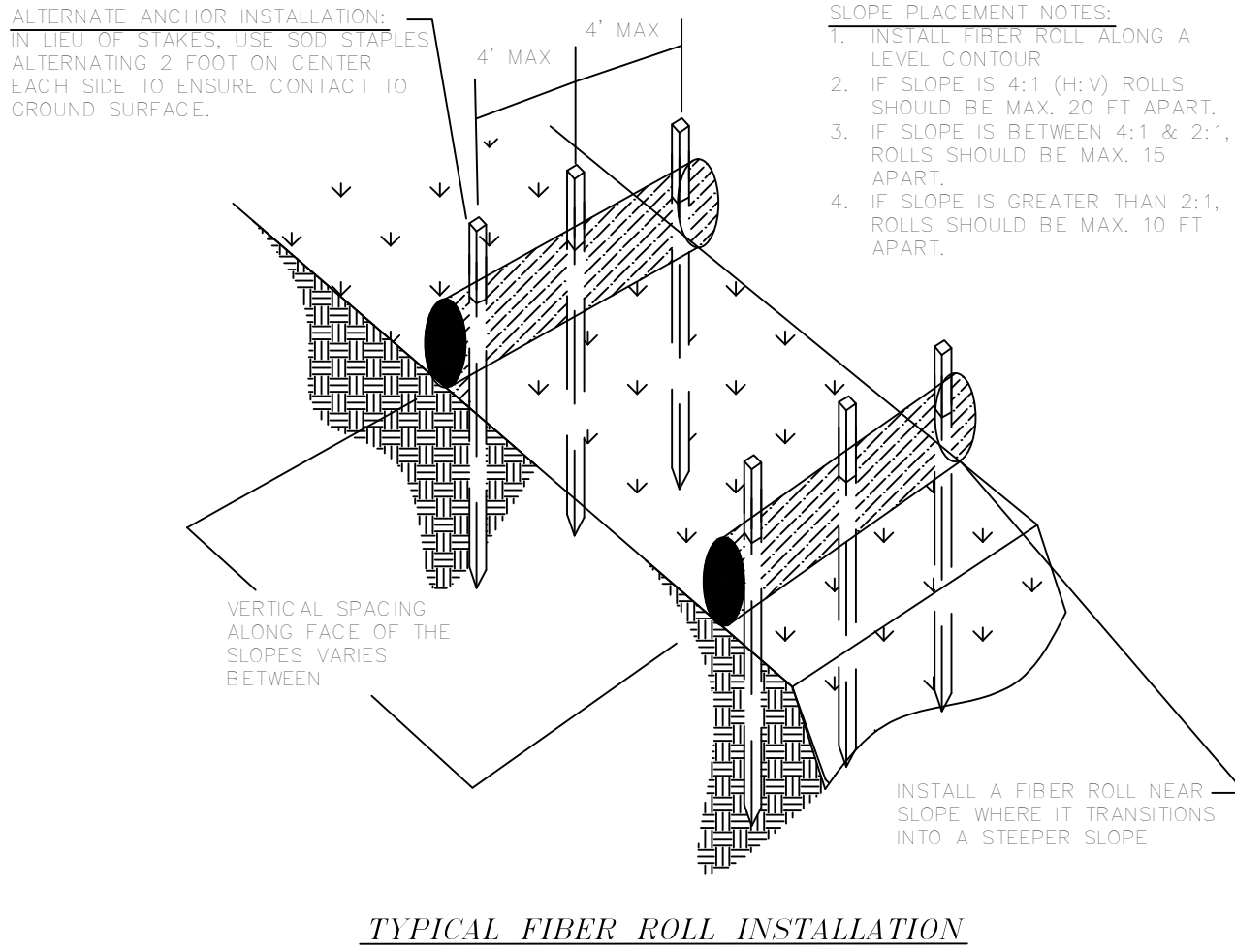


NOTES:

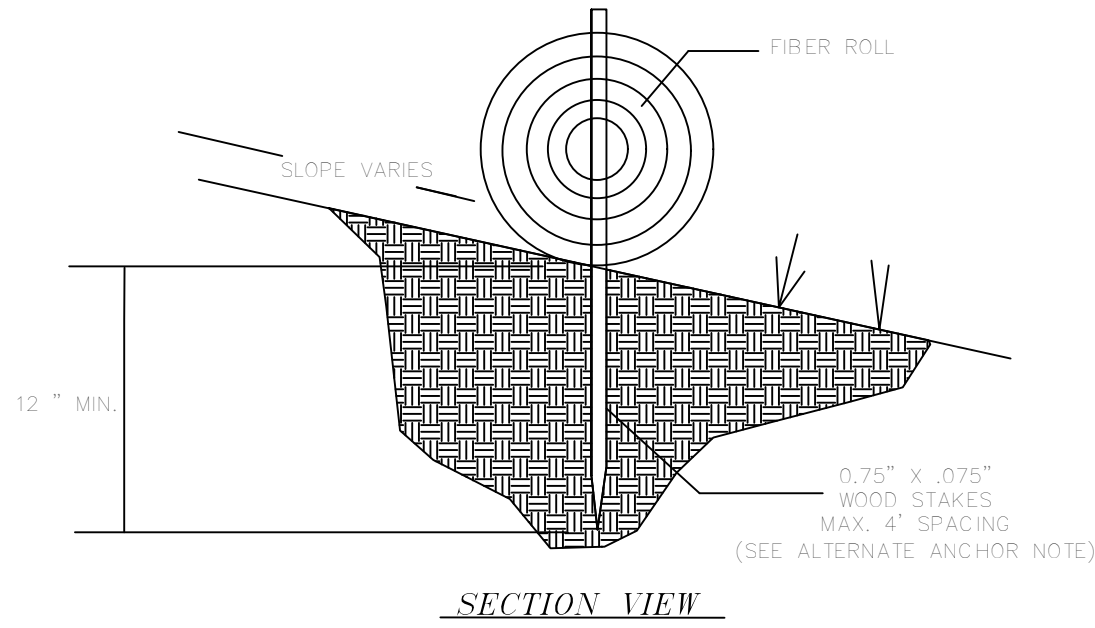
1. EROSION CONTROL LOG CONTAINMENT MESH SHALL BE 100% BIODEGRADABLE, PHOTODEGRADABLE OR RECYCLABLE; AND FILL MATERIAL SHALL CONSIST OF MULCH, ASPEN EXCELSIOR FIBERS, CHIPPED SITE VEGETATION, COCONUT FIBERS, 100% RECYCLABLE FIBERS, OR ANY OTHER ACCEPTABLE MATERIAL EXCLUDING STRAW AND HAY.
2. DAILY INSPECTION SHALL BE MADE BY THE CONTRACTOR AND SILT ACCUMULATION MUST BE REMOVED WHEN DEPTH REACHES 50% CAPACITY.
3. CONTRACTOR SHALL MONITOR THE PERFORMANCE OF INLET PROTECTION DURING EACH RAINFALL EVENT AND IMMEDIATELY REMOVE THE INLET PROTECTIONS IF THE STORM WATER BEGINS TO OVERTOP THE CURB.
4. INLET PROTECTIONS SHALL BE REMOVED AS SOON AS THE SOURCE OF SEDIMENT IS STABILIZED.

FIBER ROLL (WATTLE) CURB INLET PROTECTION

NOT TO SCALE

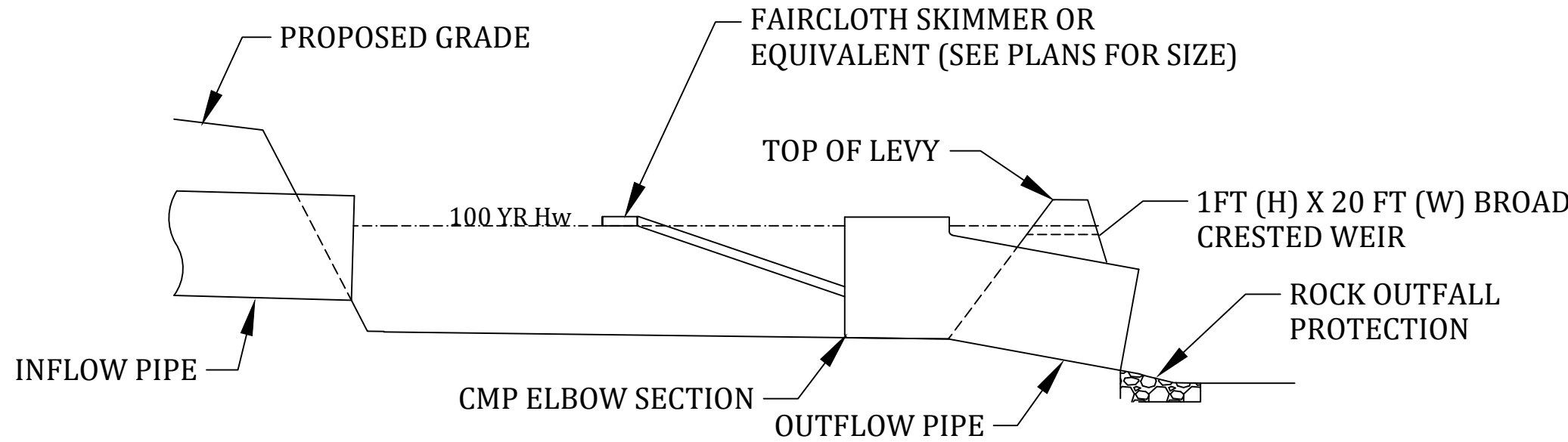


TYPICAL FIBER ROLL INSTALLATION



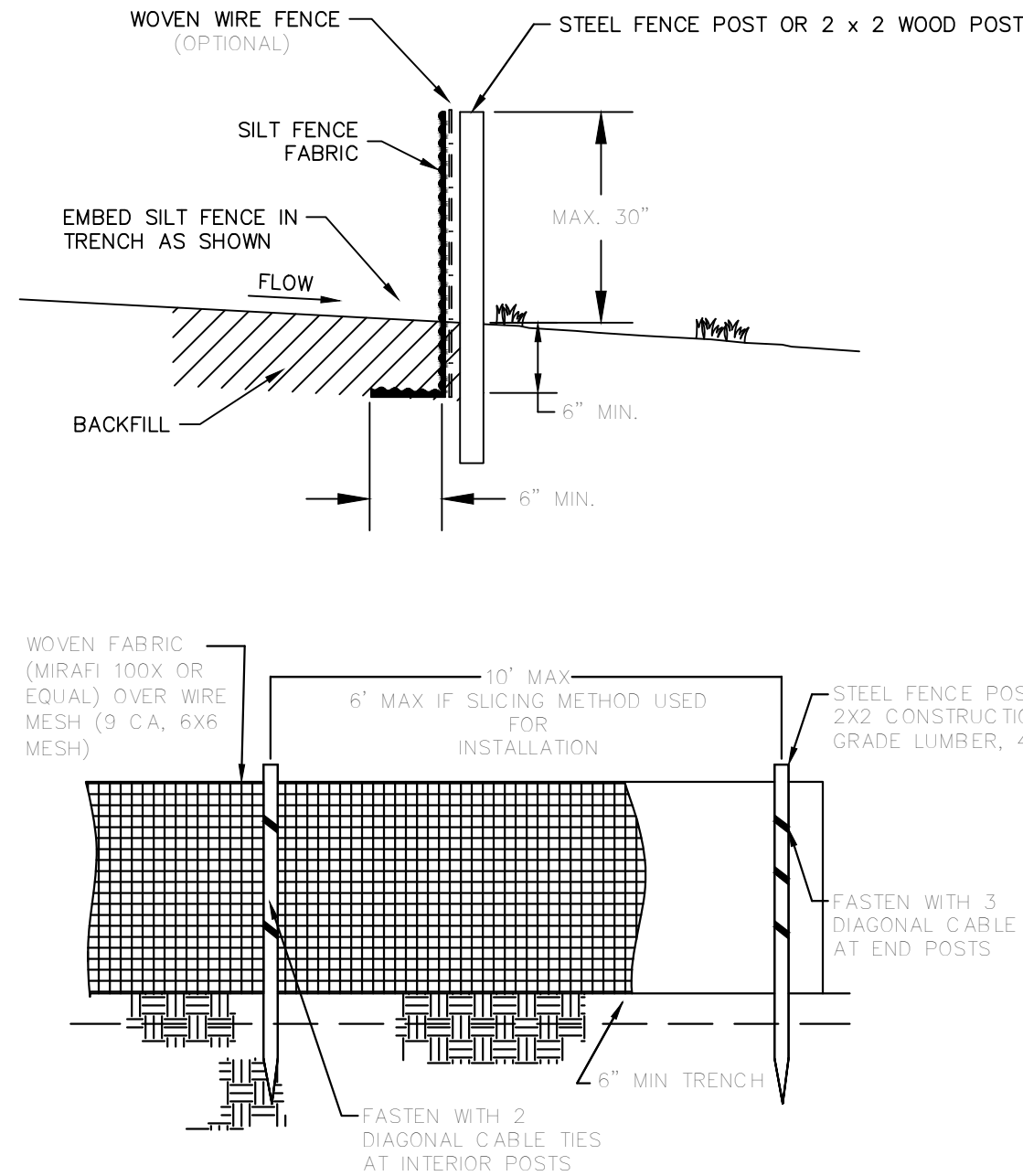
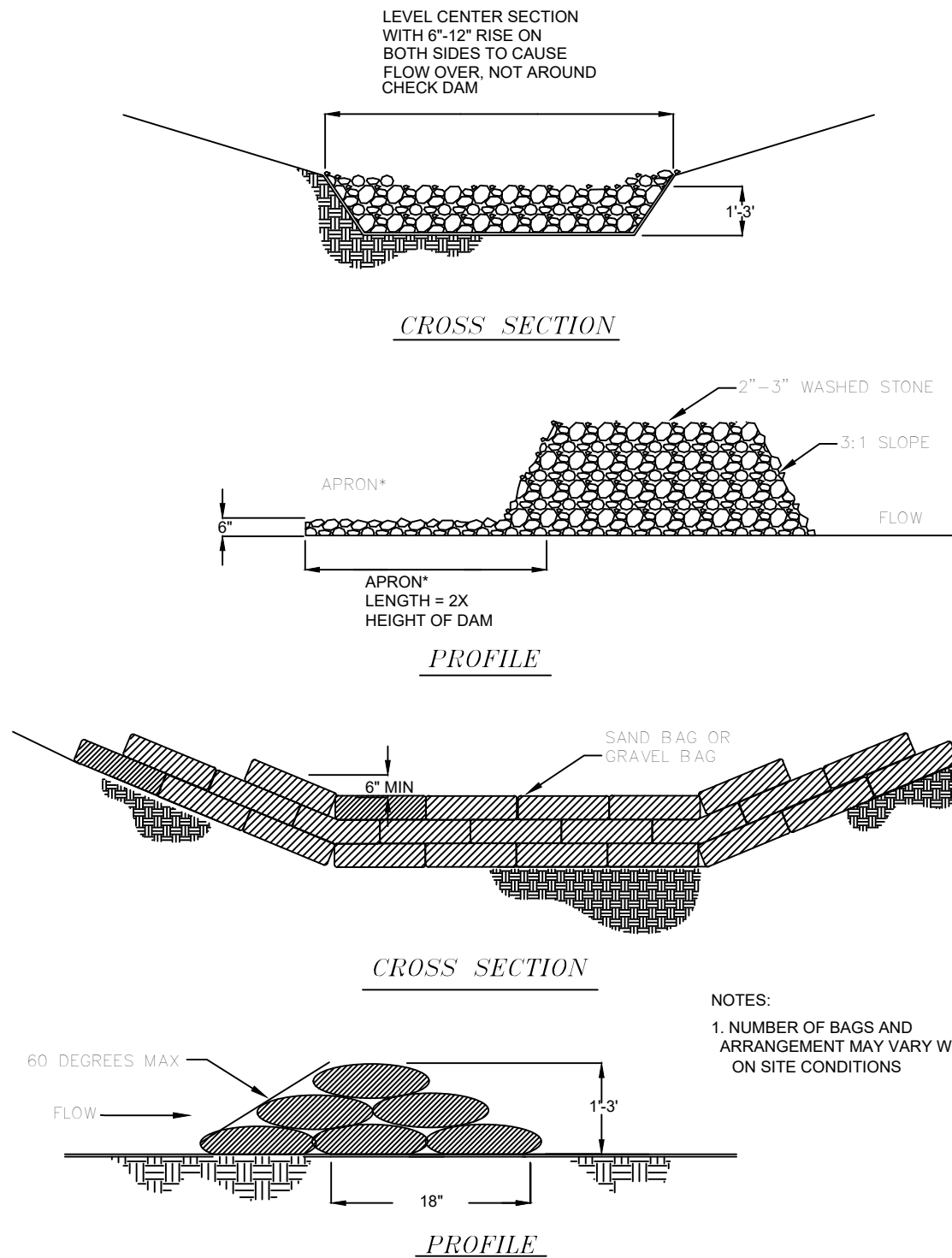
FIBER ROLLS AND WATTLES

NOT TO SCALE



SEDIMENT BASIN w/ FAIRCLOTH SKIMMER

NOT TO SCALE



NOTES:

1. PLACE SILT FENCE AT DOWNSLOPE LIMIT OF AREA TO BE GRADED.
2. SILT FENCE SHALL BE PLACED ALONG A LEVEL CONTOUR.
3. SEDIMENT TRAPPED BY THIS PRACTICE SHALL BE DISPOSED OF IN AN APPROVED SITE IN A MANNER THAT WILL NOT CONTRIBUTE TO ADDITIONAL SILTATION.
4. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH SUPPORT POST OR TO WOVEN WIRE, WHICH IS IN TURN ATTACHED TO THE STEEL FENCE POSTS.
5. INSPECTION SHALL BE FREQUENT AND REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
6. SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE.
7. ACCUMULATED SILT SHALL BE REMOVED WHEN IT REACHES 50% CAPACITY.
8. AT EACH END OF SILT FENCE, TURN FENCE UPSLOPE AND EXTEND UNTIL GROUND SURFACE RISES 18 INCHES.

DITCH CHECKS (STONE & SAND BAG)

NOT TO SCALE

SILT FENCE

NOT TO SCALE

SARAH CROSSING - PHASE IV

EROSION CONTROL DETAILS



REVISIONS

DATE	BY	DESCRIPTION

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DATE:	
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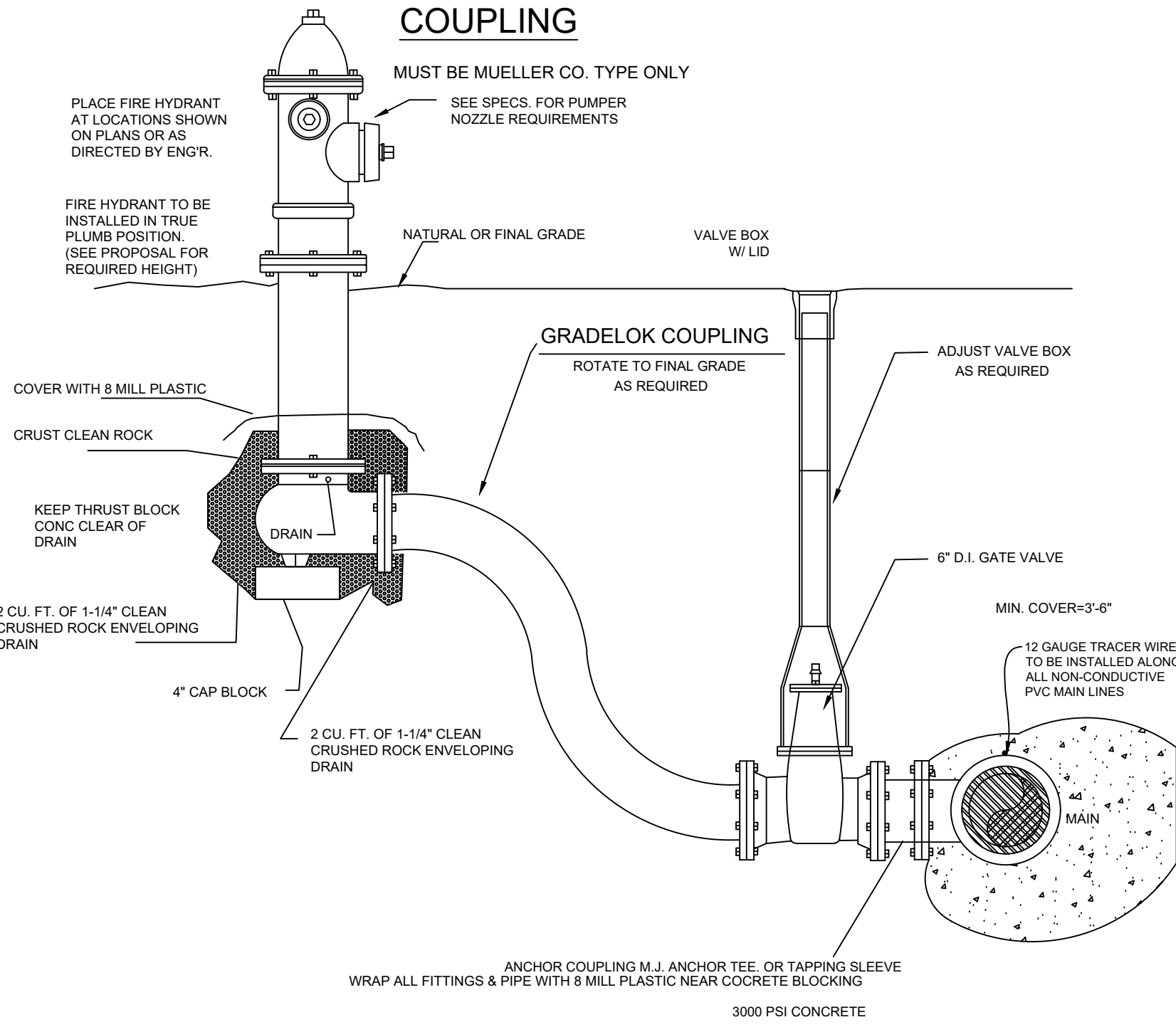
SHEET NUMBER:

DT-1

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MARK@MARKMORRISHOMES.COM

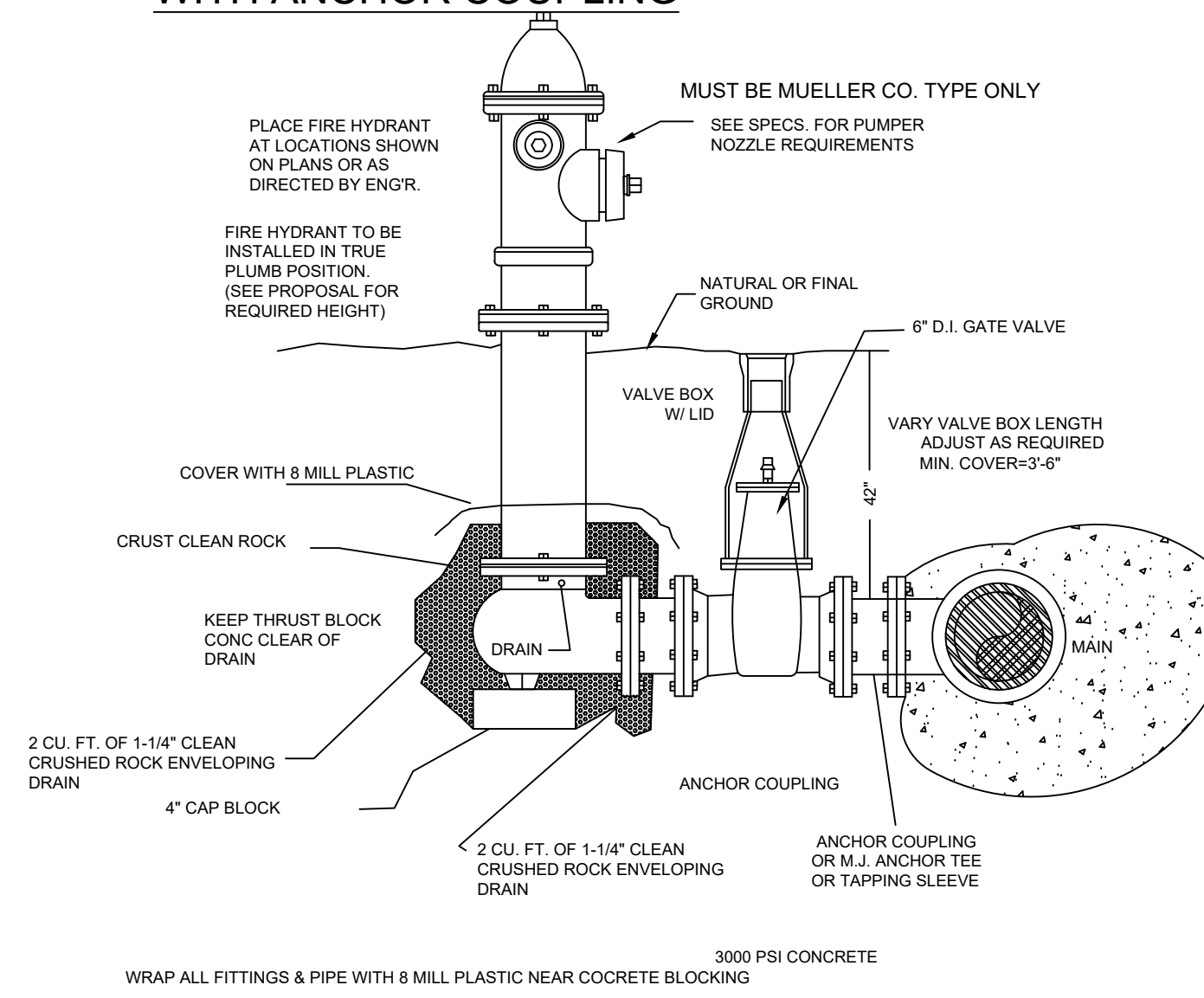
MARK MORRIS

FIRE HYDRANT DETAIL
WITH GRADELOK
COUPLING

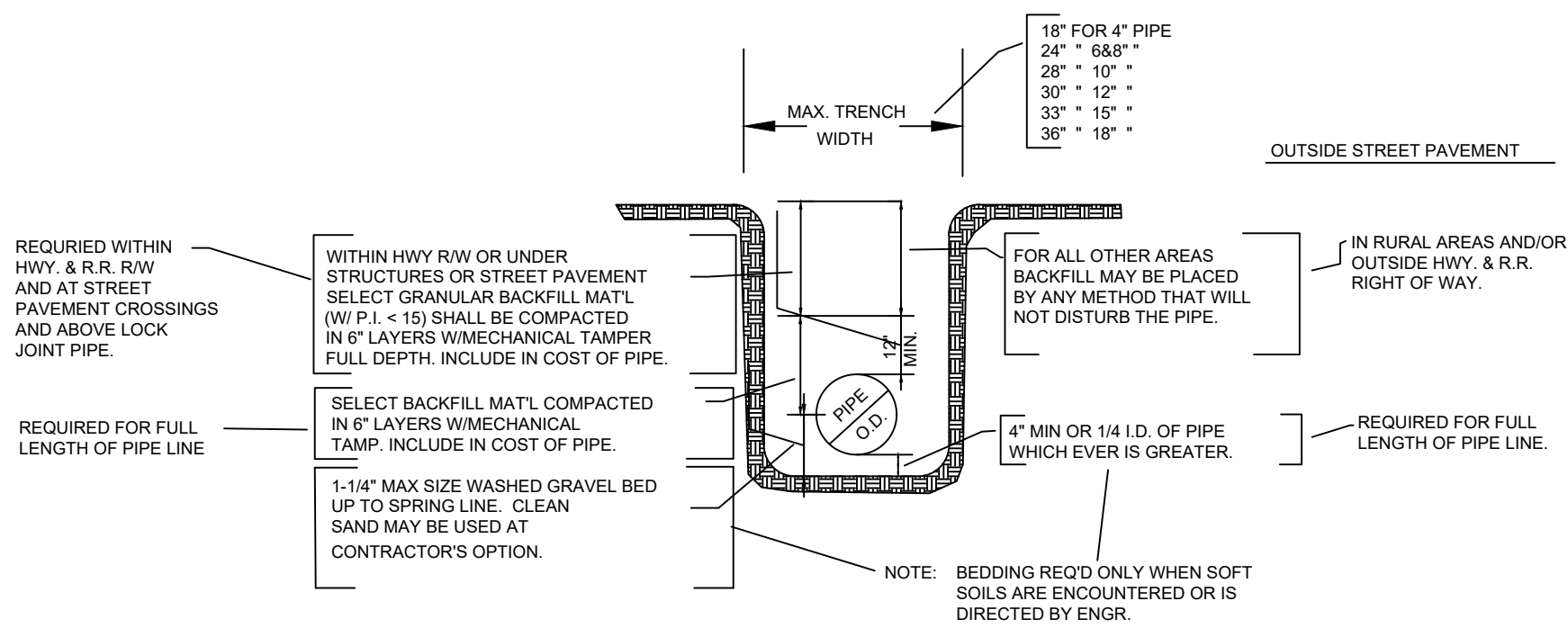


TYPICAL FIRE HYDRANT LOCATION
ALONG HWY. RIGHT-OF-WAY

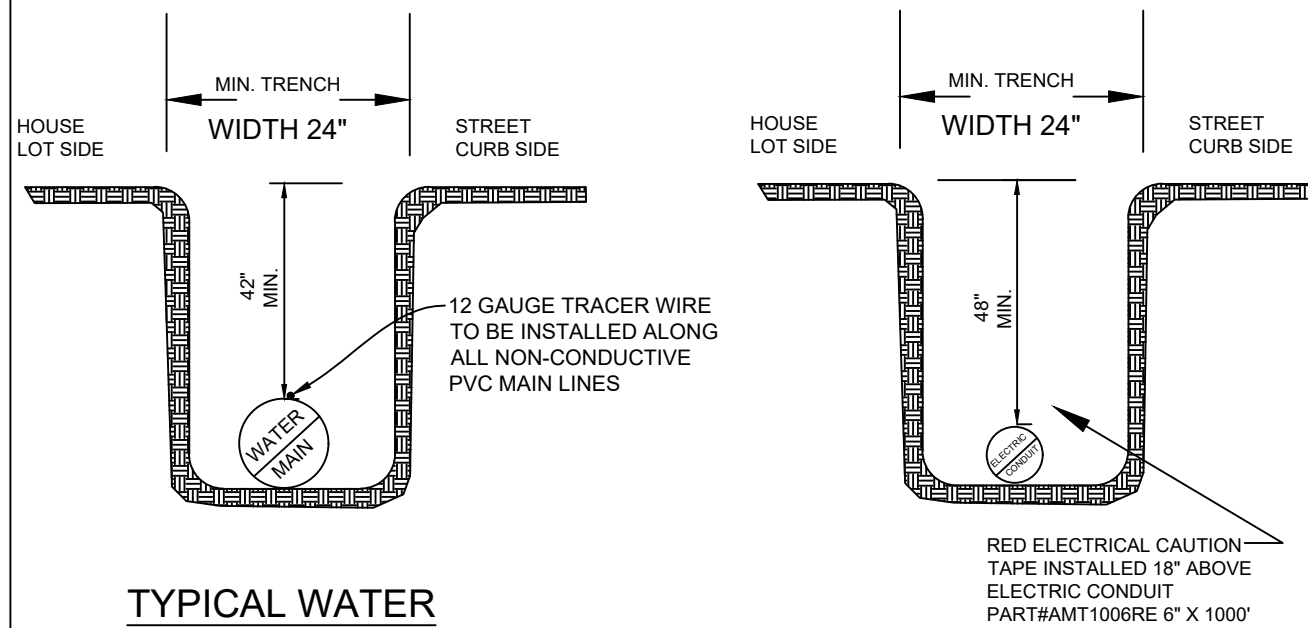
FIRE HYDRANT DETAIL
WITH ANCHOR COUPLING



TYPICAL FIRE HYDRANT
LOCATION

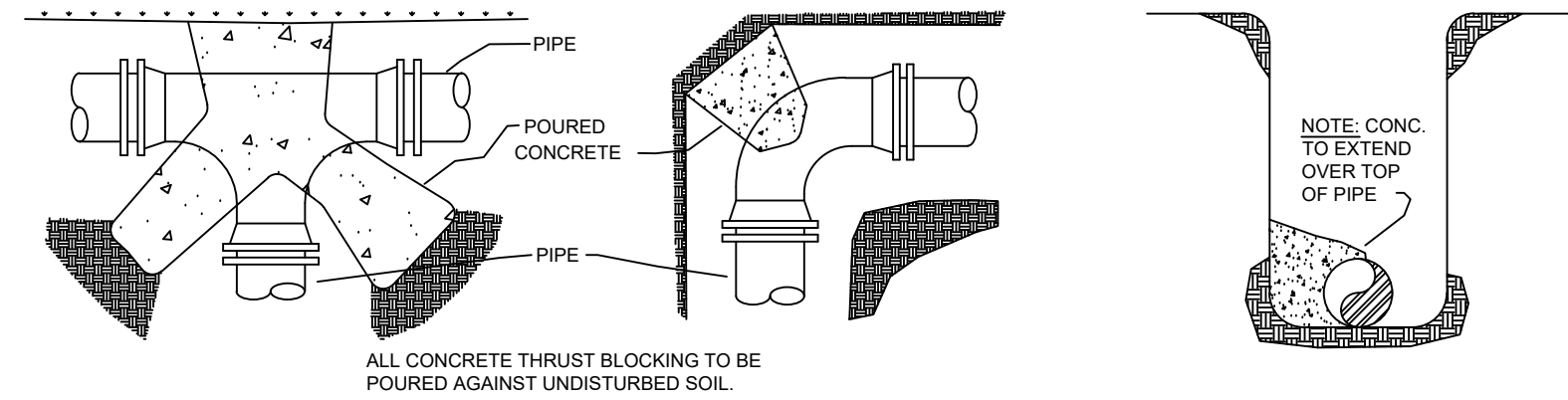


TYPICAL WATER PIPE BEDDING
AND BACKFILL DETAIL



TYPICAL WATER
TRENCH DETAIL

TYPICAL ELECTRIC
TRENCH DETAIL

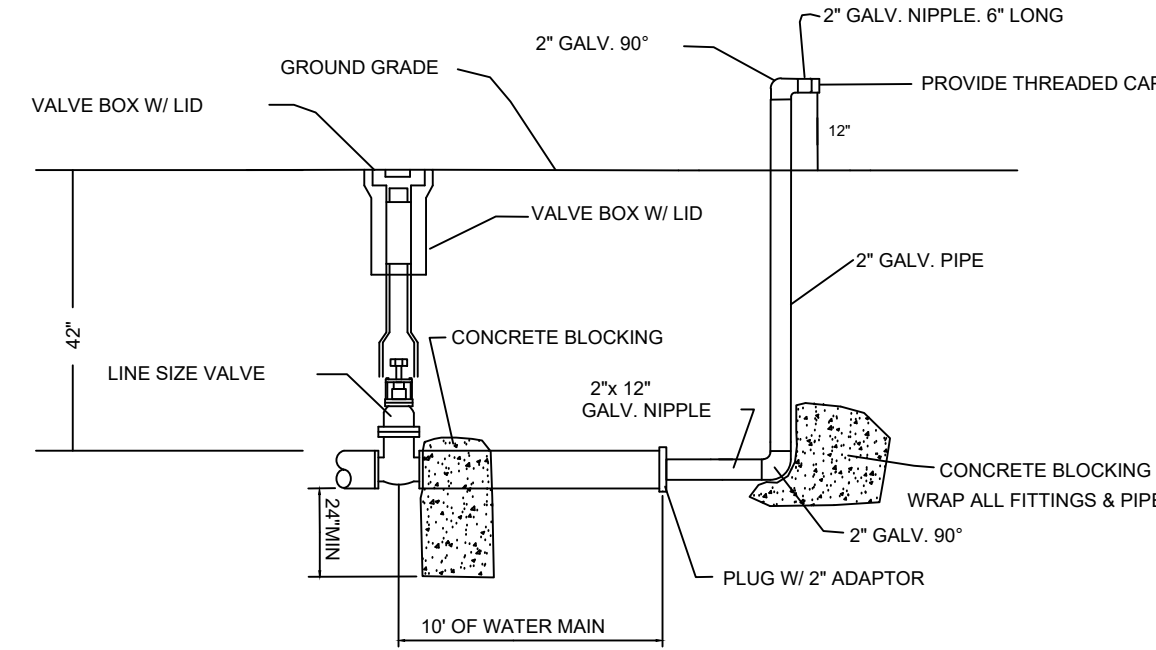


THRUST BLOCK DETAILS

- NOTES:
1. ALL BLOCKING SHALL BE AGAINST UNDISTURBED HAND DUG SOIL.
 2. WHERE SOIL CONDITIONS MAKE IT NECESSARY TO POUR CONCRETE BLOCKING OVER JOINTS, THE ENDS OF THE ADJACENT PIPES MUST HAVE A BRACE BLOCK TO RESIST MOVEMENT OF THESE JOINTS.
 3. ALL FITTINGS & PIPE SHALL BE COVERED WITH 8 MILL PLASTIC WHEN BLOCKING TO PREVENT BONDING OF CONCRETE.
 4. WHERE SHEAR IS HIGH PROPER REINFORCING MUST BE INSTALLED INTO THE BLOCKING AS DIRECTED BY ENGINEER.
 5. COVER SHALL BE A MINIMUM OF 3\"/>

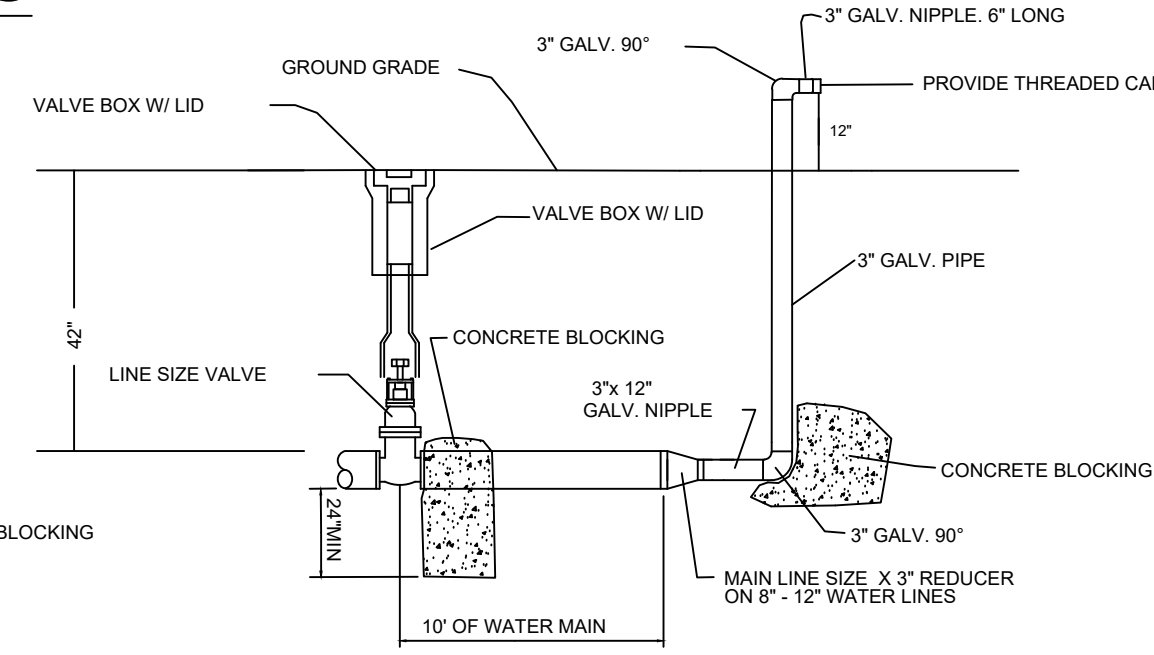
WATERLINE PIPE BEND THRUST BLOCK AREA		
SIZE & BEND DEGREE	STATIC THRUST (LB)	BLOCKING AREA (FT²)
4" - 22 ½°	1103	1
4" - 45°	2164	2
4" - 90°/TEE/PLUG	3998	4
6" - 22 1/2°	2482	2.5
6" - 45°	4867	5
6" - 90°/ TEE/PLUG	8997	9
8" - 22 1/2°	4413	4.5
8" - 45°	8656	8.5
8" - 90°/ TEE/PLUG	15994	16

BLOW-OFF ASSEMBLY DETAILS



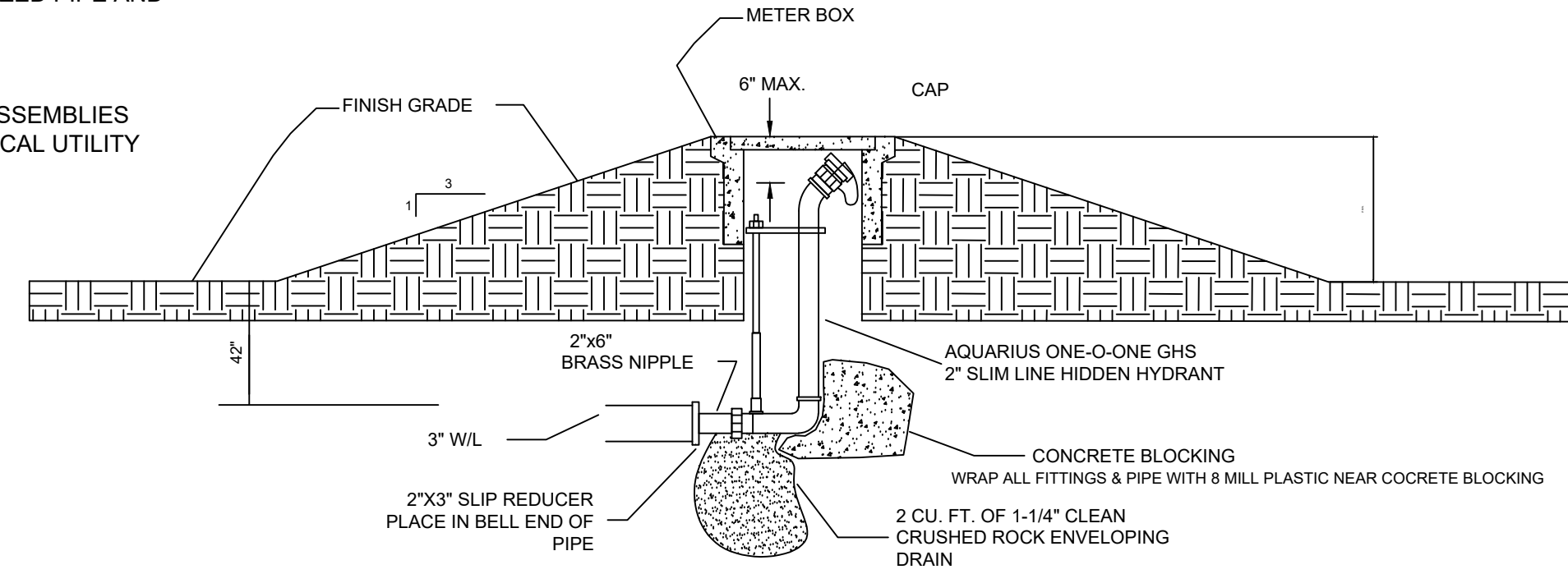
2\"/>

NOTE: ALL 2\", OR 3\" GALVANIZED PIPE AND FITTINGS ARE TO BE SCH 40



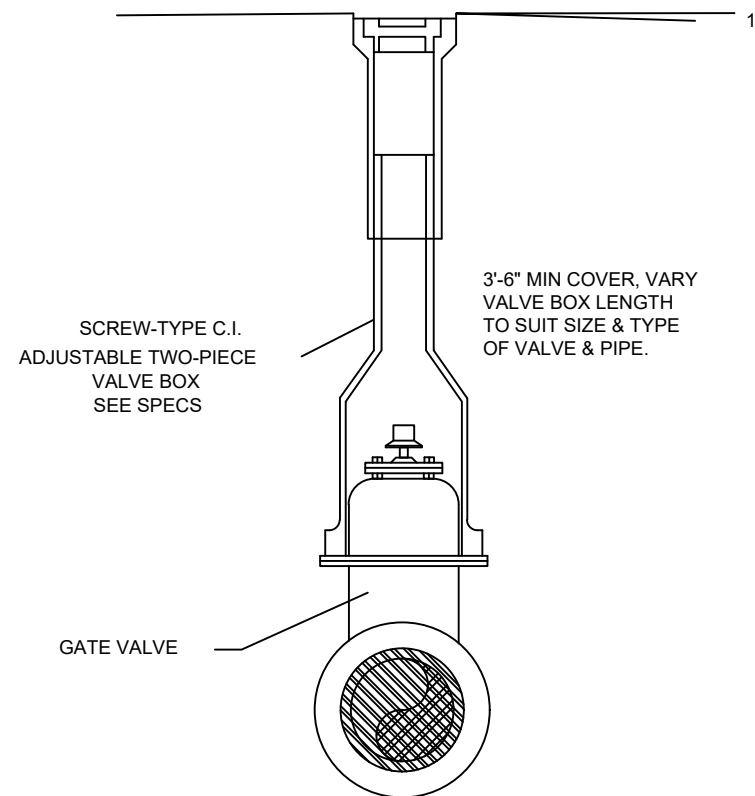
3\"/>

NOTE: OTHER BLOW-OFF ASSEMBLIES MUST BE APPROVED BY LOCAL UTILITY COMPANY

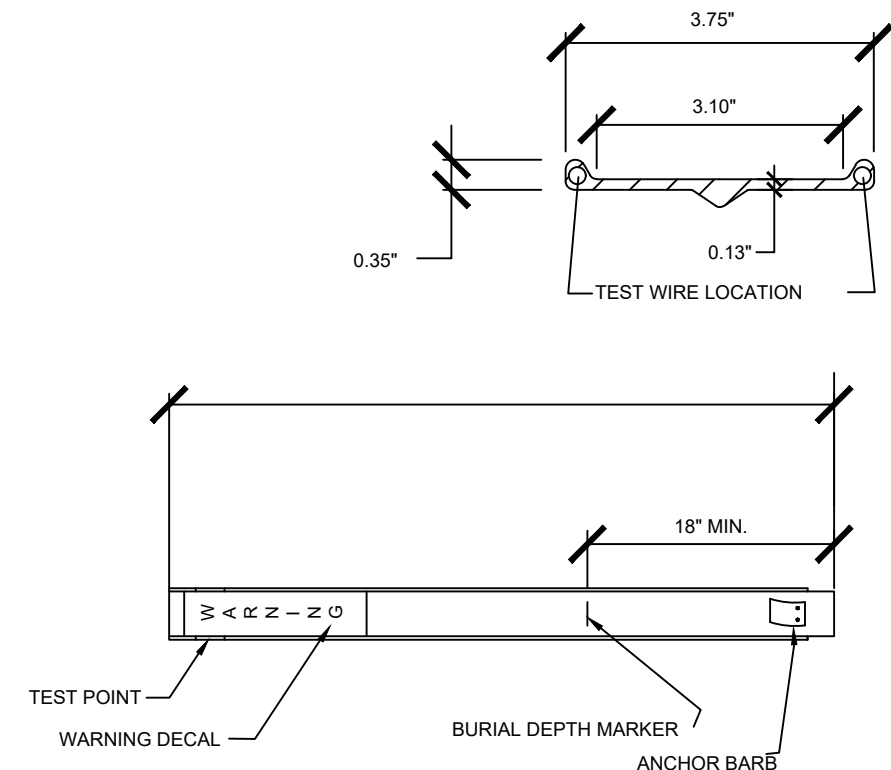
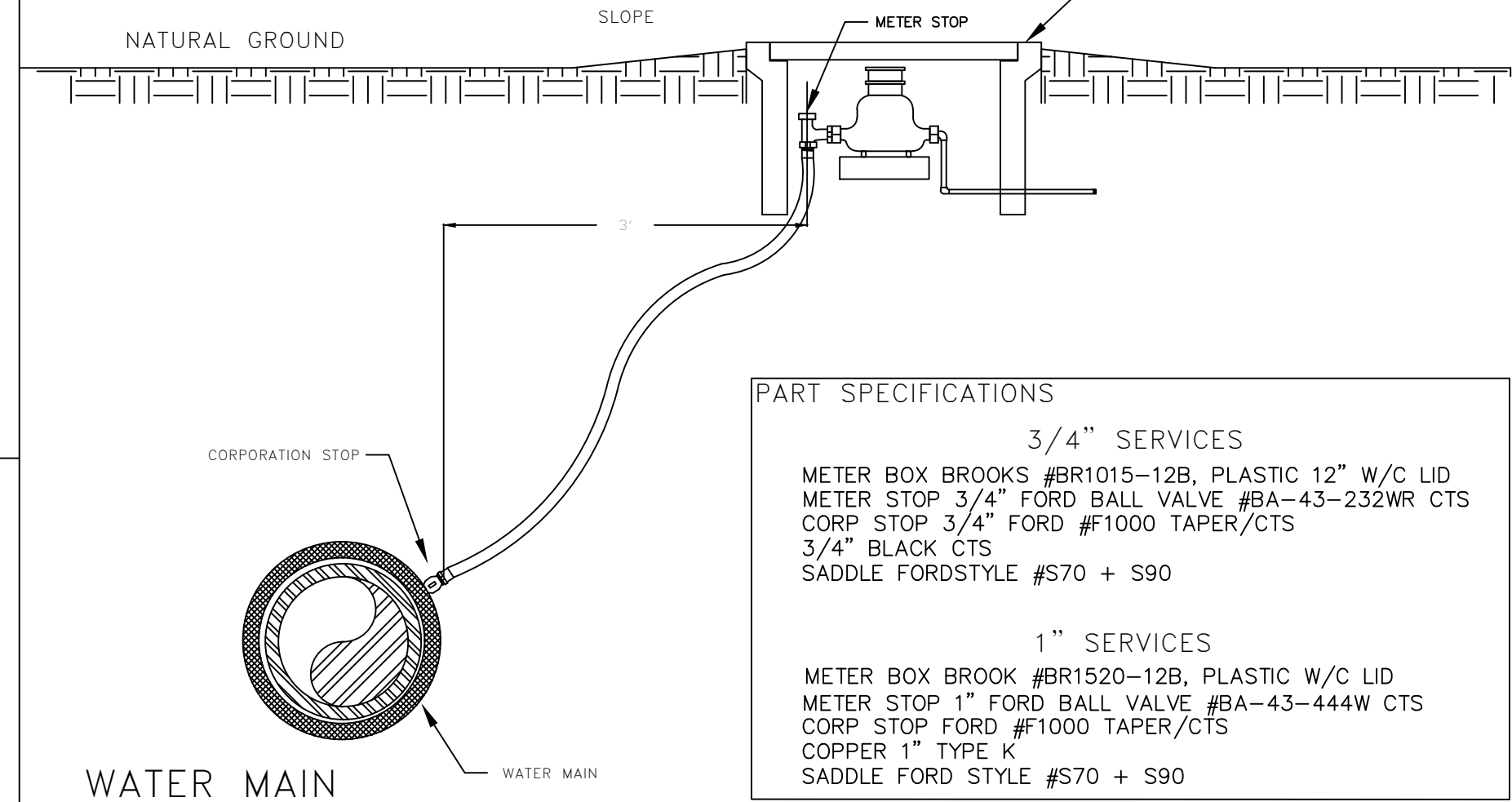


UNDERGROUND WASHOUT ASSEMBLY
FOR COVES AND DEAD END LINES WITH
NO FUTURE EXTENSION

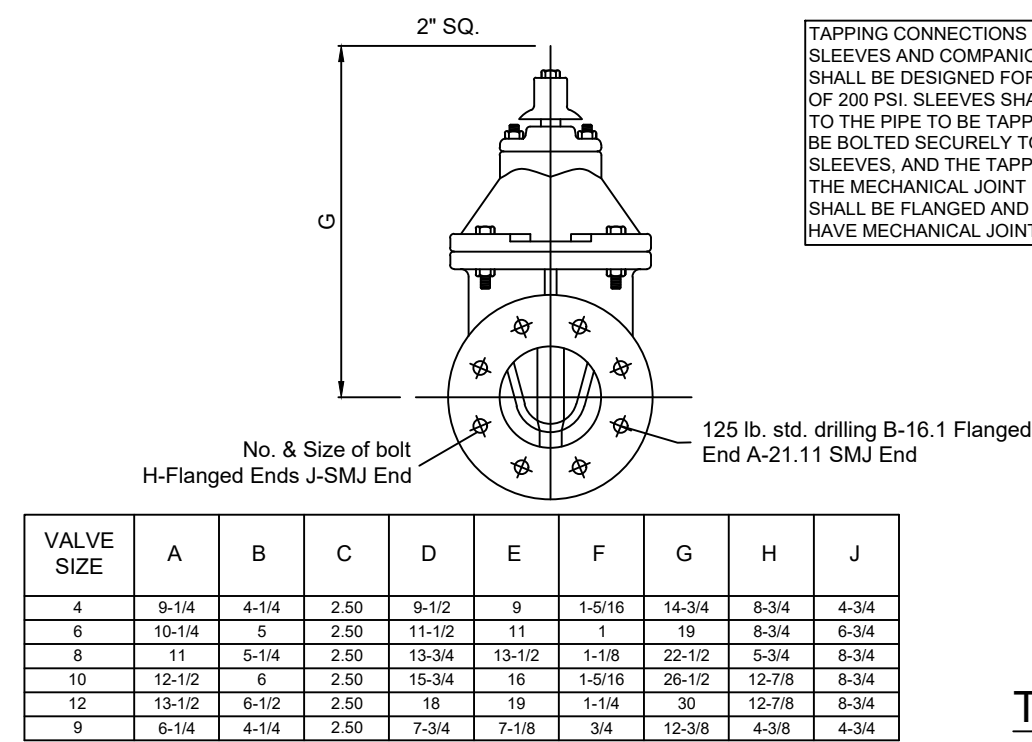
SERVICE CONNECTION AND METER SETTING



VALVE BOX DETAIL



WATER LINE & FORCE MAIN
MARKER DETAIL



TAPPING SLEEVE
TAPPING VALVE DETAIL

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SARAH CROSSING - PHASE IV

WATER DETAILS

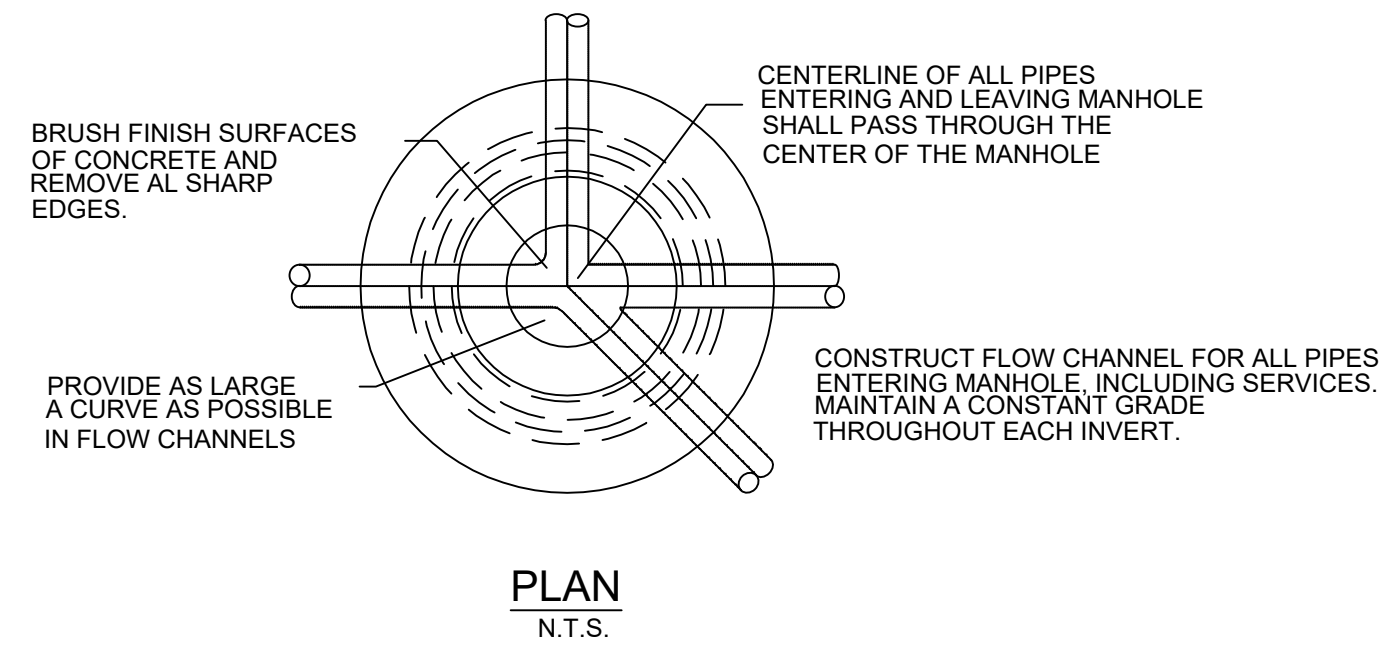


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SHEET NUMBER:		
DT-2		

NOTES TYPICAL TO ALL MANHOLES

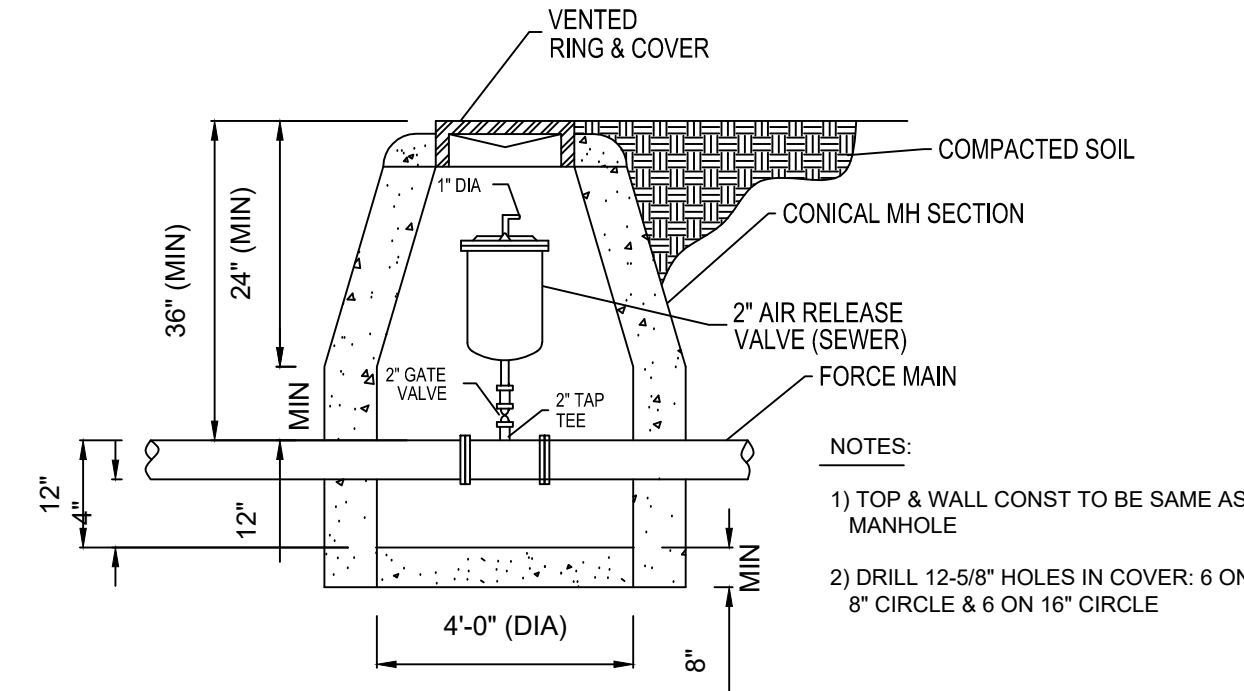
- MANHOLE BASES TO BE 4000 PSI, 28 DAY STRENGTH, LIMESTONE MIX CONCRETE ON UNDISTURBED SUB-GRADE OR ON COMPACTED SELECT FILL MATERIAL AS AUTHORIZED.
- MANHOLE SIDEWALL MATERIAL TO BE CONCRETE IN ACCORDANCE WITH PROPOSAL FORM REQUIREMENTS.
 - 4" I.D. FOR SEWER SIZES 6" THRU 24"
 - 5" I.D. FOR SEWER SIZES 24" THRU 30"
 - 6" I.D. FOR SEWER SIZES 36" THRU 42"
- SET MANHOLE TOPS AS FOLLOWS:
 - IN STREETS, ROADS, HIGHWAYS, AND OTHER PAVED AREAS: FLUSH WITH FINISHED PAVING GRADE.
 - UNDEVELOPED AREAS, SUCH AS FIELDS, WOODS, ETC...: 12" ABOVE GROUND.
 - OTHER AREAS: 1" ABOVE GROUND.
- MANHOLE FRAMES SHALL HAVE MIN. OPENING OF 22" DIA.
- ALL DOGHOUSE MANHOLES TO BE INSTALLED WITH A-LOK WEDGE STYLE WATER STOP KITS.

STANDARD MANHOLE



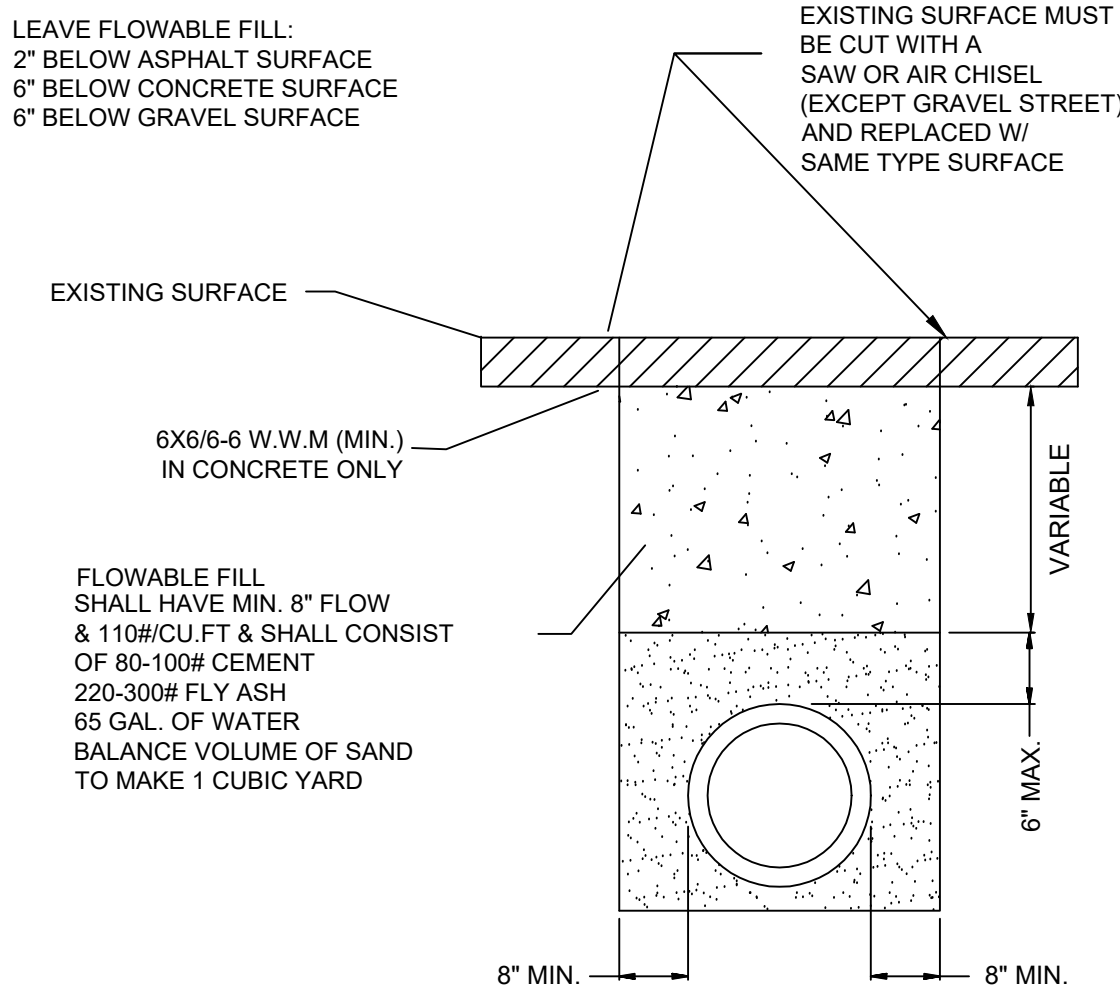
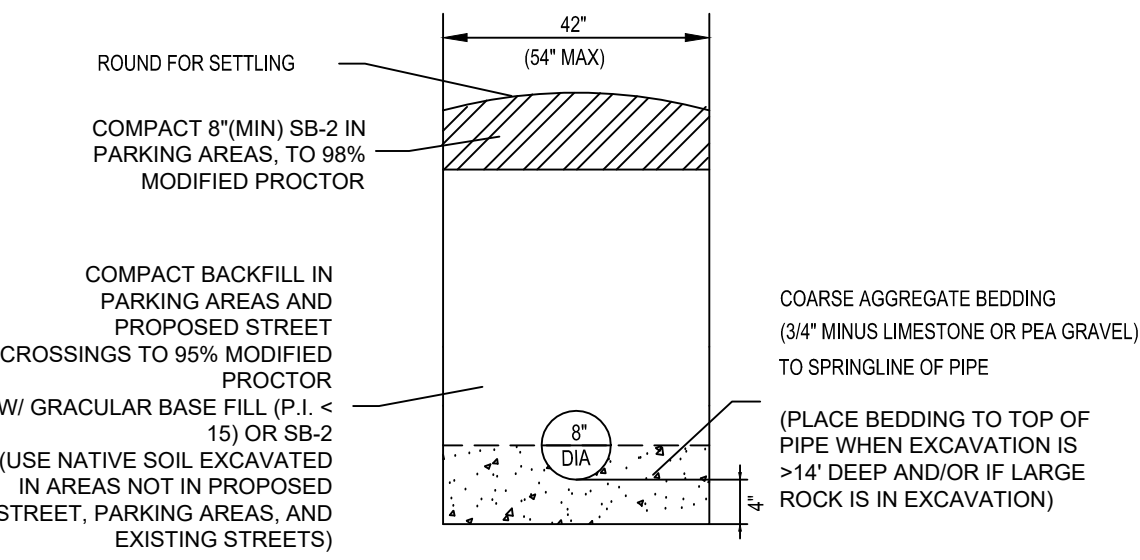
MANHOLE DETAILS

AIR/VACUUM RELEASE VALVE DETAIL



NON-EXISTING STREET LOCATIONS
SDR-26 PVC TRENCH DETAIL

- NOTES:
- COARSE AGGREGATE SHALL BE MECHANICALLY TAMPED WHEN USED.
 - PIPE IN TRENCH SHALL BE PLACED ON NO LESS THAN 4" OF 3/4" MINUS LIMESTONE. ALL OVER EXCAVATION SHALL BE COMPACTED BACK TO GRADE W/ BEDDING MATERIAL.
 - PVC PIPE SHALL BE SDR-26 AND INSTALLED IN ACCORDANCE WITH UNI-BELL PVC PIPE ASSOCIATION'S RECOMMENDATIONS FOR CONSTRUCTION.

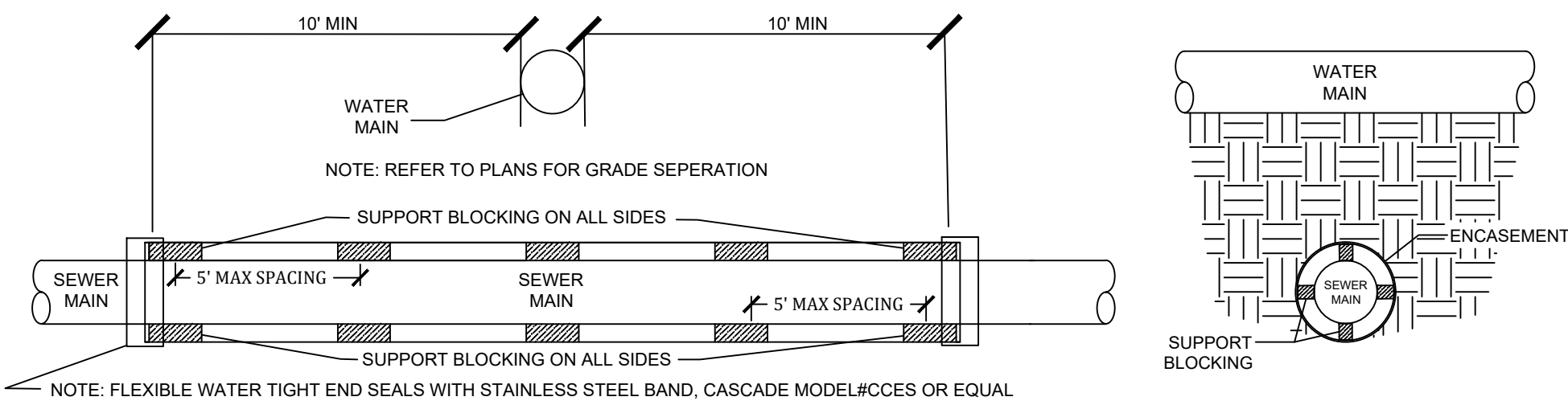


EXISTING STREET LOCATIONS

PVC TRENCH DETAIL

NOTE: 0.25 THICKNESS STEEL OR SCH 40" PVC ENCASEMENT PIPE SHALL EXTEND 10' MINIMUM EACH SIDE OF WATER MAIN

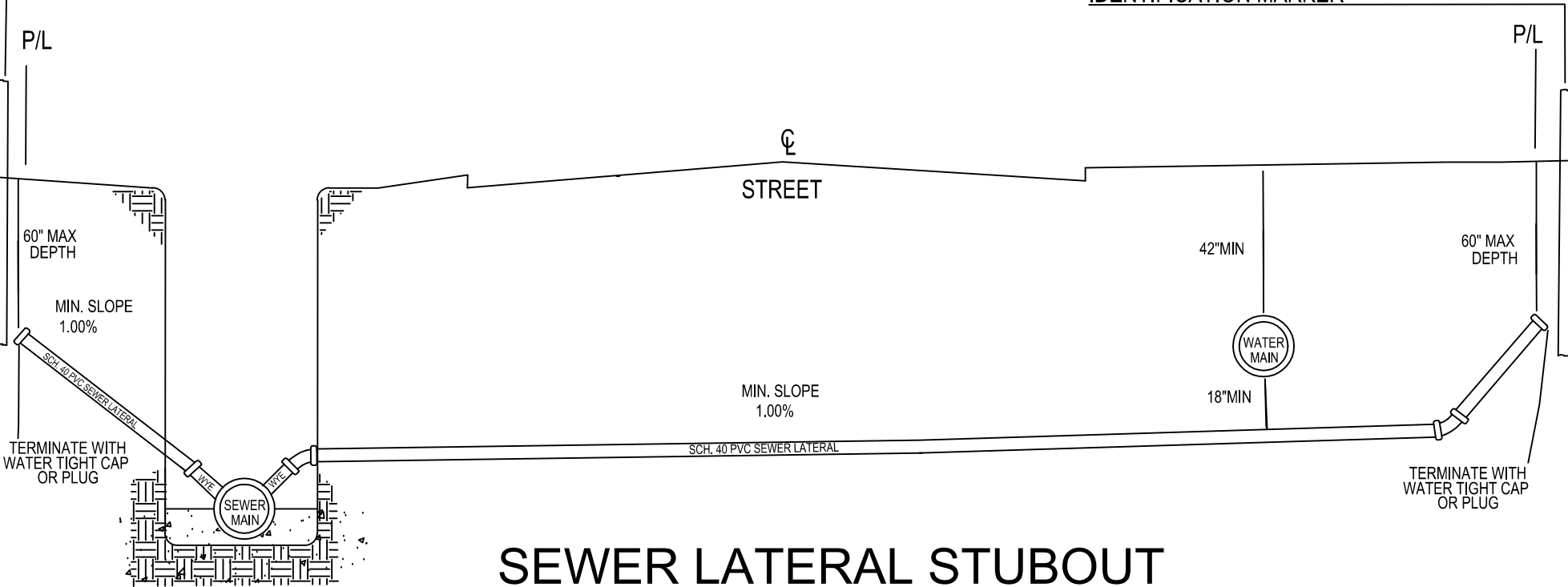
NOTE: REFER TO OPERATING ROUTING NOTE IF WATERLINE CANNOT AVOID GOING UNDER SEWERLINE



ENCASEMENT FOR SEWER MAINS AT WATER CROSSINGS

NOTE: WHEN 18" VERTICAL SEPERATION CANNOT BE MAINTAINED

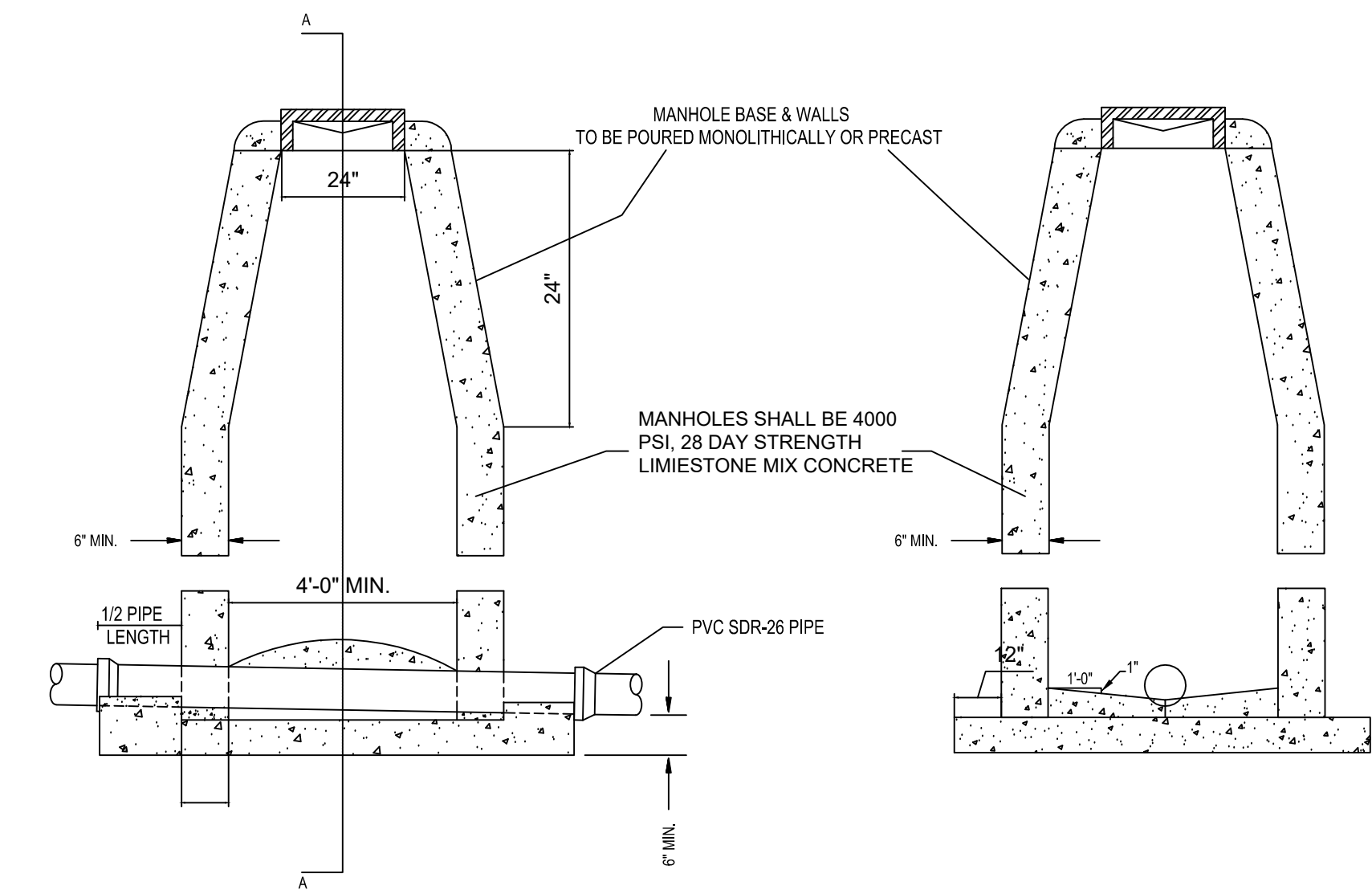
USE 4" PERFORATED SDR-35 PVC PIPE AS MARKERS @ END OF LATERALS 3'-4' ABOVE GROUND PAINTED GREEN FOR IDENTIFICATION MARKER



SEWER LATERAL STUBOUT
PERMITS REQUIRED BEFORE INSTALLATION

USE SAME CONFIGURATION FOR MANHOLE STUBOUTS

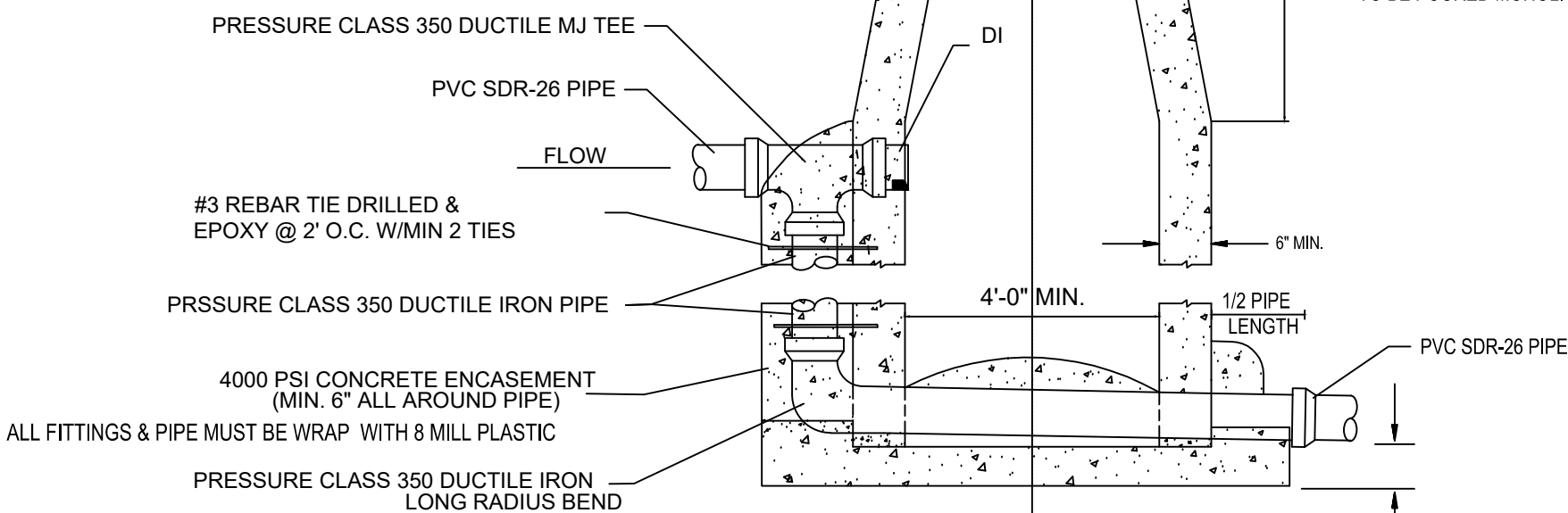
MANHOLE
CONSTRUCTION DETAIL



SIDE ELEVATION

SECTION A-A

STANDARD MANHOLE



SIDE ELEVATION

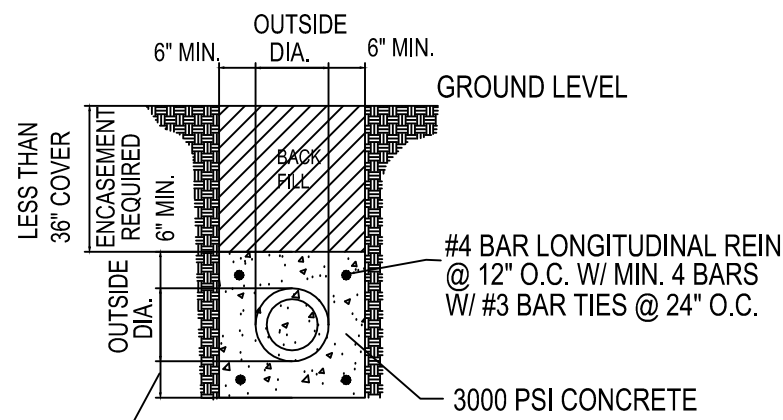
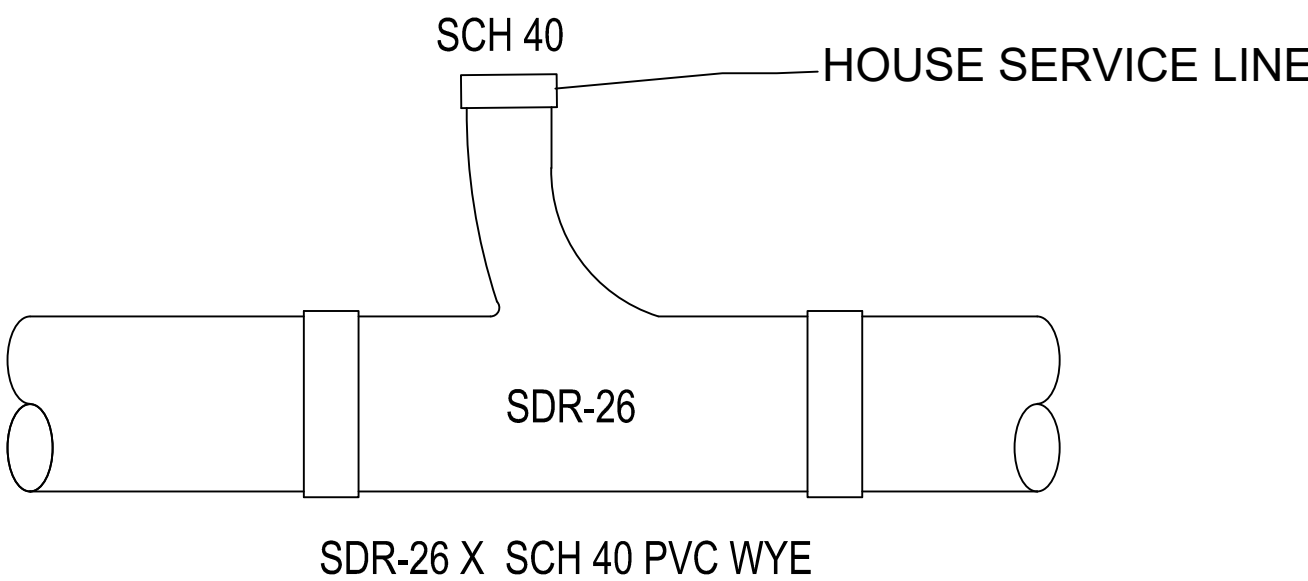
SECTION A-A

DROP MANHOLE

SINGLE WYE

ALL SERVICE LINES UNDER PAVEMENT SHALL BE PVC SCH 40 PIPE W/ PVC SCH 40 COUPLINGS

TEE WYE ONLY



CONCRETE ENCASEMENT
FOR
DITCH CROSSINGS

NOTE: ENCASEMENT PIPE MUST PRESSURE CLASS 350 DUCTILE IRON AND WRAP DUCTILE IRON PIPE WITH 8 MILL PLASTIC

SARAH CROSSING - PHASE IV

SEWER DETAILS



REVISIONS

DATE	BY	DESCRIPTION

DRAWING INFO.

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DT-3

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NOTE: CONTRACTION AND EXPANSION JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF JONESBORO SPECIFICATIONS

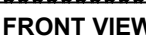
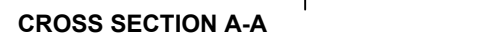
NO DRIVEWAY CUTS ALLOWED WITH THIS TYPE OF CURE



COMBINATION CURB & GUTTER

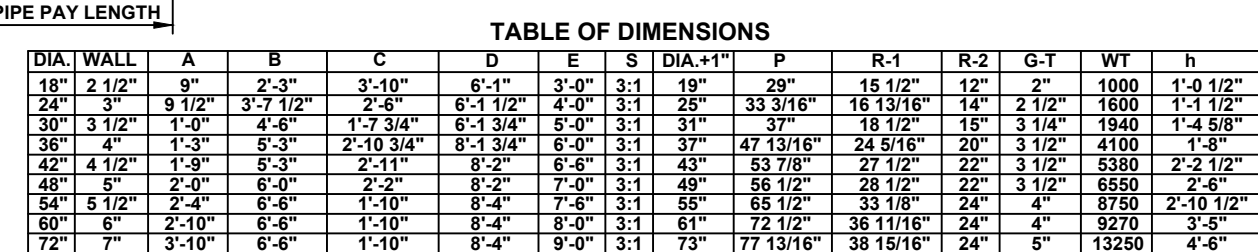
N.T.S.

NOTE: CONTRACTION AND EXPANSION JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.



SPEED TABLE

N.T.S



NOTES

1. The portion of the R.C. Curtain Wall beneath the Flared End Section (Lower 1'-0") shall be placed monolithically. The Flared End Section shall then be set in place and the remaining portions of the R.C. Curtain Wall placed.
2. All reinforcing steel are #4 Bars at 6" O.C.
3. No separate payment will be made for the Curtain Walls. They shall be considered subsidiary to the Flared End Sections.
4. Tongue End On Upstream Section. Groove End On Downstream Section.

FLARED END SECTION DETAIL FOR REINFORCED CONCRETE PIPE CULVERT

N.T.S.



NOTE:
AREA DRAIN CONSTRUCTION IS THE SAME AS
CURB INLETS AS SHOWN ON THIS DETAIL SHEET.
SEE RECTANGULAR OR CIRCULAR CURB INLET
DETAIL FOR CONSTRUCTION SPECIFICATIONS.

AREA DRAIN DETAIL

N.T.S.



N.T.S.

NOTE:
TYPE I FRAME AND COVER PERMITTED ONLY ON STRUCTURES
NOT SUBJECT TO VEHICULAR TRAFFIC.



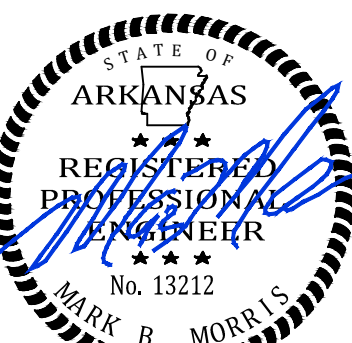
N.T.S.

STEPS SPECIFICATIONS:

- A. MANHOLE STEPS: CORROSION RESISTANT, COATED AND REINFORCED WITH STEEL PER ASTM C-478. STEEL REINFORCING MINIMUM 1/2" DIAMETER.**
- B. CAPABLE OF SUPPORTING MINIMUM 300 lb. LOAD.**
- C. NON-SLIP TEXTURED TREADS**
- D. REQUIRED IN ALL STRUCTURES 3'-0" DEEP OR DEEPER,**

SARAH CROSSING - PHASE IV

STREET DETAILS



REVISIONS

DATE	BY	DESCRIPTION

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SCALE:	

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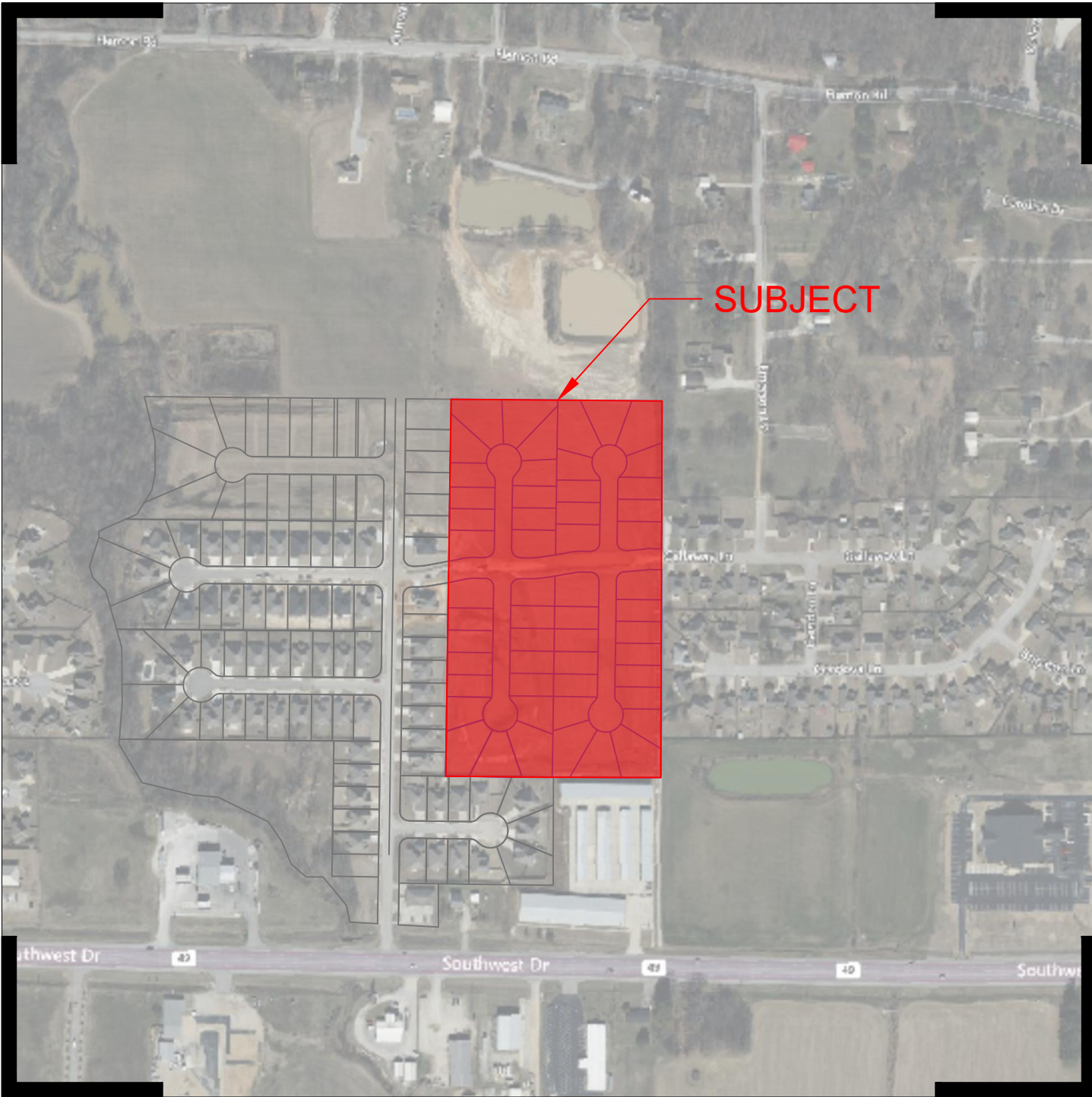
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SARAH CROSSING SUBDIVISION PHASE IV

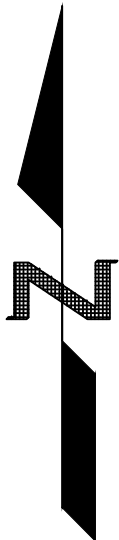
JONESBORO, CRAIGHEAD COUNTY, ARKANSAS SITE DEVELOPMENT PLAN

LOCAL CONTACTS

CITY OF JONESBORO ENGINEERING DEPT.	870-932-2438
CITY OF JONESBORO PLANNING AND ZONING	870-932-0406
CWL (WATER/ELECTRICITY)	870-935-5581
AT&T (CABLE/FIBER)	800-464-7928
RITTER (CABLE/FIBER)	870-336-4466
OPTIMUM/ALTICE (CABLE/FIBER)	866-950-3278
SUMMIT (GAS)	800-992-7552



VICINITY MAP
(NOT TO SCALE)



NAD83 Arkansas State Planes, North Zone, US Foot

INDEX OF SHEETS

PAGE	DESCRIPTION
C-1	COVER PAGE
P-1	PLAT
OV-1	KEY MAP
OV-2	OVERALL UTILITY PLAN
E-1	EROSION CONTROL PLAN
UT-1	UTILITY CROSSING PLAN
WT-1	WATER PLAN
SW-1	SEWER PLAN
SW-2	SEWER PLAN & PROFILE
GR-1	GRADING & DRAINAGE PLAN
ST-1	STREET PLAN & PROFILE
DT-1	EROSION CONTROL DETAILS
DT-2	WATER DETAILS
DT-3	SANITARY SEWER DETAILS
DT-4	CONSTRUCTION DETAILS

SARAH CROSSING - PHASE IV

COVER PAGE



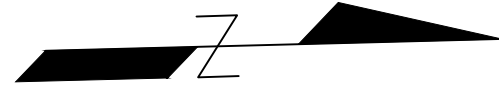
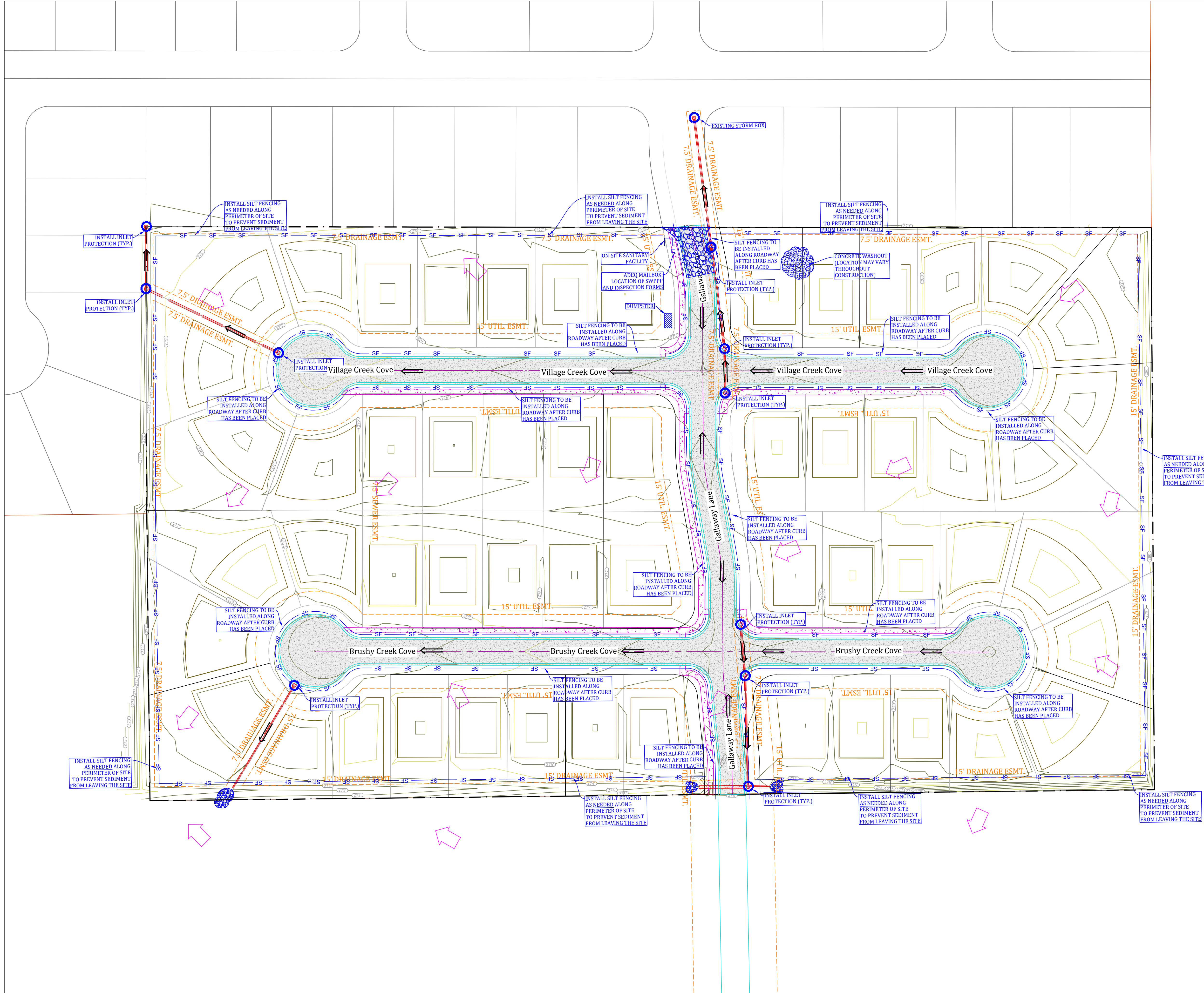
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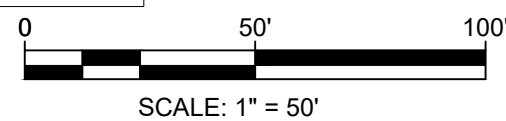
SHEET NUMBER:
C-1

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Z:\Morris Real Estate Holdings, LLC\Projects\Sarah's Crossing\Engineering\Phase IV\Sarah Crossing P4 11 2025.dwg, 7/1/2025 3:52:24 PM



- — SILT FENCE BARRIERS TO BE ERECTED AROUND INLET LOCATIONS PRIOR TO BOX TOPS BEING POURED TO PREVENT SEDIMENT FROM INFILTRATING STORM DRAIN NETWORK; ONCE BOX TOPS HAVE BEEN POURED, WATTLES SHALL BE UTILIZED TO CONTINUE PREVENTION OF SEDIMENT INFILTRATION.
- — PRE-DEVELOPMENT FLOW DIRECTION
- — POST-DEVELOPMENT FLOW DIRECTION
- SILT FENCE, INSTALL AS NECESSARY TO DIVERT FLOW AND PREVENT SEDIMENT FROM LEAVING THE SITE.



SARAH CROSSING - PHASE IV

EROSION CONTROL PLAN



REVISIONS			
DATE	BY	DESCRIPTION	

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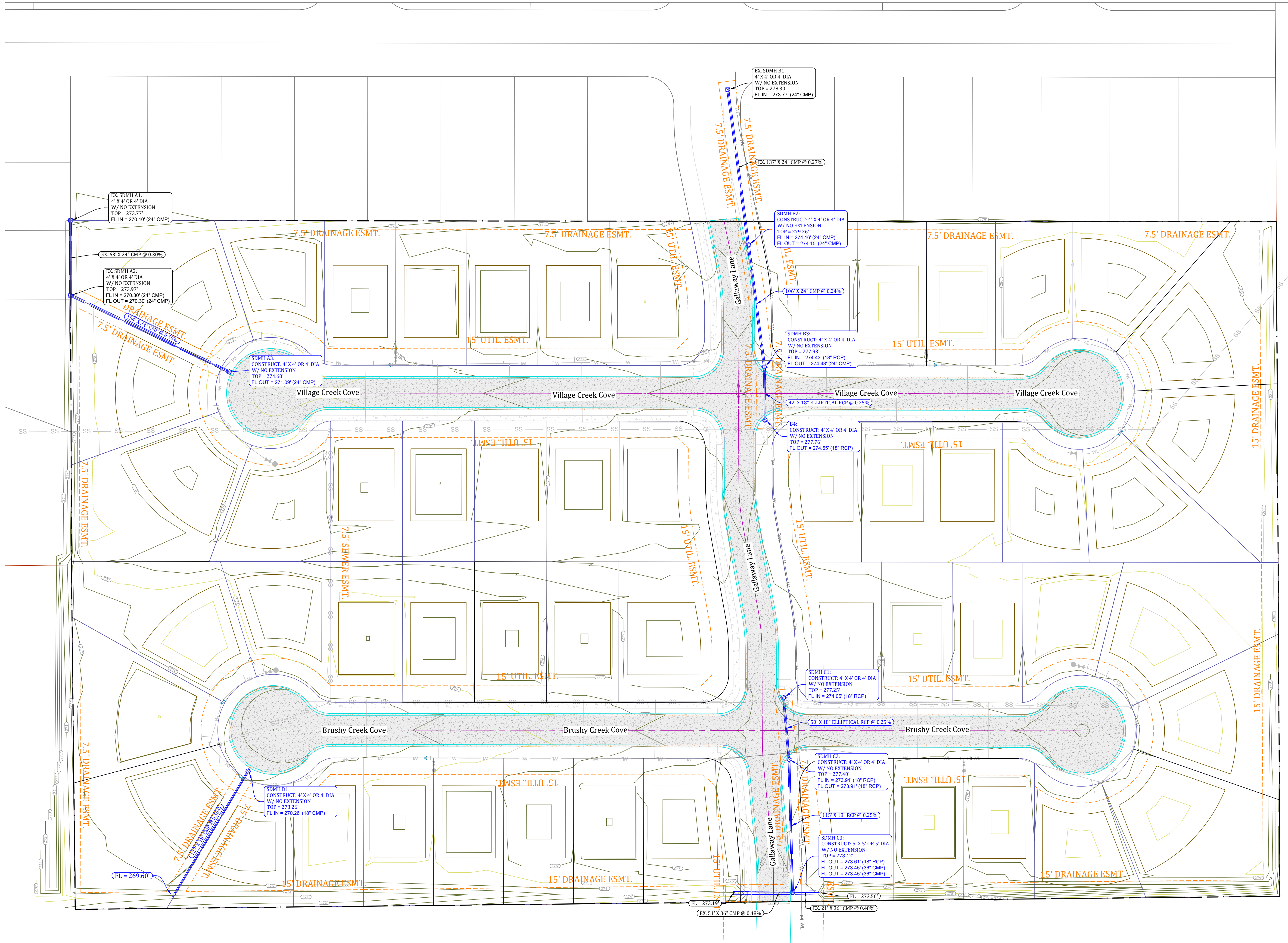
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E-1

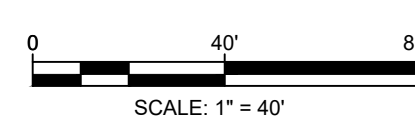
MARK MORRIS

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NOTE:
NO SIGNIFICANT TREES FOUND
ON SITE



SARAH CROSSING - PHASE IV
GRADING & DRAINAGE PLAN

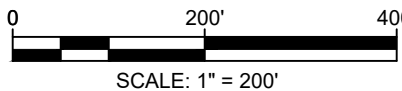
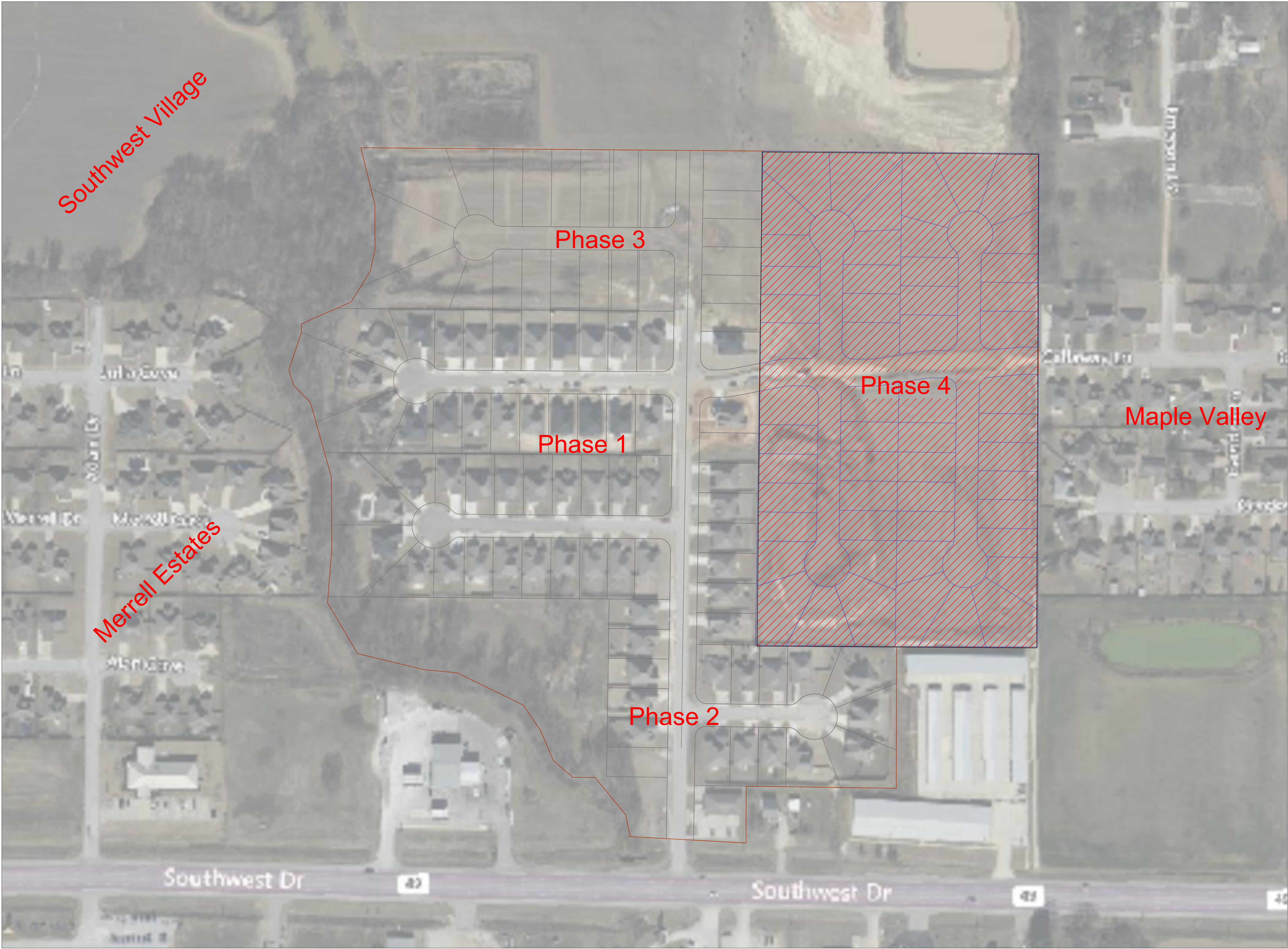


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GR-1	

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Know what's below.
Call before you dig.

SARAH CROSSING - PHASE IV

KEY MAP



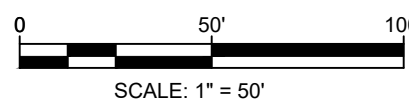
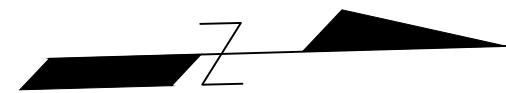
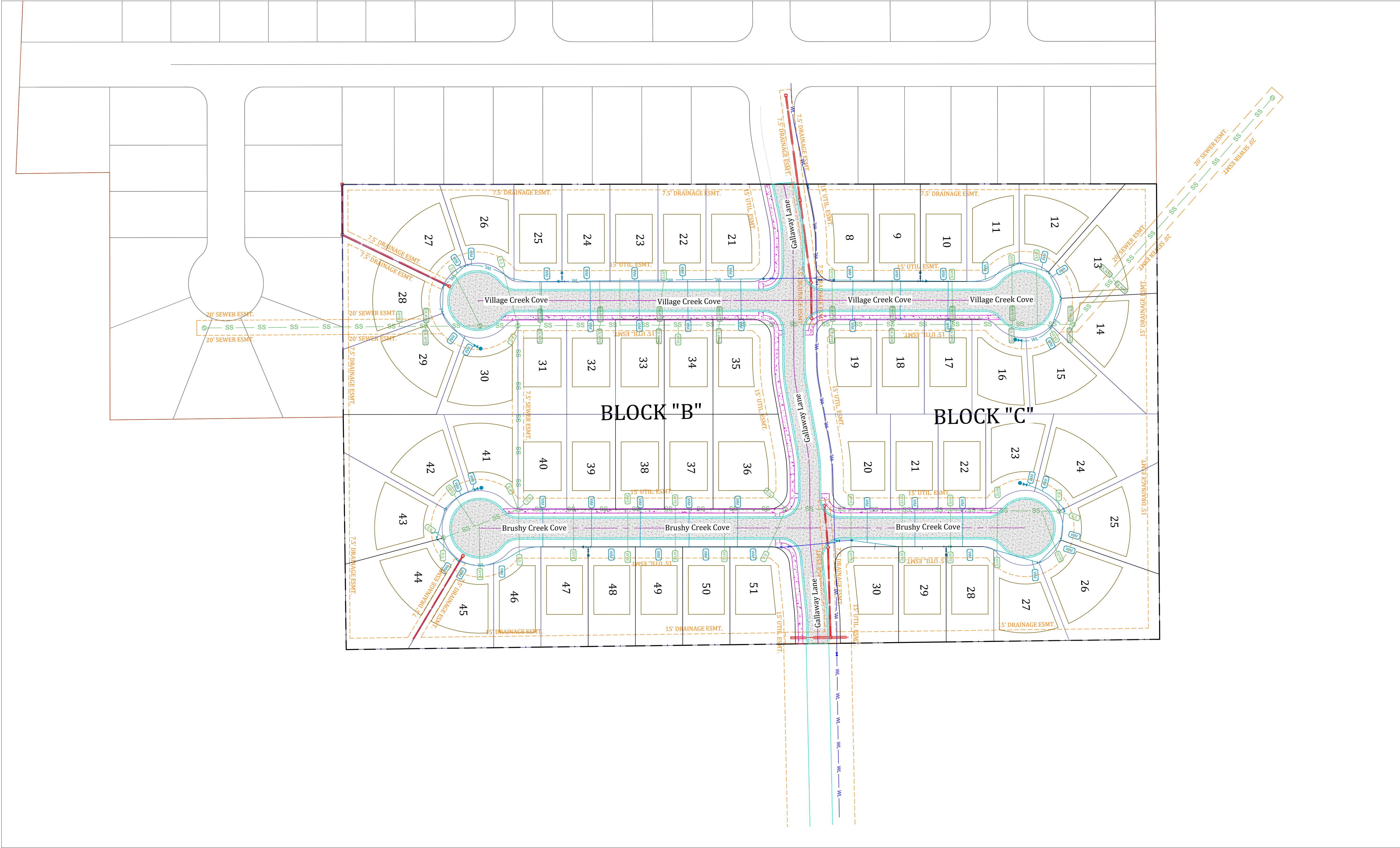
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DATE:	06/04/2025
SCALE:	1"=200'

SHEET NUMBER:
OV-1

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MARK MORRIS



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SARAH CROSSING - PHASE IV

OVERALL UTILITY PLAN



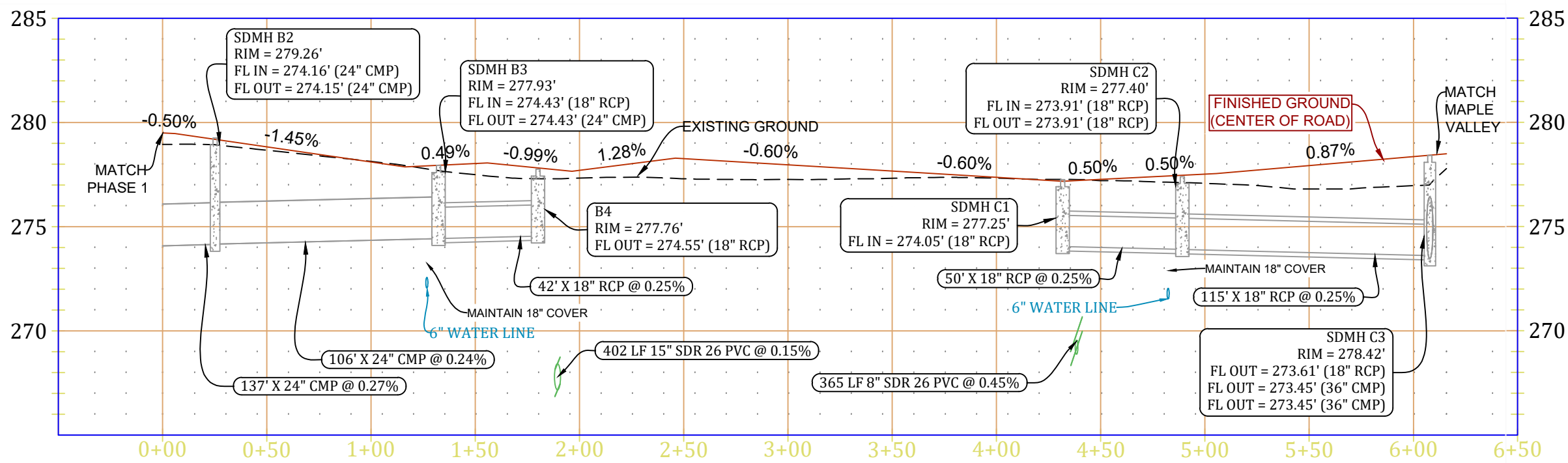
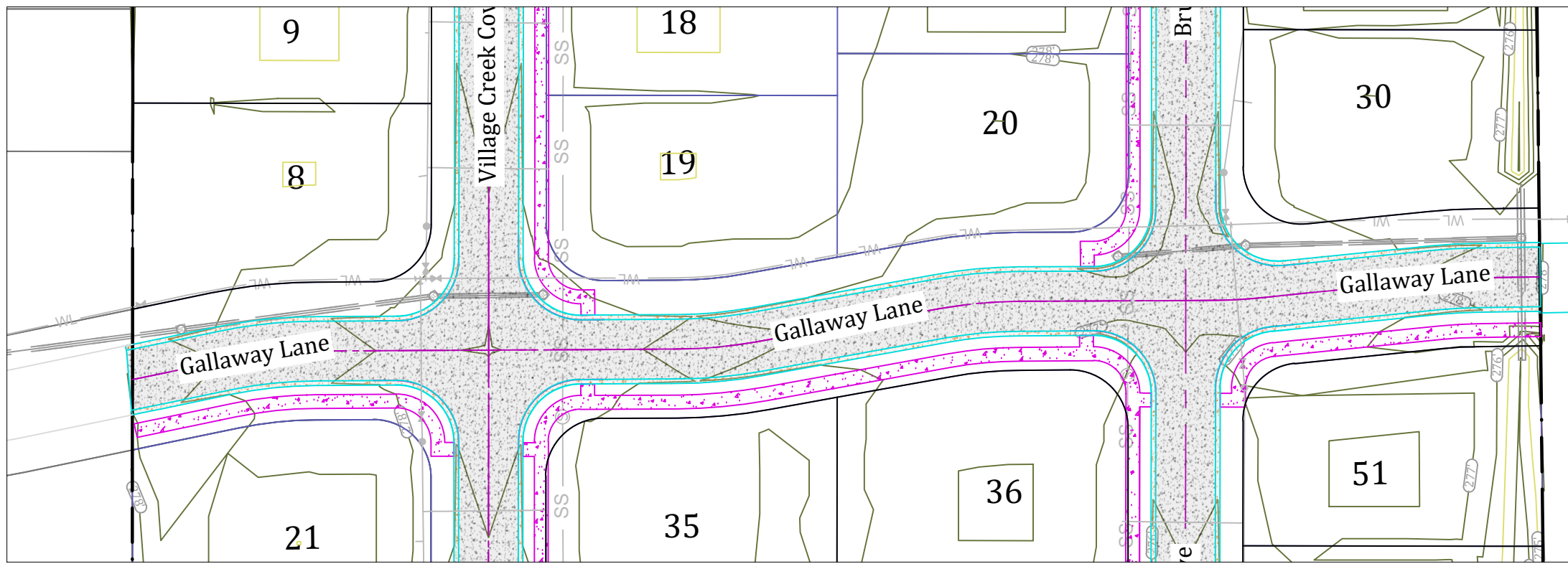
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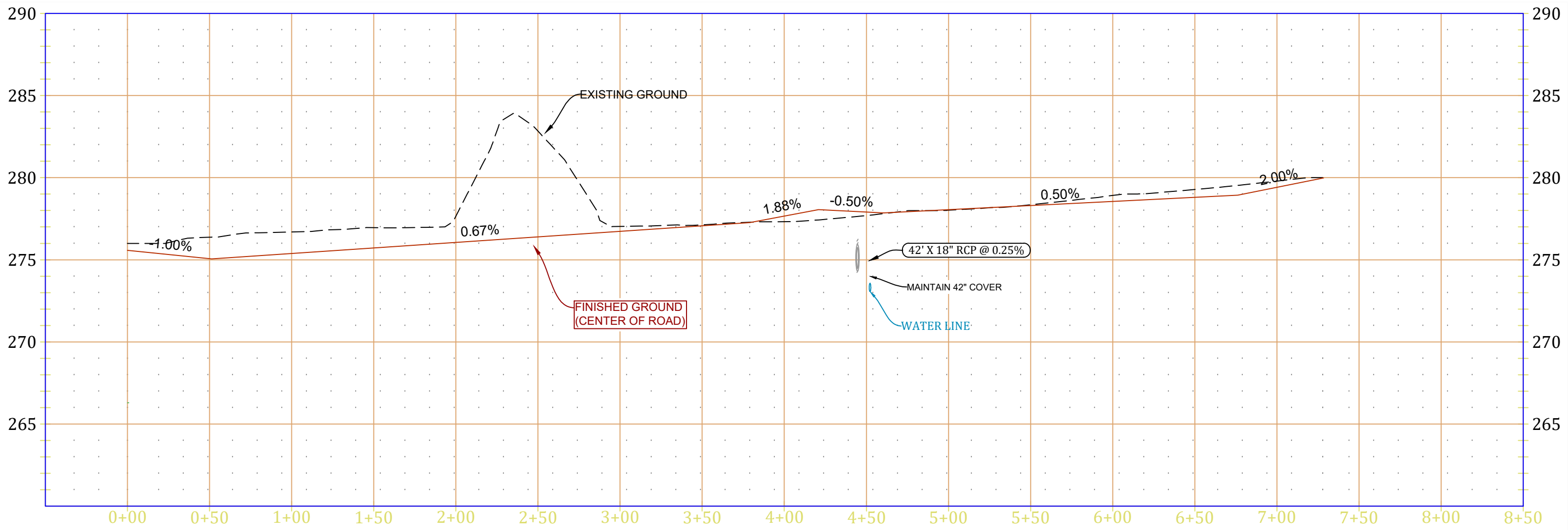
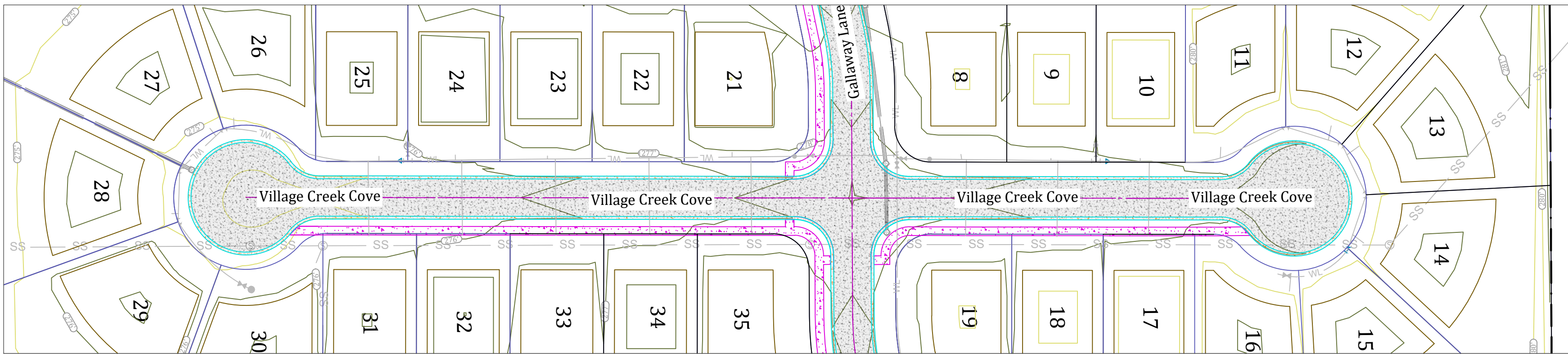
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OV-2

MARK MORRIS

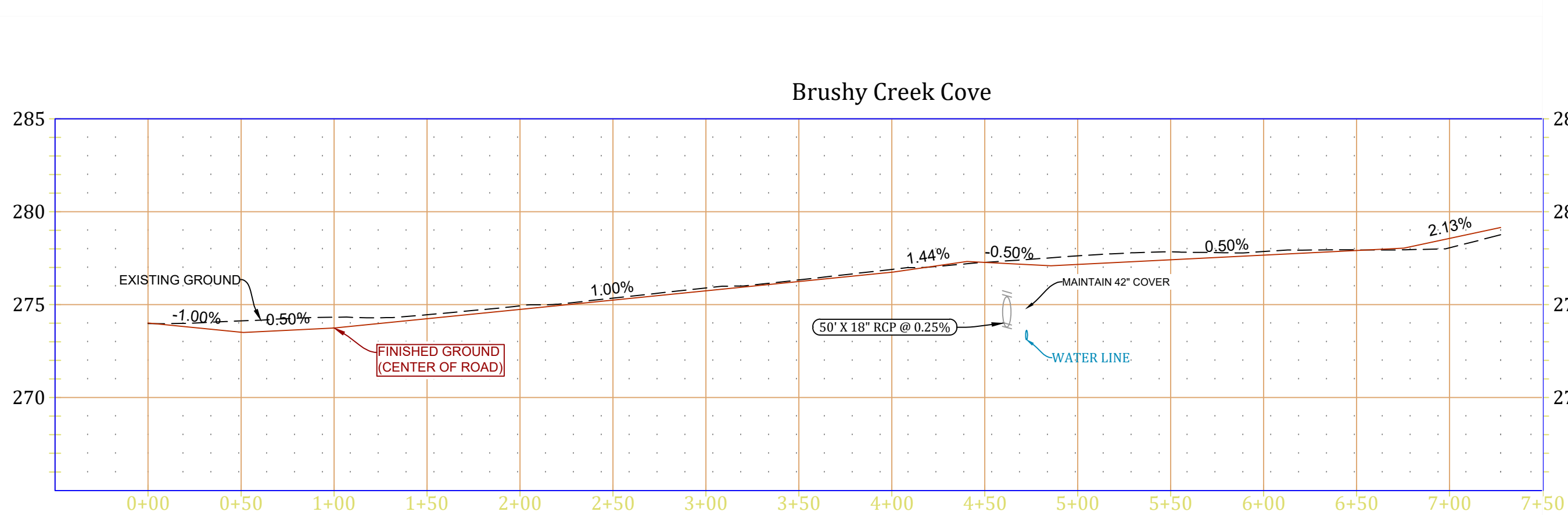
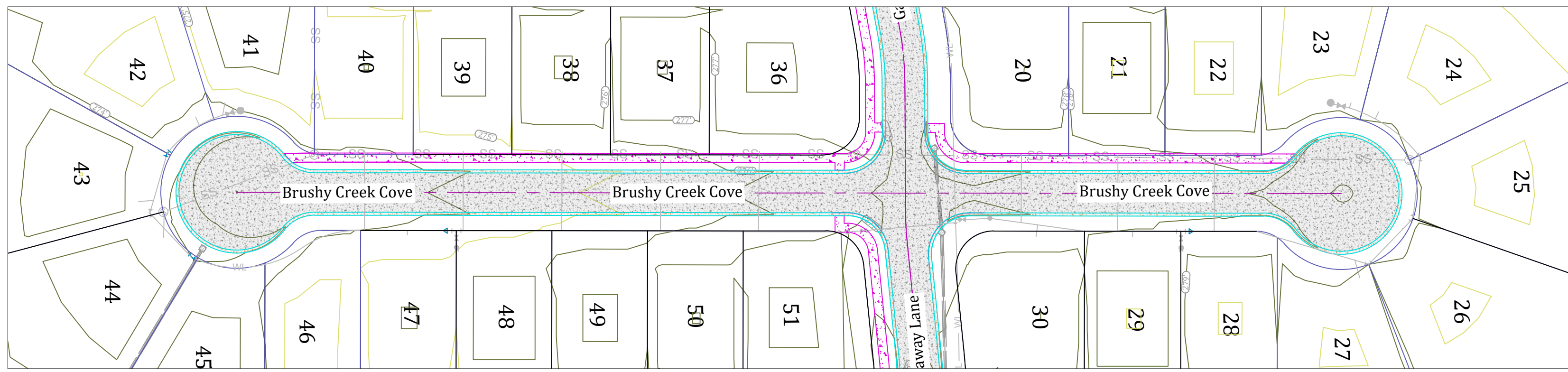
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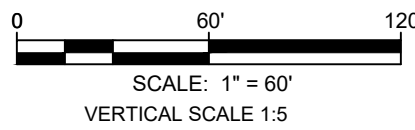
GALLAWAY LANE PLAN/PROFILE



VILLAGE CREEK COVE PLAN/PROFILE



BRUSHY CREEK COVE PLAN/PROFILE



SARAH CROSSING - PHASE IV
STREET PLAN & PROFILE



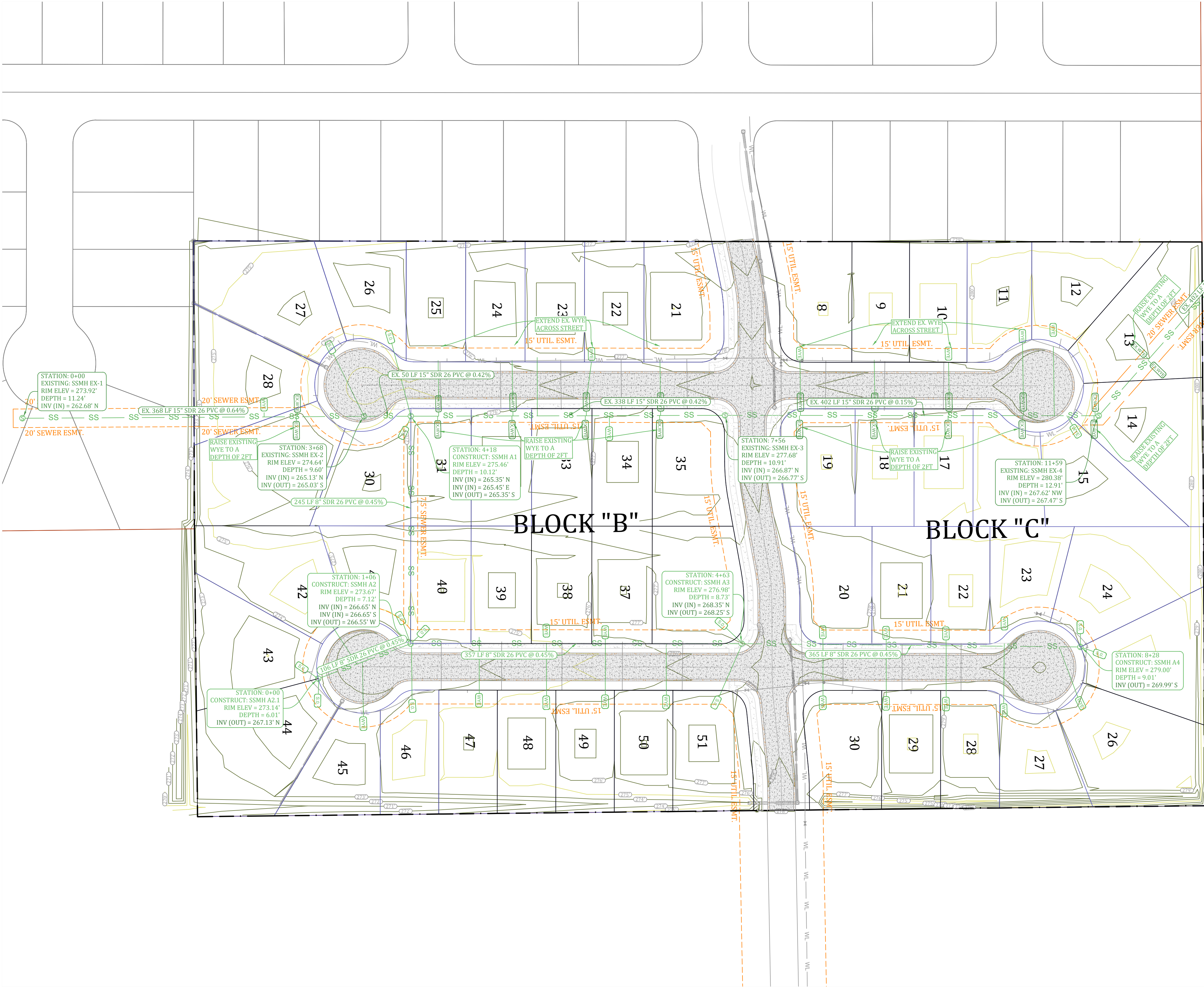
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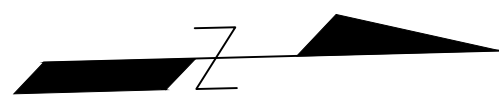
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MARK MORRIS



LEGEND			
	CURB		STORM DRAIN INLET
	SIDEWALK		STORM DRAIN PIPE
	CORRIDOR CROSS SECTION LINE		SANITARY SEWER
	WATER LINE		SEWER MANHOLE
	WATER METER		SEWER WYE
	FIRE HYDRANT		SEWER STUB-OUT
	BLOW-OFF ASSEMBLY		EX. SEWER WYE
	GATE VALVE		EX. SEWER STUB-OUT

TOTAL SEWERLINE QUANTITIES:
1073 LF - 8" SDR26 PVC
21- SHORT LATERALS
21- LONG LATERALS
8- SHORT STUB-OUTS
4- LONG STUB-OUT



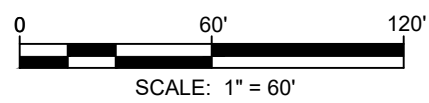
SANITARY SEWER NOTES:

- CONTRACTOR SHALL PROVIDE WYES OR STUBOUTS FOR EACH LOT. THEY SHALL BE PLACED ON THE LOW ELEVATION SIDE OF LOT OR IN THE CENTER OF THE LOT IF THE ELEVATION IS FLAT.
- CONTRACTOR SHALL PROVIDE ENGINEER W/AS-BUILT DIMENSIONS FOR WYES, VALVES, TEES, BENDS, FIRE HYDRANTS, & WATER METERS.
- CONTRACTOR SHALL PROVIDE LABOR AND MATERIALS FOR ALL SEWER, WATER, & ELECTRICAL LINE LATERALS & PROVIDE LABOR & MATERIAL TO BACKFILL ALL SEWER, WATER & ELECTRICAL STREET CUTS WITH SANDY MATERIAL (P.1 -15) TO TOP OF EXCAVATION & COMPACT FILL TO 95% COMPACTION & PROVIDE COMPACTION TESTS AT 2' INTERVALS. CONTRACTOR SHALL PROVIDE LABOR AND MATERIAL FOR ALL SEWER & WATER LINE MAINS THAT CROSS A STREET AND BACKFILL THEM WITH SANDY (P.1 -15) TO TOP OF EXCAVATION & COMPACT FILL TO 95% COMPACTION & PROVIDE MATERIAL COMPACTION TESTS AT 2' INTERVALS.
- CONTRACTOR SHALL CALL ARK ONE CALL FOR LOCATES PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL NOTIFY ENGINEER TO DETERMINE ELEVATIONS FOR MANHOLE TOPS, WATERLINE ELEVATION WHERE IT CROSSES PROPOSED STORM DRAINS, WATER METER BOX ELEVATIONS, VALVE BOX ELEVATIONS, AND FIRE HYDRANT ELEVATIONS, AND WATER LINE LOCATION AROUND INLETS. WATER LINE SHALL BE INSTALLED AT AN ELEVATION WHERE HYDRANTS ARE LOCATED TO ENSURE HYDRANT WILL BE AT PROPER HEIGHT WHEN FINISHED GRADE HAS BEEN OBTAINED.
- CONTRACTOR SHALL PROVIDE OWNER WITH ONE YEAR WARRANTY FOR MATERIALS AND WORKMANSHIP.
- CONTRACTOR SHALL PROVIDE ENGINEER WITH LIEN RELEASES FOR MATERIALS AND LABOR, BACTERIAL SAMPLE RESULTS, AND MODEL & YEAR OF WATER LINE VALVES WITHIN 30 DAYS OF COMPLETION OF WORK.
- CONTRACTOR SHALL DRESS UP PROJECT AFTER INSTALLATION TO THE CONDITION IT WAS PRIOR TO BEGINNING CONSTRUCTION. MOUND BACKFILL OVER TRENCHES 1' TO 2' TO ALLOW FOR SETTLING IN AREAS OUT OF THE STREET. ALL STREET EXCAVATION MATERIAL SHALL BE DISPOSED OF BY CONTRACTOR ON THE ADJACENT LOTS OR AS DIRECTED BY OWNER.
- CONTRACTOR SHALL REVIEW DETAIL SHEETS TO ENSURE PROPER INSTALLATION OF VALVES, FIRE HYDRANTS, CONCRETE BLOCKING, SERVICE LINES, MANHOLE INVERTS, AND DROP ASSEMBLIES.
- ALL SEWER LINES SHALL BE ASTM 2241 SDR-26 OR OTHER AS NOTED
- WATER LINE AND SANITARY SEWER LINE SHALL MAINTAIN 18" MINIMUM VERTICAL SEPARATION. WATER/SEWER LINE AND STORM DRAIN SHALL MAINTAIN 36" MINIMUM HORIZONTAL SEPARATION & 18" VERTICAL SEPARATION TO PREVENT FREEZING. MAINTAIN AS MUCH DISTANCE BETWEEN WATER AND SEWER LINE AS POSSIBLE, BUT NO LESS THAN 10" HORIZONTAL SEPARATION. WHERE 18" VERTICAL SEPARATION BETWEEN WATER AND SEWER LINES IS UNAVOIDABLE OR IN LOCATIONS WHERE WATER MUST UNAVOIDABLY CROSS UNDER SANITARY SEWER LINE, REGARDLESS OF VERTICAL SPACING, THEN WATER LINE AND/OR SEWER LINE MUST BE ENCASED 10 FEET EITHER SIDE OF THE CROSSING LOCATION. ANY JOINT IN THE ENCASEMENT PIPE SHALL BE MECHANICALLY RESTRAINED. EXCEPTIONS TO THIS REQUIREMENT MUST BE APPROVED IN WRITING BY THE ARKANSAS DEPARTMENT OF HEALTH. SEE SECTION XIV.A OF THE PUBLIC WATER SYSTEMS MANUAL FOR THE STATE OF ARKANSAS FOR FULL TEXT OF SPECIFICATIONS.
- CONSTRUCTION SHALL BEGIN ONLY AFTER APPROVED PLANS FROM THE ARKANSAS DEPARTMENT OF HEALTH ARE SUBMITTED TO CWL, AND ALL NECESSARY FEES ARE PAID IN FULL. CONSTRUCTION BY CONTRACTORS SHALL THEN BEGIN ONLY AFTER THE RIGHT OF WAY HAS BEEN CLEARED, THE ENTIRE SECTION STAKED, THE ELEVATIONS CAREFULLY CHECKED, AND A PRE-CONSTRUCTION MEETING HAS BEEN HELD. A POST CONSTRUCTION MEETING WILL BE HELD W/ CONTRACTOR PRESENT TO VERIFY ALL CONSTRUCTION IS ACCEPTABLE. TO CWL PRIOR TO THE INSTALLATION OF ELECTRICAL WIRE.
- DEVELOPER MUST SHOW PROOF OF ADEQUATE INSURANCE COVERAGE. (ARKANSAS WORKER'S COMPENSATION, \$1,000,000.00 MINIMUM GENERAL LIABILITY, AND \$1,000,000.00 MINIMUM AUTO LIABILITY.) CONTRACTOR MUST HAVE A STATE CONTRACTORS LICENSE. CONTRACTOR MUST HAVE A COMPETENT PERSON AT THE JOB SITE AS DEFINED BY OSHA.
- DURING CONSTRUCTION, WORK SHALL BE INSPECTED BY THE DEVELOPER'S CONSULTING ENGINEER FOR NECESSARY SAFETY PRACTICES, PROPER MATERIALS, AND WORKMANSHIP. CWL WILL PROVIDE RANDOM INSPECTIONS TO INSURE THAT THE PLANS APPROVED BY THE ARKANSAS DEPARTMENT OF HEALTH AND CWL ARE FOLLOWED CONCERNING WORKMANSHIP AND MATERIALS.
- IF SEWER AND WATER LINES HAVE NOT BEEN ACCEPTED BY CWL WITHIN 360 DAYS AFTER MANHOLE TESTS, LOW PRESSURE AIR TESTS, HYDROSTATIC TESTS AND BACTERIAL TESTS, ALL TESTS WILL BE REQUIRED TO BE REPEATED AND RE-CERTIFIED.
- CWL ENGINEERING DEPT. MUST APPROVE ANY WATER OR SEWER LINES THAT WILL BE INSTALLED CLOSER THAN 5' PARALLEL WITH EDGE OF PAVED STREET SURFACE.
- REPAIRS SHALL BE MADE IN ACCORDANCE WITH AWWA.
- ALL SEWER WYES & STUBOUTS SHALL BE EXTENDED ACROSS STREETS TO THE APPROPRIATE LOT LINES. CONTRACTOR IS RESPONSIBLE FOR ENSURING ALL LOTS HAVE A SEWER SERVICE.

TRENCH SETTLEMENT NOTE:
CITY WATER & LIGHT IS NOT RESPONSIBLE FOR ANY SETTLEMENT OVER WATER & SEWER TRENCHES THAT OCCURS BEFORE OR AFTER THE WARRANTY PERIOD.

ALL SEWER WYES SHALL BE A MINIMUM OF 10' FOOT AWAY FROM PROPERTY CORNERS.

NOTE: IF UNSAFE PRACTICES ARE DISCOVERED BY CWL DURING OUR INSPECTIONS OF WORKMANSHIP AND MATERIALS, CWL WILL NOTIFY THE OSHA. THIS NO WAY OBLIGATES CWL FOR THE RESPONSIBILITY OF THE CONTRACTORS SAFETY PRACTICES.



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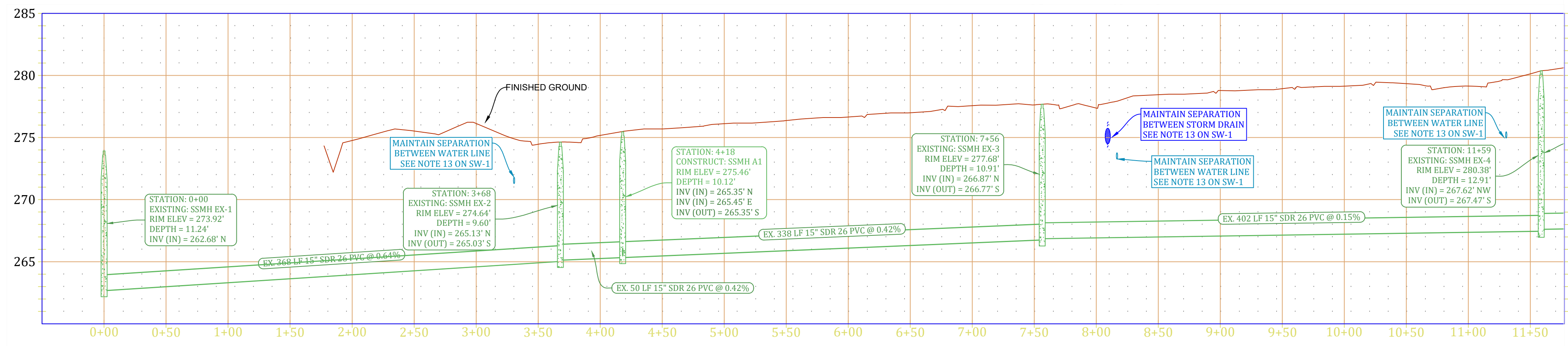
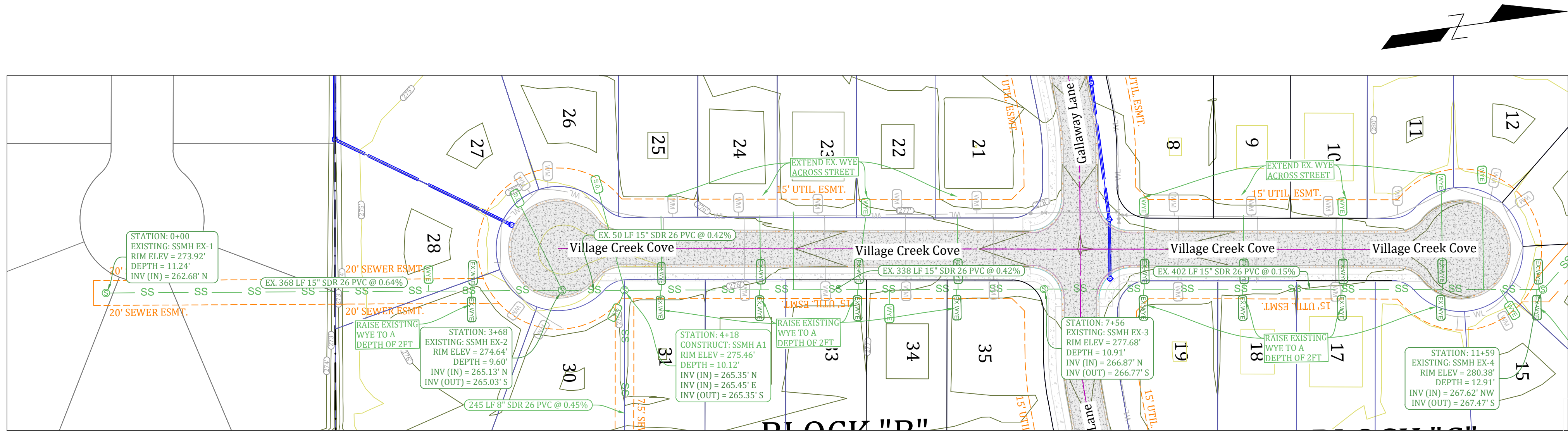
SARAH CROSSING - PHASE IV
GENERAL SEWER PLAN



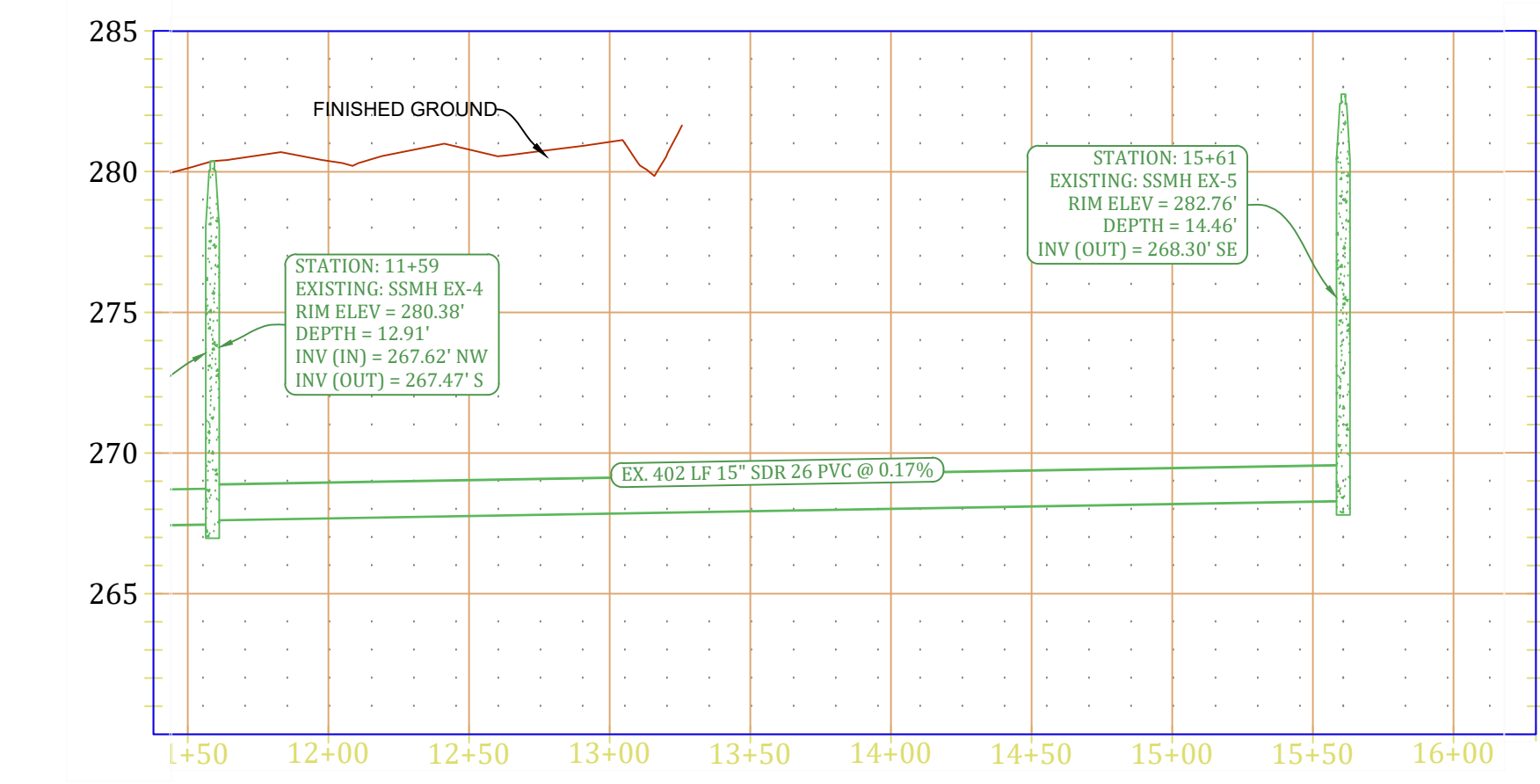
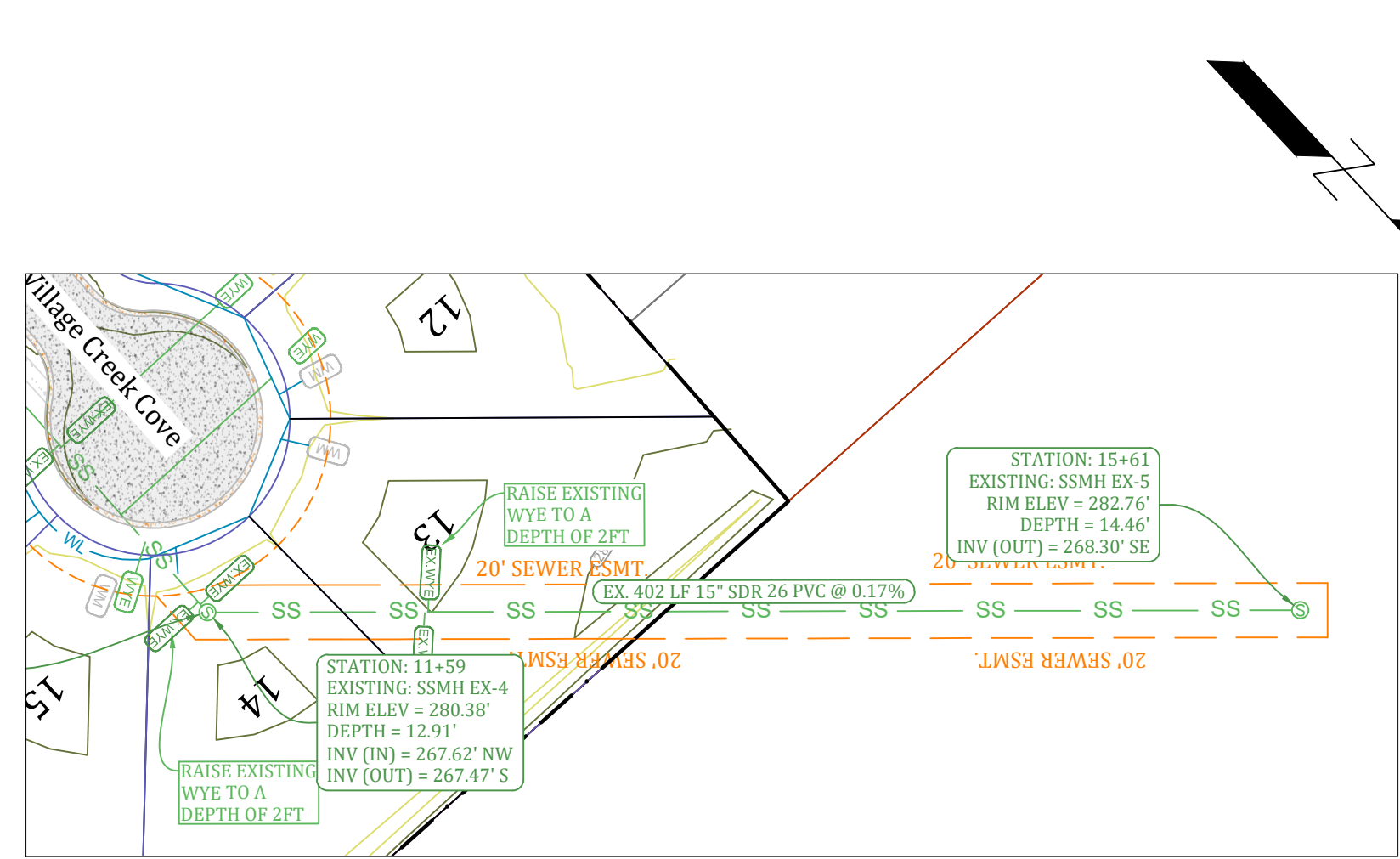
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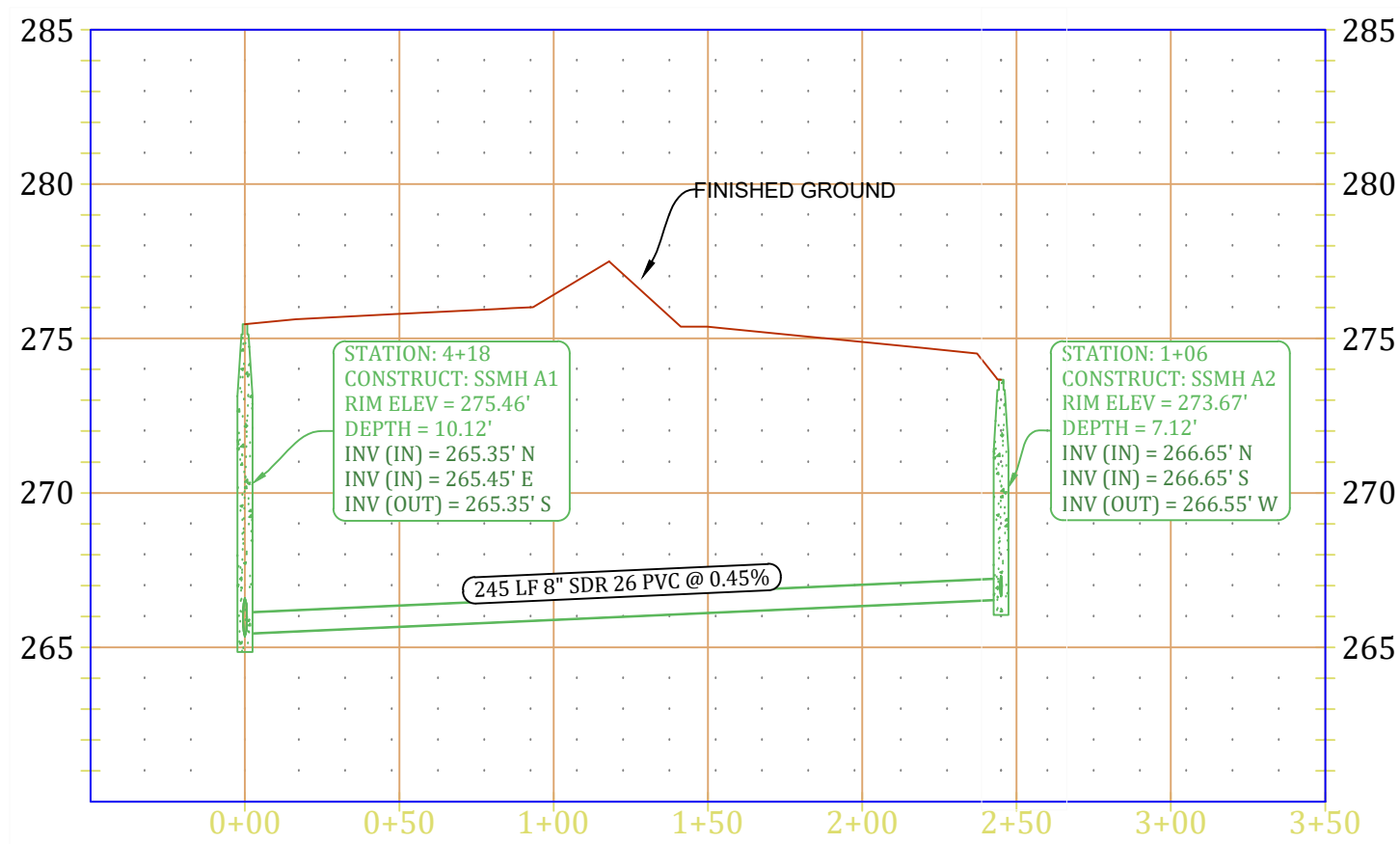
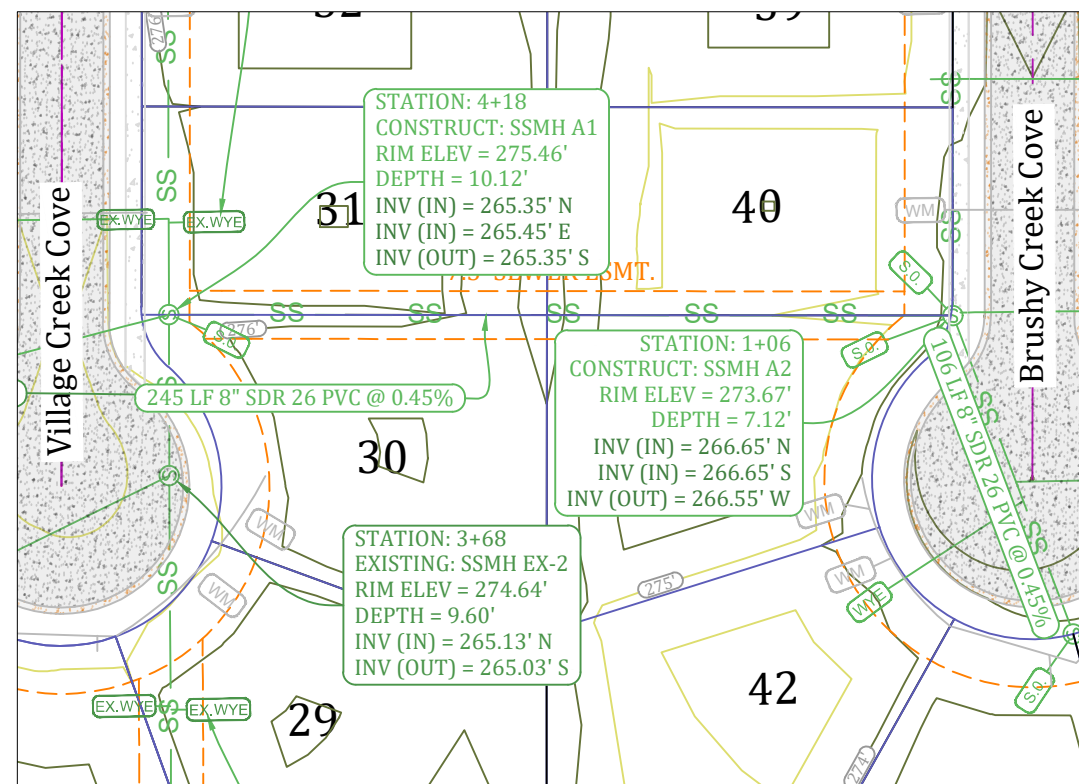
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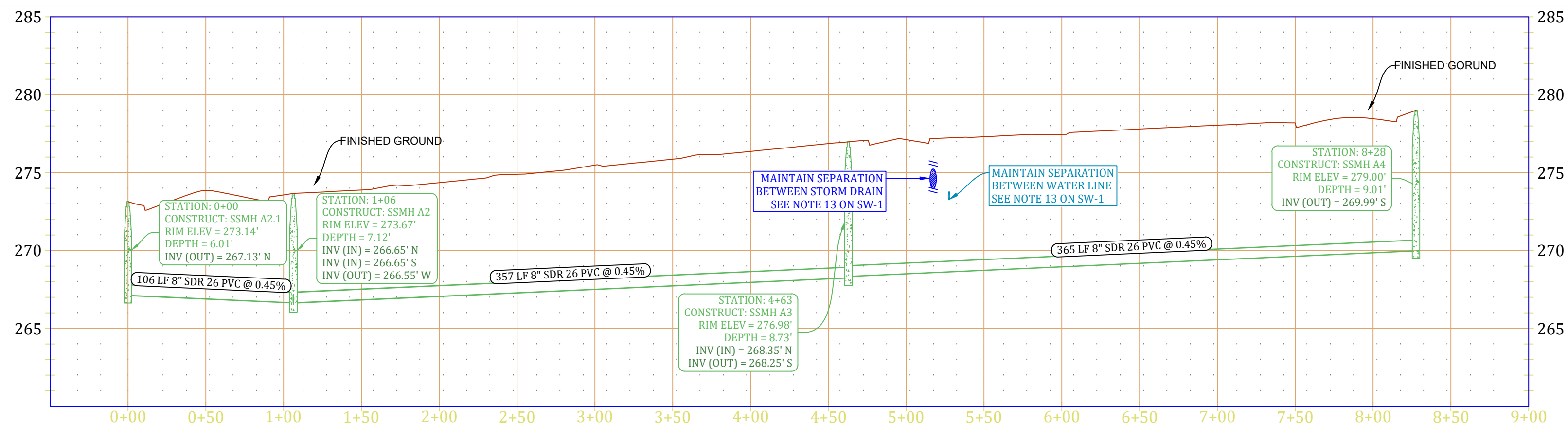
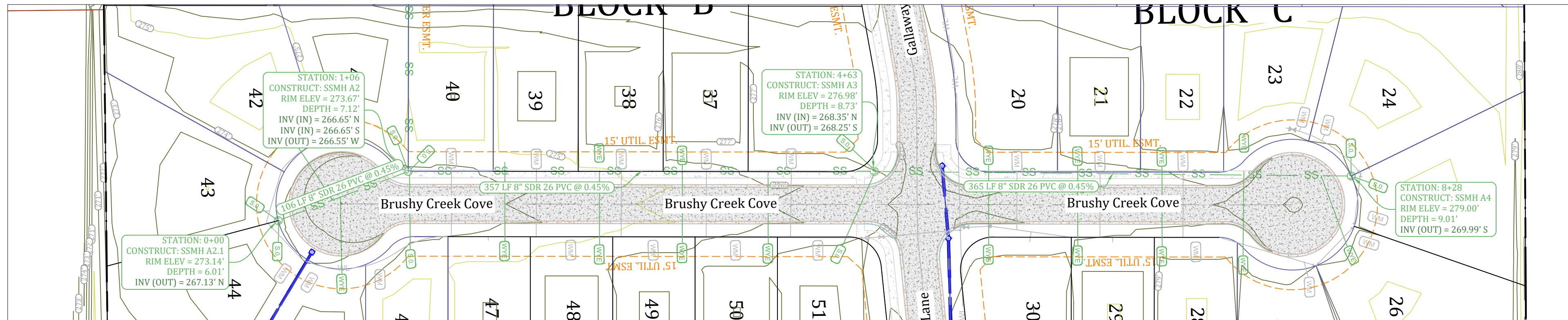
EXISTING SEWER LINE - PLAN/PROFILE (SSMH EX-1 TO SSMH EX-4)



EXISTING SEWER LINE - PLAN/PROFILE (SSMH EX-4 TO EX-5)



SEWER LINE A - PLAN/PROFILE (SSMH A1 TO A2)

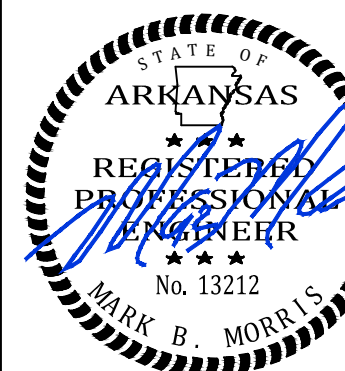


SEWER LINE A - PLAN/PROFILE (SSMH A2.1 TO A4)

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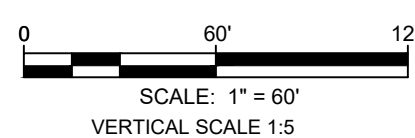
SARAH CROSSING - PHASE IV
SEWER PLAN & PROFILE

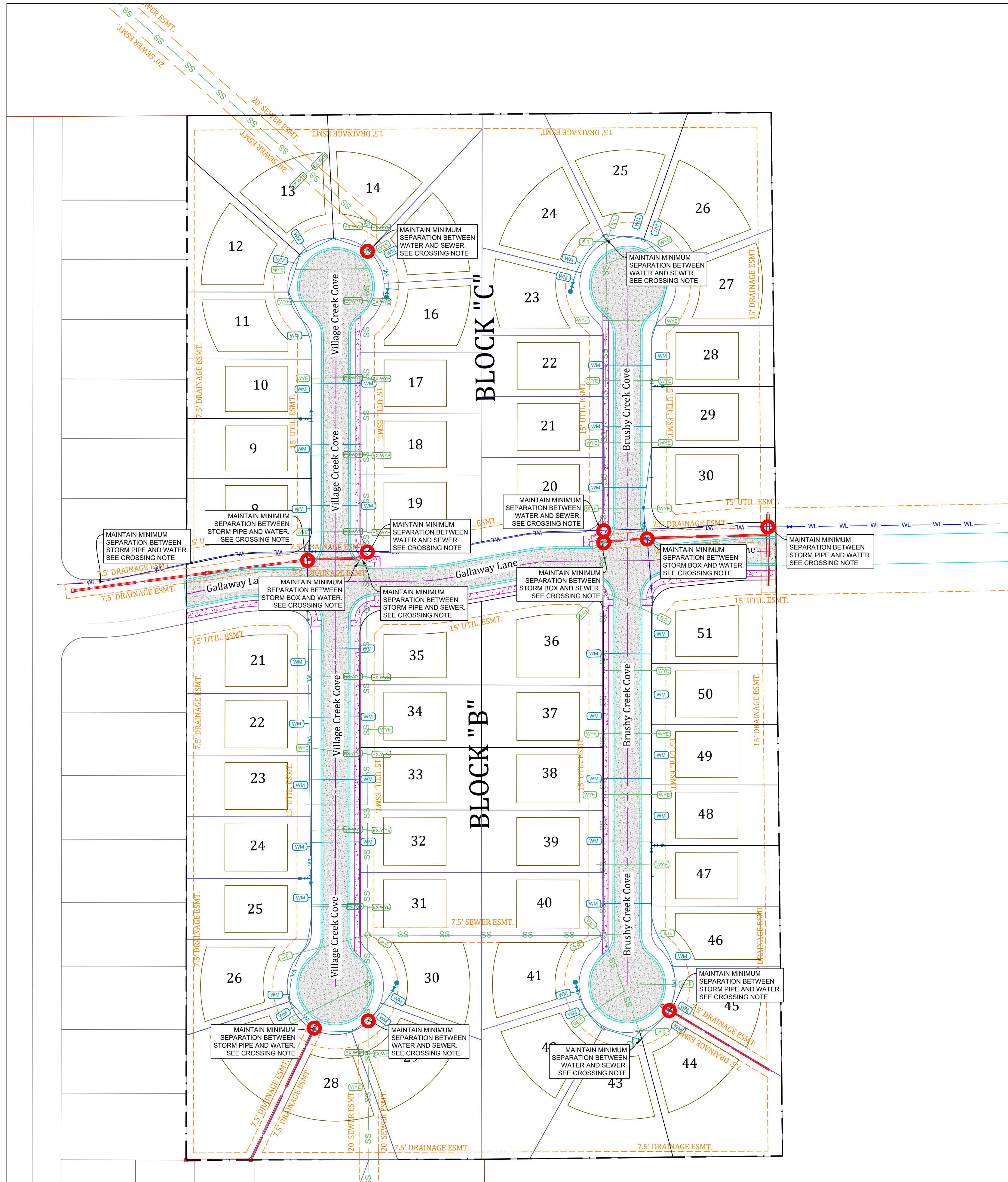


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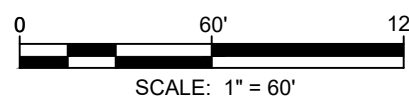
CROSSING NOTES

> WATER LINE AND SANITARY SEWER LINE SHALL MAINTAIN 18" MINIMUM VERTICAL SEPARATION.

> WATER/SEWER LINE AND STORM DRAIN NETWORK SHALL MAINTAIN 36" MINIMUM HORIZONTAL SEPARATION AND 18" VERTICAL SEPARATION TO PREVENT FREEZING.

> MAINTAIN MINIMUM 10' HORIZONTAL SEPARATION BETWEEN WATER AND SANITARY SEWER LINES.

> SEE SEWER PLAN AND PROFILE SHEETS FOR CROSSING DEPICTIONS.



SARAH CROSSING - PHASE IV

UTILITY CROSSING PLAN



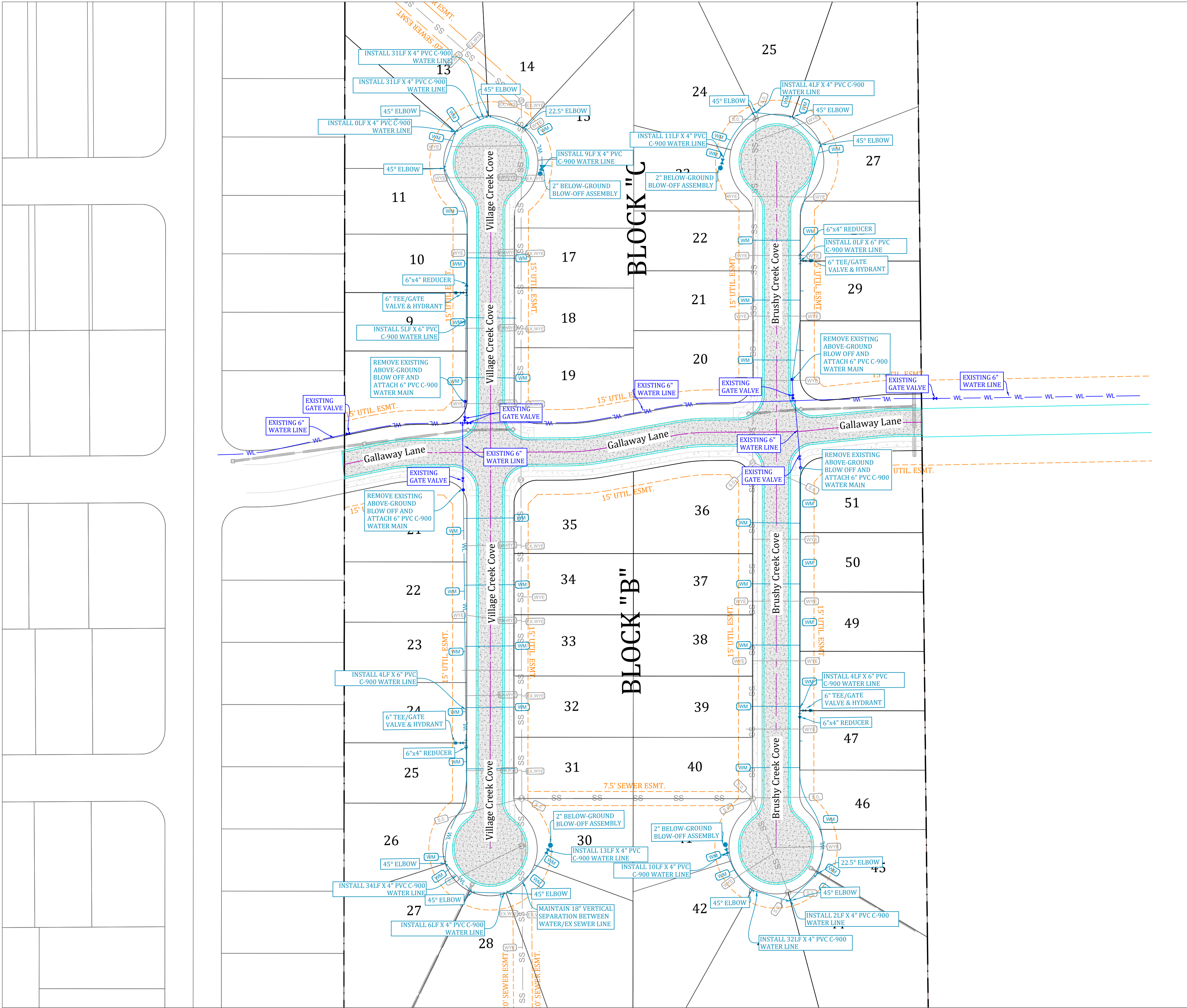
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SHEET NUMBER:
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OPERATING NOTE:

THE OPERATING ROUTINE SHALL INCLUDE NECESSARY PROTECTIVE MEASURES TO DETECT AND REMOVE OR DESTROY ANY CONTAMINANT OF CONCERN OR REGULATION THAT MIGHT ENTER THE DISTRIBUTION SYSTEM. EVERY PRECAUTION MUST BE TAKEN AGAINST THE POSSIBILITY OF SEWAGE CONTAMINATION OF THE WATER IN THE DISTRIBUTION SYSTEM. WATER MAINS AND SANITARY SEWERS SHALL BE CONSTRUCTED AS FAR APART AS PRACTICABLE, AND SHALL BE SEPARATED BY UNDISTURBED AND COMPACTED EARTH. A MINIMUM HORIZONTAL DISTANCE OF 10' SHOULD BE MAINTAINED BETWEEN WATER LINES AND SEWER LINE OR OTHER SOURCES OF CONTAMINATION. WATER LINES AND SEWER SHALL NOT BE LAID IN THE SAME TRENCH EXCEPT OF THE WRITTEN APPROVAL OF THE ARKANSAS DEPARTMENT OF HEALTH. WATER MAINS NECESSARILY CLOSE PROXIMITY TO SEWERS MUST BE PLACED SO THAT THE BOTTOM OF THE WATERLINE WILL BE AT LEAST 18" ABOVE THE TOP OF THE SEWER LINE AT ITS HIGHEST POINT. IF THIS DISTANCE MUST UNAVOIDABLY BE REDUCED, THE WATER LINE OR THE SEWER LINE MUST BE ENCASED IN A WATERTIGHT PIPE WITH SEALED WATERTIGHT ENDS EXTENDING AT LEAST 10' EITHER SIDE OF THE CROSSING. ANY JOINT IN THE ENCASEMENT PIPE IS TO BE MECHANICALLY RESTRAINED. THE ENCASEMENT PIPE MAY BE VENTED TO THE SURFACE IF CARRYING WATER OR SEWER UNDER PRESSURE. WHERE A WATER LINE MUST UNAVOIDABLY PASS BENEATH THE SEWER LINE, AT LEAST 18" OF SEPARATION MUST BE MAINTAINED BETWEEN THE OUTSIDE OF THE TWO PIPES IN ADDITION TO THE PRECEDING ENCASEMENT REQUIREMENTS. EXCEPTIONS TO THIS MUST BE APPROVED IN WRITING BY THE ARKANSAS DEPARTMENT OF HEALTH. A MINIMUM HORIZONTAL DISTANCE OF 3' SHALL BE MAINTAINED BETWEEN WATER LINES AND OTHER UNDERGROUND UTILITIES OF A NON-SANITARY NATURE (GAS, ELECTRIC, ETC.), EXCEPTIONS TO THIS MUST BE APPROVED IN WRITING BY THE ARKANSAS DEPARTMENT OF HEALTH AND HUMAN SERVICES.

TRENCH SETTLEMENT NOTE:

CITY WATER & LIGHT IS NOT RESPONSIBLE FOR ANY SETTLEMENT OVER WATER & SEWER TRENCHES THAT OCCURS BEFORE OR AFTER THE WARRANTY PERIOD.

WATER METER NOTE:

WATER METERS ARE SHOWN PICTORIALLY ONLY. WATER METERS SHALL BE LOCATED AT THE RIGHT OF WAY APPROXIMATELY AT THE CENTER OF THE LOT. THERE SHOULD BE A MINIMUM OF 10 FEET SEPARATION BETWEEN WATER METERS AND SANITARY SEWER SERVICES. THERE SHOULD BE A MINIMUM OF 3 FEET SEPARATION BETWEEN WATER METERS AND PROPERTY LINES.

WATER CONNECTION NOTE:

CONTRACTOR SHALL NOT INITIATE TIE IN CONNECTION TO EXISTING WATER MAIN AFTER 3 P.M. DURING NORMAL WORKING DAYS OR ON FRIDAY AFTER 12 P.M., UNLESS APPROVED BY CITY WATER & LIGHT ENGINEERING DEPT. CONTRACTOR SHALL NOTIFY CIVL INSPECTOR OF TENTATIVE TIME FRAME FOR CONNECTION 24 HOURS PRIOR TO ACTIVITY.

ALL SEWER WYES SHALL BE A MINIMUM OF 10' FOOT AWAY FROM PROPERTY CORNERS.

ELECTRICAL CONDUIT NOTE:

ALL ELECTRICAL CONDUIT SHALL HAVE A MINIMUM 48" OF COVER BELOW FINISHED GRADE.

NO TRANSFORMER, JUNCTION BOX, OR PULL BOX TO BE PLACED OVER WATER FITTINGS WITHOUT CITY WATER AND LIGHT APPROVAL.

LEGEND			
	CURB		STORM DRAIN INLET
	SIDEWALK		STORM DRAIN PIPE
	SL-1		SANITARY SEWER
	WATER LINE		SEWER MANHOLE
	WATER METER		SEWER WYE
	FIRE HYDRANT		SEWER STUB-OUT
	BLOW-OFF ASSEMBLY		EX. SEWER WYE
	GATE VALVE		EX. SEWER STUB-OUT
NOTE: SYMBOLS REPRESENTED IN LEGEND MAY NOT ALL BE INCLUDED ON THIS SHEET "RESPECTIVE SYMBOLS REPRESENTED IN PLAN VIEW ARE NOT TO SCALE"			

TOTAL WATERLINE QUANTITIES:
768 LF - 6" C-900 PVC WATERLINE
1224 LF - 4" C-900 PVC WATERLINE
38- 3/4" SHORT SERVICES
16- 3/4" LONG SERVICES

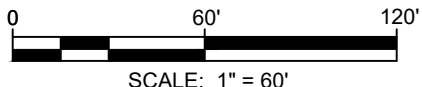
PRESSURES:
STATIC PRESSURE = 64 PSI
RESIDUAL PRESSURE = 55 PSI

WATER AND ELECTRIC NOTES:

- CONTRACTOR SHALL PROVIDE NECESSARY ELECTRICAL CONDUIT & PERMANENT MARKERS W/ GRAY PVC SCH 40 (2") & PVC SCH 40 (4") & 36" RAD ELBOWS FOR RISERS AND A STRING & MOUSE BLOWN IN THE CONDUIT. CONTRACTOR SHALL OBTAIN AN ELECTRICAL PLAN FROM CWL AND INSTALL THE ELECTRICAL CONDUIT AS SPECIFIED BY CWL (CONTRACTOR SHALL OBTAIN CWL SPECIFICATIONS).
- CONTRACTOR SHALL PROVIDE 3/4" METER STOP, TAP, SERVICE LINE & METER BOX AS REQUIRED BY CWL & INSTALL AS REQUIRED BY CWL.
- CONTRACTOR SHALL PROVIDE ENGINEER W/ "AS-BUILT" DIMENSIONS FOR WYES, VALVES, TEES, BENDS, FIRE HYDRANTS, & WATER METERS.
- CONTRACTOR SHALL PROVIDE LABOR AND MATERIALS FOR ALL SEWER, WATER, & ELECTRICAL LINE LATERALS & PROVIDE LABOR & MATERIAL TO BACKFILL ALL SEWER, WATER & ELECTRICAL STREET CUTS WITH SANDY MATERIAL (P.I. <10) TO TOP OF EXCAVATION & COMPACT FILL TO 95% COMPACTION & PROVIDE COMPACTION TESTS AT 2' INTERVALS. CONTRACTOR SHALL PROVIDE LABOR AND MATERIAL FOR ALL SEWER & WATER LINE MAINS THAT CROSS A STREET AND BACKFILL THEM WITH SANDY (P.I. <10) TO TOP OF EXCAVATION & COMPACT FILL TO 95% COMPACTION & PROVIDE MATERIAL COMPACTION TESTS AT 2' INTERVALS.
- A 12 GA SOLID TRACER WIRE SHALL BE INSTALLED WITH WATER LINE MAIN WHERE THERE SHALL BE A PERMANENT WATER LINE MARKER INSTALLED @ 10' INTERVALS IN BETWEEN WHERE THE TRACER WIRE IS INSTALLED. THE TRACER WIRE SHALL BE INSTALLED ABOVE THE WATER LINE AND CONNECTED TO THE PERMANENT MARKERS IN A MANNER THAT WILL INSURE A CONTINUOUS CONNECTION OF THE TRACER LINE.
- CONTRACTOR SHALL CALL ARK ONE CALL FOR LOCATES PRIOR TO CONSTRUCTION.
- CWL SHALL PROVIDE FIRE HYDRANT, 6" D.I. GATE VALVE, VALVE BOX W/ LID, AND 6" BY LINE SIZE D.I. TEE. CONTRACTOR SHALL PROVIDE LABOR FOR INSTALLATION AND CONCRETE BLOCKING. (SEE DETAIL SHEET)
- CONTRACTOR SHALL NOTIFY ENGINEER TO DETERMINE ELEVATIONS FOR MANHOLE TOPS, WATERLINE ELEVATION WHERE IT CROSSES PROPOSED STORM DRAINS, WATER METER BOX ELEVATIONS, VALVE BOX ELEVATIONS, AND FIRE HYDRANT ELEVATIONS. AND WATER LINE LOCATION AROUND INLETS. WATER LINE SHALL BE INSTALLED AT AN ELEVATION WHERE HYDRANTS ARE LOCATED TO ENSURE HYDRANT WILL BE AT PROPER HEIGHT WHEN FINISHED GRADE HAS BEEN OBTAINED.
- CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH PLANS TO INSURE THE WATER LINE DEPTH OF INSTALLATION IS BELOW THE PROPOSED STORM DRAIN ELEVATION. CONTRACTOR WILL BE REQUIRED TO COME BACK AND LOWER WATER LINE (AT THE CONTRACTOR'S EXPENSE) IF IT INTERFERES WITH THE STORM DRAIN INSTALLATION.
- IF CONTRACTOR CAN NOT INSURE WATER LINE INSTALLATION TO BE BELOW STORM DRAIN, THEN THE CONTRACTOR SHALL POSTPONE WATER LINE INSTALLATION UNTIL STORM DRAIN HAS BEEN INSTALLED.
- CONTRACTOR SHALL PROVIDE OWNER WITH ONE YEAR WARRANTY FOR MATERIALS AND WORKMANSHIP.
- CONTRACTOR SHALL PROVIDE ENGINEER WITH LIEN RELEASES FOR MATERIALS AND LABOR; BACTERIAL SAMPLE RESULTS; AND MODEL & YEAR OF WATER LINE VALVES WITHIN 30 DAYS OF COMPLETION OF WORK.
- CONTRACTOR SHALL DRESS UP PROJECT AFTER INSTALLATION TO THE CONDITION IT AS PRIOR TO BEGINNING CONSTRUCTION. MOUND BACKFILL OVER TRENCHES 1' TO 2' TO ALLOW FOR SETTLING IN AREAS OUT OF THE STREET.
- CONTRACTOR SHALL REVIEW DETAIL SHEETS TO ENSURE PROPER INSTALLATION OF VALVES, FIRE HYDRANTS, CONCRETE BLOCKING, SERVICE LINES, MANHOLE INVERTS, AND DROP ASSEMBLIES.
- CONTRACTOR SHALL OBTAIN APPROVAL SET OUT AND INSTALLATION OF WATER LINES. SPECIFICATIONS ARE TO BE ADHERED TO AND MADE A PART OF THIS SET OF PLANS. CONTRACTOR SHALL USE CWL SPECIFICATIONS TO INCLUDE ITEMS NOT SHOWN OR REFERRED TO IN THIS SET OF PLANS. ALL STREET EXCAVATION MATERIAL SHALL BE DISPOSED OF BY CONTRACTOR AT THE ADJACENT LOTS OR AS DIRECTED BY OWNER.
- ALL STREET EXCAVATION MATERIAL SHALL BE DISPOSED OF BY CONTRACTOR ON THE ADJACENT LOTS OR AS DIRECTED BY OWNER.
- CONTRACTOR SHALL PLACE 8 MIL POLYWRAP OVER DUCTILE IRON FITTINGS PRIOR TO PLACING CONCRETE BLOCKING TO PREVENT CONCRETE FROM ENCASEING BOLTS & NUTS.
- ALL WATER LINES 4" TO 12" DIAMETER SHALL BE AWWA C-900 PRESSURE GLASS 150 DR 18 ALL 2" AND 3" DIAMETER WATER LINES SHALL BE SDR 21 CLASS 200 (ASTM 2241). ALL SEWER LINES SHALL BE ASTM 2241 SDR-26 OR AS SHOWN.
- WATER LINE AND SANITARY SEWER LINE SHALL MAINTAIN 18" MINIMUM VERTICAL SEPARATION. WATER/SEWER LINE AND STORM DRAIN SHALL MAINTAIN 36" MINIMUM HORIZONTAL SEPARATION & 18" VERTICAL SEPARATION TO PREVENT FREEZING. MAINTAIN AS MUCH DISTANCE BETWEEN WATER AND SEWER LINE AS POSSIBLE, BUT NOT LESS THAN 10' HORIZONTAL SEPARATION. SEPARATION BETWEEN WATER AND SEWER LINES ARE UNAVOIDABLE OR IN LOCATIONS WHERE WATER MUST UNAVOIDABLY CROSS UNDER SANITARY SEWER LINE, REGARDLESS OF VERTICAL SPACING, THEN WATER LINE AND/OR SEWER LINE MUST BE ENCASED 10 FEET EITHER SIDE OF THE CROSSING LOCATION. ANY JOINT IN THE ENCASEMENT PIPE SHALL BE MECHANICALLY RESTRAINED. EXCEPTIONS TO THIS REQUIREMENT MUST BE APPROVED IN WRITING BY THE ARKANSAS DEPARTMENT OF HEALTH. SEE SECTION XIV.A. OF THE PUBLIC WATER SYSTEMS MANUAL FOR THE STATE OF ARKANSAS FOR FULL TEXT OF SPECIFICATIONS.
- CONSTRUCTION SHALL BEGIN ONLY AFTER APPROVED PLANS FROM THE ARKANSAS DEPARTMENT OF HEALTH ARE SUBMITTED TO CWL AND NECESSARY FEES ARE PAID IN FULL. CONSTRUCTION BY CONTRACTORS SHALL THEN BEGIN ONLY AFTER THE RIGHT OF WAY HAS BEEN CLEARED, THE ENTIRE SECTION STAKED, THE ELEVATIONS CAREFULLY CHECKED, AND A PRE-CONSTRUCTION MEETING HAS BEEN HELD. A POST CONSTRUCTION MEETING WILL BE HELD W/ CONTRACTOR PRESENT TO VERIFY ALL CONSTRUCTION IS ACCEPTABLE TO CWL PRIOR TO THE INSTALLATION OF ELECTRICAL WIRE.
- DEVELOPER MUST SHOW PROOF OF ADEQUATE INSURANCE COVERAGE. (ARKANSAS WORKERS COMPENSATION, \$1,000,000.00 MINIMUM AUTO LIABILITY, CONTRACTOR MUST HAVE A STATE CONTRACTORS LICENSE. CONTRACTOR MUST HAVE A COMPETENT PERSON AT THE JOB SITE AS DEFINED BY OSHA.
- FINAL CLEANUP OF THE SITE SHALL INCLUDE THE REMOVAL AND DISPOSAL OFF-SITE OF ALL CONSTRUCTION DEBRIS, AND SURPLUS MATERIALS.
- DURING CONSTRUCTION, WORK SHALL BE INSPECTED BY THE DEVELOPER'S CONSULTING ENGINEER FOR NECESSARY SAFETY PRACTICES, PROPER MATERIALS, AND WORKMANSHIP. CWL WILL PROVIDE RANDOM INSPECTIONS TO INSURE THAT THE PLANS APPROVED BY THE ARKANSAS DEPARTMENT OF HEALTH AND CWL ARE FOLLOWED CONCERNING WORKMANSHIP AND MATERIALS.
- IF SEWER AND WATER LINES HAVE NOT BEEN ACCEPTED BY CWL WITHIN 360 DAYS AFTER MANDREL TESTS, LOW PRESSURE AIR TESTS, HYDROSTATIC TESTS AND BACTERIAL TESTS, ALL TESTS WILL BE REQUIRED TO BE REPEATED AND RE-CERTIFIED.
- INSTALLATION OF DOMESTIC WATER METER TAPS ARE REQUIRED TO BE MADE DURING THE INSTALLATION OF THE NEW WATER MAINS.
- OBSTRUCTING DRAINAGE PIPES, SHALL BE REMOVED. IF IN SOUND, UNDAMAGED CONDITION, THEY MAY BE CLEANED AND RE-INSTALLED. IF UNSOUND OR DAMAGED, THEY SHALL BE REPLACED WITH THE SAME TYPE OF MATERIAL, WHEN THE ANGLE BETWEEN CENTER LINES OF A DRAINAGE PIPE AND WATER MAIN WILL PERMIT. THE WATER MAIN MAY BE INSTALLED BY TUNNELING, PROVIDED THE DRAINAGE PIPE IS NOT DAMAGED AND THE WATER MAIN IS LAID PROPERLY, AS AN ALTERNATIVE IN THE CASE OF CORRUGATED METAL PIPE, A SECTION MAY BE CUT FROM THE DRAINAGE PIPE, AND RESTORATION ACCOMPLISHED WITH NEW PIPE AND COUPLING BANDS.
- CWL ENGINEERING DEPT. MUST APPROVE ANY WATER OR SEWER LINES THAT WILL BE INSTALLED CLOSER THAN 5' PARALLEL WITH EDGE OF PAVED STREET SURFACE.
- REPAIRS SHALL BE MADE IN ACCORDANCE WITH AWWA. IF VALVE IS CLOSED BY THE CONTRACTOR WITHOUT CWL'S KNOWLEDGE, THE NEW SECTION MUST BE TESTED FOR WATER QUALITY AND FLUSHED. THE CWL INSPECTOR WILL WITNESS ALL REPAIRS.
- WATER LINE FITTINGS SHALL BE CLASS 350 DUCTILE IRON OR CAST IRON.
- TRENCH DEPTH WILL BE SUCH AS TO ALLOW A MINIMUM COVER OF 42" OVER WATER MAIN.
- LEAKAGE TESTS SHALL BE MADE PRIOR TO STERILIZATION OPERATIONS. THE TEST PERIOD SHALL BE TWO (2) HOURS. TEST PRESSURE SHALL BE 1.5 TIMES THE CALCULATED WORKING PRESSURE OF THE MAIN, BUT NOT LESS THAN 100 PSI. THE LINE WILL NOT BE ACCEPTED UNLESS OR UNTIL THE TOTAL IS LESS THAN THAT SPECIFIED IN AWWA C-600-93 FOR DUCTILE IRON AND AWWA C-605-94 FOR PVC PIPE.
- ALL WATER LINES SHALL BE STERILIZED IN ACCORDANCE WITH AWWA C-651-94. ANY NEW CONSTRUCTION OR REPAIRED WATER MAIN MUST BE THOROUGHLY CLEANED (FLUSHED), DISINFECTED, AND TESTED FOR BACTERIOLOGICAL QUALITY BEFORE IT CAN BE PLACED IN SERVICE. FOLLOWING A CONTACT PERIOD OF NOT LESS THAN 24 HOURS, THE CHLORINATED WATER SHALL BE FLUSHED FROM THE SYSTEM, AND THE SYSTEM FILLED WITH WATER OF NORMAL CHLORINE CONTENT. SAMPLES OF WATER THEN SHALL BE TAKEN ON TWO CONSECUTIVE DAYS FROM THE LINES AND DELIVERED TO THE CWL - LABORATORY FOR BACTERIAL ANALYSIS. THIS PROCESS SHALL BE CONTINUED UNTIL THE SAMPLES SHOW THE WATER IS SAFE FOR DOMESTIC REQUIREMENTS. FLUSHING DEVICES SHOULD BE SIZED TO PROVIDE FLOWS WHICH WILL GIVE A VELOCITY OF AT LEAST 2.5 FEET PER SECOND IN THE WATER MAIN BEING FLUSHED. NO FLUSHING DEVICE SHALL BE DIRECTLY CONNECTED TO ANY SEWER. IF THE LINE HAS SET FOR A ONE MONTH OR LONGER BEFORE FINAL ACCEPTANCE, IT SHALL BE RESTERILIZED, FLUSHED, AND BACTERIAL TESTED.
- THE DEVELOPER AND ENGINEER ARE RESPONSIBLE FOR METER BOX LOCATIONS. ANY CONFLICTS THAT REQUIRE PLASTIC BOX(S) TO BE CHANGED OUT TO CONCRETE BOX(S) WILL BE AT THE EXPENSE OF THE DEVELOPER AND/OR CONTRACTOR.
- IF UNSAFE PRACTICES ARE DISCOVERED BY CWL DURING INSPECTIONS OF WORKMANSHIP AND MATERIALS, CWL WILL NOTIFY OSHA. THIS PROCEDURE IN NO WAY OBLIGATES CWL FOR THE RESPONSIBILITY OF THE CONTRACTOR'S SAFETY PRACTICES.
- DURING CONSTRUCTION, WORK SHALL BE INSPECTED DAILY BY THE ENGINEER OF RECORD FOR PROPER MATERIALS, WORKMANSHIP, AND INSTALLATION IN ACCORDANCE WITH THE APPROVED ARKANSAS DEPARTMENT OF HEALTH AND CITY WATER AND LIGHT APPROVED PLANS.

NOTE: ALL 12" GATE VALVES TO BE BUTTERFLY VALVES

NOTE: TRACER WIRE AND PEDESTAL SHOULD BE INSTALLED WHEN WATER LINE NOT ACCOMPANIED WITH ELECTRIC LINE.



SARAH CROSSING - PHASE IV

GENERAL WATER PLAN



REVISIONS		
DATE	BY	DESCRIPTION

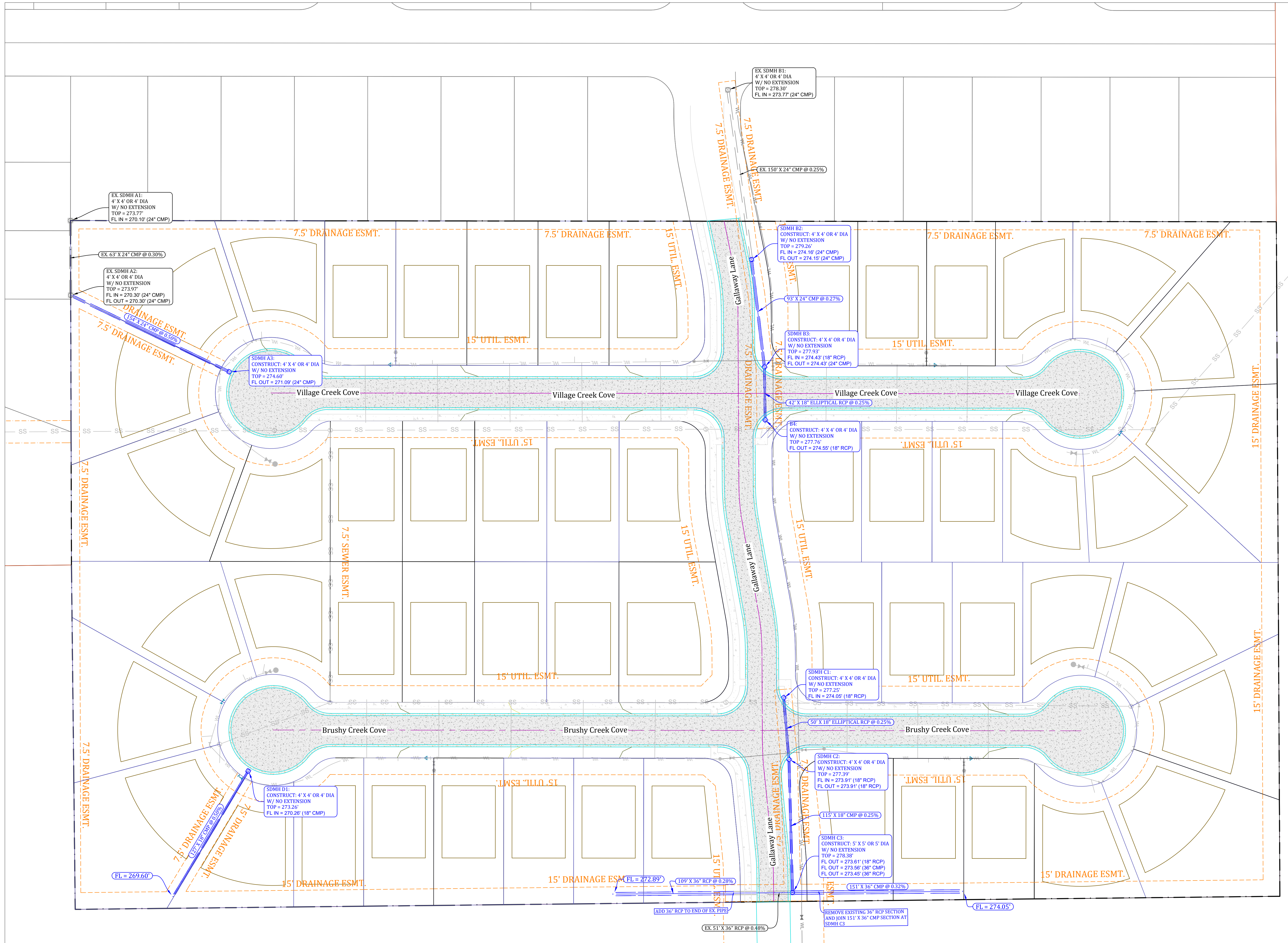
DRAWING INFO.
DRAWN BY: IMH
DATE: 06/04/2025
SCALE: 1"=60'

SHEET NUMBER:
WT-1

MARK MORRIS P.E.
6011 SOUTHWEST DRIVE
JONESBORO, AR 72404
PH: 1-870-919-7700
MARK@MARKMORRISHOMES.COM

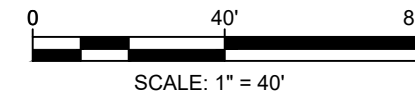


Z:\Morris Real Estate Holdings, LLC\Projects\Sarah's Crossing\Engineering\Plans\Phase 4\Sarah Crossing P4.dwg, 6/1/2025 2:32:08 PM



241 800 456 4566 (T) 800 456 4566 (F)

NOTE:
NO SIGNIFICANT TREES FOUND
ON SITE



811

Know what's below.
Call before you dig.

SARAH CROSSING - PHASE IV

GRADING & DRAINAGE PLAN



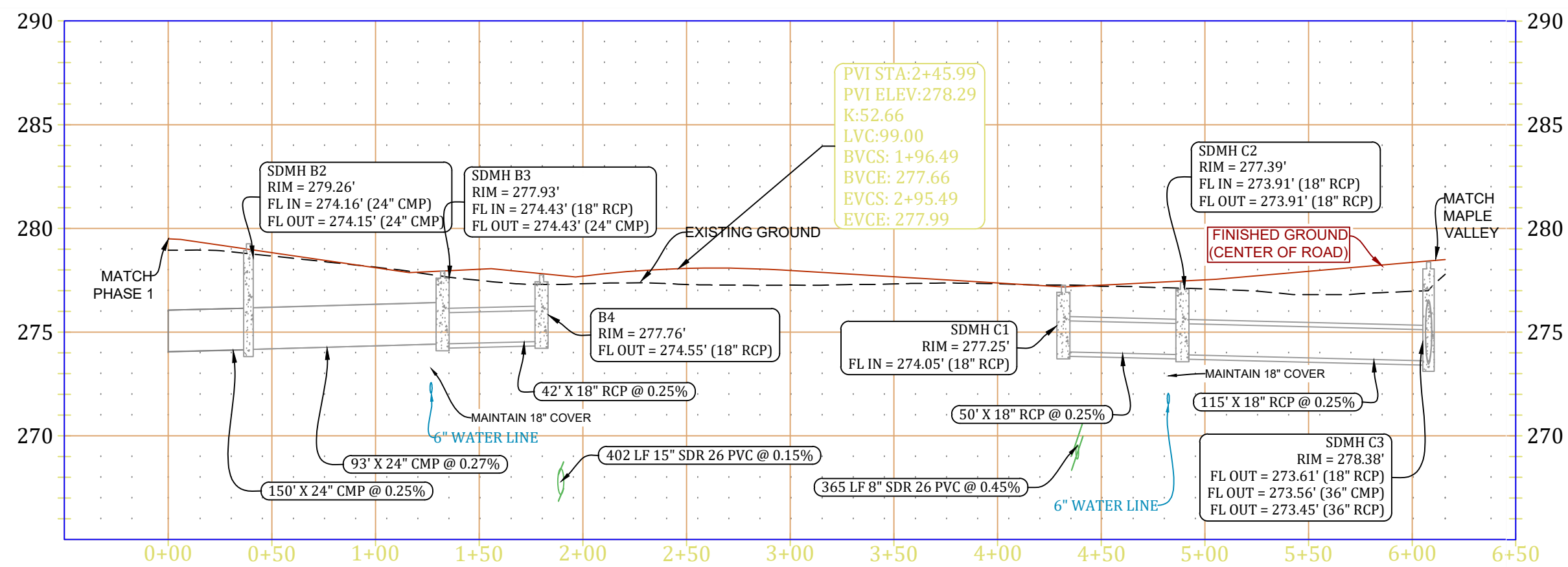
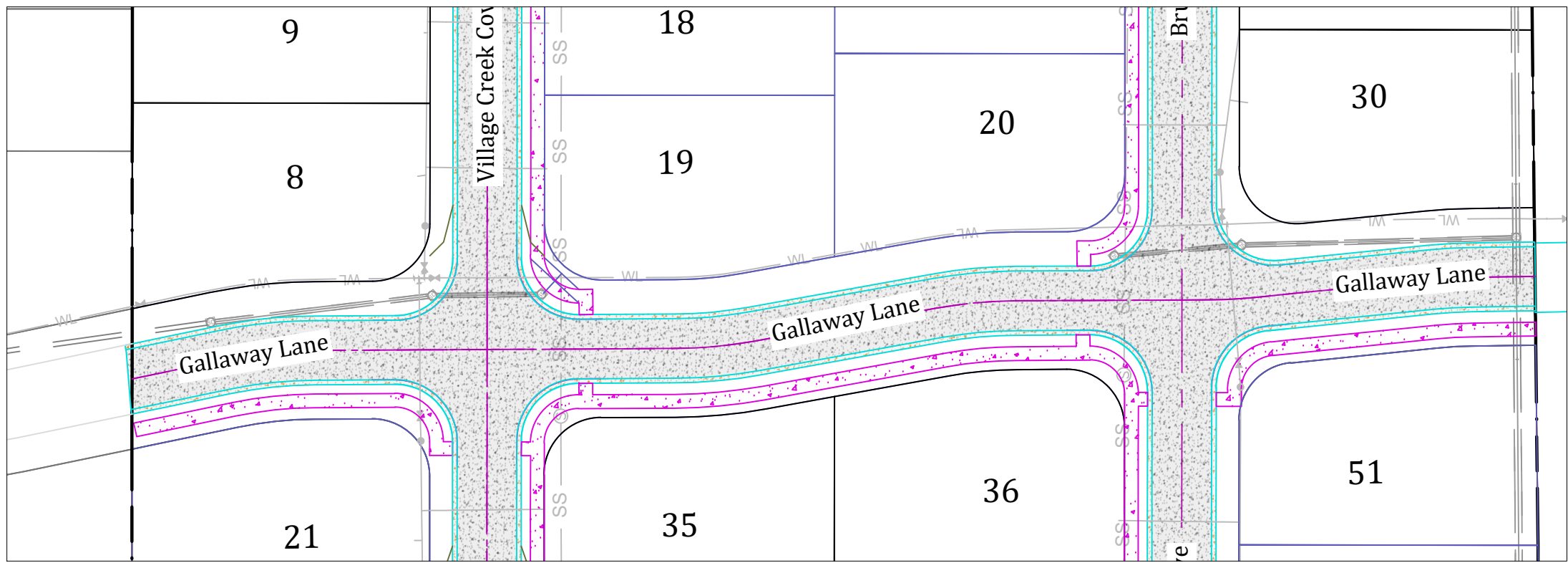
REVISIONS		
DATE	BY	DESCRIPTION

DRAWING INFO.	
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DATE:	06/04/2025
SCALE:	1"=40'

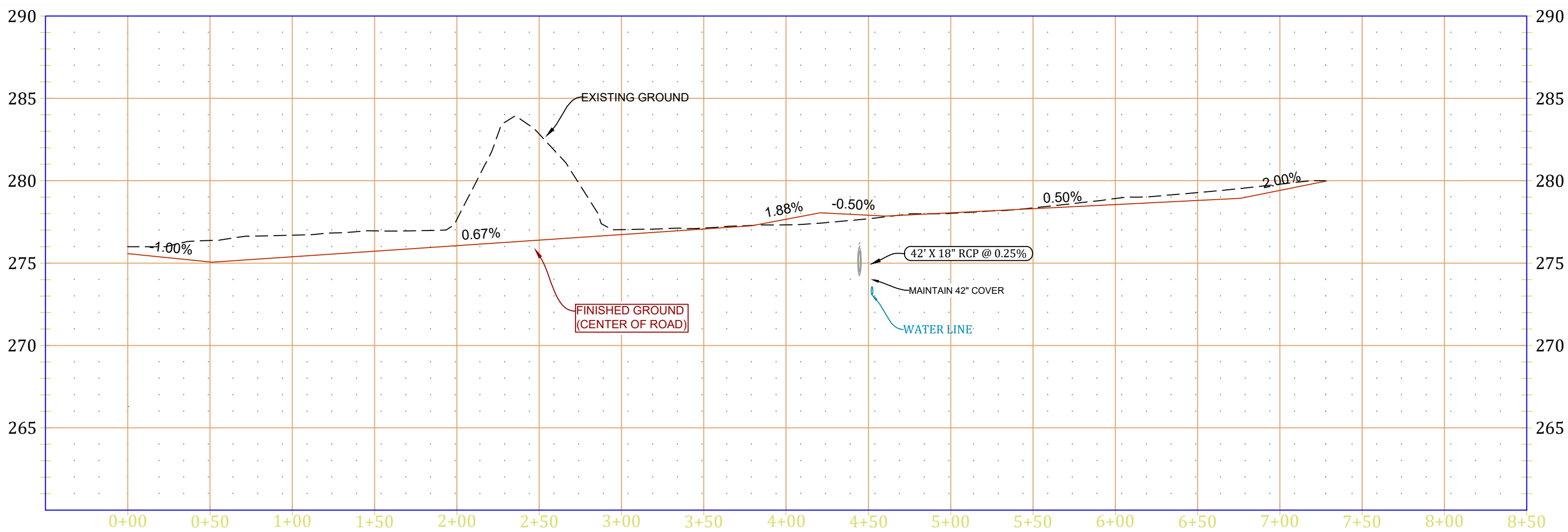
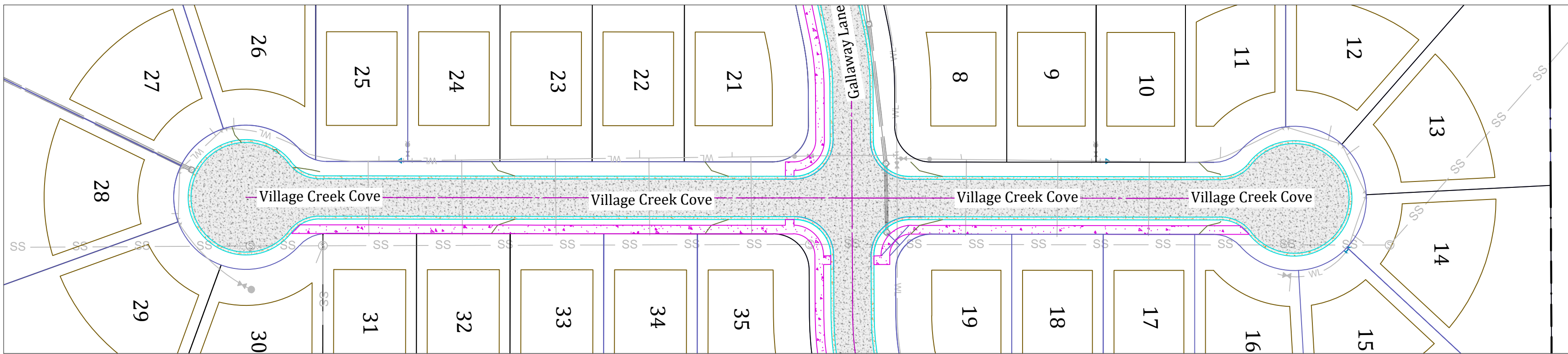
SHEET NUMBER:
GR-1

MARK MORRIS

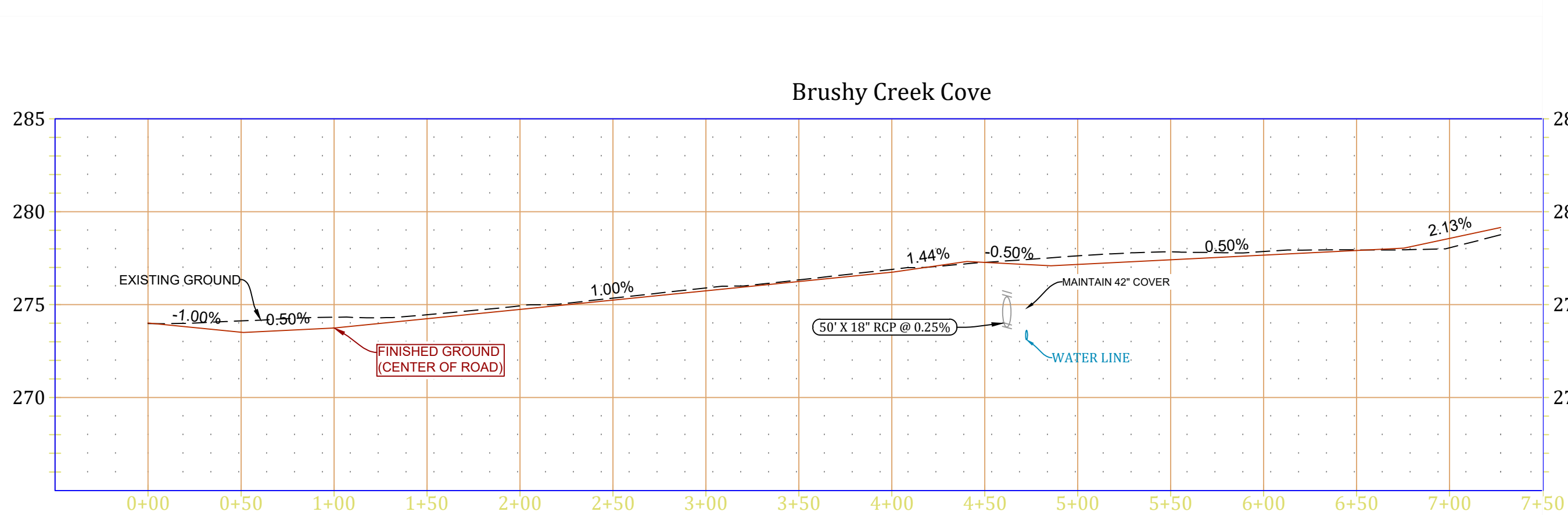
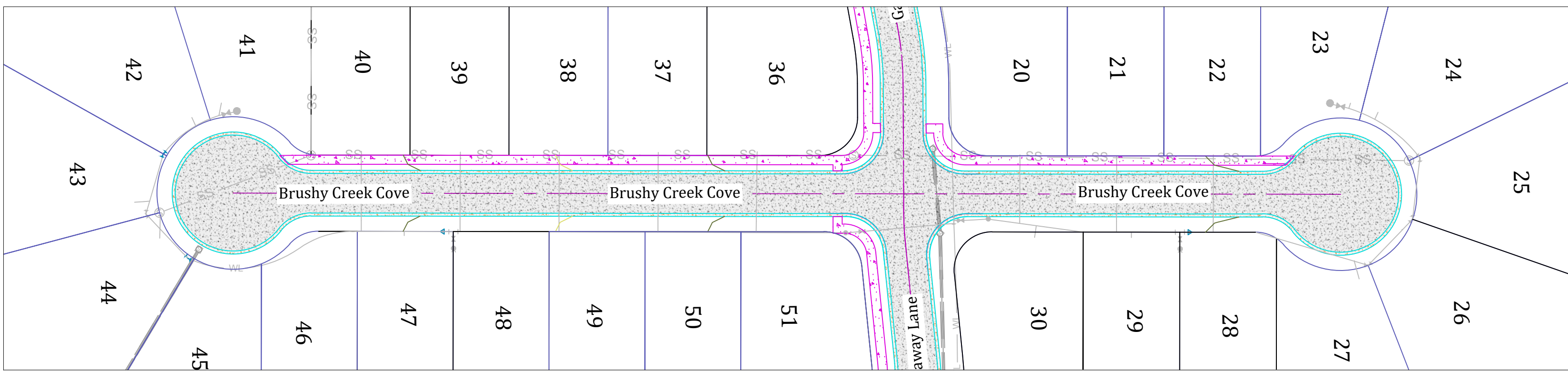
MARK MORRIS P.E.
6011 SOUTHWEST DRIVE
JONESBORO, AR 72404
PH: 1-870-919-7700
MARK@MARKMORRISHOMES.COM



GALLAWAY LANE PLAN/PROFILE



VILLAGE CREEK COVE PLAN/PROFILE



BRUSHY CREEK COVE PLAN/PROFILE

SARAH CROSSING - PHASE IV

STREET PLAN & PROFILE

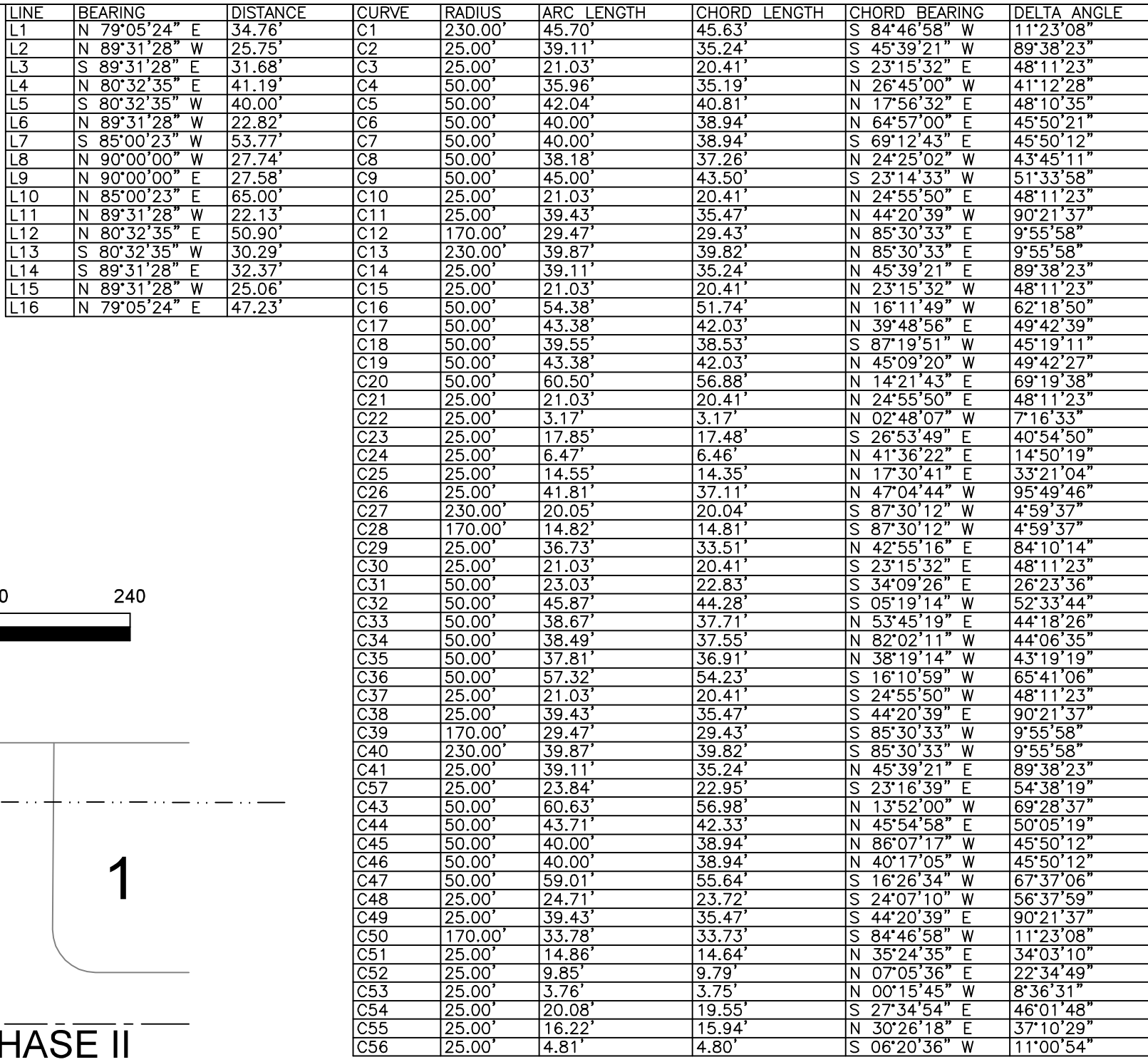


REVISIONS		
DATE	BY	DESCRIPTION

DRAWING INFO.	
DRAWN BY:	IMH
DATE:	06/04/2025
SCALE:	1"=60'

SHEET NUMBER:
ST-1

VERTICAL SCALE 1:5



A PART OF THE SOUTHWEST QUARTER OF SECTION 3, TOWNSHIP 13 NORTH, RANGE 3 EAST, CRAIGHED COUNTY, ARKANSAS AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

Commencing at the South Quarter corner of said Section 3; thence North 00°09'27" East 97.76 feet to the North right of way Highway No. 49; thence North 86°53'44" West 429.90 feet along said right of way; Thence North 00°09'27" East 133.73 feet; Thence South 89°11'32" East 119.33 feet; Thence North 00°09'27" East 311.00 feet to the Point of Beginning of Proper;

Thence North 89°23'10" West 310.00 feet; Thence North 00°50'09" East 1085.62 feet; Thence South 89°31'28" East 607.11 feet; Thence South 00°09'49" West 325.99 feet; Thence South 00°09'09" West 758.63 feet; Thence North 89°50'48" West 310.00 feet to the Point of Beginning of Proper, containing 666124.60 Sq. Feet, 15.29 Acres, more or less and being subject to all public and private roads and easements.

1. BEARINGS ARE STATE PLANE GRID BEARINGS(NAD 83 DATUM)AS PER GPS OBSERVATION ON SITE DURING SURVEY AND ARE TO BE USED TO DEFINE THE GENERAL DIRECTION AND TRUE ANGLE AT THE INTERSECTION OF THE PROPERTY AND LAND LINES.

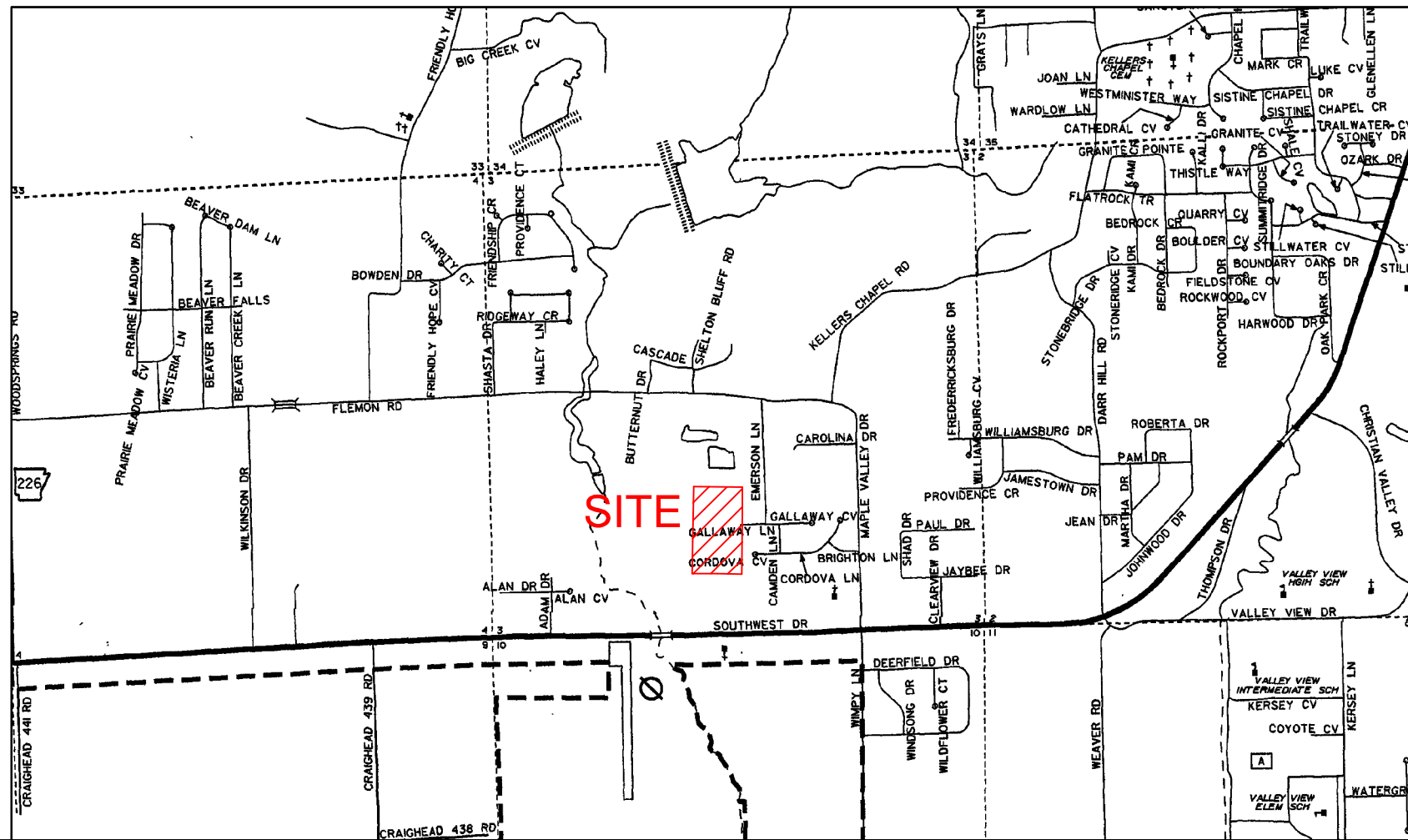
2. PROPERTY IS SUBJECT TO UTILITY SERVICES AND UNDERGROUND UTILITY LINES NOT LOCATED.

THIS IS TO CERTIFY I HAVE ON THIS DATE SURVEYED THE ABOVE DESCRIBED
PROPERTY IN ACCORDANCE WITH MONUMENTS FOUND THIS PLAT CONFORMS
TO THAT SURVEYED.

H&S HIME PROFESSIONAL SURVEYING SERVICES
1817 WOODSPRINGS RD STE. "F"
JONESBORO, ARKANSAS 72401

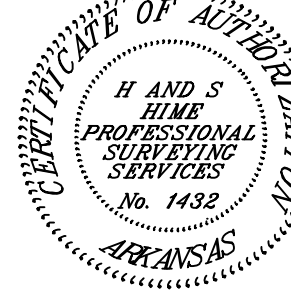
WE HEREBY CERTIFY THAT WE THE OWNERS OF THE PROPERTY SHOWN AND DESCRIBED
HEREON AND THAT WE ADOPT THE PLAN OF THE SUBDIVISION AND DEDICATED PERPETUAL USE OF ALL
STREETS AND EASEMENTS AS NOTED AND WE FURTHER CERTIFY THAT WE HAVE READ AND
ACKNOWLEDGED THE FOLLOWING STATEMENT.

DATE:



H&S HIME PROFESSIONAL SURVEYING SERVICES

**SARAH CROSSING SUBDIVISION, PHASE 4
TO THE CITY OF JONESBORO, ARKANSAS.**



SHAWN L. HIME



1817 WOODSPRINGS RD. - STE. "F"
LONGFORD, ARIZONA 85905

113 NORTH RANGE 3 EAST CRAIGHEAD COUNTY ARKANSAS

CLIENT: MARK MORRIS



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Text File

File Number: RZ-25-13

Agenda Date:

Version: 1

Status: To Be Introduced

In Control: Metropolitan Area Planning Commission

File Type: Rezoning

Rezoning RZ 25-13 2106 Bridger Rd. Rezone approximately 6.79 acres

rezone 6.79 acres from R-1 medium density residential to RM-16 residential multifamily 16 units per acre on Bridger Road east of Johnson Ave. Proposed by David Baker with Fisher Arnold Engineers for Dr. Surinda Sra.



Application for a Zoning Ordinance Map Amendment

METROPOLITAN AREA
PLANNING COMMISSION
Jonesboro, Arkansas

Meeting Date: August 12, 2025

Date Received: _____

Meeting Deadline: July 17, 2025

Case Number: _____

LOCATION:

Site Address: 2106 Bridger Road

Side of Street: East between East Johnson Road and Tracy Dr

Quarter: _____ Section: 02 Township: 14 Range: 04

Attach a survey plat and legal description of the property proposed for rezoning. A Registered Land Surveyor must prepare this plat.

SITE INFORMATION:

Existing Zoning: R-1 Proposed Zoning: RM-16

Size of site (square feet and acres): 6.79 AC / 295,753 SF Street frontage (feet): 650'

Existing Use of the Site: Single Family Residential

Character and adequacy of adjoining streets: _____

Does public water serve the site? Yes It is provided in Bridger Rd

If not, how would water service be provided? _____

Does public sanitary sewer serve the site? Yes, sewer is located in Bridger Rd near the medical office building

If not, how would sewer service be provided? _____

Use of adjoining properties:

North Vacant

South Vacant

East Vacant

West Mobile Home Park

Physical characteristics of the site: Predominately open grass with trees along the perimeter of the property and around the existing house

Characteristics of the neighborhood: Fringe Zone that is a transition between Medical (Hospital) District, higher density residential and rural. Future Land Use Plan calls for High Density Residential

Applications will not be considered complete until all items have been supplied. Incomplete applications will not be placed on the Metropolitan Area Planning Commission agenda and will be returned to the applicant. The deadline for submittal of an application is on the public meeting schedule. The Planning staff must determine that the application is complete and adequate before it will be placed on the MAPC agenda.

REZONING INFORMATION:

The applicant is responsible for explaining and justifying the proposed rezoning. *Please prepare an attachment to this application answering each of the following questions in detail:* Please see attached file.

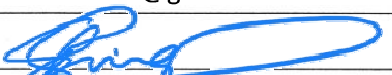
- (1). How was the property zoned when the current owner purchased it?
- (2). What is the purpose of the proposed rezoning? Why is the rezoning necessary?
- (3). If rezoned, how would the property be developed and used?
- (4). What would be the density or intensity of development (e.g. number of residential units; square footage of commercial, institutional, or industrial buildings)?
- (5). Is the proposed rezoning consistent with the *Jonesboro Comprehensive Plan* and the *Future Land Use Plan*?
- (6). How would the proposed rezoning be the public interest and benefit the community?
- (7). How would the proposed rezoning be compatible with the zoning, uses, and character of the surrounding area?
- (8). Are there substantial reasons why the property cannot be used in accordance with existing zoning?
- (9). How would the proposed rezoning affect nearby property including impact on property value, traffic, drainage, visual appearance, odor, noise, light, vibration, hours of use or operation and any restriction to the normal and customary use of the affected property.
- (10). How long has the property remained vacant?
- (11). What impact would the proposed rezoning and resulting development have on utilities, streets, drainage, parks, open space, fire, police, and emergency medical services?
- (12). If the rezoning is approved, when would development or redevelopment begin?
- (13). How do neighbors feel about the proposed rezoning? Please attach minutes of the neighborhood meeting held to discuss the proposed rezoning or notes from individual discussions. *If the proposal has not been discussed with neighbors, please attach a statement explaining the reason. Failure to consult with neighbors may result in delay in hearing the application.*
- (14). If this application is for a Limited Use Overlay (LUO), the applicant must specify all uses desired to be permitted.

OWNERSHIP INFORMATION:

All parties to this application understand that the burden of proof in justifying and demonstrating the need for the proposed rezoning rests with the applicant named below.

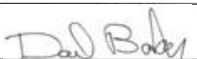
Owner of Record:

I certify that I am the owner of the property that is the subject of this rezoning application and that I represent all owners, including spouses, of the property to be rezoned. I further certify that all information in this application is true and correct to the best of my knowledge.

Name: Dr. Surinda Sra
Address: 11 Katsina Circle
City, State: Cherokee Village, AR ZIP 72405
Telephone: 870-847-3039
Facsimile: email - drssra@gmail.com
Signature: 

Applicant:

If you are not the Owner of Record, please describe your relationship to the rezoning proposal:

Name: Fisher Arnold, Inc. - David Baker
Address: 404 Creeth Ave
City, State: Jonesboro, AR ZIP 72401
Telephone: 888-583-9724
Facsimile: email- dbaker@fisherarnold.com
Signature: 

Deed: Please attach a copy of the deed for the subject property.

Applications will not be considered complete until all items have been supplied. Incomplete applications will not be placed on the Metropolitan Area Planning Commission agenda and will be returned to the applicant. The deadline for submittal of an application is on the public meeting schedule. The Planning staff must determine that the application is complete and adequate before it will be placed on the MAPC agenda.



CITY OF JONESBORO
REZONING ADJOINING PROPERTY OWNER NOTIFICATION

The Metropolitan Area Planning Commission, City of Jonesboro, Arkansas, will hold a public hearing at the City of Jonesboro Municipal Center, 300 S. Church St., Council Chambers, 1st Floor, Jonesboro, Arkansas, on:

Tuesday, August 12, 2025 at 5:30 p.m.

One item on the agenda for this meeting is a request to the Commission to approve a Rezoning to the zoning ordinance concerning property that is adjacent to your property. You have the opportunity to attend this meeting to voice your approval or disapproval if you wish. If you have information that you feel should be taken into consideration before a decision is rendered, you are encouraged to submit such information to the Commission. If the Commission renders a decision you feel is unfair or unjust, you may appeal the decision to Circuit Court.

REZONING REQUESTED BY: KN Systems Inc. / Dr. Sra
DATE: Meeting Date: Tuesday, August 12, 2025 at 5:30 p.m.
SUBJECT PROPERTY ADDRESS: 2106 Bridger Road
DESCRIPTION OF REZONING REQUESTED: ±6.79 Acres From R-1, Single Family Residential to RM-16 , Residential Multifamily

In affixing my signature below, I am acknowledging my understanding of this request for a Rezoning. I further understand that my signature only indicates my receipt of notification of the request for a Rezoning and does not imply an approval by me or the Rezoning, unless so written by me to the Commission.

Printed Name of Property Adjacent Owner

(Signature)

Date

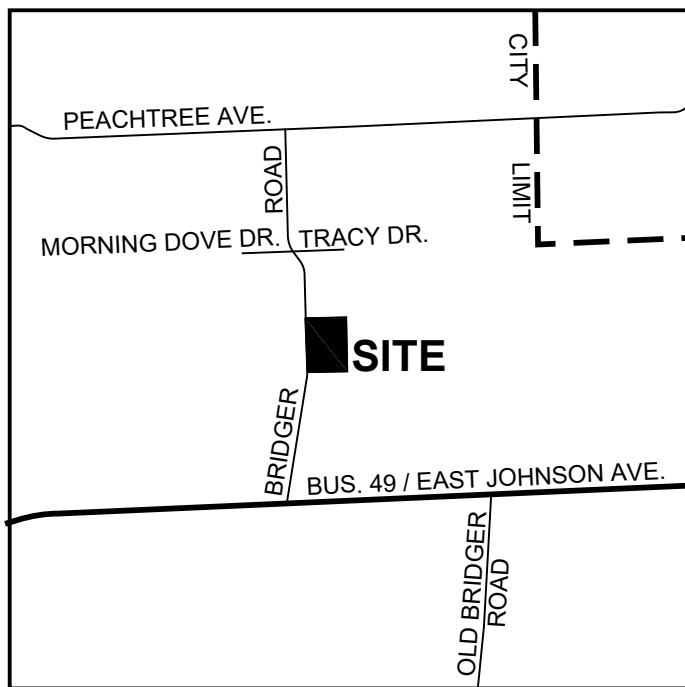
Address

Phone

If you would like to obtain additional information, or voice an opinion regarding this request, you may do so by contacting the Planning Department, at 300 S. Church St., or by calling 870-932-0406, between the hours of 8:00 a.m. and 5:00 p.m., Monday through Friday.



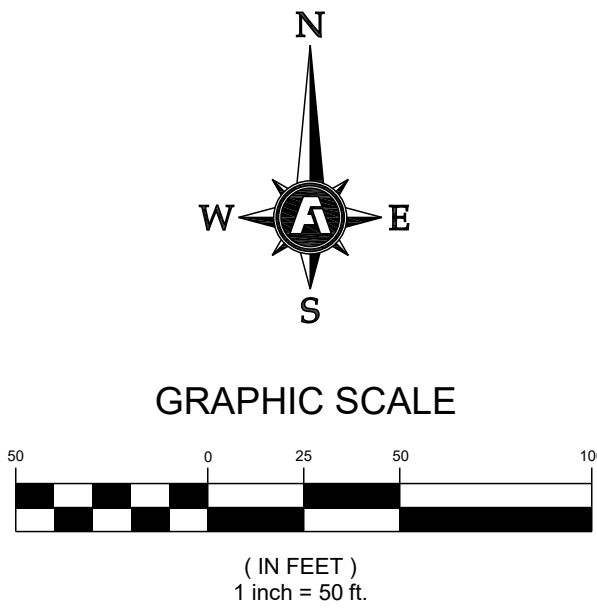
PROPOSED BUILDING STYLE:
SIMILAR TO THE POINTE AT NORTH HILLS, LITTLE ROCK, ARKANSAS



VICINITY MAP NTS

PLAN DATA CHART:

SITE AREA:	6.79 ACRES / 295,753 SF
TOTAL UNITS:	80 UNITS
DENSITY:	12 DU/AC
MINIMUM LOT AREA (2,722 SF/DU)	217,760 SF
PARKING REQUIRED:	1.75 / ONE-BEDROOM UNIT (TOTAL 140)
PARKING PROVIDED:	143
EXISTING ZONING:	R-1
PROPOSED ZONING:	RM-16
(CONSISTENT WITH THE JONESBORO COMPREHENSIVE PLAN)	



CONCEPT PLAN
BRIDGER OAKS
JONESBORO, ARKANSAS
AUGUST, 2025
TOTAL AREA: 295,753 Sq. Ft. / 6.79 Ac.
FEMA PANEL NO. 05031C0257 D / ELEV. N/A
PARCEL 01-144024-02100

NOTE:
THIS PROPERTY IS NOT WITHIN THE LIMITS OF A
FEMA/FIRM IDENTIFIED SPECIAL FLOOD HAZARD AREA
ACCORDING TO PANEL 05031C0257 D DATED
SEPTEMBER 26, 2024.

PREPARED FOR:
KN SYSTEMS, INC.
DR. SRA
11 KATSINA CIRCLE
CHEROKEE VILLAGE, AR 72529

FISHER ARNOLD
ENGINEERS | ARCHITECTS | CONSULTANTS | PLANNERS
404 Creath Avenue | Jonesboro, Arkansas 72404
870.932.2019 | Fax: 870.932.1076 | www.fisherarnold.com



July 17, 2025

Mr. Derrel Smith
Director of Planning
300 Church Street
Jonesboro, AR 72401

**RE: 2106 BRIDGER ROAD REZONING
PARCEL 01-144024-02100
JONESBORO, ARKANSAS**

Dear Derrell:

We appreciate the opportunity to submit this application for rezoning 6.79 acres located at 2106 Bridger Road in Jonesboro. The subject property is located on the east side of Bridger Road north of Medical Blvd. The subject property is zoned R-1 Single Family Residential. We are requesting the property be rezoned to RM-16.

We base our justification of the rezoning request upon several factors:

- a. there has been substantial change in the area with the construction of the hospital and support businesses
- b. the rezoning complies with the Comprehensive Plan which designates this area as High Density Residential.
- c. there is a public need for housing. This will support the hospital and retail businesses along E. Johnson Ave.
- d. this use will provide a logical transition between the higher intensive commercial uses and the single family uses to the north

Attached to our application is a concept plan of how this property is planned to be developed.

We are looking forward to working with you on this application. If you have any questions regarding this request, please do not hesitate to call. We will be waiting for your approval to proceed.

Sincerely,

FISHER & ARNOLD, INC.

David Baker
Manager – Planning & Landscape Architecture

404 Creath Avenue
Jonesboro, AR 72401
870.932.2019
Toll Free: 1.888.583.9724
www.fisherarnold.com



July 17, 2025

RE: REQUEST FOR REZONING FROM R-1 TO RM-16
PROPERTY LOCATION: 2106 BRIDGER ROAD
PARCEL: 01-144024-02100
JONESBORO, ARKANSAS

Dear Neighbor:

In regard to the rezoning for the referenced property, please see below a detailed explanation outlining the rezoning request.

On behalf of the owner of the referenced property, we would like to inform you of a rezoning request in your community. The subject property is located at 2106 Bridger Road. The property is currently zoned R-1 Single Family Residential and contains approximately 6.79 acres that need to be rezoned to allow development as multi-family residential housing.

The proposed use will provide a logical transition between intensive commercial uses to the south and existing single-family homes in the vicinity and is in compliance with the Jonesboro Comprehensive Plan, which identifies this area for high-density residential uses. It supports recent developments in the area, including the hospital and related medical uses, as well as the retail businesses along East Johnson Avenue. Most importantly, it responds to the current high demand for housing.

If the amendment to the zoning map request is approved, the RM-16 zoning will allow for the development of 80 multi-family residential units with support amenities and parking. Please see the attached Rezoning Plat and Concept Plan that were submitted to the City of Jonesboro Planning Department, and the City of Jonesboro Rezoning Property Owner Notification for additional information.

We anticipate the application to be heard during the Metropolitan Area Planning Commission meeting during the August 12, 2025 meeting, at 5:30 PM in the Council Chambers. If you have any questions related to our application, please do not hesitate to contact me at (888) 583-9724 .

Sincerely,

FISHER & ARNOLD, INC.

David Baker
Manager – Planning & Landscape Architecture

404 Creath Avenue
Jonesboro, AR 72401
870.932.2019
Toll Free: 1.888.583.9724
www.fisherarnold.com

REZONING INFORMATION:

- (1). How was the property zoned when the current owner purchased it? [R-1 Single Family Residential](#)
- (2). What is the purpose of the proposed Rezoning? Why is the rezoning necessary? [The proposed rezoning would provide for Higher Density Residential to support the Medical/Hospital and retail in the northeast portion of the city.](#)
- (3). If rezoned, how would the property be developed and used? [The property would be developed with multi-family residential.](#)
- (4). What would be the density or intensity of development (e.g. number of residential units: square footage of commercial, institutional, or industrial buildings? [The proposed development would have a maximum of 16 du/ac.](#)
- (5). Is the proposed rezoning consistent with the Jonesboro Comprehensive Plan and the Future Land Use Plan? [Yes, the Future Land Use Plan calls for this area to be High Density Residential.](#)
- (6). How would the proposed rezoning be in the public interest and benefit the community? [This rezoning would benefit the public by providing residential uses to support the growth around the hospital and medical facilities as well as the retail along E Johnson Ave.](#)
- (7). How would the proposed rezoning be compatible with the zoning, uses and character of the surrounding area? [This rezoning is compatible with the existing higher density mobile home community directly west of our property. Additionally, there is an existing senior housing facility to our south and east that has a similar density as what our proposed rezoning allows.](#)
- (8). Are there substantial reasons why the property cannot be used in accordance with existing zoning? [This property is on the fringe or transition area between the medical and retail areas and lower density residential areas. The higher density would support the medical and commercial districts and provide a logical land use transition between the higher intensive commercial uses and the lower density residential areas.](#)
- (9). How would the proposed rezoning affect nearby property including impact on property value, traffic, drainage, visual appearance, odor, noise, light, vibration, hours of use or operation and any restriction to the normal and customary use of the affected area? [The proposed rezoning would not have a negative impact on the neighborhood because it is located across the street from a mobile home park, and is located near higher intensive medical uses. The traffic patterns would predominately flow southward to E Johnson Ave and would have a minimal negative impact on traffic. Our proposed project would follow the City's Design Guidelines that are created to minimize impact on the community.](#)

- (10). How long has the property remained vacant? [There is a single family residence on the property that is occupied.](#)
- (11). What impact would the proposed rezoning and resulting development have on utilities, streets, drainage, parks, open space, fire, police and emergency medical services? [Due to the proximity of the hospital and retail services the project would have minimal impact on emergency services. There are existing utilities in the area that are adequate to serve our proposed](#)
- (12). If the rezoning is approved, when would development or redevelopment begin? [If the rezoning is approved, it is anticipated that construction would begin in the fall of 2026. This timeline would give time to prepare the final building and site designs, and go through reviews with the City of Jonesboro.](#)
- (13). How do neighbors feel about the proposed rezoning? Please attach minutes of the neighborhood meeting held to discuss the proposed rezoning or notes from individual discussions. If the proposal has not been discussed with neighbors, please attach a statement explaining the reason. Failure to consult with neighbors may result in delay in hearing the application. [An informational letter will be mailed via certified mail to property owners within 200 feet of the property. No meeting has been held to date, but contact information has been provided, with an offer to discuss any aspect of the proposed rezoning.](#)
- (14). If this application is for a Limited Use Overlay (LUO), the applicant must specify all uses desired to be permitted. [Our application is not for a LUO.](#)



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Text File

File Number: RZ-25-14

Agenda Date:

Version: 2

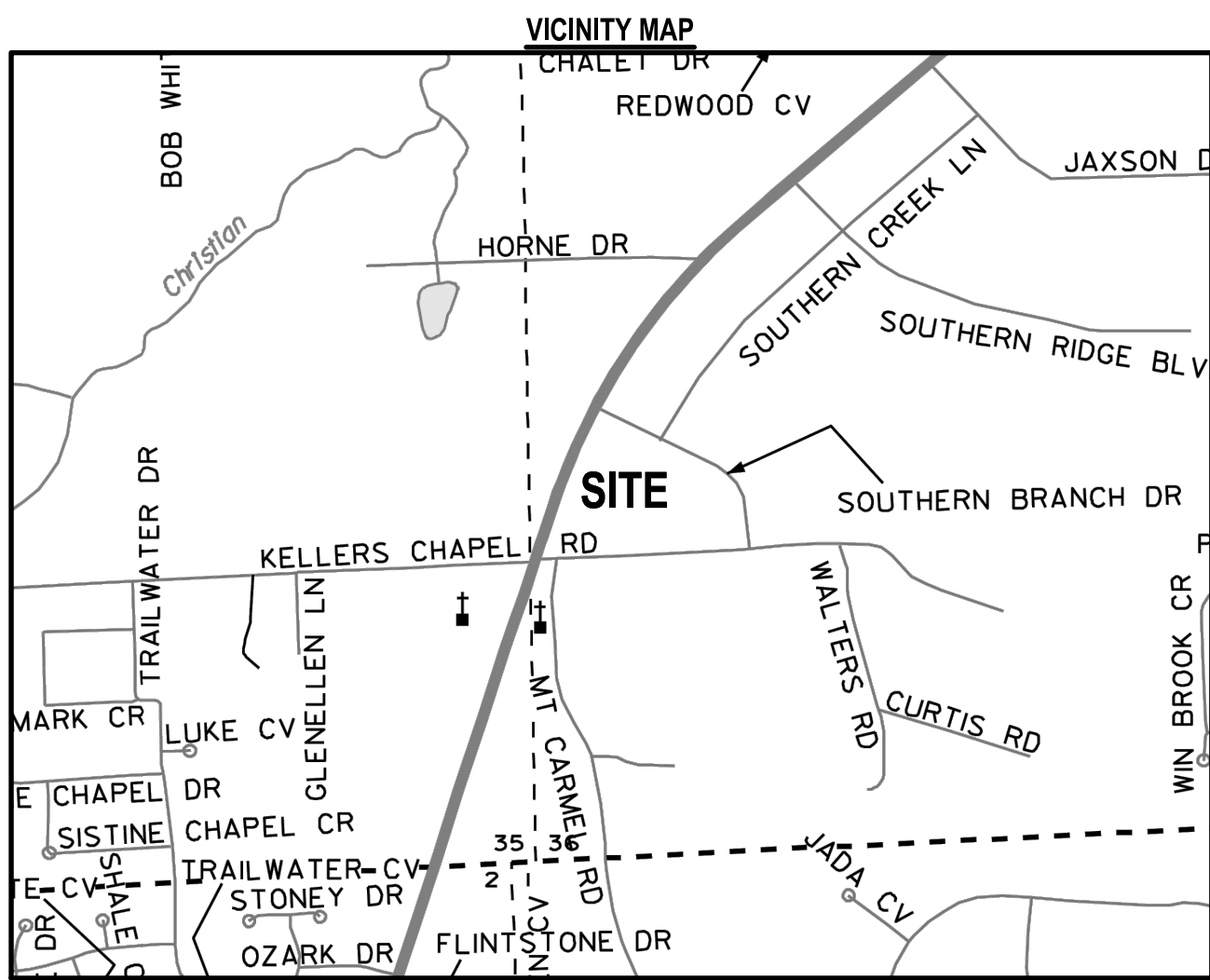
Status: To Be Introduced

In Control: Metropolitan Area Planning Commission

File Type: Rezoning

Rezone 7 acres +/- from PD-M Planned Development Multi Family to PD-M Commercial

Rezone 7 acres from PD-M multifamily to PD-M commercial in the Southern Hills Development. Jeremy Bevill of CTA is representing Southern Hills Real Estate, LLC to change the pattern book of the Southern Hills Development along Southern Branch Drive and Southern Ridge Blvd.



**BLOCK B
SOUTHERN HILLS,
JONESBORO, CRAIGHEAD
COUNTY, ARKANSAS**

THE FOLLOWING DOCUMENTS WERE USED IN CONSTRUCTION OF THIS SURVEY

- A. Arkansas State Highway Commission Right of Way map, Job # 100454. Dated 5/03/2001. obtained from Arkansas Department of Transportation.
- B. Survey by Daniels land Surveying, LLC. Dated 5/05/2021. Filed for record in Plat Book C, Page 362 of Craighead County records.
- C. Survey by Daniels land Surveying, LLC. Dated 8/23/2022. Filed for record in Plat Book C, Page 378 of Craighead County records.

SURVEYOR'S NOTES

1. Except as specifically stated or shown on this plat, this survey does not purport to reflect any of the following which may be applicable to the subject real estate: easements, other than possible easements which were visible at the time of making this survey; building setback lines; restrictive covenants; subdivision restrictions; zoning or other land use regulations, and any other facts which an accurate and current title search may disclose.
2. Surveyor has made no investigation or independent search for easements of record, encumbrances, restrictive covenants, owners' title evidence, or any other facts which an accurate and current title search may disclose.
3. Plat represents a boundary survey of a parcel described in Craighead County Deed Records at Book C, Page 378.
4. Survey is valid only if print has original seal and signature of surveyor present.
5. This survey meets current "Arkansas Minimum Standards for Property Surveys and Plats." Field work for this survey was completed on 03/26/2025.
6. Every document of record reviewed and considered as a part of this survey is noted hereon. No abstract of title, nor title commitment nor results of title searches were furnished to the surveyor. There may exist other documents of record which would affect this parcel.
7. The subject properties designated as Parcel 01-143363-05300 and Parcel 01-143363-05400 by the Craighead County, Arkansas, Assessor's Office.
8. All buildings, surface and subsurface improvements on and adjacent to the site are not necessarily shown.
9. The location and/or existence of utility service lines to the property surveyed are unknown and are not shown.
10. No attempt has been made as a part of this boundary survey to obtain or show data concerning existence, size, depth, condition, capacity, or location of any utility or municipal/public service facility. For information regarding these utilities or facilities, please contact the appropriate agencies or other surveys.

LEGEND

SET 5/8" REBAR P.S. 1563
 FOUND REBAR P.S. 1563
 FOUND RIGHT-OF-WAY
 5/8" REBAR (AS NOTED)

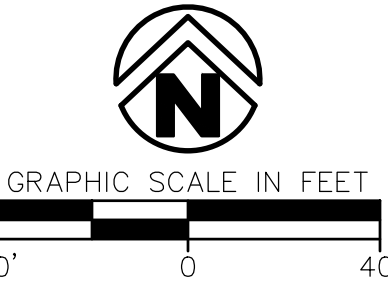
PROPERTY LINE
 ADJACENT PROPERTY
 LINE
 CENTERLINE ROAD
 EXISTING EASEMENT
 EXISTING RIGHT-OF-WAY

SURVEY DESCRIPTION:

A part of Block B, lot 1R of Southern Hills, Jonesboro, Craighead County, Arkansas, being more particularly described as follows:

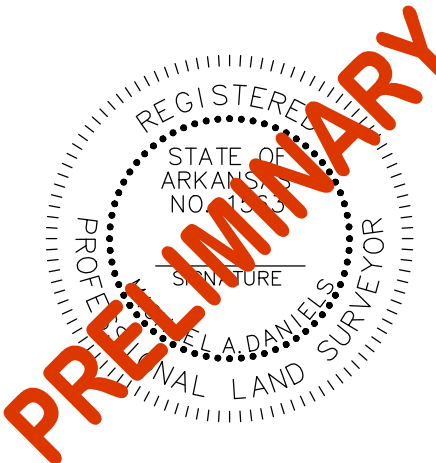
BEGINNING at the North corner of Block B, lot 1R; Thence South 58°40'40" East, a distance of 146.41 feet; Thence South 31°11'20" West, a distance of 174.96 feet; Thence South 61°00'44" West, a distance of 22.07 feet; Thence North 65°02'12" West, a distance of 122.70 feet; Thence North 34°21'37" East, a distance of 49.44 feet; Thence North 25°20'20" East, a distance of 158.87 feet to a POINT OF BEGINNING, containing 0.63 acres, more or less, and being subject to all Rights-of-Way- and easements of record.

BASIS OF BEARINGS:
RECORD PLAT BOOK C, PAGE
378 OF CRAIGHEAD COUNTY
CIRCUIT CLERK OFFICE



STATE PLAT CODE:
500-14N-03E-0-36-401-16-1563

SEAL



REZONING PLAT
BLOCK B
SOUTHERN HILLS, JONESBORO,
CRAIGHEAD COUNTY, ARKANSAS
PREPARED FOR:
SOUTHERN HILLS REAL ESTATE, LLC.

ISSUE DATE:	07/21/2025
PROJECT NO:	24902900
CONTACT:	SAM / MAD

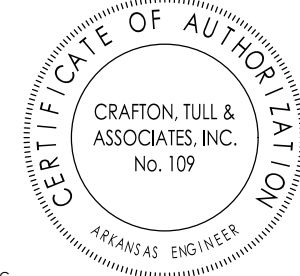
2520 Alexander Drive Suite C
Jonesboro, AR 72401-7194



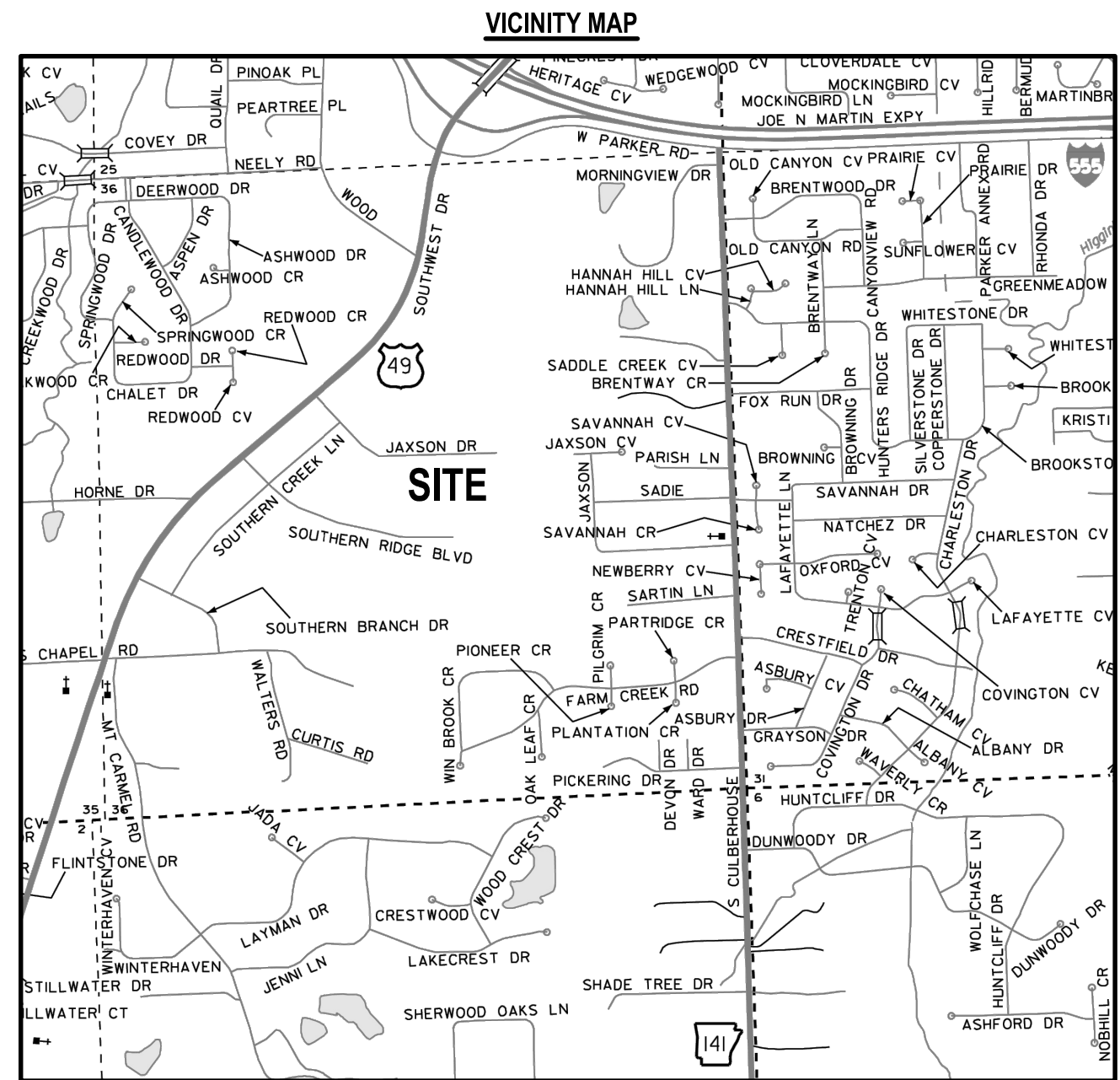
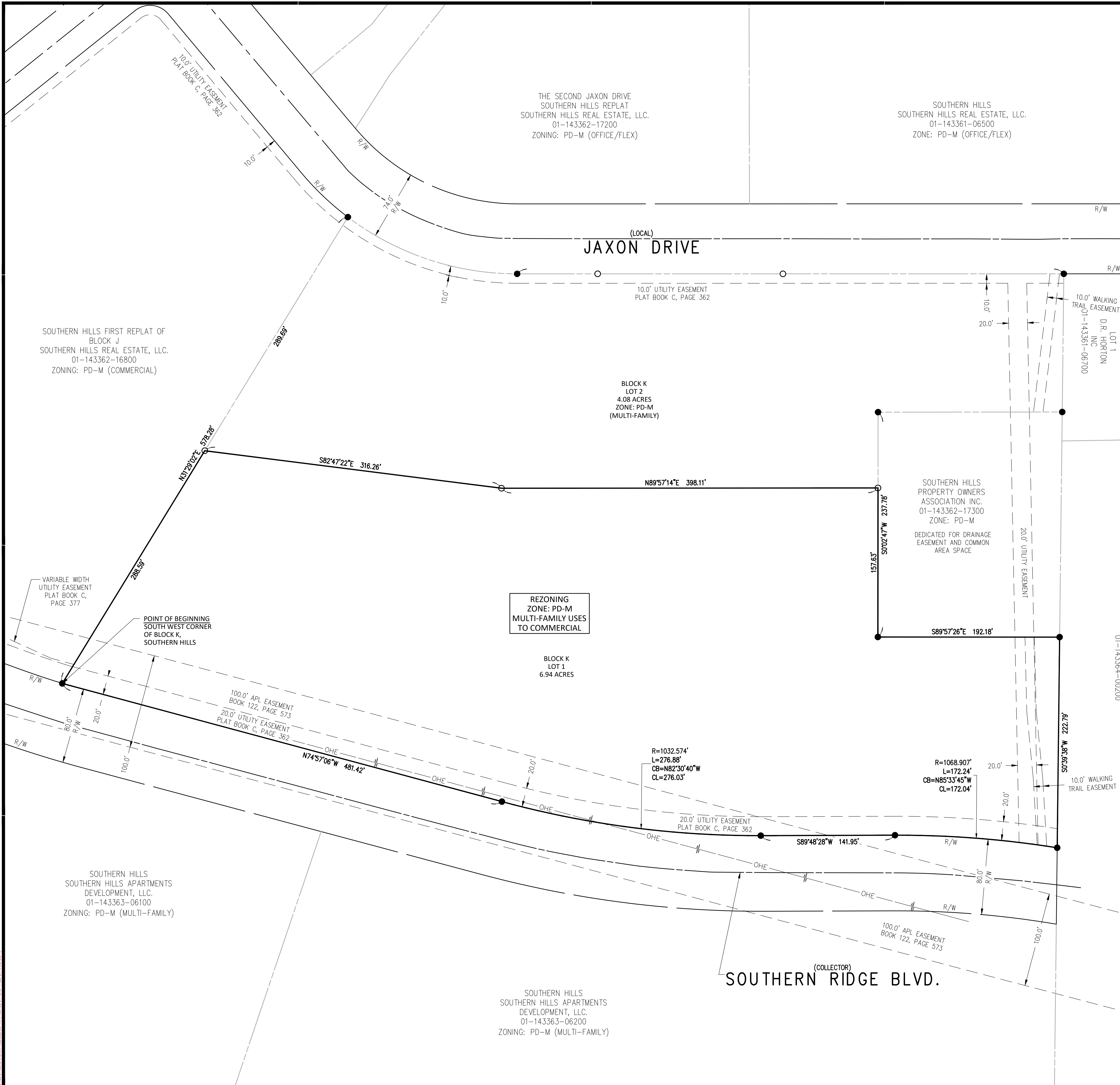
870.203.7876 t
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CERTIFICATE OF AUTHORIZATION

[illegible]

SHEET NO.



**BLOCK K
SOUTHERN HILLS,
JONESBORO, CRAIGHEAD
COUNTY, ARKANSAS**

SURVEYOR'S NOTES:

1. Except as specifically stated or shown on this plat, this survey does not purport to reflect any of the following which may be applicable to the subject real estate: easements, other than possible easements which were visible at the time of making this survey, building setback lines; restrictive covenants; subdivision restrictions; zoning or other land use regulations, and any other facts which an accurate and current title search may disclose.
2. Surveyor has made no investigation or independent search for easements of record, encumbrances, restrictive covenants, ownership title evidence, or any other facts which an accurate and current title search may disclose.
3. Survey is valid only if print has original seal and signature of surveyor present.
4. This survey meets current "Arkansas Minimum Standards for Property Surveys and Plats." Field work for this survey was completed on 05/30/2025.
5. Every document of record reviewed and considered as a part of this survey is noted hereon. No abstract of title, nor title commitment nor results of title searches were furnished to the surveyor. There may exist other documents of record which would affect this parcel.
6. The subject properties designated as Parcel 01-143362-16900 by the Craighead County, Arkansas, Assessor's Office.
7. All buildings, surface and subsurface improvements on and adjacent to the site are not necessarily shown.
8. The location and/or existence of utility service lines to the property surveyed are unknown and are not shown.
9. No attempt has been made as a part of this boundary survey to obtain or show data concerning existence, size, depth, condition, capacity, or location of any utility or municipal/public service facility. For information regarding these utilities or facilities, please contact the appropriate agencies or other surveys.

THE FOLLOWING DOCUMENT WAS USED IN THE CONSTRUCTION OF THIS SURVEY:

- A. Final Subdivision Plat Southern Hills. Filed for record in Plat Book C, Page 362, in the Circuit Clerk's office of Craighead County Arkansas.

LEGEND

- SET 5/8" REBAR (P.S. 1563)
FOUND REBAR (AS NOTED)

PROPERTY LINE _____

ADJACENT PROPERTY LINE _____

CENTERLINE ROAD _____

EXISTING EASEMENT _____

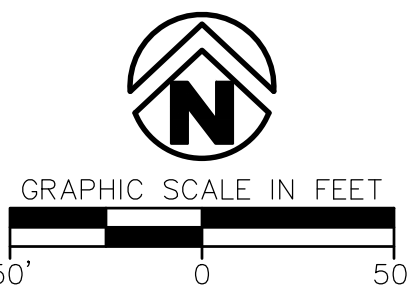
EXISTING RIGHT-OF-WAY _____

SURVEY DESCRIPTION:

A part of Block K of Southern Hills, Jonesboro, Craighead County, Arkansas, being more particularly described as follows:

BEGINNING at the South West corner of Block K; Thence North 31° 29'02" East, a distance of 288.59 feet; Thence South 82° 47'22" West, a distance of 316.26 feet; Thence North 89° 57'14" East, a distance of 398.11 feet; Thence South 00° 02'44" West, a distance of 157.63 feet; Thence South 89° 57'26" East, a distance of 192.18 feet; Thence South 00° 39'38" East, a distance of 222.79 feet to the beginning of a curve, Said curve turning to the left, having a radius of 1,068.907 feet, whose long chord bears N89° 33'45" West a distance of 172.04 feet; Thence South 89° 45'24" West, a distance of 141.66 feet to the beginning of a curve, Said curve turning right having a radius of 1,068.907 feet, whose long chord bears N82° 30'40" West a distance of 176.03 feet; Thence North 74° 57'00" West, a distance of 481.42 feet to a **POINT OF BEGINNING**, containing 6.94 acres, more or less, and being subject to all Rights-of-Way, and easements of record.

BASIS OF BEARINGS:
RECORD PLAT BOOK C, PAGE
362 OF CRAIGHEAD COUNTY
CIRCUIT CLERK OFFICE

STATE PLAT CODE:
500-14N-03E-0-36-000-16-1563

SEAL



REZONING PLAT
BLOCK K, SOUTHERN HILLS, JONESBORO,
CRAIGHEAD COUNTY, ARKANSAS
PREPARED FOR:
SOUTHERN HILLS REAL ESTATE, LLC.

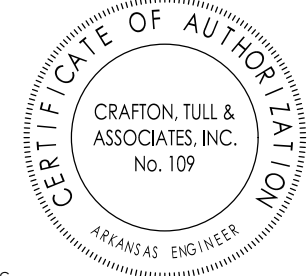
ISSUE DATE:	07/21/2025
PROJECT NO:	24902900
CONTACT:	SAM / MAD



870.203.7876 †
www.craftontull.com

OF A

CERTIFICATE OF AUTHORIZATION



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[illegible]

SHEET NO.



Application for a Zoning Ordinance Map Amendment

METROPOLITAN AREA
PLANNING COMMISSION
Jonesboro, Arkansas

Meeting Date: _____ Date Received: _____
Meeting Deadline: _____ Case Number: _____

LOCATION:

Site Address: 3412 Southwest Drive

Side of Street: East between Jaxon Drive and Kellers Chapel Road

Quarter: SE Section: 36 Township: 14N Range: 3E

Attach a survey plat and legal description of the property proposed for rezoning. A Registered Land Surveyor must prepare this plat.

SITE INFORMATION:

Existing Zoning: PD-M Mult-family & Office/Senior Living Proposed Zoning: PD- M Commercial

Size of site (square feet and acres): 7 Acres +/- Street frontage (feet): 1,100'

Existing Use of the Site: Vacant

Character and adequacy of adjoining streets: Southwest Drive (Principal Arterial) is 5-lane Hwy.

Does public water serve the site? Yes 8" Water main is on property.

If not, how would water service be provided? N/A

Does public sanitary sewer serve the site? Yes, existing sewer is in-place.

If not, how would sewer service be provided? N/A

Use of adjoining properties:

North Vacant

South Vacant & Convenience Store

East Vacant

West Vacant

Physical characteristics of the site: Land sloping east.

Characteristics of the neighborhood: Commercial

Applications will not be considered complete until all items have been supplied. Incomplete applications will not be placed on the Metropolitan Area Planning Commission agenda and will be returned to the applicant. The deadline for submittal of an application is on the public meeting schedule. The Planning staff must determine that the application is complete and adequate before it will be placed on the MAPC agenda.

REZONING INFORMATION:

The applicant is responsible for explaining and justifying the proposed rezoning. Please prepare an attachment to this application answering each of the following questions in detail:

- (1). How was the property zoned when the current owner purchased it?
Mixed Zoning, RS-1, C-3
- (2). What is the purpose of the proposed rezoning? Why is the rezoning necessary?
Highest & best use of the property.
- (3). If rezoned, how would the property be developed and used?
Commercial Development.
- (4). What would be the density or intensity of development (e.g. number of residential units; square footage of commercial, institutional, or industrial buildings)?
Square footage of commercial is estimated at 80,000.
- (5). Is the proposed rezoning consistent with the Jonesboro Comprehensive Plan and the Future Land Use Plan?
Yes, this portion of Southwest Drive is High Intensity.
- (6). How would the proposed rezoning be the public interest and benefit the community?
Yes, retail and commercial businesses.
- (7). How would the proposed rezoning be compatible with the zoning, uses, and character of the surrounding area?
The subject property is adjacent to commercial and multi-family zoning.
- (8). Are there substantial reasons why the property cannot be used in accordance with existing zoning?
Current housing demands.
- (9). How would the proposed rezoning affect nearby property including impact on property value, traffic, drainage, visual appearance, odor, noise, light, vibration, hours of use or operation and any restriction to the normal and customary use of the affected property.
The rezoning and future development should have a positive impact on adjacent properties.
- (10). How long has the property remained vacant?
Lots are vacant. The land has been developed into a commercial subdivision recently
- (11). What impact would the proposed rezoning and resulting development have on utilities, streets, drainage, parks, open space, fire, police, and emergency medical services?
Impacts, if any, can be mitigated through the design phase.
- (12). If the rezoning is approved, when would development or redevelopment begin?
Within 1 month.
- (13). How do neighbors feel about the proposed rezoning? Please attach minutes of the neighborhood meeting held to discuss the proposed rezoning or notes from individual discussions. *If the proposal has not been discussed with neighbors, please attach a statement explaining the reason. Failure to consult with neighbors may result in delay in hearing the application.*
Adjacent properties are vacant.
- (14). If this application is for a Limited Use Overlay (LUO), the applicant must specify all uses desired to be permitted.

OWNERSHIP INFORMATION:

All parties to this application understand that the burden of proof in justifying and demonstrating the need for the proposed rezoning rests with the applicant named below.

Owner of Record:

I certify that I am the owner of the property that is the subject of this rezoning application and that I represent all owners, including spouses, of the property to be rezoned. I further certify that all information in this application is true and correct to the best of my knowledge.

Applicant:

If you are not the Owner of Record, please describe your relationship to the rezoning proposal:

Name:	<u>Southern Hills Real Estate, LLC</u>	Name:	<u>Jeremy Bevill, PE</u>
Address:	<u>2704 S. Culberhouse</u>	Address:	<u>2520 Alexander Dr, Ste C</u>
City, State:	<u>Jonesboro, AR</u> ZIP <u>72401</u>	City, State:	<u>Jonesboro, AR</u> ZIP <u>72401</u>
Telephone:	<u>870-935-7800</u>	Telephone:	<u>870-203-7876</u>
Facsimile:	<u>N/A</u>	Facsimile:	<u>N/A</u>
Signature:	<u></u>	Signature:	<u></u>

Deed: Please attach a copy of the deed for the subject property.

Applications will not be considered complete until all items have been supplied. Incomplete applications will not be placed on the Metropolitan Area Planning Commission agenda and will be returned to the applicant. The deadline for submittal of an application is on the public meeting schedule. The Planning staff must determine that the application is complete and adequate before it will be placed on the MAPC agenda.

Southern Hills

Planned Development - Major Modification (ORD-20:018, v. 1)
Development Guidelines

Submitted to:
City of Jonesboro
Planning & Zoning Department
300 S. Church St
Jonesboro, AR 72401

Job No. 24902900

Prepared by:



Crafton Tull

Prepared for:
Southern Hills Real Estate, LLC
2505 Southwest Square
Jonesboro, AR 72401

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Street Sections and Buffer Yards16

Character Images16

Site and Project Information

Owner Information:

Name: Southern Hills Real Estate, LLC
Address: 2110 Fair Park Boulevard, Suite E, Jonesboro, AR 72401

Project Information:

Parcels: 01-143362-17100, 01-143362-17000, 01-143362-17120,
01-143362-17110, 01-143362-17200, 01-143362-06500,
01-143362-06600, 01-143362-16600, 01-143362-16500,
01-143362-16400, 01-143362-16700, 01-143362-16800,
01-143362-16900, 01-143362-17300, 01-143362-05700,
01-143362-05620, 01-143362-05610, 01-143362-05600,
01-143362-05500, 01-143362-06000, 01-143362-05900,
01-143362-05800, 01-143362-06100, 01-143362-06200,
01-143362-06400, 01-143362-06300, 01-143362-05400,
01-143362-05300, 01-143362-05210
Address: 3412 Southwest Dr, Jonesboro, AR
Acreage: ±118.34 acres
Existing Zoning: PD-M
Existing Use: Mixed-Use
Proposed Zoning: PD-M
Proposed Use: Mixed-Use

Project Narrative

The Southern Hills is an approved Planned Development District (PD-M, ORD-20:018 v. 1) that allows for a variety of residential, commercial and related uses. Many lots in the district are being designed, under construction, or fully constructed. Crafton Tull has helped design many of the various lots in the PD and seeks a modification to the previously approved Final Development Plan.

The proposed modifications to the Final Development Plan will expand the boundaries of Area A (retail/small lot use) and reduce the size of Area B (Office/Senior Living Uses). Area K (condominiums, townhomes, multi-family uses) will also be modified so the lots fronting Southern Ridge Blvd are commercial and the lots fronting Jaxon Dr. are remaining condos, townhomes, and multi-family. All the proposed changes align with the intent of the existing ordinance and permitted uses.

All other development guidelines as approved in the ordinance will remain unchanged.

Vicinity Map



Legal Description

A part of the Southwest Quarter of the Northwest Quarter , a part of the Southeast Quarter of the Northwest Quarter, & a part of the Southwest Quarter of the Northeast Quarter & a part of the Northwest Quarter of the Southwest Quarter & a part of the Northeast Quarter of the Southwest Quarter, & a part of the Northwest Quarter of the Southeast Quarter, and all being in Section 36, Township 14 North, Range 3 East, Craighead County, Arkansas and being more particularly described as follows:

COMMENCING at the Southwest corner of the Northwest Quarter of the Southwest Quarter of said Section 36; thence North 88°56'51" East, 181.29 feet to the POINT OF BEGINNING; Thence North 00°44'51" West, 24.89 feet; Thence North 87°12'05" West, 68.02 feet; Thence North 16°47'37" West, 24.92 feet; Thence North 19°52'49" East, 148.99 feet; Thence North 21°32'46" East, 163.36 feet; Thence North 23°49'52" East, 197.11 feet; Thence with a curve to the right, with a radius of 2864.79 feet, 92.83 feet, to a point which bears North 28°11'21" East, 92.83 feet from the last said point; Thence South 61°02'05" East, 9.95 feet; Thence with a curve to the right, with a radius of 2878.33 feet, 307.41 feet, to a point which bears North 32°07'04" East, 307.27 feet from the last said point; Thence North 37°17'30" East, 129.76 feet; Thence North 37°24'18" East, 59.69 feet; Thence North 41°11'15" East, 196.50 feet; Thence North 45°10'17" East, 196.38 feet; Thence North 49°17'02" East, 202.12 feet; Thence North 51°19'12" East, 568.98 feet; Thence North 56°40'44" East, 113.17 feet; Thence North 56°38'48" East, 209.43 feet; Thence North 40°22'27" East, 156.89 feet; Thence North 51°36'11" East, 247.58 feet; Thence North 38°23'54" West, 10.00 feet; Thence North 51°36'06" East, 171.70 feet; Thence with a curve to the left, with a radius of 1733.51 feet, 175.32 feet, to a point which bears North 48°19'18" East, 175.25 feet from the last said point; Thence South 37°12'39" East, 110.56 feet; Thence South 37°01'48" East, 143.79 feet; Thence South 89°00'31" West, 35.43 feet; Thence South 00°04'39" West, 208.81 feet; Thence North 89°05'10" East, 209.73 feet; Thence North 00°10'04" West, 629.18 feet; Thence North 88°59'44" East, 656.02 feet; Thence South 00°00'17" East, 1074.26 feet; Thence South 89°59'48" West, 319.95 feet; Thence South 00°39'38" West, 250.80 feet; Thence South 00°39'38" West, 1320.06 feet; Thence South 88°56'31" West, 329.96 feet; Thence South 88°48'06" West, 1314.89 feet; Thence South 88°56'51" West, 1135.34 feet to the POINT OF BEGINNING, containing 118.34 acres more or less and being subject to all rights of way and easements of record.

Approved PD Master Development Plan

Areas A, B, and K from the approved PD are proposed to be amended. No modifications are requested for the remaining areas in the PD.



Figure 1: Previously Approved PD Final Development Plan

Proposed PD Master Development Plan

Modifications to the ordinance are necessary as lots are being developed. Eight lots have been developed with more under design. The majority of the PD will remain as approved. The two proposed changes are highlighted below.



Figure 2: Proposed Modifications to the PD

Area A-B Approved & Proposed

The modifications to Areas A & B are as follows:

	Approved	Proposed
Area A	± 4.06 acres	± 4.32 acres
Area B	± 3.86 acres	± 2.69 acres

The uses of each area will not be modified. Area A will be slightly expanded and Area B will be reduced. The expansion of Area A will allow for expanded retail/commercial development on the Southwest Drive corridor. Total acreages differ from approved due to minor shifts in road alignments during site planning and construction.

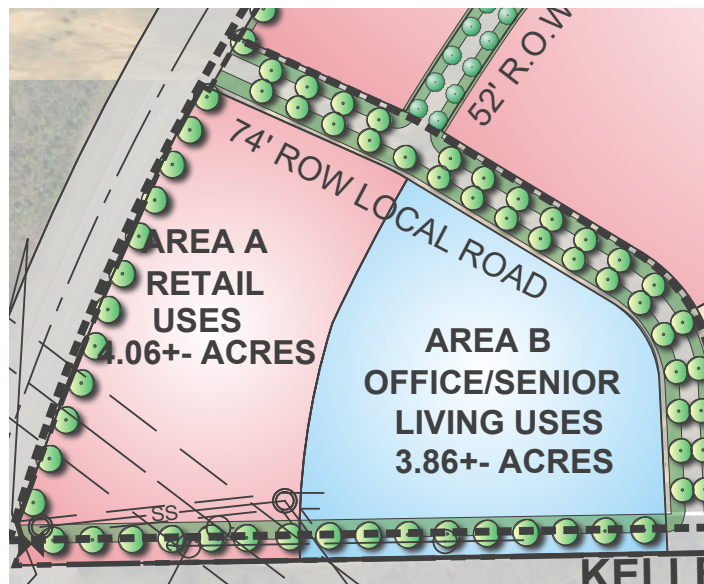


Figure 3: Approved Areas A & B



Figure 4: Proposed Areas A & B

Area K Approved & Proposed

The modification to Area K are shown in the table below:

Area K	Approved		Proposed	
	Condos, Townhomes, Multi-Family	Commercial	Condos, Townhomes, Multi-Family	Commercial
	± 12.14 acres	± 0.00 acres	± 4.08 acres	± 6.94 acres

The proposed commercial lots will front Southern Ridge Blvd, a new collector street that connects Southwest Drive, the PD site, and future development. These lots, located in Area K, will be governed by the same C-3 base zoning as other areas within the approved PD, which permits uses such as grocery stores. The largest of these lots is planned for a grocery store, providing a much-needed service to both the PD’s residential areas and the broader community. Adjacent lots along Jaxon Dr. will remain designated for condos, townhomes, and multi-family housing. Minor shifts in road alignments during site planning and construction have resulted in slight changes to total acreages compared to the original approval.

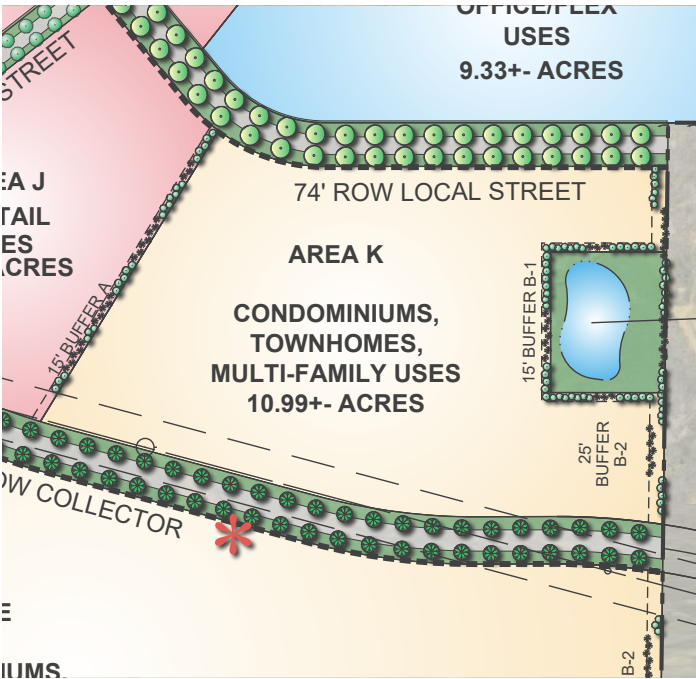


Figure 5: Approved Area K

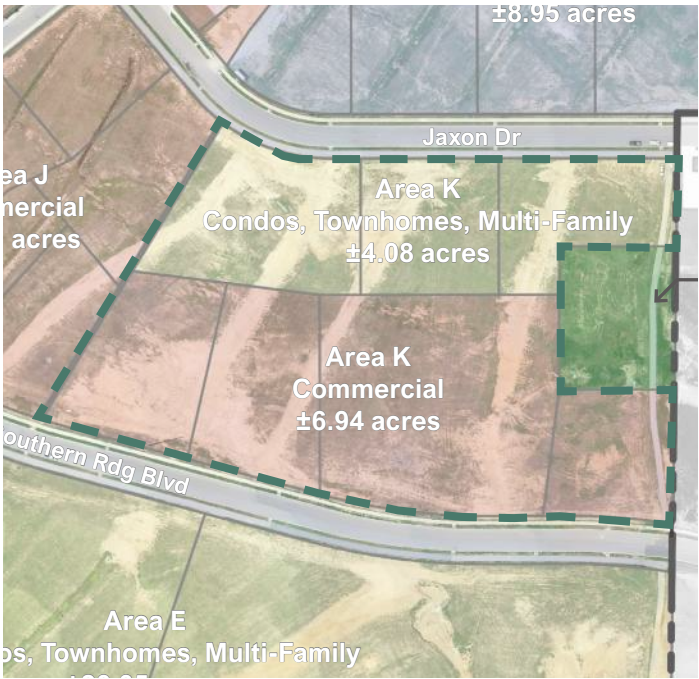


Figure 6: Proposed Area K

Modified Development Guidelines

I. GENERAL

Proposed modifications to the PD are written below and marked in **red**. All other permitted uses, dimensional regulations, access and circulation, design criteria, etc for the project will remain the same as approved. The approved development guidelines have been listed below for reference.

- A. **Areas A, C, D, H, J, K, and L - The planned uses are limited to commercial and retail services arranged on separate lots, as regulated by the C-3 General Commercial District as modified herein.**
 - B. Area B - The planned uses are limited to office and assisted living services as regulated by the CR-1 Commercial Residence Mixed Use District.
 - C. Areas E and K - The planned uses are limited to multi-family residential as regulated by the RM-16 Residential Multifamily District, including condominiums, townhouses, and multi-family.
 - D. Area F - The planned uses are limited to self-storage facilities as regulated by the C-3 General Commercial District.
 - E. Area G, N and O - The planned uses are limited to common open space.
 - F. Area M - The planned uses are limited to office flex space.
 - G. All areas - Quasi-public facilities are allowed in all development areas. These uses may include churches, public or quasi-public amenity areas, schools, and public or quasi-public governmental facilities.
- and regulated in the C-3, General Commercial District as modified herein. The following uses are excluded:**
- 1. Adult entertainment
 - 2. Auto wrecking yards, salvage yards, and junkyards
 - 3. Cemetery
 - 4. Columbarium
 - 5. Funeral home
 - 6. Manufactured housing units
 - 7. Pawn shop
 - 8. Recreational vehicle park
- B. Office and Assisted Living Uses shall be permitted in Area B, as permitted and regulated in the CR-1 Commercial Residence Mixed Use District, as modified herein.
 - C. Commercial Uses shall be permitted in Areas D and J, as permitted and regulated in the C-3 General Commercial District, as modified herein.
 - D. Multi-family Residential Uses shall be permitted in Areas E and K, as permitted and regulated in the RM-16 Multifamily Residential District, as modified herein. Such uses include condominiums, townhouses, and multi-family.
 - E. Office and Commercial Uses shall be permitted in Area M as permitted and regulated in the C-4 Neighborhood Commercial District, as modified herein.
 - F. Common Open Spaces shall be provided in Areas G, N, and P as regulated herein.
 - G. Commercial Self-Storage Use shall be permitted in Area F, as permitted and regulated in the C-3 General Commercial District, as modified herein.
 - H. A Declaration of Covenants, Conditions and Restrictions, hereinafter the “Declaration” shall be applicable to all properties within the development and an Owner’s Association shall be maintained to assure that development is completed to assure that all

II. USES PERMITTED

- A. **Commercial Uses shall be permitted in Areas A, C, D, H, K, and L as permitted**

features and amenities of the community which are considered to be common assets are continuously maintained in a quality manner. Multiple declarations will be allowed within different areas.

III. BULK REQUIREMENTS

Development will be generally guided by the Outline Plan, Site Plan and Master Plan, and the area use descriptions provided above. Development of individual parcels will be specifically in compliance with the provisions for dimensional regulations, and access and circulation conditions provided below.

Each area is also designated with a Referenced District, referring to zoning districts in the Jonesboro Zoning Ordinance. Other restrictions not specifically addressed here, including but not limited to, setbacks, side and rear yards, minimum lot area, accessory uses and parking and loading shall be as defined in the Jonesboro Zoning Ordinance as applicable to the Reference District for the particular parcel in question. Where provisions of this agreement are in conflict with the above referenced Ordinance, these provisions shall apply. Where two (2) or more provisions of this agreement are in conflict, the more stringent of these provisions shall apply, unless an alternative is approved by the Planning Commission.

A. Maximum densities: Dwelling Units per Acre (DUA) and floor area ratio (FAR) for Areas and uses shall be as follows:

1. 0.25 FAR - Commercial Uses
2. 0.40 FAR - Office Uses
3. 0.50 FAR - Office/Distribution Use
4. 0.70 FAR - Hotel, Motel, or Hospital Uses
5. 16 DUA - Condominium, Townhouse, Multi-Family Residential, or Assisted Living Use

B. Minimum Setbacks for Non-Residential

Uses (Areas A, C, D, H, J, **K**, and L)

1. For buildings greater than 15 feet or one story in height, the minimum side and rear building setbacks as set forth below shall be increased 8 feet for each additional 15-foot increase in building height.
2. Minimum building setback from Southwest Drive shall be 25 feet.
3. Minimum building setback from Kellers Chapel Road shall be 25 feet.
4. Minimum building setback from minor collector streets shall be 25 feet.
5. Minimum building setback from all other public and private rights-of-way shall be 20 feet.
6. Minimum side yard shall be 10 feet unless adjacent to residential use, which shall be 15 feet.
7. Minimum rear yard shall be 20 feet unless adjacent to residential use, which shall have a rear yard of 25 feet.

C. Minimum Building Setbacks for Multi-Family Residential Uses (Areas E and K)

1. Minimum building setback from public rights-of-way shall be 25 feet.
2. Minimum building setback from private drives shall be 20 feet.
3. Maximum building height shall be 45 feet.
4. Minimum side yard adjacent to non-residential uses shall be 15 for single-story structures, 20 feet for two-story structures, and 30 feet for three-story or greater structures. Where adjacent to single-family residential use, the minimum side yard shall be 30 feet.
5. Minimum rear yard shall be 20 feet unless adjacent to single-family residential use, which shall have a rear yard of 35 feet.
6. Buildings with balconies and / or porticos are allowed to infringe into the setback zone with the balcony and / or portico no more than 10 feet.

D. Maximum building heights shall be as follows:

1. All Commercial uses: 35 feet
2. Office uses: 60 feet *
3. Hotel / Motel: 60 feet *
4. Hospitals: 60 feet *
5. All distribution or showroom uses: 50 feet
6. Multi-Family Residential uses: 45 feet

* For every one foot of additional setback from site perimeter and public roads, a gain of one-half foot in building height shall be allowed with a maximum height of 70 feet for non-residential structures.

- E. Minimum required site area for each non-residential use shall be in conformance with the Zoning Ordinance requirements and shall conform to the density requirements set forth in Condition III. A.
- F. Air conditioning, heating, electrical, and other mechanical and utility equipment shall be screened using architectural features, planting, fences, or other means from motorists on public rights-of-way and the site perimeter.
- G. Outdoor storage on Areas A, C, D, H, J, **K**, L, and M shall be buffered in public rights-of-way or adjacent residential properties.

IV. ROADS, ACCESS, AND CIRCULATION

- A. Southwest Drive shall be full improved 50 feet from the centerline. Improvements shall include sidewalks (or an alternative pedestrian system), street lighting, and relocation of utilities (if necessary).
- B. Kellers Chapel Road shall be dedicated and fully improved 40 feet from the centerline. Improvements shall include curb and gutter, sidewalks (or an alternative pedestrian system), street lighting, and relocation of utilities (if necessary).
- C. All internal public streets shall be dedicated

and improved in accordance with the Street Sections.

- D. The maximum number of curb cuts to Southwest Drive shall be eight (8), distributed as follows, with no curb cut located closer than 300 feet from the intersection with Kellers Chapel Road:
 1. Area A: 1
 2. Area C: 3
 3. Area H: 2
 4. Area L: 2
- E. The maximum number of curb cuts to Kellers Chapel Road shall be five (5), distributed as follows:
 1. Area A: 1
 2. Area B: 1
 3. Area F: 2
 4. Area G: 1
- F. For all other streets, the number and location of curb cuts to parcels shall be subject to the approval of the City Engineer.
- G. Parking and loading spaces shall be in accordance with the Zoning Ordinance requirements. Except that common parking areas may be approved on Final Plans serving several uses which have variations in the accumulation of vehicles and have relationships between land uses that result in multiple visits to different uses in the same trip, based on the Urban Land Institute's Shared Parking formula. Common loading space(s) shall be in accordance with the anticipated intensity of use of the loading space(s) for the various uses. Both parking and loading requirements shall be subject to the approval of the Planning Commission.
- H. All public improvements required herein shall be made to the specifications of the City of Jonesboro.
- I. All private drives shall be constructed to meet applicable Subdivision Regulations and shall provide a minimum pavement width of twenty-four (24) feet, exclusive of curb and gutter

except where modified herein.

- J. All private drives and alleys shall remain private and maintained by the Owner's Association in perpetuity. Full Disclosure shall be made through the private covenants regarding the private status, and maintenance responsibilities, of all alleys and private drives.
- K. All development within Areas A, C, D, F, H, J, **K**, and L shall interconnect in such a manner that interchange between parcels will not require access on Southwest Drive or Kellers Chapel Road.
- L. All utility and pedestrian easements shall be delineated on the Final Plan.
- M. A five-foot utility easement shall be placed along property lines as needed to service development.
- N. The placement and location of utilities within the development, excepting those along Southwest Drive and Kellers Chapel Road, shall be underground.

V. LANDSCAPING, SCREENING, OPEN SPACE

- A. Streetscape areas along Southwest Drive shall be a minimum of 25 feet in width, which includes a green buffer, 6-foot sidewalk, and utility corridor within the right-of-way.
- B. Streetscape areas along Kellers Chapel Road shall be a minimum of 20.5 feet in width (see streetscape plate).
- C. Streetscape areas along internal drives shall adhere to the Streetscape Plates indicated on the Outline Plan, (see plates).
- D. A minimum 15-foot wide Buffer Yard A shall be provided between all commercial and residential uses.
- E. A minimum 15-foot wide Buffer Yard B-1 shall be provided where multi-family residential use adjoins single-family and common open space uses. Where shared

use paths are indicated in the buffer yard separating single-family residential and multi-family residential uses, a minimum 25-foot wide Buffer Yard B-2 shall be provided.

- F. All parking shall be screened from public and private streets with low-growing shrubs and / or berms in conformance with the Street Plates. Landscaping internal to the parking lots shall be provided at a minimum ratio of three hundred (300) square feet of landscaping area and one (1) shade tree per every twenty (20) parking spaces. Landscape areas with twenty (20) or more parking spaces should not be less than two hundred (200) square feet in area in any single location and should be located so that no parking space is further than seventy-five (75) feet from a shade tree. Modifications may be approved by the Planning Commission. Area O is exempt from this requirement.
- G. All construction and improvements within the development shall be in compliance with erosion and sediment control guidelines and ordinances of the City of Jonesboro.
- H. All required landscaping shall not conflict with any existing easements.
- I. Refuse containers and loading areas shall be screened from view of adjacent public roadways and residential properties.

VI. SIGNS

- A. All signs shall be designed, located, and constructed in accordance with a comprehensive sign plan submitted to and approved by the Planning Commission.
- B. The minimum sign setback from any public right-of-way shall be 15 feet, excluding directional and stop signs.
- C. Portable and temporary signs shall not be permitted except temporary construction signs approved by the City of Jonesboro.
- D. Off-premise, advertising, and billboard signs shall not be permitted.

VII. Stipulations

- A. That the proposed site shall satisfy all requirements of the City Engineer, all requirements of the current Stormwater Drainage Design Manual and Flood Plain Regulations regarding any new construction.
- B. A final site plan subject to all ordinance requirements shall be submitted, reviewed, and approved by the Planning Department, prior to any redevelopment of the property.
- C. Any change of use shall be subject to Planning Department approval in the future.
- D. A final site plan illustrating compliance with site requirements for parking, signage, landscaping, fencing, buffering, outdoor storage, dumpster enclosure, sidewalks etc. shall be submitted to the Planning Department prior to any redevelopment.
- E. The Rezoning will have to comply with all the Planned Development District Standards.
- F. The City will allow three permanent roads and up to five temporary driveways along Southwest Drive until such time as a traffic signal is installed by the developer at Drive "E". At that time, any driveway in conflict with the traffic signal shall be removed by the developer. All such, permanent and temporary drives shall meet the spacing and other requirements set forth in the City's Access Management Ordinance, including the need for deceleration lanes or tapers.

Street Sections and Buffer Yards

No modifications are proposed to the approved street sections and buffer yards. Parking on streets will not be allowed.

Character Images

Multi-Family Uses



Retail / Commercial



Office

