



City of Jonesboro

Municipal Center
300 S. Church Street
Jonesboro, AR 72401

Meeting Agenda Public Works Council Committee

Tuesday, May 2, 2017

5:00 PM

Municipal Center

1. Call To Order

2. Roll Call by City Clerk Donna Jackson

3. Approval of minutes

MIN-17:039 Minutes for the Public Works Committee meeting on April 4, 2017

Attachments: [Minutes](#)

MIN-17:046 Minutes for the special called Public Works Committee meeting on April 18, 2017

Attachments: [Minutes](#)

4. New Business

Resolutions To Be Introduced

RES-17:042 RESOLUTION BY THE CITY COUNCIL OF THE CITY OF JONESBORO, ARKANSAS AUTHORIZING THE MAYOR TO ENTER INTO AN AGREEMENT WITH THE ARKANSAS DEPARTMENT OF COMMUNITY CORRECTIONS FOR COMMUNITY SERVICE WORKERS

Sponsors: Sanitation

Attachments: [Copy of Community Service Worksite Agreement.pdf](#)

RES-17:047 RESOLUTION OF THE CITY OF JONESBORO, ARKANSAS TO AMEND RESOLUTION 2001:34 AND AUTHORIZING THE MAYOR TO EXECUTE A WARRANTY DEED WITH A CORRECTED PROPERTY DESCRIPTION

Sponsors: Mayor's Office

Attachments: [Highfill Deed.docx](#)

RES-17:050 A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO AUTHORIZE THE MAYOR AND CITY CLERK TO ACCEPT A PERMANENT DRAINAGE EASEMENT FROM MARK WEBB (WOODFIELD PROPERTIES, LLC) FOR THE PURPOSE OF MAKING DRAINAGE IMPROVEMENTS

Sponsors: Engineering

RES-17:051 A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO ENTER INTO AN AGREEMENT WITH UNION PACIFIC RAILROAD COMPANY TO IMPROVE EXISTING PUBLIC PEDESTRIAN CROSSING

Sponsors: Engineering

Attachments: [Exhibit A](#)
[Exhibit B](#)
[Jonesboro Ped Agreement](#)

5. Pending Items

6. Other Business

7. Public Comments

8. Adjournment



City of Jonesboro

300 S. Church Street
Jonesboro, AR 72401

Legislation Details (With Text)

File #: MIN-17:039 **Version:** 1 **Name:** Minutes for the Public Works Committee meeting on April 4, 2017
Type: Minutes **Status:** To Be Introduced
File created: 4/5/2017 **In control:** Public Works Council Committee
On agenda: **Final action:**
Title: Minutes for the Public Works Committee meeting on April 4, 2017
Sponsors:
Indexes:
Code sections:
Attachments: [Minutes](#)

Date	Ver.	Action By	Action	Result
------	------	-----------	--------	--------

Minutes for the Public Works Committee meeting on April 4, 2017



City of Jonesboro

Municipal Center
300 S. Church Street
Jonesboro, AR 72401

Meeting Minutes Public Works Council Committee

Tuesday, April 4, 2017

5:00 PM

Municipal Center

1. Call To Order

2. Roll Call by City Clerk Donna Jackson

Present 5 - Gene Vance;John Street;Mitch Johnson;Darrel Dover and Ann Williams

Absent 2 - Chris Moore and Charles Coleman

3. Approval of minutes

[MIN-17:026](#)

Minutes for the Public Works Committee Meeting on March 7, 2017

Attachments: [Minutes](#)

A motion was made by Councilman Mitch Johnson, seconded by Councilman Gene Vance, that this matter be Passed . The motion PASSED with the following vote.

Aye: 4 - Gene Vance;Mitch Johnson;Darrel Dover and Ann Williams

Absent: 2 - Chris Moore and Charles Coleman

[MIN-17:033](#)

Minutes for the Public Works Committee Special Called Meeting on March 21, 2017

Attachments: [Minutes.pdf](#)

A motion was made by Councilman Mitch Johnson, seconded by Councilwoman Ann Williams, that this matter be Passed . The motion PASSED with the following vote.

Aye: 4 - Gene Vance;Mitch Johnson;Darrel Dover and Ann Williams

Absent: 2 - Chris Moore and Charles Coleman

4. New Business

Resolutions To Be Introduced

[RES-17:027](#)

A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO ENTER INTO AN AGREEMENT WTH FISHER & ARNOLD, INC. TO PERFORM PROFESSIONAL ENGINEERING SERVICES

Sponsors: Engineering

Attachments: [Proposal](#)

City Engineer Craig Light stated this item was discussed on March 21, 2017, and was passed by City Council. City Clerk Donna Jackson explained a resolution for the contract was entered twice into Legistar. The other resolution has already been adopted.

This item was Filed.

RES-17:036

A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO ACCEPT THE LOW BID AND ENTER INTO A CONTRACT WITH GILLIS, INC. FOR THE PATRICK STREET WIDENING - 2017:14

Sponsors: Engineering

Chairman Street noted the city has been working on this for quite a while. Mayor Perrin explained the contract is for \$127,860 and the company has 60 days to get the work done. CWL got in quickly and moved the lines. If the committee will go ahead and forward this on to tonight's Council meeting due to the good weather conditions, they can go ahead and get the bond work started. Councilman Dover asked if they would like to walk it on the Council meeting. Mayor Perrin answered yes. The quicker they can get it done during the nice weather, the better off the city will be.

A motion was made by Councilman Darrel Dover, seconded by Councilman Mitch Johnson, that this matter be Recommended to Council . The motion PASSED with the following vote.

Aye: 4 - Gene Vance;Mitch Johnson;Darrel Dover and Ann Williams

Absent: 2 - Chris Moore and Charles Coleman

5. Pending Items

6. Other Business

7. Public Comments

8. Adjournment

A motion was made by Councilman Mitch Johnson, seconded by Councilman Gene Vance, that this meeting be Adjourned . The motion PASSED with the following vote.

Aye: 4 - Gene Vance;Mitch Johnson;Darrel Dover and Ann Williams

Absent: 2 - Chris Moore and Charles Coleman



Legislation Details (With Text)

File #: MIN-17:046 **Version:** 1 **Name:** Minutes for the special called Public Works Committee meeting on April 18, 2017

Type: Minutes **Status:** To Be Introduced

File created: 4/19/2017 **In control:** Public Works Council Committee

On agenda: **Final action:**

Title: Minutes for the special called Public Works Committee meeting on April 18, 2017

Sponsors:

Indexes:

Code sections:

Attachments: [Minutes](#)

Date	Ver.	Action By	Action	Result
------	------	-----------	--------	--------

Minutes for the special called Public Works Committee meeting on April 18, 2017



City of Jonesboro

Municipal Center
300 S. Church Street
Jonesboro, AR 72401

Meeting Minutes Public Works Council Committee

Tuesday, April 18, 2017

5:15 PM

Municipal Center

Special Called Meeting

1. Call To Order

2. Roll Call by City Clerk Donna Jackson

In the absence of Chairman Street, Councilman Dover presided over the meeting.

Present 6 - Gene Vance;Chris Moore;Mitch Johnson;Darrel Dover;Charles Coleman and Ann Williams

Absent 1 - John Street

3. New Business

[ORD-17:017](#)

AN ORDINANCE TO WAIVE BIDS AND PURCHASE BRIDGE PANELS FOR THE REPLACEMENT OF WEST COLLEGE BRIDGE BY THE STREET DEPARTMENT, AND DECLARING AN EMERGENCY

Sponsors: Streets

Councilman Dover noted this ordinance was brought up at the Finance meeting and there were questions about the bid process and which committee it should go through before being presented to Council. City Engineer Craig Light explained part of the problem was with the title in that it wasn't waiving the bidding. That has been corrected. They did their annual advertisement for bids for pipes and bridge parts earlier this year. They did not receive any bids for the bridge deck. They contacted a few manufacturers and ended up getting proposals for two. But, only one met all the requirements and provided the bolts, everything that was needed to construct the bridge. That was the only bid they were able to obtain. The other company was unwilling to provide a full submittal. They need to get the bridge panels ordered so they can get the bridge replaced.

Councilman Dover asked if they had all the proper paper in regards to the bid sheets. He then stated bid sheets wouldn't be needed if waiving the bids. City Attorney Carol Duncan explained that's why they did it as waiving competitive bidding. Even though it was bid in the bid packets in January, no one submitted a response. Since they reached out to people, they didn't really go through the bidding process on the second go around. As far as they know, it's a sole source.

A motion was made by Councilman Mitch Johnson, seconded by Councilman Gene Vance, that this matter be Recommended to Council . The motion PASSED with the following vote.

Aye: 5 - Gene Vance;Chris Moore;Mitch Johnson;Charles Coleman and Ann Williams

Absent: 1 - John Street

4. Adjournment

A motion was made by Councilman Mitch Johnson, seconded by Councilman Gene Vance, that this meeting be Adjourned . The motion PASSED with the following vote.

Aye: 5 - Gene Vance;Chris Moore;Mitch Johnson;Charles Coleman and Ann Williams

Absent: 1 - John Street



Legislation Details (With Text)

File #:	RES-17:042	Version:	1	Name:	Resolution authorizing the Mayor to enter into an agreement with the Arkansas Department of Community Corrections for Community Service Workers
Type:	Resolution	Status:		Status:	To Be Introduced
File created:	4/11/2017	In control:		In control:	Public Works Council Committee
On agenda:		Final action:		Final action:	
Title:	RESOLUTION BY THE CITY COUNCIL OF THE CITY OF JONESBORO, ARKANSAS AUTHORIZING THE MAYOR TO ENTER INTO AN AGREEMENT WITH THE ARKANSAS DEPARTMENT OF COMMUNITY CORRECTIONS FOR COMMUNITY SERVICE WORKERS				
Sponsors:	Sanitation				
Indexes:	Contract				
Code sections:					
Attachments:	Copy of Community Service Worksite Agreement.pdf				

Date	Ver.	Action By	Action	Result
------	------	-----------	--------	--------

RESOLUTION BY THE CITY COUNCIL OF THE CITY OF JONESBORO, ARKANSAS AUTHORIZING THE MAYOR TO ENTER INTO AN AGREEMENT WITH THE ARKANSAS DEPARTMENT OF COMMUNITY CORRECTIONS FOR COMMUNITY SERVICE WORKERS

WHEREAS, the City of Jonesboro, Arkansas and the Arkansas Department of Corrections desire to enter into an agreement wherein the Arkansas Department of Corrections will provide offenders to do community service work for the City of Jonesboro; and

WHEREAS, said agreement is attached hereto and the terms set out therein;

NOW THEREFORE BE IT RESOLVED BY THE CITY COUNCIL FOR THE CITY OF JONESBORO, ARKANSAS THAT:

Section 1: The City of Jonesboro approves the Agreement with the Arkansas Department of Community Corrections to supply offenders to do community service work for the City of Jonesboro. That the terms of the Agreement are set out in the attachment.

Section 2: The Mayor, Harold Perrin, and City Clerk, Donna Jackson, are hereby authorized by the City Council for the City of Jonesboro to execute all documents necessary to effectuate this agreement.

**Arkansas Department of Community Correction
COMMUNITY SERVICE WORKSITE TIMESHEET**

PAROLE

PROBATION

Purpose of the Agreement

This agreement formalizes the responsibilities of the Arkansas Department of Community Correction and the agency receiving Community service work.

Arkansas Department of Community Correction Responsibilities

- 1 Complete a thorough screening of all offenders prior to their referral for placement. Advise the worksite of current offenses, if requested
- 2 Adhere to worksite guidelines and requests when referring to offenders
- 3 Advise the worksite of any special conditions of the case which relate to the proposed service
- 4 Deal with any problems, such as poor performance, attendance, or attitude, in a prompt and affirmative manner
- 5 Conduct a worksite visit, Visit or contact the worksite contact person when appropriate for such reasons as inquiring about offender performance, obtaining offender timesheets, maintaining rapport.
- 6 Inform worksite of known instances of inclusion in new stories

Worksite Responsibilities

The worksite or worksite representative will perform the following

- 1 Designate a specific staff person through whom referral will be made
- 2 Keep all information concerning offenders confidential. Only exception FOIA (freedom of information act)
- 3 Provide descriptions of available work assignments
- 4 Establish a work schedule and assignments, if the offender is accepted by the worksite. If the offender is not accepted, the worksite will advise Career Planning and Placement Specialist.
- 5 Not assign offenders to perform any tasks which pose obvious danger to the offender or others, no allow offenders to drive worksite vehicles without prior permission of the Career Planning and Placement Specialist and appropriate training and license
- 6 Have Offenders sign a waiver of liability if necessary.
- 7 Ensure that proper Emergency medical care is provided if an injury occurs; but it will be the offenders responsibility to pay all medical bills incurred
- 8 Provide the offender with instructions and training required for the job assignment
- 9 Familiarize the offender with the mission and activities of the worksite and give a clear explanation of explanation of expectations and the rules.
- 10 Supervise the actual performance of work by the offender
- 11 Keep accurate records of hours worked by offenders on timesheets provided by the Career Planing and Placement Specialist.
- 12 Inform the Carrer Planning and Placement Specialists of the offender's performance when requested

The undersigned representatives have read and accept the provisions of this agreement.

City of Jonesboro, AR

Agency/Worksite

300 S Church Street

Address

Jonesboro AR 72401

City, State, Zip

870-932-1052

Phone

Fax Number

Worksite email address

Mayor Harold Perrin

Date

Donna Jackson City Clerk

Date

Carreer Planning and Placement Specialist

Date



Legislation Details (With Text)

File #: RES-17:047 **Version:** 1 **Name:** Resolution to amend Resolution 2001:34 and Authorizing the Mayor to Execute a Warranty Deed with a corrected property description

Type: Resolution **Status:** To Be Introduced

File created: 4/19/2017 **In control:** Public Works Council Committee

On agenda: **Final action:**

Title: RESOLUTION OF THE CITY OF JONESBORO, ARKANSAS TO AMEND RESOLUTION 2001:34 AND AUTHORIZING THE MAYOR TO EXECUTE A WARRANTY DEED WITH A CORRECTED PROPERTY DESCRIPTION

Sponsors: Mayor's Office

Indexes:

Code sections:

Attachments: [Highfill Deed.pdf](#)

Date	Ver.	Action By	Action	Result
------	------	-----------	--------	--------

RESOLUTION OF THE CITY OF JONESBORO, ARKANSAS TO AMEND RESOLUTION 2001:34 AND AUTHORIZING THE MAYOR TO EXECUTE A WARRANTY DEED WITH A CORRECTED PROPERTY DESCRIPTION

WHEREAS, the City of Jonesboro, Arkansas approved Resolution 2001:34 and agreed to sell property to Skip Highfill by way of a Warranty Deed for the purchase price of \$2,700;

WHEREAS, during a title search on said property it has come to the attention of the purchasing party that the Warranty Deed executed in 2001 contained a property description which did not match the property description on the survey of the property in question, and;

WHEREAS, a corrected Warranty Deed has been prepared and needs to be executed to correct the previous error;

NOW THEREFORE BE IT RESOLVED BY THE CITY COUNCIL FOR THE CITY OF JONESBORO, ARKANSAS THAT:

1. The City of Jonesboro amends Resolution 2001:34 and approves the Warranty Deed attached hereto, which corrects an error in the property description in the previously approved deed.
2. The Mayor, Harold Perrin and City Clerk, Donna Jackson are hereby authorized by the City Council for the City of Jonesboro to execute all documents necessary to effectuate this agreement.

PASSED AND APPROVED this ____ day of _____, 2017.

HAROLD PERRIN, MAYOR

ATTEST:

DONNA JACKSON, CITY CLERK

This instrument prepared by:
Carol M. Duncan
City Attorney, City of Jonesboro
410 W. Washington Ave.
Jonesboro, AR 72401

WARRANTY DEED

KNOW ALL BY THESE PRESENTS:

THAT the City of Jonesboro, an Arkansas City of the First Class, GRANTOR, by its authorized representative, duly authorized so to act by proper resolution of its City Council, for an in consideration of the sum of Ten and 00/100 Dollars (\$10.00) and other good and valuable consideration, paid by Skip Highfill, GRANTEE, the receipt of which is hereby acknowledged, hereby grants, bargains, sells, and conveys unto the Grantee, and unto his heirs and assigns forever, the following described lands located in the County of Craighead, State of Arkansas:

A part of the Southeast Quarter of the Southeast Quarter of Section 20, Township 14 North, Range 4 East, Craighead County, Arkansas to wit: From the Northeast corner of said Southeast Quarter Southeast Quarter, run South 00°44' West 20 feet to the North right-of-way line of Wilkins Street; then run North 89°16' West along said right-of-way line 210 feet to a point of curvature and the point of beginning; then run along said right-of-way and along a curve to the right, central angle 07°20' and radius 156.7 feet for a distance of 20 feet to a curb; then run along curb line and along a curve to the right, central angle 38°31', radius 25 feet for a distance of 16.5 feet; then run North 00°44' East along a curb 32.1 feet; then leaving said curb run South 89°16' East 24.3 feet; then run South 00°44' West 50 feet to the point of beginning, containing 0.027 acres.

To have and to hold unto the Grantee and unto his heirs and assigns forever, with all tenements, appurtenances and hereditaments thereunto belonging.

And Grantor hereby covenants with the Grantee that it will forever warrant and defend the title to such lands against all lawful claims whatsoever.

IN WITNESS WHEREOF, the name of the Grantor is hereunto affixed by its authorized representative this ____ day of _____, 2017.

City of Jonesboro, Arkansas

By: _____
Harold Perrin, Mayor

Attested by:

Donna Jackson, City Clerk

ACKNOWLEDGEMENT

STATE OF ARKANSAS

CITY OF CRAIGHEAD

BE IT REMEMBERED, that on this ____ day of _____, 2017, came before me, the undersigned, a Notary Public, duly commissioned, qualified and acting, within and for the said County and State, Harold Perrin and Donna Jackson, to me personally well known (or satisfactorily proven to be), who stated that they were the Mayor and the City Clerk of the City of Jonesboro, Arkansas, a corporation, and were duly authorized in their respective capacities to execute the foregoing instrument for and in the name and on behalf of said corporation, and further stated and acknowledged that they had so signed, executed and delivered the foregoing instrument for the consideration, uses and purpose therein mentioned and set forth.

IN TESTIMONY WHEREOF, I have hereunto set my hand and official seal this ____ day of _____, 2017.

Notary Public

My commission expires:



Legislation Details (With Text)

File #:	RES-17:050	Version:	1	Name:	Resolution to accept a permanent drainage easement from Mark Webb for the purpose of making drainage improvements
Type:	Resolution	Status:			To Be Introduced
File created:	4/21/2017	In control:			Public Works Council Committee
On agenda:		Final action:			
Title:	A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO AUTHORIZE THE MAYOR AND CITY CLERK TO ACCEPT A PERMANENT DRAINAGE EASEMENT FROM MARK WEBB (WOODFIELD PROPERTIES, LLC) FOR THE PURPOSE OF MAKING DRAINAGE IMPROVEMENTS				
Sponsors:	Engineering				
Indexes:					
Code sections:					
Attachments:					

Date	Ver.	Action By	Action	Result
------	------	-----------	--------	--------

A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO AUTHORIZE THE MAYOR AND CITY CLERK TO ACCEPT A PERMANENT DRAINAGE EASEMENT FROM MARK WEBB (WOODFIELD PROPERTIES, LLC) FOR THE PURPOSE OF MAKING DRAINAGE IMPROVEMENTS

WHEREAS, the City of Jonesboro, Arkansas desires to accept the following described easement for the purpose of making drainage improvements:

A part of Woodfield Properties, LLC Replat of Lots 1 & 2 of Sach's Subdivision of Block 12 of Nisbetts Second Addition; Section 13, Township 14, Range 03 being more particularly described as follows: A 15' x 100' easement along north property lines starting on the NE corner of property also known as 301 Chestnut Street, hence West 100', South 15', East 100', North 15' to the point of beginning proper.

NOW THEREFORE BE IT RESOLVED BY THE CITY COUNCIL FOR THE CITY OF JONESBORO, ARKANSAS THAT:

Section 1: The Mayor and City Clerk are hereby authorized by the City Council for the City of Jonesboro, Arkansas to accept the easement described above.



Legislation Details (With Text)

File #:	RES-17:051	Version:	1	Name:	Agreement with Union Pacific Railroad Company to improve existing public pedestrian crossing
Type:	Resolution	Status:		Status:	To Be Introduced
File created:	4/24/2017	In control:		In control:	Public Works Council Committee
On agenda:		Final action:			
Title:	A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO ENTER INTO AN AGREEMENT WITH UNION PACIFIC RAILROAD COMPANY TO IMPROVE EXISITING PUBLIC PEDESTRIAN CROSSING				
Sponsors:	Engineering				
Indexes:	Contract				
Code sections:					
Attachments:	Exhibit A Exhibit B Jonesboro Ped Agreement				

Date	Ver.	Action By	Action	Result
------	------	-----------	--------	--------

A RESOLUTION TO THE CITY OF JONESBORO, ARKANSAS TO ENTER INTO AN AGREEMENT WITH UNION PACIFIC RAILROAD COMPANY TO IMPROVE EXISITING PUBLIC PEDESTRIAN CROSSING

WHEREAS, the City of Jonesboro has desires to enter into an agreement with Union Pacific Railroad Company to improve the existing Public Pedestrian crossing, at grade along, over and across Railroad's track and right of way at Railroad Mile Post 238.44, on Railroad's Wynne Subdivision DOT No. 440571 in City of Jonesboro.

WHEREAS, funding for the execution of the contract shall come from the Capital Improvement budget and compensation shall be paid in accordance with the attached agreement.

NOW THEREFORE BE IT RESOLVED BY THE CITY COUNCIL FOR THE CITY OF JONESBORO, ARKANSAS THAT:

Section 1: That the City of Jonesboro shall enter into an agreement with Union Pacific Railroad Company to improve the existing Public Pedestrian crossing at the location described in the attached agreement.

Section 2: That funding for the execution of the contract shall come from the Capital Improvement budget and compensation shall be paid in accordance with the attached agreement.

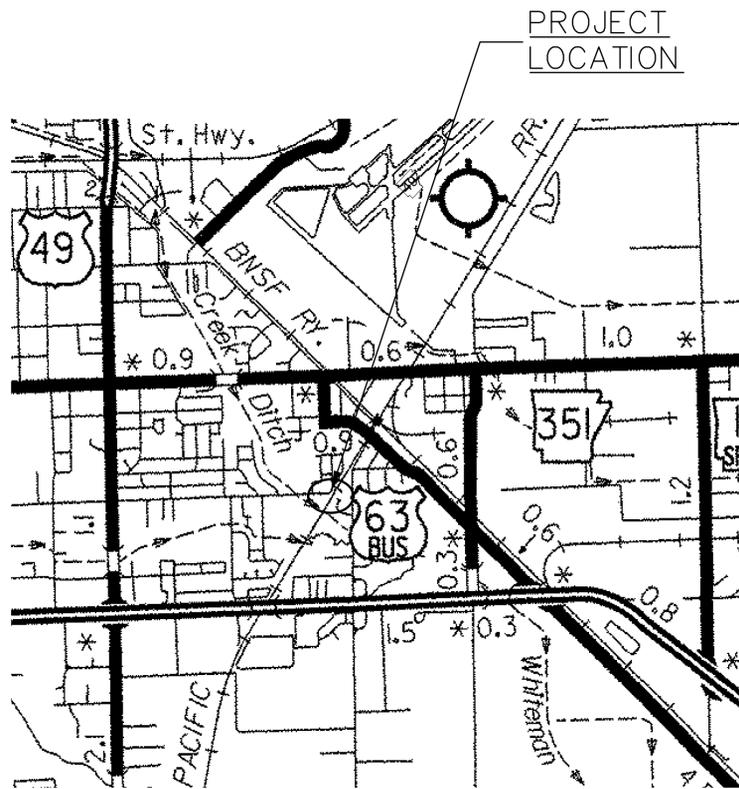
Section 3: The Mayor and the City Clerk are hereby authorized by the City Council for the City of Jonesboro to execute all documents necessary to effectuate this agreement.

CITY OF JONESBORO CONSTRUCTION PLANS

RACE ST. SIDEWALK & RR PED. CROSSING
(JONESBORO) (TAP-15) (S)

CRAIGHEAD COUNTY

JOB 100857

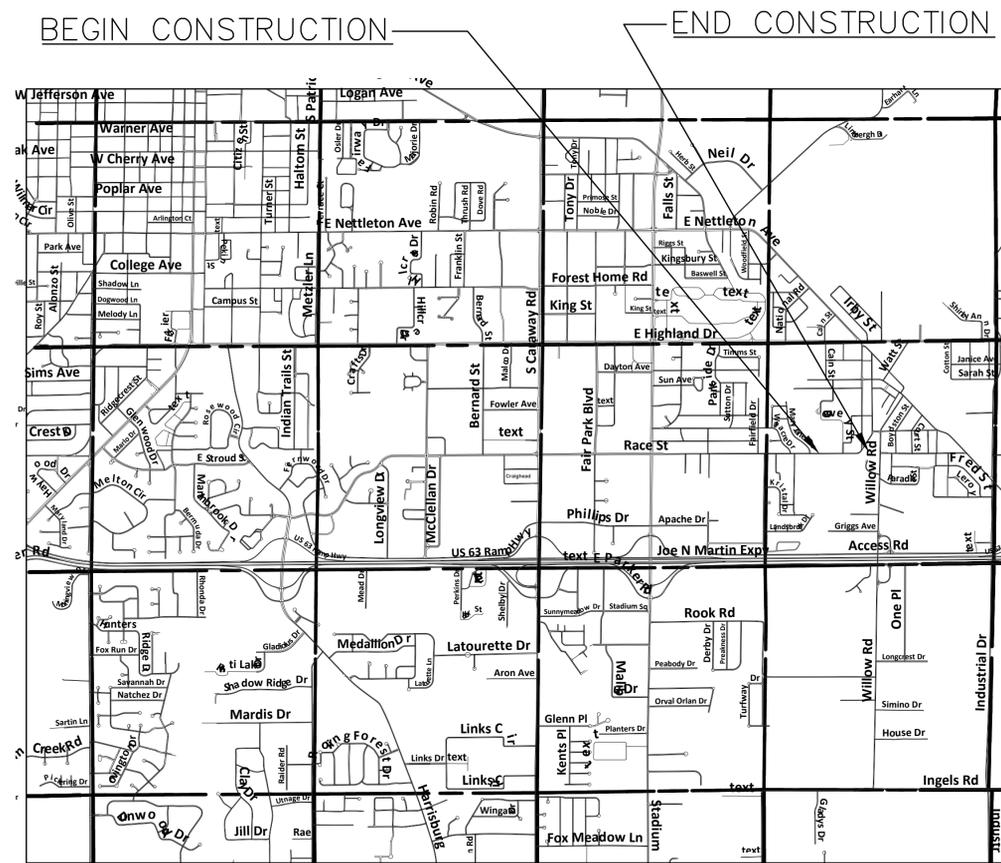


VICINITY MAP

BEGIN-POINT OF PROJECT
LAT. 35°48'49" N
LONG. 90°39'18" W

MID-POINT OF PROJECT
LAT. 35°48'49" N
LONG. 90°39'14" W

END-POINT OF PROJECT
LAT. 35°48'09" N
LONG. 90°40'07" W



R3E R4E

CITY OF JONESBORO

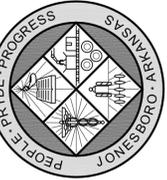
NOT TO SCALE



AHTD DISTRICT 10



Know what's below.
Call before you dig.



Engineering Department
Phone: (870) 832-2438
P.O. Box 1945
307 Vine Street
Jonesboro, AR 72401
Email: engineer@jonesboro.org

City of
Jonesboro
ARKANSAS

RACE ST. SIDEWALK AND RR PED. CROSSING



REVISIONS		
DATE	BY	DESCRIPTION

DRAWING INFO.	
DRAWN BY:	MEMORRIS
DATE:	8/16/2016
SCALE:	1" = 2000'
JOB NO.:	100857
CAD NO.:	

COVER

SHEET NUMBER:
1 of **23**

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		2	23
						JOB NO.	100857	

② INDEX OF SHEETS, GOVN. SPECS & GEN. NOTES

GENERAL NOTES

- GRADE LINE DENOTES FINISHED GRADE OF SIDEWALKS WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U.S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATION.
- ALL PARTS OF THIS DESIGN & INSTALLATION SHALL BE IN ACCORDANCE WITH AASHTO, THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARDS AND DETAILS, ADA STANDARDS FOR ACCESSIBLE DESIGN, AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITIONS.
- CONTRACTOR SHALL NOTIFY ALL EXISTING UTILITY OWNERS BEFORE BEGINNING WORK ON THIS PROJECT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITY SERVICES LINE CROSSED OR EXPOSED BY CONSTRUCTION OPERATIONS. WHERE EXISTING UTILITIES OR SERVICE LINES ARE CUT, BROKEN OR DAMAGED, THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE UTILITY IMMEDIATELY TO COORDINATE THE REPLACEMENT OR REPAIR OF THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER , AT THE CONTRACTORS'S EXPENSE.
- THE CONTRACTOR SHALL PROVIDE THE DIVERSION OF PEDESTRIANS AND VEHICLE DURING THE PROGRESS OF WORK IN A MANNER SATISFACTORY TO THE CONSTRUCTION ENGINEER. NO SEPARATE PAY ITEM.
- THE CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS AND SIDEWALKS ADJACENT TO PROJECT FREE OF MUD AND DEBRIS.
- ALL MODIFICATIONS TO THE CONSTRUCTION PLANS SHALL MEET THE LATEST VERSION OF ADA STANDARDS FOR ACCESSIBLE DESIGN AND BE APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT ARKANSAS ONE CALL (800)482-8998) PRIOR TO THE START OF ANY WORK.
- CONTRACTOR SHALL BE RESPONSIBLE TO ADJUST WATER VALVE TO GRADE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION STAKING. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- NEITHER THE OWNER NOR THE ENGINEER SHALL BE RESPONSIBLE FOR SUB-SURFACE CONDITIONS. THE CONTRACTOR SHALL MAKE HIS OWN DETERMINATION CONCERNING SUB-SURFACE CONDITIONS.
- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT CONTRACTOR'S OWN EXPENSE.
- IT SHALL BE THE DUTY OF THE CONTRACTOR TO CONTROL AND PROVIDE SAFETY SIGNAGE FOR ALL VEHICULAR TRAFFIC WHEN CONSTRUCTION IMPROVEMENTS IMPAIR NORMAL STREET USAGE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE AMOUNT OF BORROW MATERIAL USED ON THE PROJECT. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINAL SOIL STABILIZATION OF THE PROJECT LIMITS INCLUDING THE ESTABLISHMENT OF VEGETATIVE COVER AND INSTALLATION OF PERMANENT EROSION CONTROL MEASURES AS SPECIFIED.
- CONSTRUCTION LIMITS TO BE CLEARED BY THE CONTRACTOR. ALL REMAINING DEBRIS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT THE CONTRACTORS'S OWN EXPENSE. ALL DEBRIS FORM THE DEMOLISHED SITE SHALL BE REMOVED AND DISCARDED BY THE CONTRACTOR FROM THE SITE EACH DAY.
- ALL CURB AND GUTTER SHALL HAVE A MINIMUM RUNNING SLOPE OF 0.50%.
- USE EXPANSION JOINTS WHERE ANY PERMANENT OBJECTS (STREET LIGHTS, POWER POLES, INLETS, FIRE HYDRANTS, ETC.) ARE LOCATED.
- CONTRACTOR SHALL SAW CUT EXISTING ASPHALT AND/OR CONCRETE TO ACCOMMODATE PROPOSED IMPROVEMENTS.
- ALL SIDEWALKS ARE TO HAVE A MAXIMUM OF 2.0% CROSS SLOPE AND 5.0% RUNNING SLOPE. CURB RAMPS ARE TO HAVE A MAXIMUM OF 2.0% CROSS SLOPE AND RUNNING SLOPE BETWEEN 5.0% AND 8.33%.
- WHEN CONNECTING TO EXISTING SIDEWALK, TIE-INS ARE TO BE LOCATED AT EXISTING EXPANSION JOINT. IN THE EVENT THERE IS NO EXPANSION JOINT LOCATED WHERE PROPOSED CONSTRUCTION IS TO BE LOCATED, PROVIDE AN EXPANSION JOINT AND MATERIAL.

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR CONSTRUCTION, EDITION OF 2003, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER

ERRATA-----ERRATA FOR THE BOOK OF STANDARDS SPECIFICATIONS
JOB 100857-----SIDEWALK DRAIN

TITLE



INDEX OF SHEETS

SHEET NO.	TITLE	DRWG NO.	DATE
1	TITLE SHEET		
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS & GENERAL NOTES		
3	QUANTITY SHEET		
4	TYPICAL SECTION/SPECIAL DETAIL		
5	SURVEY CONTROL		
6	MAINTENANCE OF TRAFFIC		
7-8	SIDEWALK PLAN		
9-11	CROSS SECTIONS		
12	CURBING DETAILS	CG-1	11/29/07
13	DETAILS OF DRIVEWAYS & ISLANDS	DR-1	2/27/14
14	MAILBOX DETAILS	MB-1	11/18/04
15	DETAIL OF DROP INLETS	FCP-9	11/16/01
16	DETAIL OF DROP INLETS (TYPE C)	FPC-9E	8/22/02
17	STANDARD HIGHWAY SIGNS AND SUPPORT ASSEMBLIES	SHS-1	9/12/13
18	U-CHANNEL POST ASSEMBLIES	SHS-2	2/27/14
19	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-1	9/2/15
20	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-2	9/2/15
21	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	TC-3	9/2/15
22	WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS	WR-1	11/10/05
23	WHEELCHAIR RAMPS ALTERATIONS ONLY	WR-2	10/9/03

SUMMARY OF QUANTITIES

Item No.	AHTD Ref	Description	Quantity	Unit
1	202	REMOVAL AND DISPOSAL OF ASPHALT PAVEMENT	18	SQ. YD.
2	202	REMOVAL AND DISPOSAL OF CONCRETE PAVEMENT	15	SQ. YD.
3	202	REMOVAL AND DISPOSAL OF CURB & GUTTER	100	LIN. FT
4	202	REMOVAL AND DISPOSAL OF CONCRETE WALK	550	EACH
5	210	BORROW	500	CU. YD.
6	303	AGGR. BASE COURSE (CLASS 7)	100	TON
7	505	PORTLAND CEMENT CONCRETE DRIVEWAY	40	SQ. YD.
8	601	MOBILIZATION	1	LUMP SUM
9	603	MAINTENANCE OF TRAFFIC	1	LUMP SUM
10	604	SIGNS	48	SQ. FT.
11	604	TRAFFIC DRUMS	20	EACH
14	624	SOLID SOD	600	SQ. YD.
15	633	CONCRETE WALKS	545	SQ. YD.
17	634	CC CURB & GUTTER - A(1'-6")	100	LIN. FT
18	640	MODIFY DROP INLET	2	EACH
21	SP	SIDEWALK DRAIN	6	EACH

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		3	23
				JOB NO.		100857		

② QUANTITY SHEET



ADVANCED WARNING SIGNS & DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	TOTAL QUANTITY REQUIRED	TOTAL SIGNS REQUIRED (SQ. FT.)	TRAFFIC DRUMS
G20-2	END ROAD WORK	48" x 24"	2	16	
W20-1	ROAD WORK AHEAD	48" x 48"	2	32	
	TRAFFIC DRUMS				20
TOTAL				48	20

NOTE: ADDITIONAL SIGNS AND DEVICES MAY BE REQUIRED FOR TEMPORARY LANE CLOSURES AND FLAGGING OPERATIONS IN ACCORDANCE WITH STANDARD DRAWINGS AND WILL NOT BE PAID FOR DIRECTLY BUT BE CONSIDERED INCLUDED IN VARIOUS PAY ITEMS.

REMOVAL AND DISPOSAL OF ITEMS

STATION	STATION	PC CONCRETE DRIVEWAY SQ. YD.	ASPHALT PAVEMENT SQ. YD.	CONCRETE WALKS SQ. YD.	CURB & GUTTER LIN. FT
41+13	41+76			35	
41+76	44+38			144	
44+38	45+67			72	
45+67	45+76	15			
45+76	46+25			27	
46+25	46+36		18		
46+36	48+08			96	
48+08	49+28			67	
49+86	50+83			109	
41+13	50+83				100
TOTAL		15	18	550	100

PC CONCRETE DRIVEWAY

STATION	DESCRIPTION	PC CONCRETE DRIVEWAY SQ. YD.
45+71	DRIVEWAY LEFT	17
46+31	DRIVEWAY LEFT	21
TOTAL		38
USE:		40

CONCRETE WALKS

STATION	STATION	DESCRIPTION	CONCRETE WALKS SQ. YD.	SIDEWALK DRAIN* EACH
41+13	41+75	CONCRETE WALK RT CL	35	
41+80	44+37	CONCRETE WALK RT CL	143	
44+38		SIDEWALK DRAIN		1
44+39	45+67	CONCRETE WALK RT CL	72	
45+76	45+95	CONCRETE WALK RT CL	11	
45+96		SIDEWALK DRAIN		1
45+96	46+25	CONCRETE WALK RT CL	17	
46+36	46+46	CONCRETE WALK RT CL	6	
46+47		SIDEWALK DRAIN		1
46+48	48+06	CONCRETE WALK RT CL	88	
48+11	49+51	CONCRETE WALK RT CL	78	
49+60	50+83	CONCRETE WALK RT CL	69	
TOTAL			519	3
USE:			545	6

*TO BE USED IF AND WHERE AS DIRECTED BY ENGINEER. QUANTITIES ESTIMATED

BASE AND SURFACING

STATION	DESCRIPTION	AGGREGATE BASE COURSE (TON)
ENTIRE PROJECT	RIGHT OF CENTERLINE	100
TOTAL		100

ENTIRE PROJECT IS TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

CONCRETE COMBINATION CURB AND GUTTER

STATION	DESCRIPTION	TYPE A LIN FT
	LEFT CENTERLINE	95
TOTAL		95
USE :		100

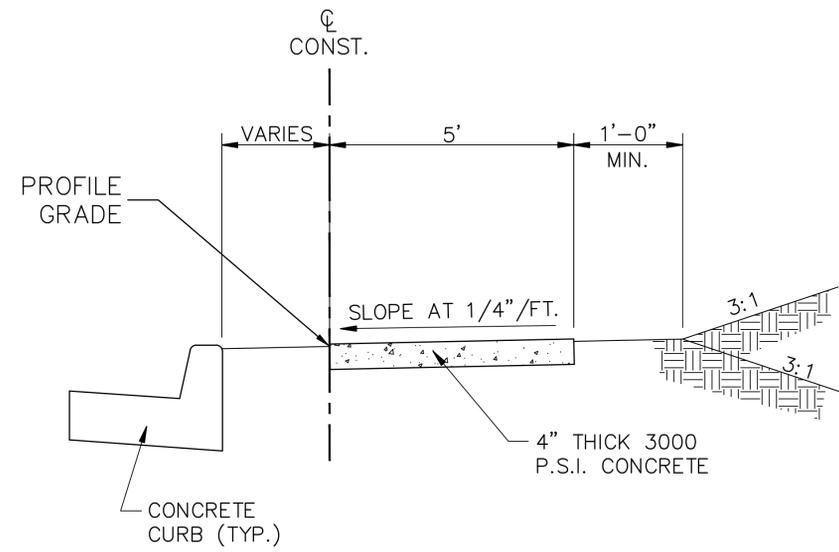
ENTIRE PROJECT IS TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

STRUCTURES

STATION	DESCRIPTION	MODIFY INLET EACH
41+77	MODIFY D.I. ON RT.	1
48+08	MODIFY D.I. ON RT.	1
TOTAL		2

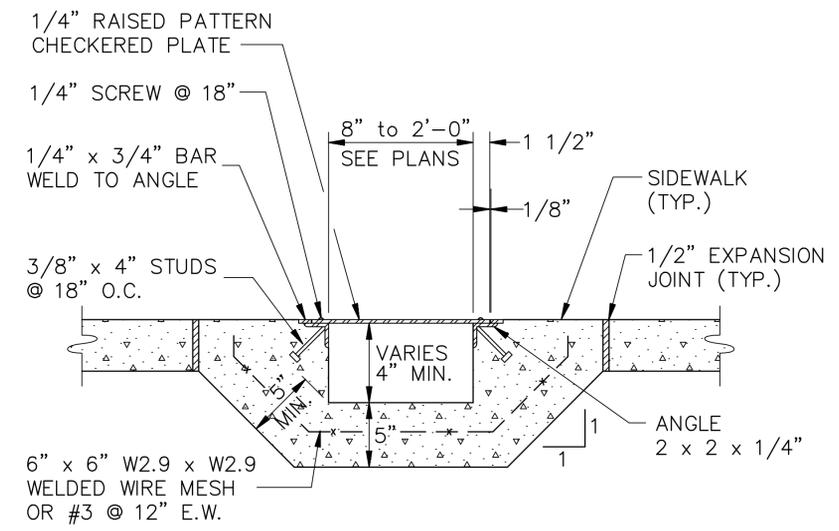
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		4	23
				JOB NO.		100857		

② TYPICAL SECTION & SPECIAL DETAIL



TYPICAL SECTION — SIDEWALK
N.T.S.

* MINIMUM WIDTH FOR SIDEWALK ADJACENT TO CURB IS 5' FOR CITY STREETS AND 6' FOR STATE OR U.S. HIGHWAYS.



SIDEWALK DRAIN
N.T.S. (SECTION)

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		5	23
				JOB NO.		100857		

② SURVEY CONTROL



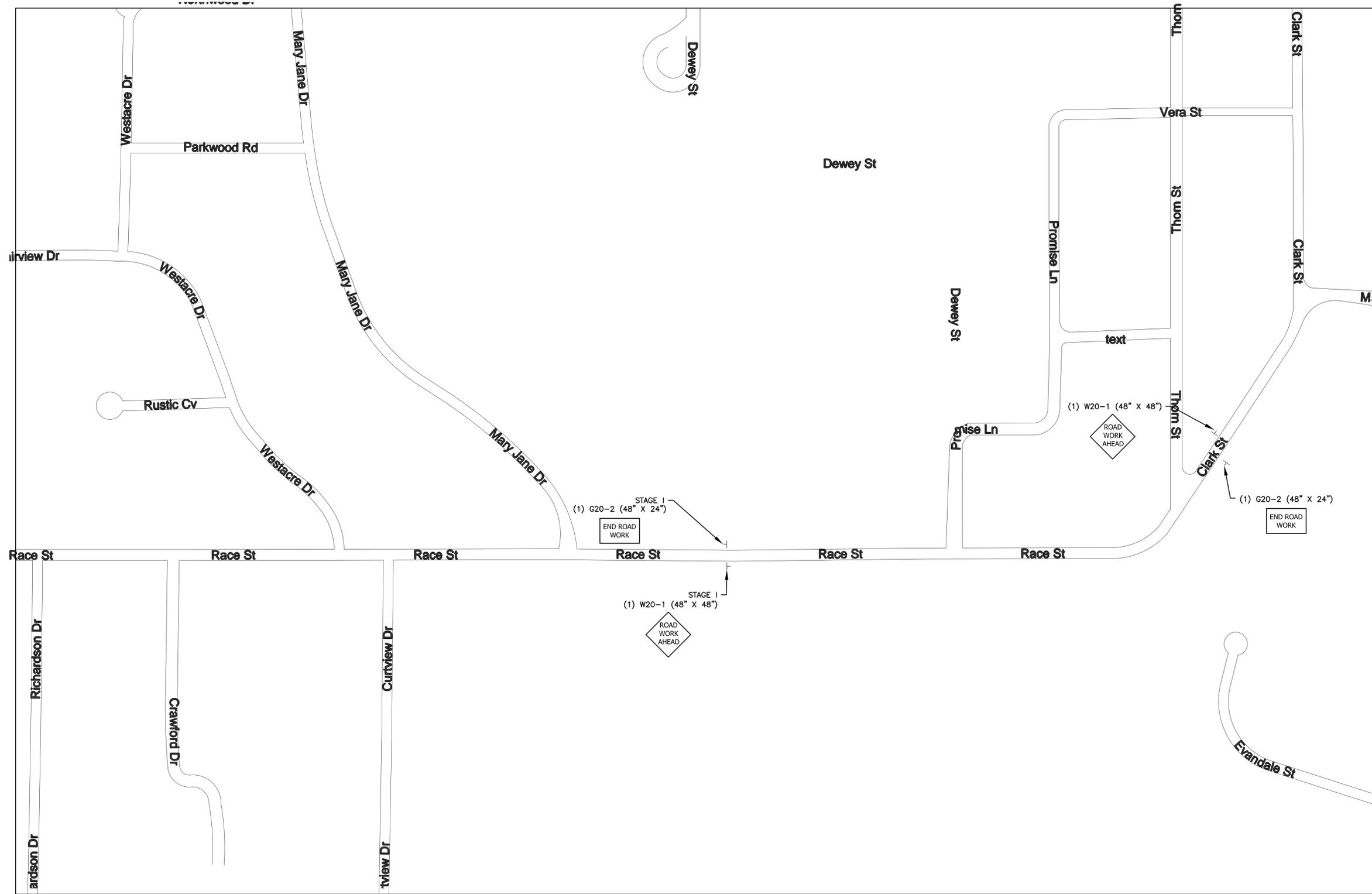
Number	Type	Length	Radius	Direction	Start Station	End Station	Delta angle	PI Station	Start Point		End Point	
									Northing	Easting	Northing	Easting
1	Line	603.53'		N89° 37' 01.03"E	41+00.00'	47+03.53'			541572.6618	1711057.099	541576.6966	1711660.616
2	Line	100.08'		N87° 00' 10.90"E	47+03.53'	48+03.61'			541576.6966	1711660.616	541581.9292	1711760.562
3	Line	0.00'		S52° 37' 09.21"E	48+03.61'	48+03.61'			541581.9292	1711760.562	541581.9283	1711760.563
4	Line	8.27'		S89° 43' 43.54"E	48+03.61'	48+11.88'			541581.9283	1711760.563	541581.8892	1711768.83
5	Line	3.53'		N88° 47' 16.90"E	48+11.88'	48+15.41'			541581.8892	1711768.83	541581.9637	1711772.355
6	Line	19.69'		N85° 52' 20.31"E	48+15.41'	48+35.10'			541581.9637	1711772.355	541583.381	1711791.994
7	Line	89.70'		N79° 20' 49.99"E	48+35.10'	49+24.80'			541583.381	1711791.994	541599.9635	1711880.152
8	Curve	4.03'	5.00'		49+24.80'	49+28.83'	46.2135 (d)	49+26.94'	541599.9635	1711880.152	541602.1271	1711883.426
9	Line	10.73'		N33° 26' 09.90"E	49+28.83'	49+39.57'			541602.1271	1711883.426	541611.084	1711889.34
10	Line	26.45'		S56° 33' 50.10"E	49+39.57'	49+66.01'			541611.084	1711889.34	541596.5117	1711911.41
11	Line	5.63'		N38° 51' 37.96"E	49+66.01'	49+71.65'			541596.5117	1711911.41	541600.8993	1711914.946
12	Curve	12.83'	15.00'		49+71.65'	49+84.48'	49.0215 (d)	49+78.49'	541600.8993	1711914.946	541606.4777	1711926.071
13	Line	39.43'		N87° 52' 55.53"E	49+84.48'	50+23.92'			541606.4777	1711926.071	541607.935	1711965.479
14	Line	12.01'		N88° 18' 24.35"E	50+23.92'	50+35.93'			541607.935	1711965.479	541608.29	1711977.488
15	Line	29.20'		N88° 03' 19.52"E	50+35.93'	50+65.14'			541608.29	1711977.488	541609.281	1712006.676
16	Line	28.57'		N88° 33' 06.90"E	50+65.14'	50+93.71'			541609.281	1712006.676	541610.003	1712035.237



INSIDE EDGE OF SIDEWALK ALIGNMENT

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		6	23
						JOB NO.	100857	

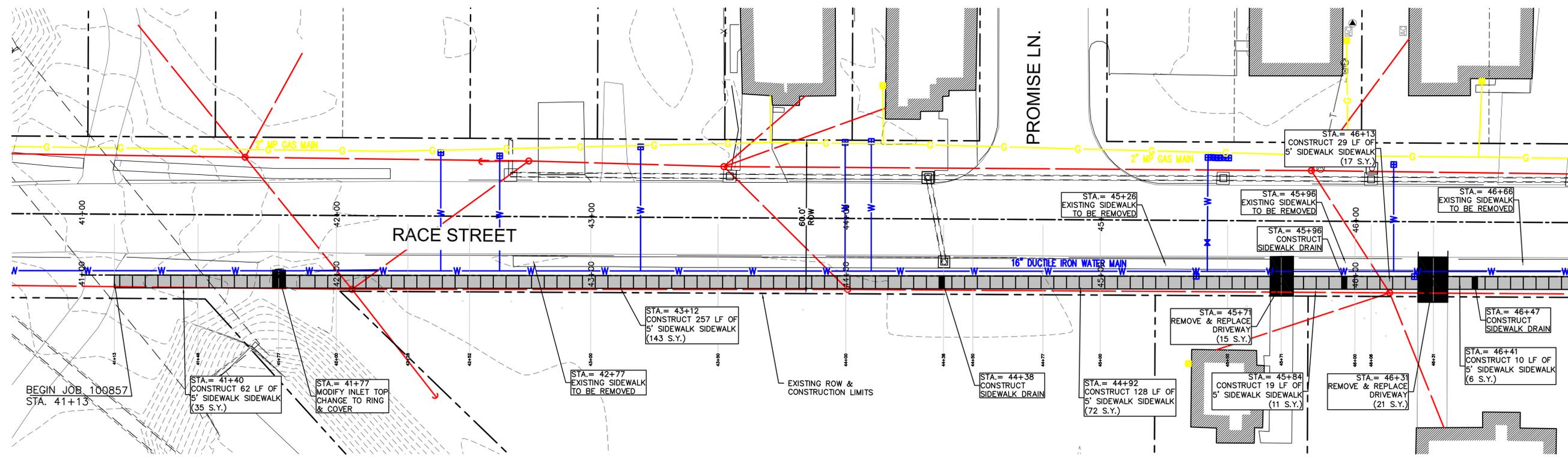
② MAINTENANCE OF TRAFFIC



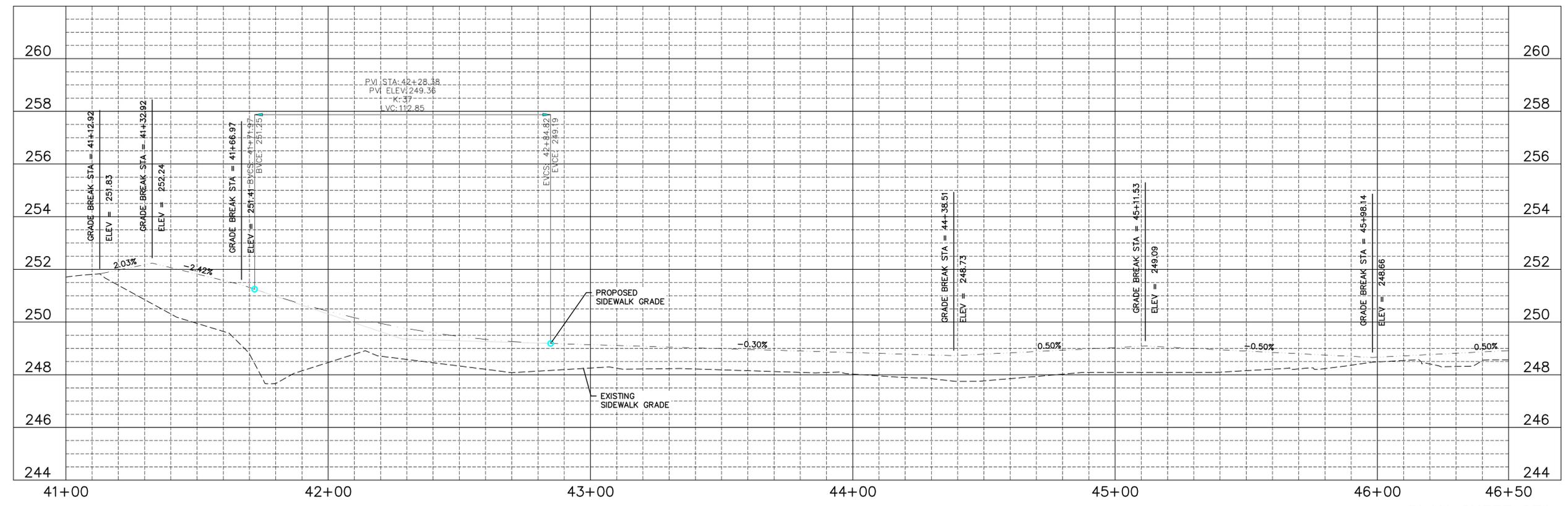
MAINTENANCE OF TRAFFIC

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		7	23
						JOB NO.	100857	

② PLAN SHEETS



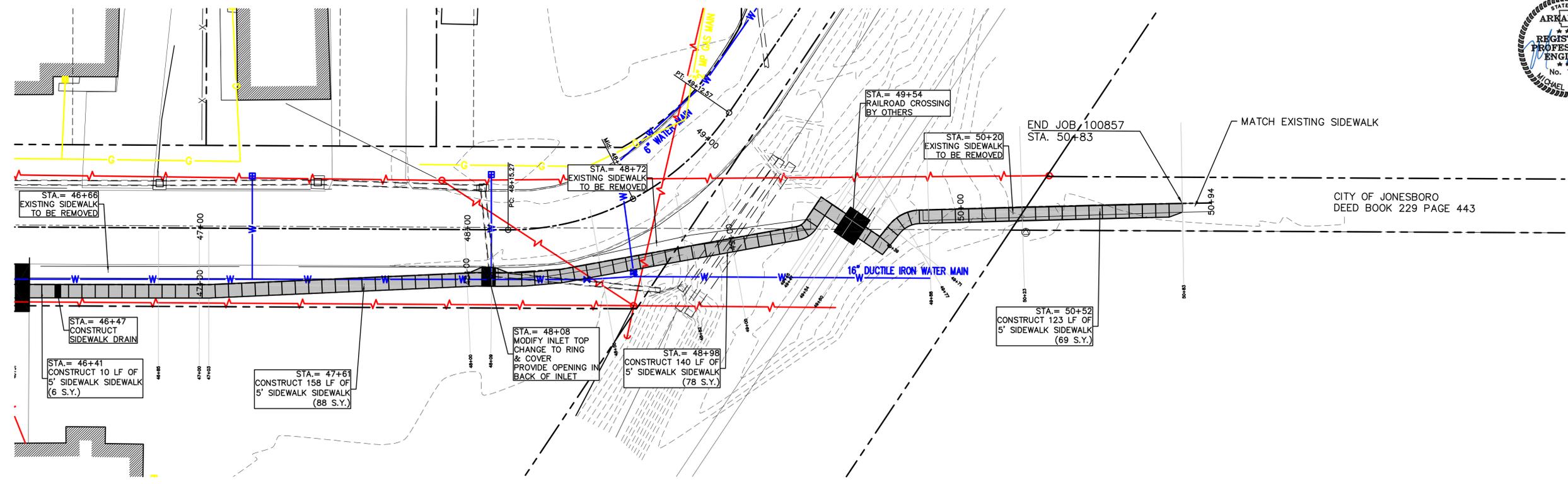
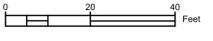
EDGE OF SIDEWALK PROFILE



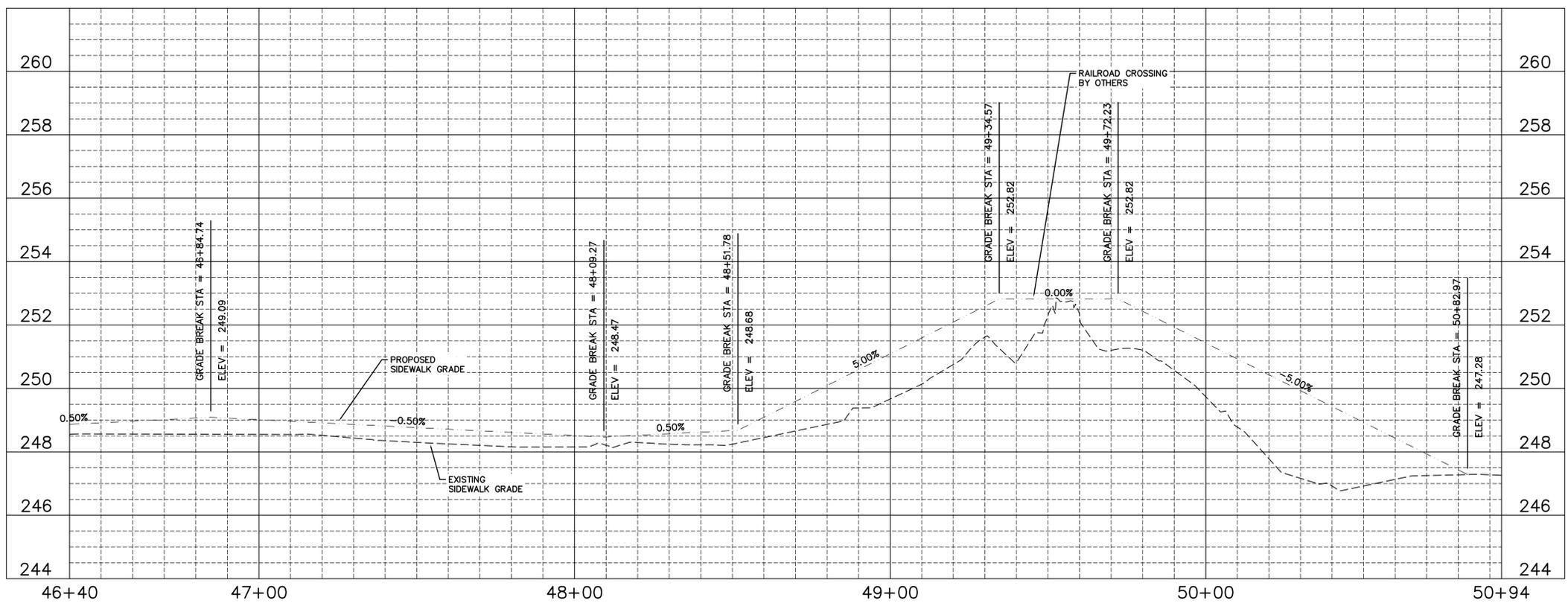
PLAN SHEET STA. 41+13 TO 46+50

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		8	23
						JOB NO.	100857	

② PLAN SHEETS

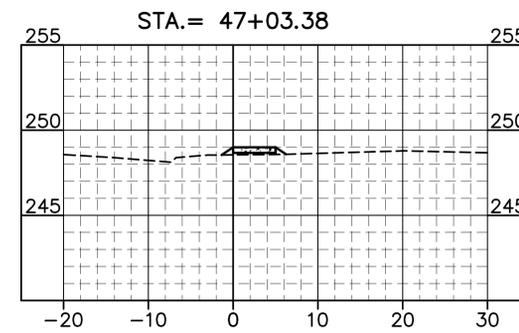
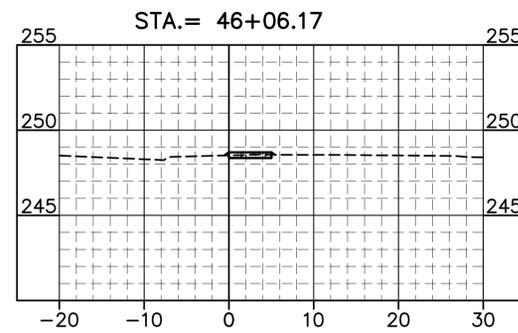
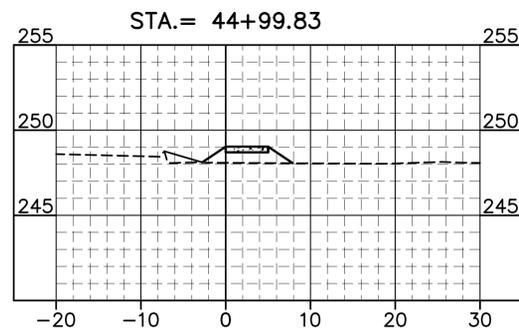
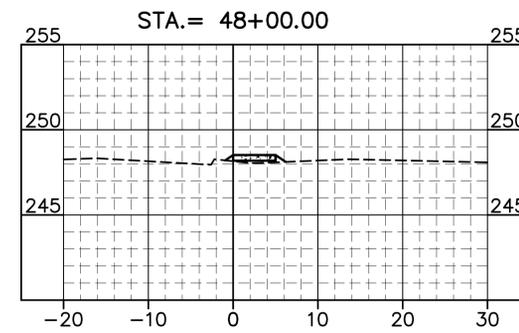
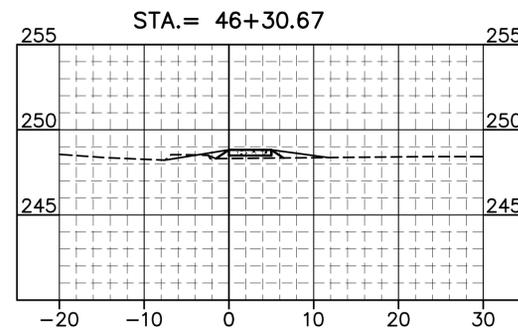
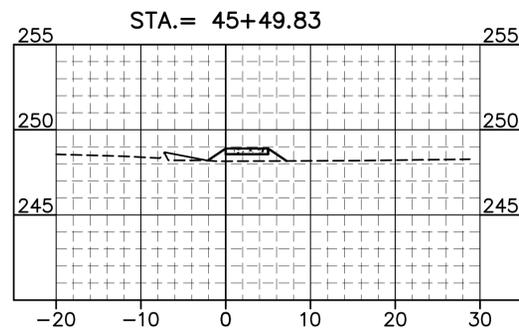
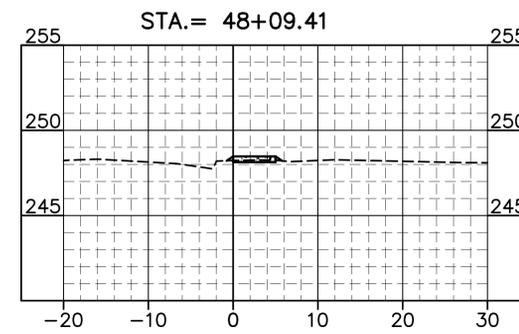
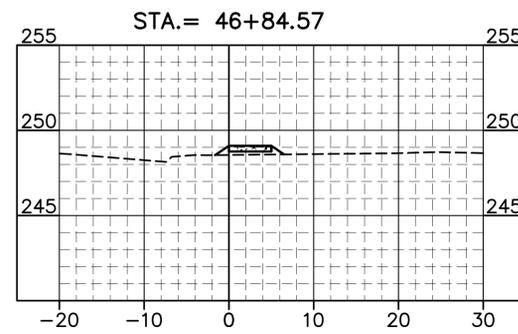
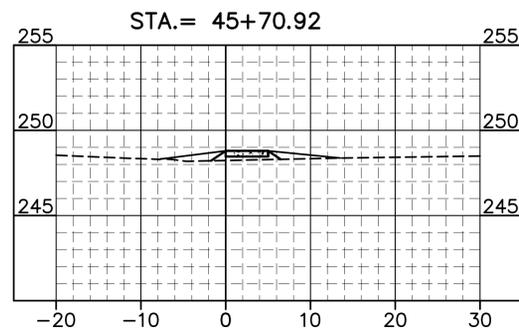
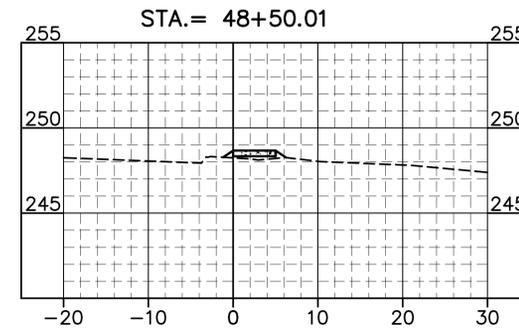
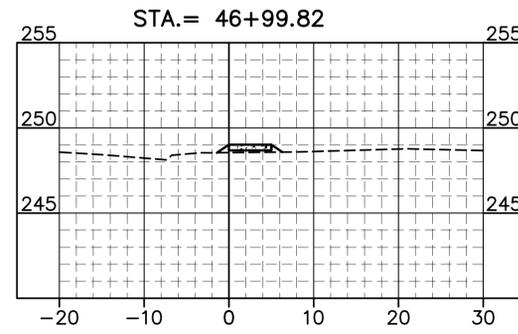
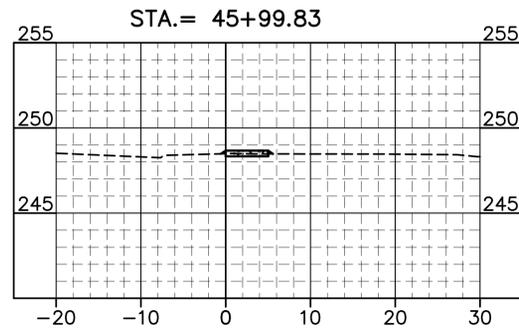


EDGE OF SIDEWALK PROFILE



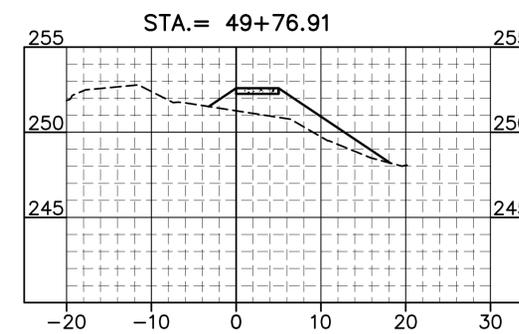
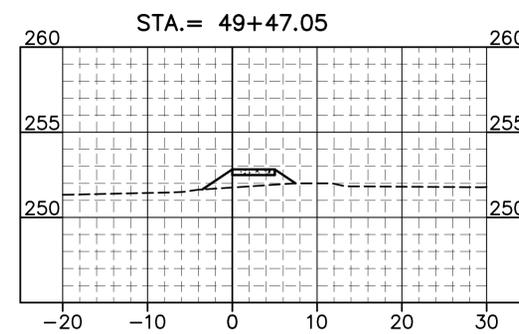
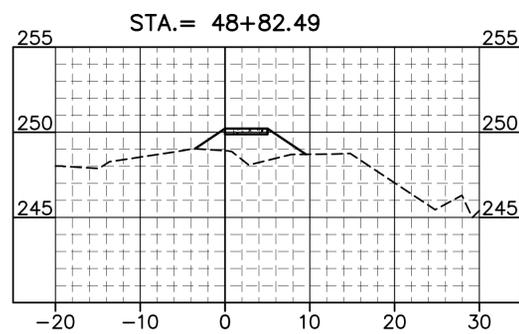
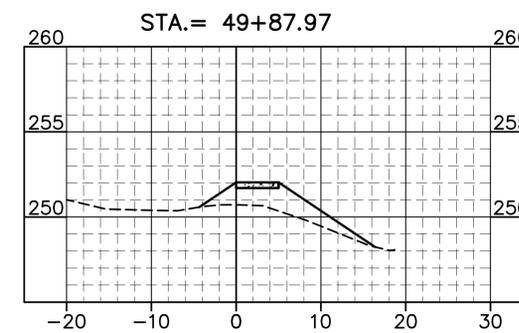
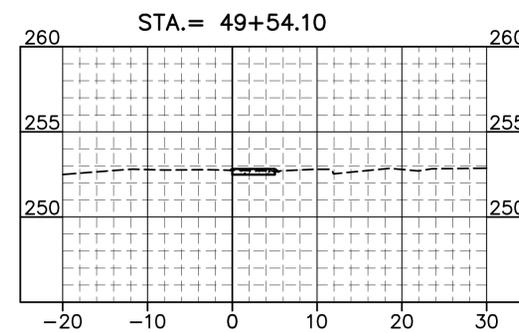
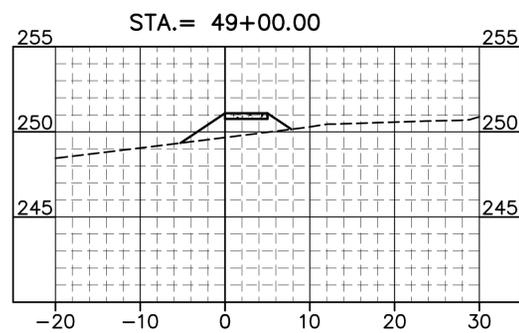
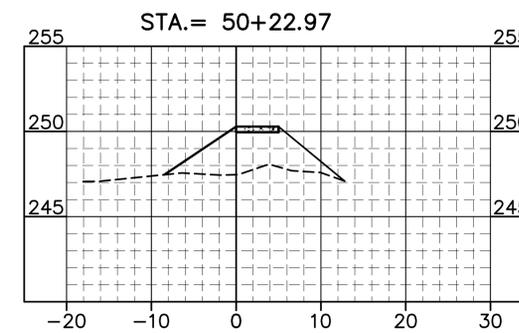
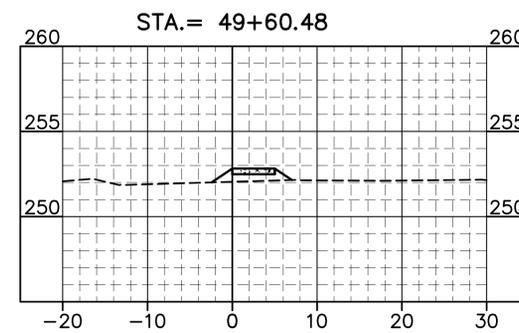
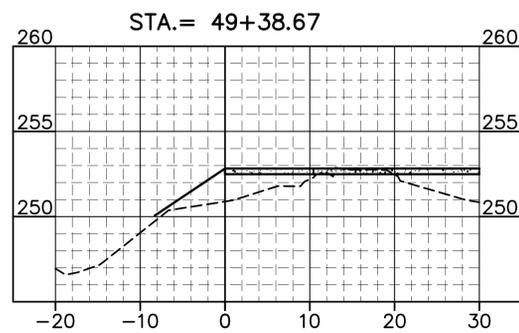
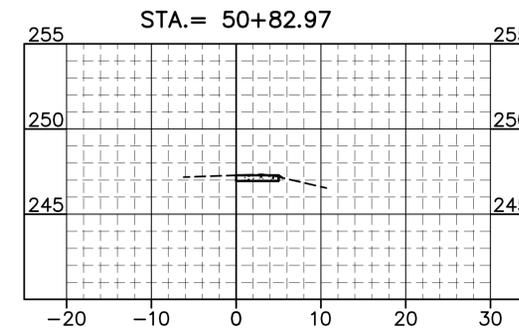
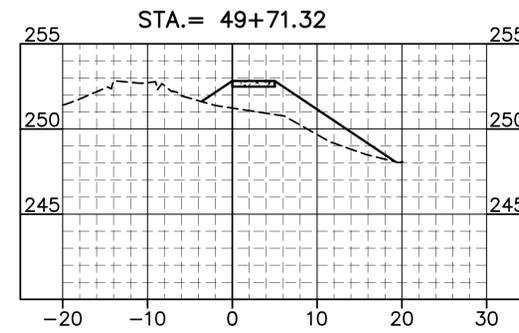
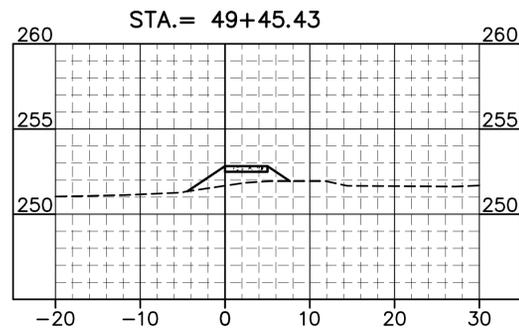
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		10	23
						JOB NO.	100857	

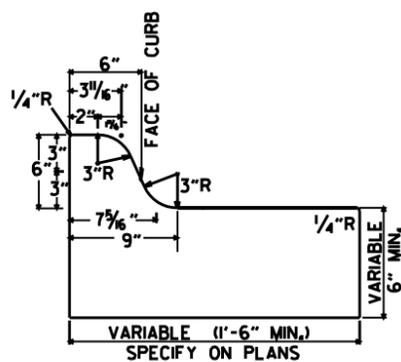
② CROSS SECTIONS



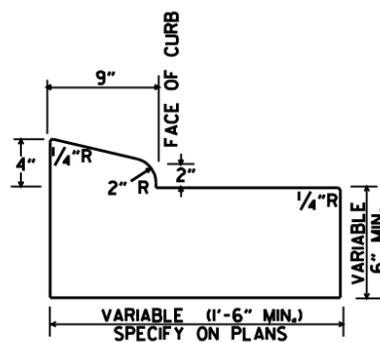
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		11	23
						JOB NO.	100857	

② CROSS SECTIONS

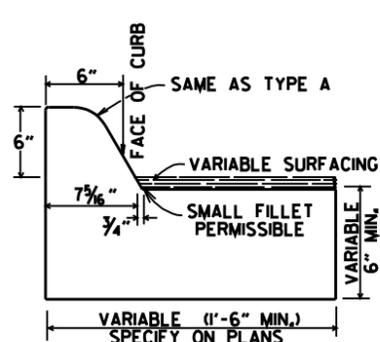




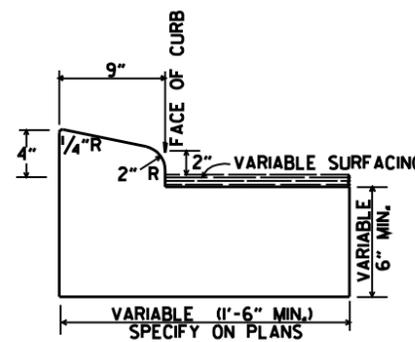
TYPE A



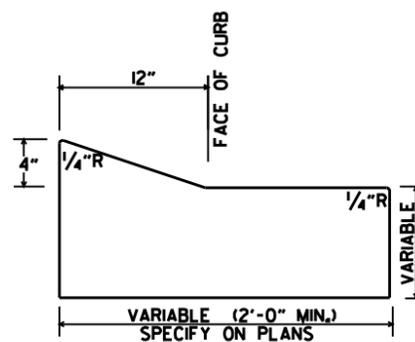
TYPE B-1



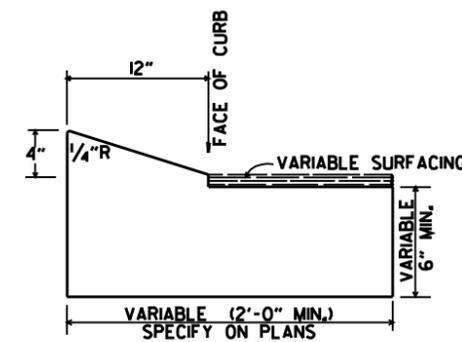
TYPE C



TYPE B-2

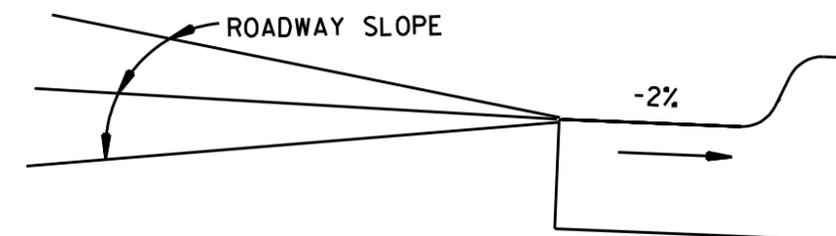


TYPE E-1

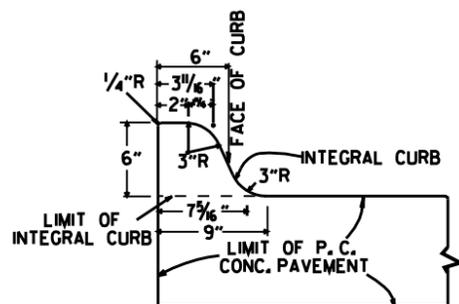


TYPE E-2

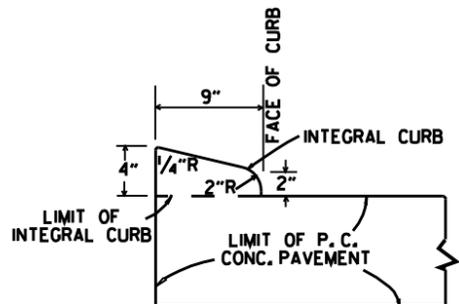
CONCRETE COMBINATION CURB AND GUTTER



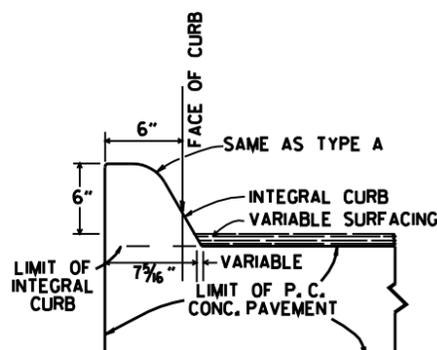
DETAIL OF GUTTER SLOPE
GUTTER SHALL BE CONSTRUCTED ON 2% SLOPE AWAY FROM ROADWAY, REGARDLESS OF ROADWAY SLOPE.



TYPE A

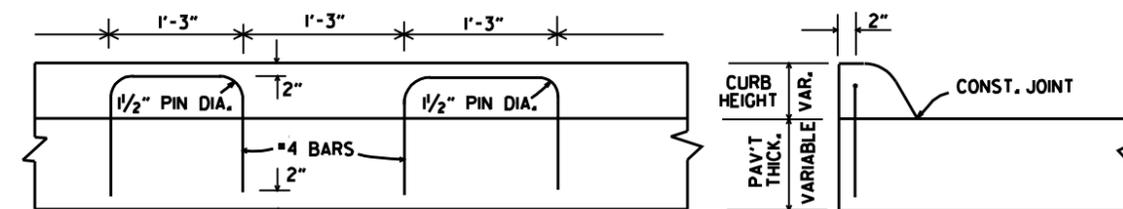


TYPE B



TYPE C

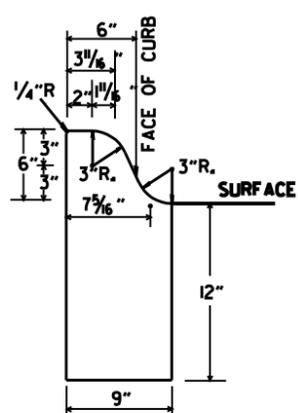
INTEGRAL CURB



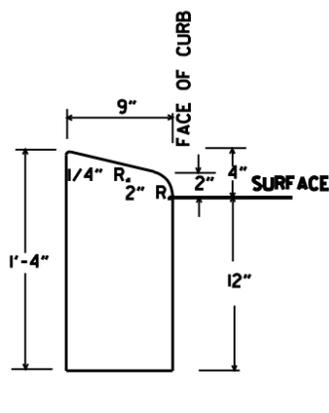
LONGITUDINAL SECTION

ELEVATION

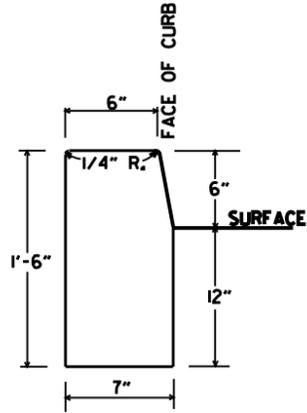
ALTERNATE CONSTRUCTION METHOD FOR INTEGRAL CURB



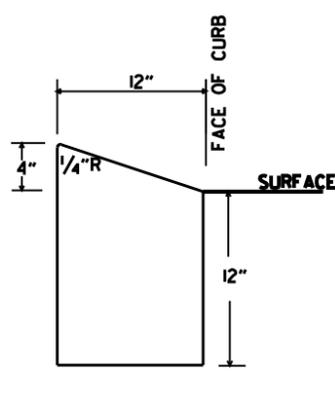
TYPE A



TYPE B

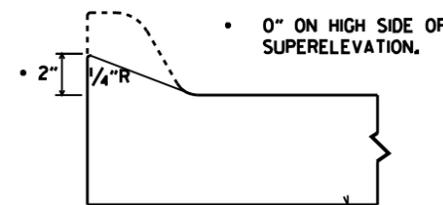


TYPE D



TYPE E

CONCRETE CURB



NOTE: USE MODIFIED CURB AS SPECIFIED ON STD. DR-1. COMPENSATION FOR MODIFIED CURB WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE TYPE OF CURB OR CURB AND GUTTER SPECIFIED.

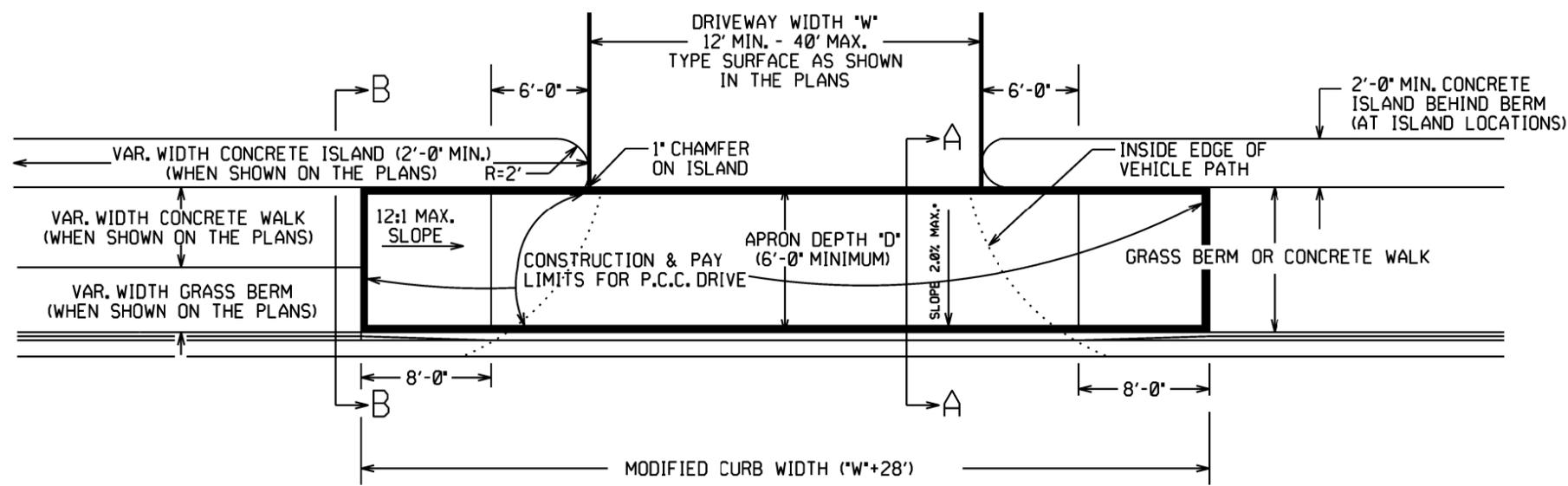
DETAILS OF MODIFIED CURB

DATE	REVISION	DATE FILMED
11-29-07	REVISED GUTTER SLOPE & MODIFIED CURB DETAILS	
11-10-05	ADDED DETAILS OF TYPE E CURBS	
11-16-01	REVISED CONCRETE CURB TYPE B	
11-18-98	REVISED MODIFIED CURB	
6-2-94	ADDED NOTE TO SPECIAL MODIFIED CURB	
8-5-93	CORRECTED GUTTER SLOPE	8-5-93
10-1-92	ADDED DETAILS OF GUTTER SLOPE	10-1-92
5-24-90	ADDED DETAILS OF MODIFIED CURB	5-24-90
11-30-89	VARIABLE DEPTH TYPE A & B 1	11-30-89
7-15-88	REVISED MODIFIED CURB	630-7-15-88
11-1-73	REVISED MODIFIED CURB	500-11-1-73
10-2-72	REVISED AND REDRAWN	512-10-2-72

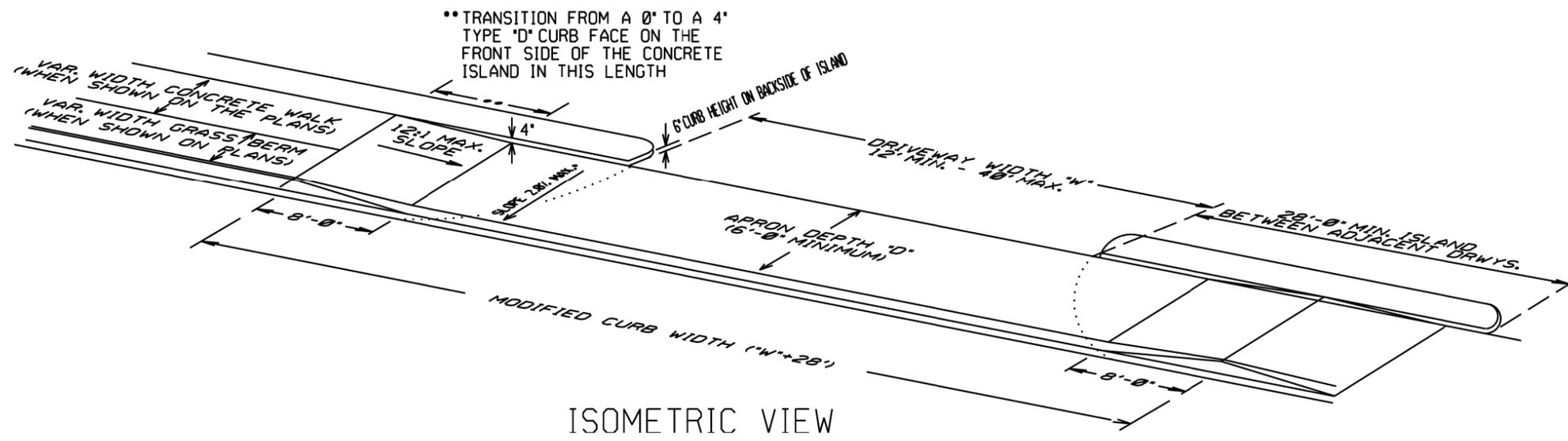
ARKANSAS STATE HIGHWAY COMMISSION

CURBING DETAILS

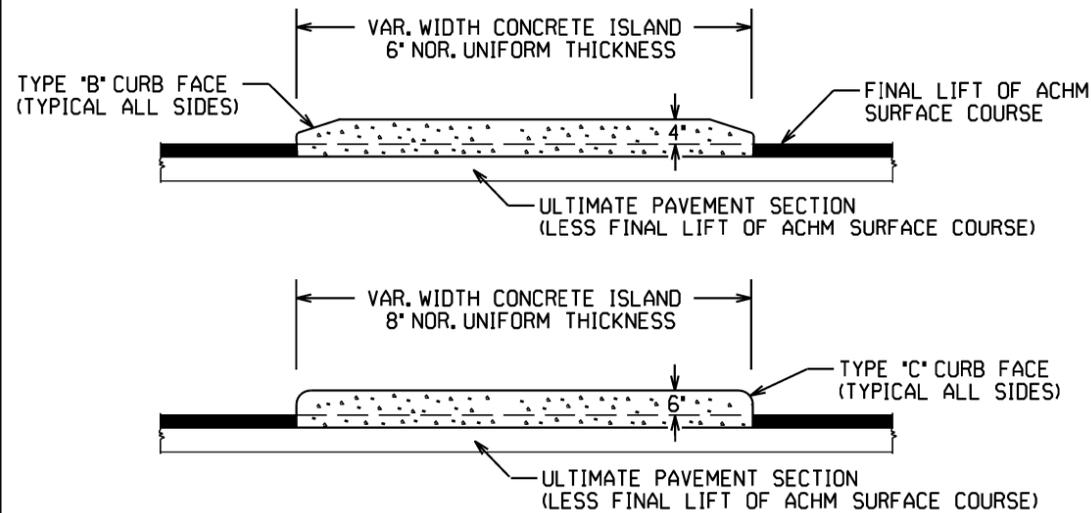
STANDARD DRAWING CG-1



PLAN VIEW

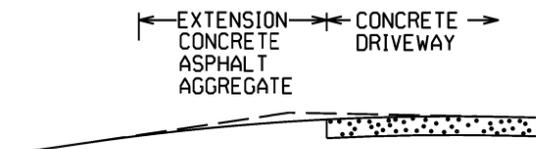


ISOMETRIC VIEW



CURBED ISLANDS FOR CHANNELIZATION

REFER TO PLANS FOR TYPE OF CURB FACE TO BE USED.
NO DIRECT PAYMENT WILL BE MADE FOR THE CURB FACES
SHOWN ON THE ISLAND DETAILS. PAYMENT FOR THE CURB
FACE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE
ITEM "CONCRETE ISLAND".

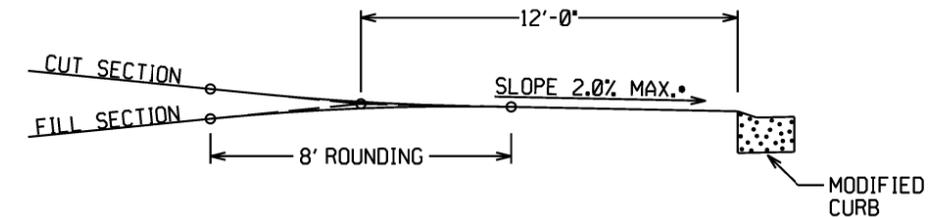


EXTENSION TYPICAL SECTIONS

- 1: CONCRETE - 6" P.C. CONCRETE DRIVEWAY
- 2: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
4" ACHM BINDER COURSE (1") OR
4" ACHM BASE COURSE (1-1/2")
- 3: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
7" AGGREGATE BASE COURSE
- 4: AGGREGATE - 6" AGGREGATE BASE COURSE

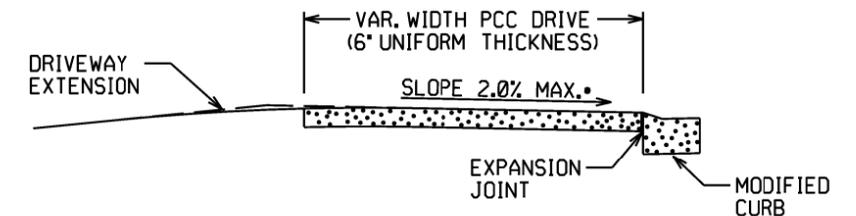
THE TYPE OF EXTENSION SHALL BE AS SHOWN IN THE PLANS.
THE CONTRACTOR MAY, WITH THE APPROVAL OF THE ENGINEER,
SUBSTITUTE A LOWER NUMBERED TYPE OF EXTENSION IN LIEU
OF THE TYPE SPECIFIED IN THE PLANS, BUT AT NO ADDITIONAL
COST TO THE DEPARTMENT.

DRIVEWAY EXTENSION DETAILS

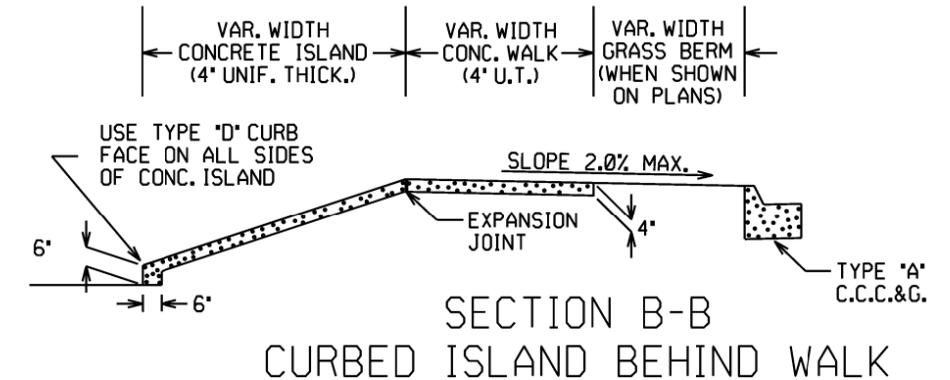


DRIVEWAY VERTICAL ALIGNMENT DETAILS

NOTE: DRIVEWAYS MAY NOT BE SLOPED AWAY
FROM THE ROADWAY UNLESS APPROVED
BY THE ENGINEER.

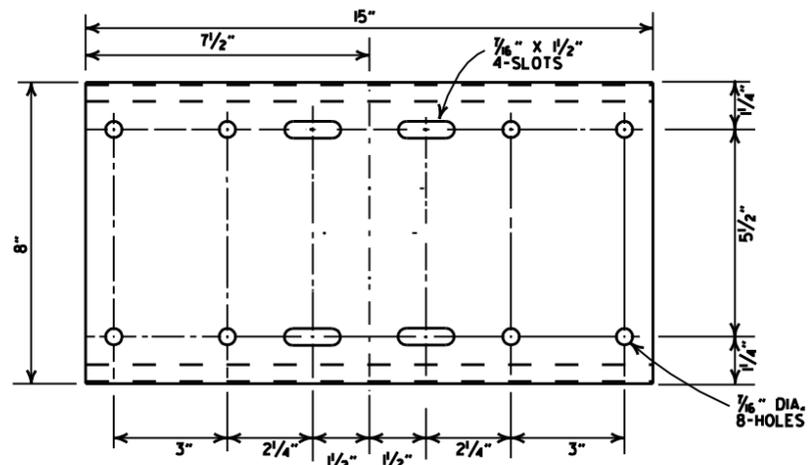


SECTION A-A

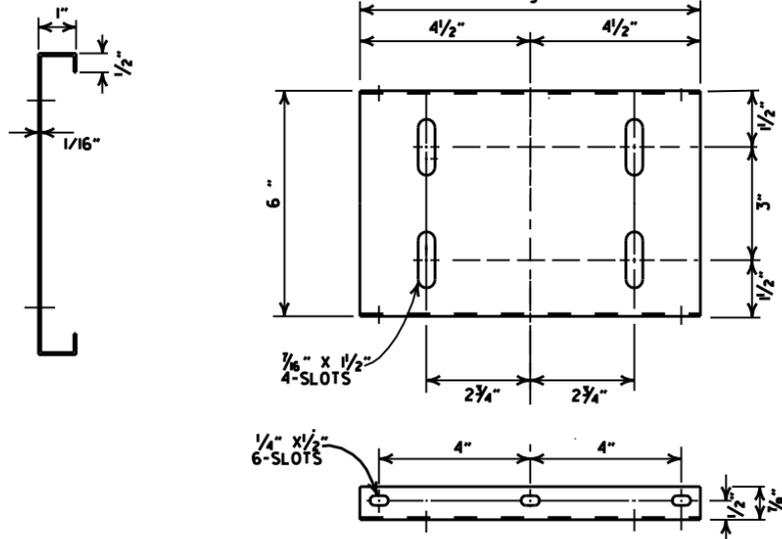


SECTION B-B
CURBED ISLAND BEHIND WALK

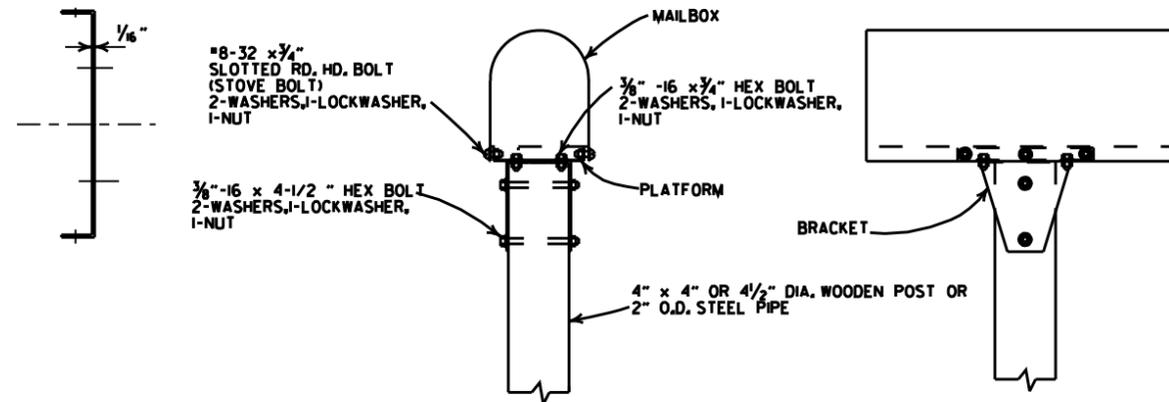
DATE	REV	DESCRIPTION
2-27-14		REVISED PLAN & ISOMETRIC VIEW
11-29-07		ADDED CHANNELIZATION ISLAND WITH TYPE C CURB FACE & REVISED DRIVEWAY SLOPE NOTE & VERTICAL ALIGNMENT DETAIL
11-10-05		REV. APRON SLOPE & DEPTH OF AGG. BASE.
8-22-02		ADDED ISLAND DETAILS & NOTES
3-30-00		REV. MOD. CURB WIDTH & TRANS. NOTE
11-19-98		REVISED NOTES
11-18-98		REDRAWN AND REISSUED
		DATE REV DATE FILMED DESCRIPTION



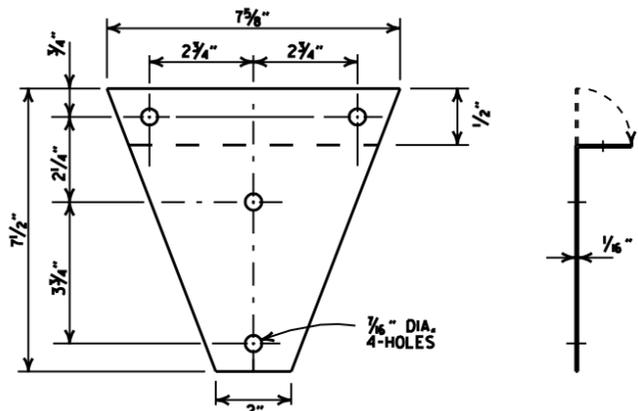
SHELF



PLATFORM



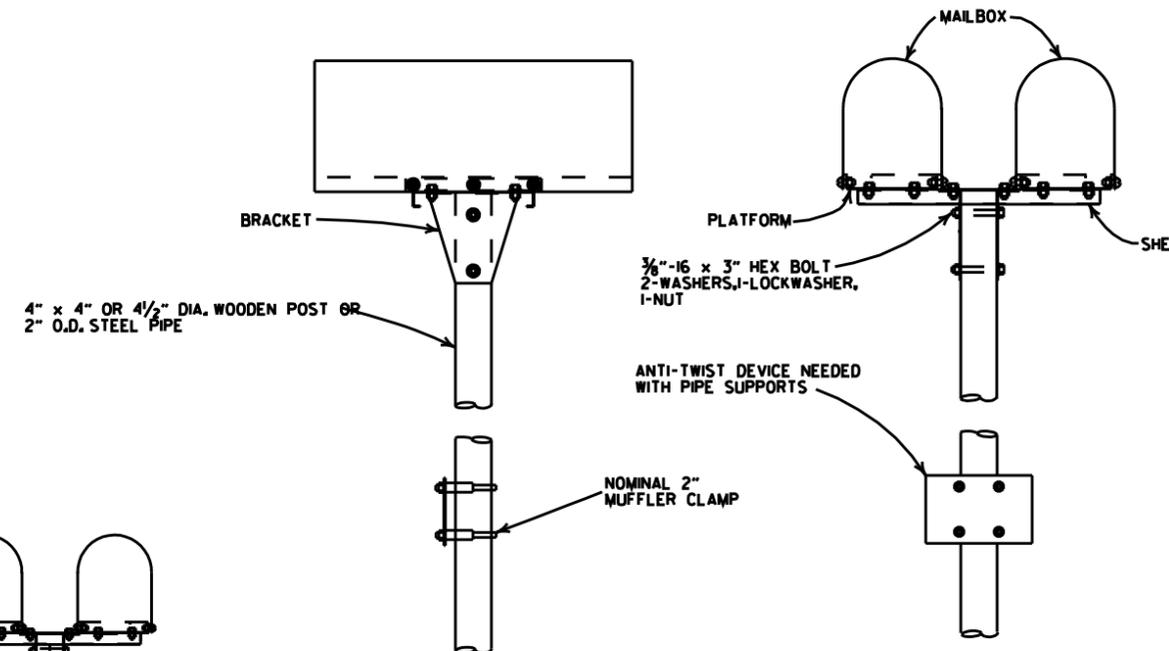
SINGLE INSTALLATION



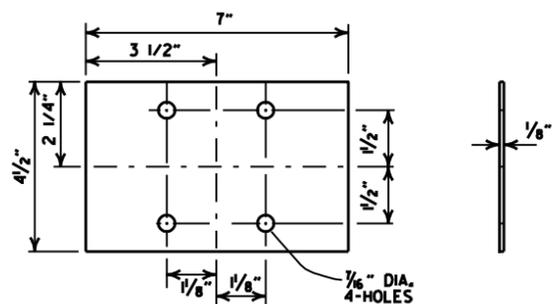
BRACKET

GENERAL NOTES

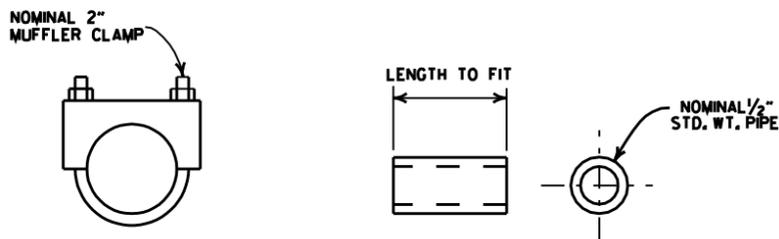
1. MAILBOX POSTS MAY BE WOOD OR METAL. WOOD POSTS SHALL BE PRESSURE TREATED FOR GROUND CONTACT IN ACCORDANCE WITH SECTION 637.02 OF THE STANDARD SPECIFICATIONS.
2. ANTI-TWIST PLATES SHALL BE USED ONLY ON METAL POSTS.
3. MAILBOX SHELF, BRACKET & PLATFORM SHALL BE GALVANIZED OR PAINTED STEEL, HOWEVER TREATED WOOD MAY BE USED WITH WOODEN POSTS. THE WOODEN SHELF, BRACKET & PLATFORM SHALL BE A MINIMUM OF 3/4" THICK AND SHALL BE ASSEMBLED WITH BOLTS OF THE APPROPRIATE LENGTH WITH SIX 8 X 3/4" FLATHEAD WOOD SCREWS USED TO ATTACH THE MAILBOX TO THE PLATFORM.
4. THE MAILBOX SHELF AND PLATFORM THAT IS SHOWN IS FOR STANDARD SIZE MAILBOXES. THE SHELF AND PLATFORM SIZE SHALL BE MODIFIED TO FIT MAILBOXES OF A DIFFERENT SIZE.
5. METAL PIPE FOR MAILBOX SUPPORT SHALL BE 2" OUTSIDE DIAMETER STEEL WITH A WALL THICKNESS OF 0.145" AND A WEIGHT OF 2.72 LBS PER FT. OUTSIDE DIAMETER AND WEIGHT SHALL HAVE A TOLERANCE OF +/- 5% ACCORDING TO AASHTO M 181.
6. MAILBOX SUPPORT SYSTEM DIFFERING FROM THOSE SHOWN MAY BE USED, PROVIDED THEY ARE ON THE AHTD QUALIFIED PRODUCTS LIST FOR MAILBOX SUPPORTS.



DOUBLE INSTALLATION

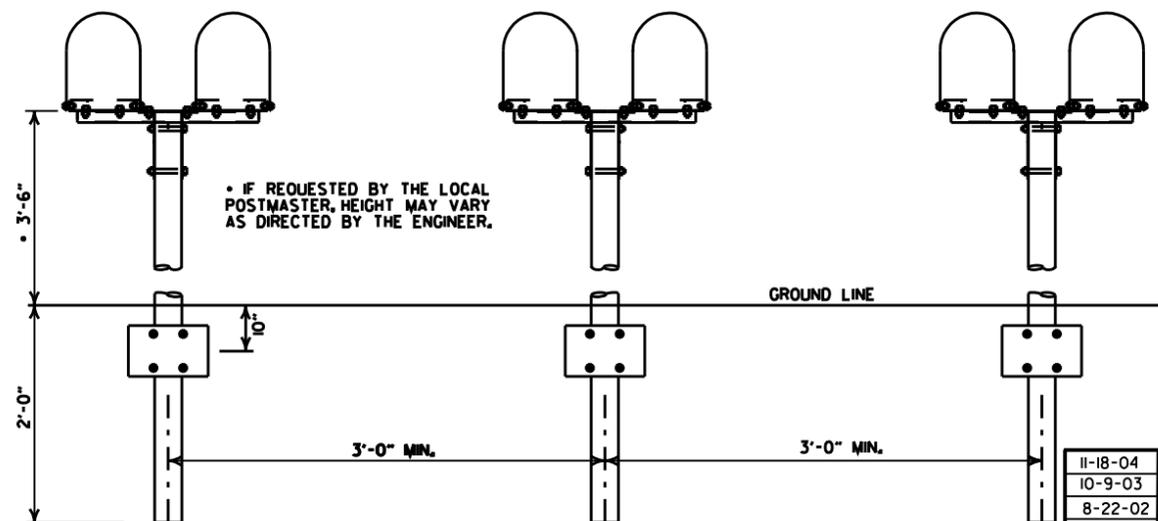


ANTI-TWIST PLATE



CLAMP

SPACER

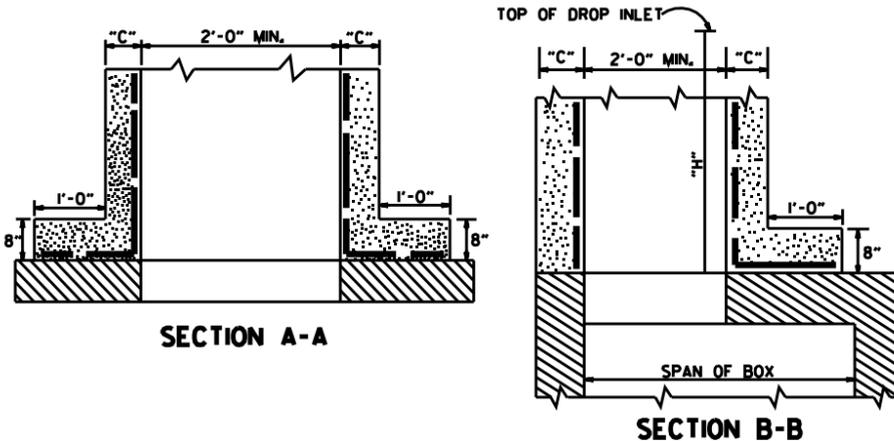
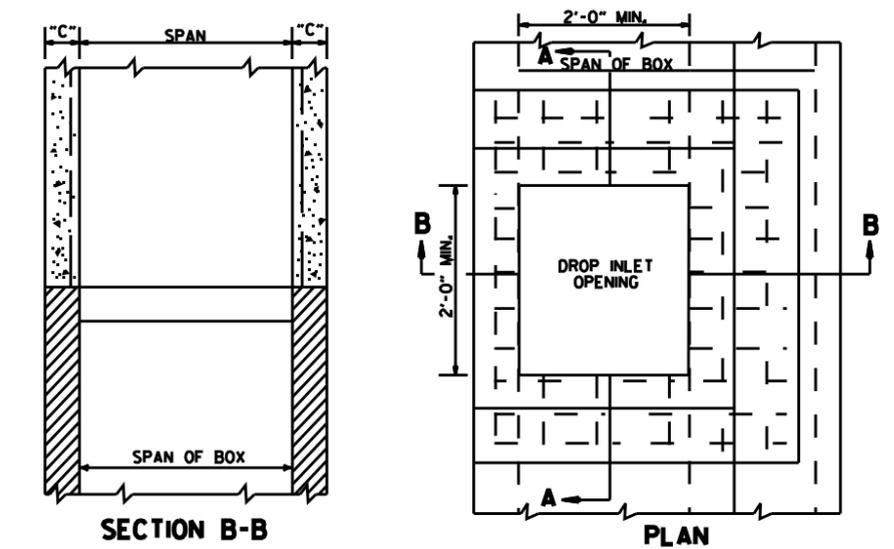


SPACING FOR MULTIPLE POST INSTALLATION

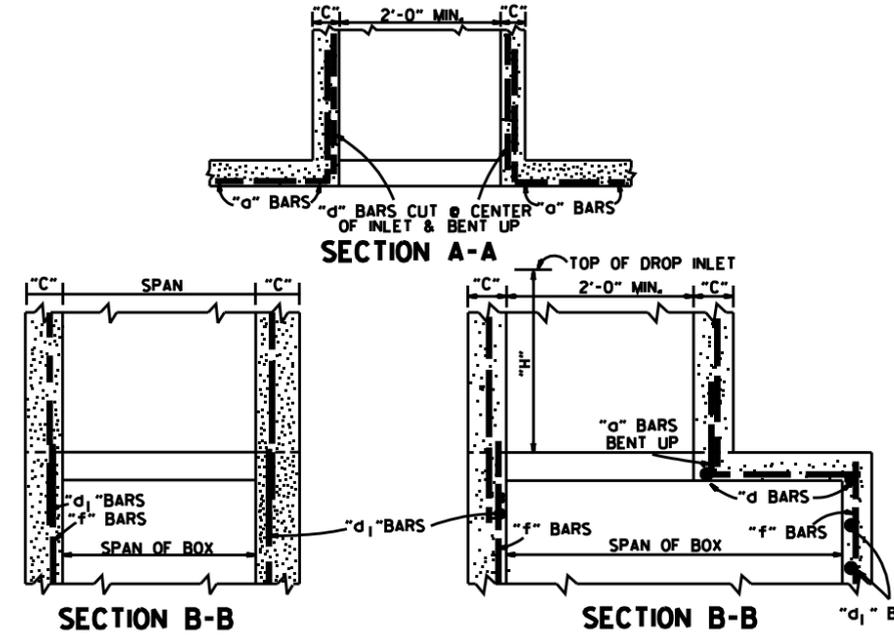
DATE	FILMED	REVISION
11-18-04		REVISED NOTES
10-9-03		REVISED NOTE 6
8-22-02		REVISED NOTE 6
10-18-96		CORRECTED AASHTO
10-1-92		CORRECTED SPELLING
9-26-91		NEW PHONE NUMBER
8-15-91		ADDED NOTE
11-30-89		ADJUSTED HEIGHT & ADDED NOTE
2-16-89		DELETED SLOTS FROM SHELF & PLTF
11-17-88	10-1-92	ADJUSTED DIMENSIONS OF STEEL POSTS
7-15-88	120-7-15-88	ISSUED

ARKANSAS STATE HIGHWAY COMMISSION

MAILBOX DETAILS
STANDARD DRAWING MB-1

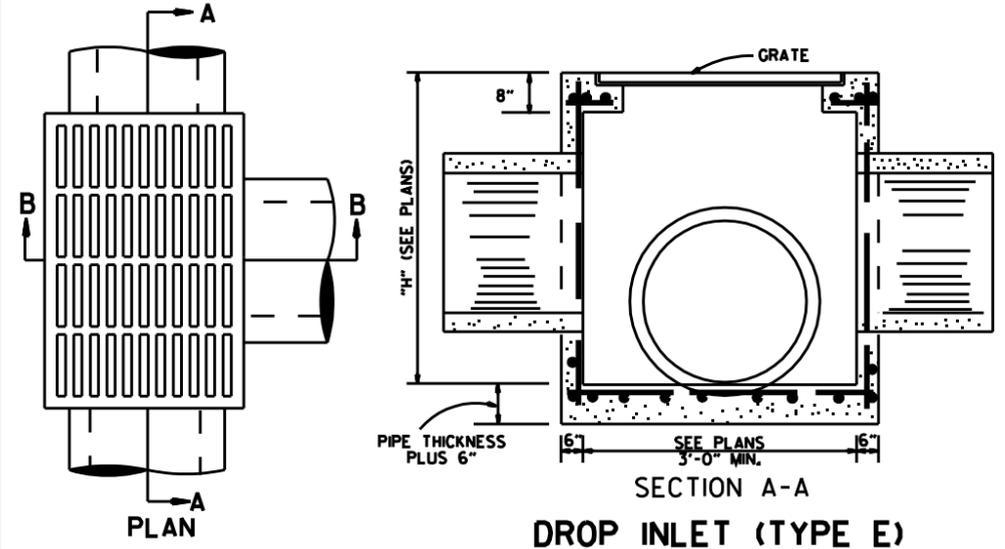


METHOD OF CONSTRUCTING DROP INLET ON EXISTING R.C. BOX CULVERT



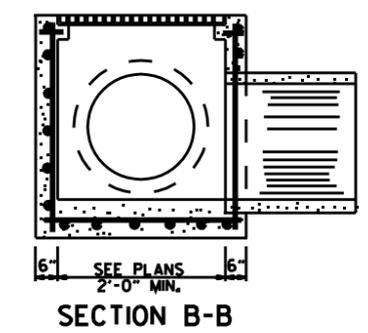
METHOD OF CONSTRUCTING DROP INLET ON NEW R.C. BOX CULVERT

NOTE: "C" DIMENSIONS AND REINFORCING BAR SIZES, SHALL CONFORM TO THOSE SHOWN ON STANDARD DRAWING FOR DROP INLET.

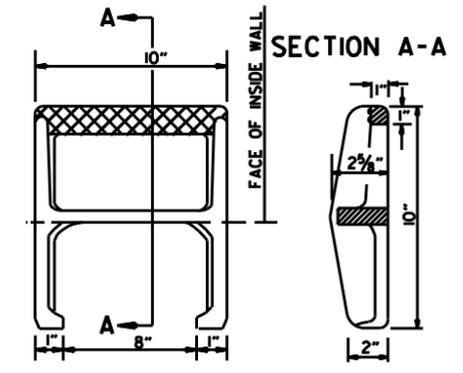


DROP INLET (TYPE E)

NOTE: REINF. BARS TO BE #4 BARS ON 6" CTRS. WITH 1/2" MIN. COVER. THIS TYPE DROP INLET TO BE USED WHERE NOT SUBJECTED TO TRAFFIC.

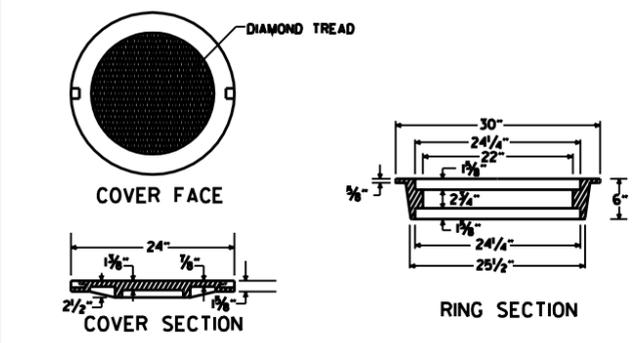


SECTION B-B



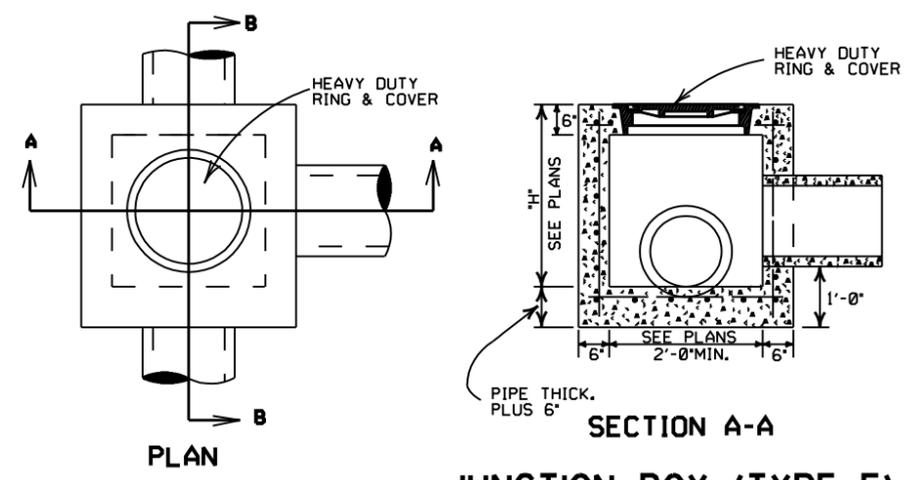
APPROX. WEIGHT = 11 LBS. (CAST IRON)
PLAN
NOTE: THIS DETAIL IS TYPICAL. OTHERS MAY BE USED WITH PRIOR APPROVAL OF THE ENGINEER.

DETAIL OF STEP FOR DROP INLET



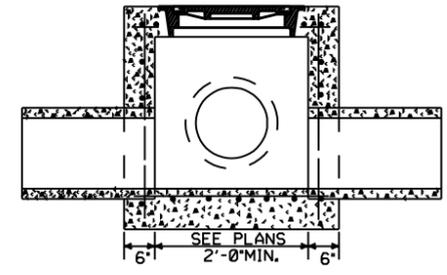
APPROXIMATE TOTAL WEIGHT = 333 LBS.

HEAVY DUTY RING & COVER

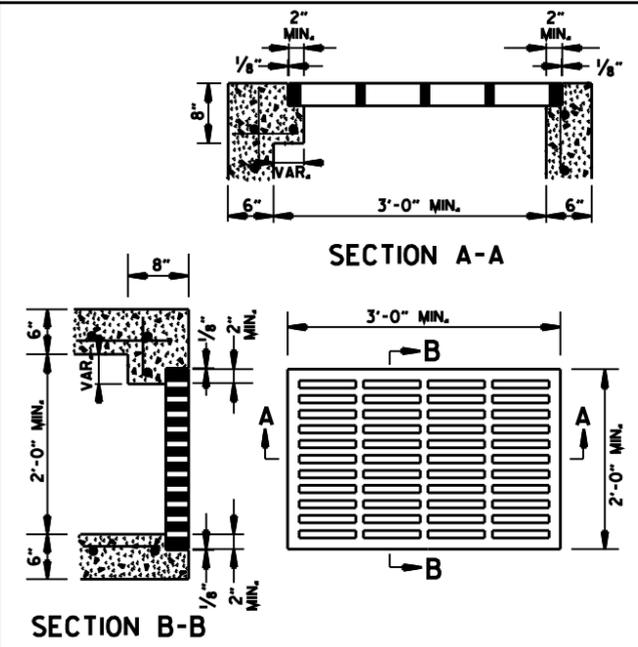


JUNCTION BOX (TYPE E)

NOTE: REINF. BARS TO BE #4 BARS ON 6" CTRS. WITH 1/2" MIN. COVER. THIS TYPE JUNCTION BOX TO BE USED WHERE NOT SUBJECTED TO TRAFFIC.

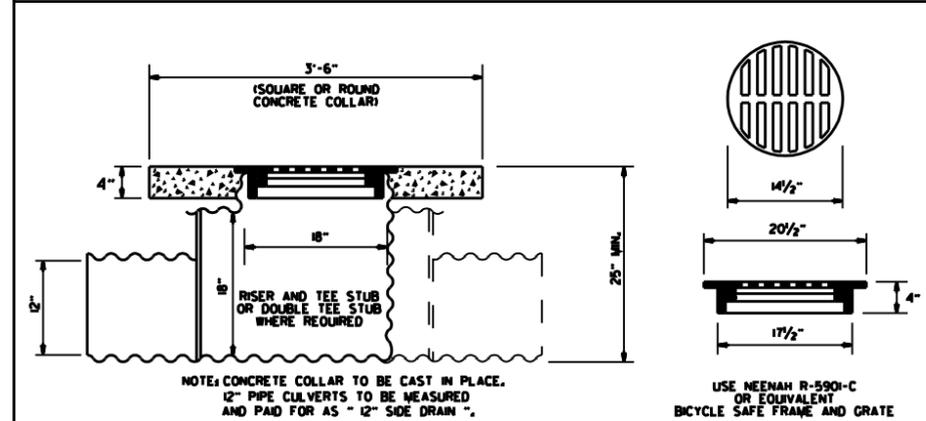


SECTION B-B



APPROXIMATE MINIMUM WATERWAY OPENING = 260 SQ. IN.

GRATE FOR TYPE E DROP INLET



NOTE: CONCRETE COLLAR TO BE CAST IN PLACE. 12" PIPE CULVERTS TO BE MEASURED AND PAID FOR AS "12" SIZE DRAIN".

USE NEEMAH R-5901-C OR EQUIVALENT BICYCLE SAFE FRAME AND GRATE

DETAIL OF YARD DRAIN

- GENERAL NOTES:
1. ALL EXPOSED CORNERS SHALL BE 3/4" CHAMFERED.
 2. STEPS SHALL BE INSTALLED ON 16" CENTERS ON ALL INLETS 4'-0" HIGH OR OVER, OR AS APPROVED BY THE ENGINEER.
 3. EXPANSION JOINT MATERIAL SHALL BE 3/4" PREFORMED FIBER.
 4. GRATE OR GRATE AND FRAME SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M 105 CLASS 35B. GRATE MAY BE USED WITHOUT FRAME.
 5. GRATE AND FRAME SHALL NOT BE PAINTED.
 6. GRATE SHALL BE BICYCLE SAFE.
 7. HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.
 8. HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M105 CLASS 35B & AASHTO M306.
 9. HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
 10. DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.

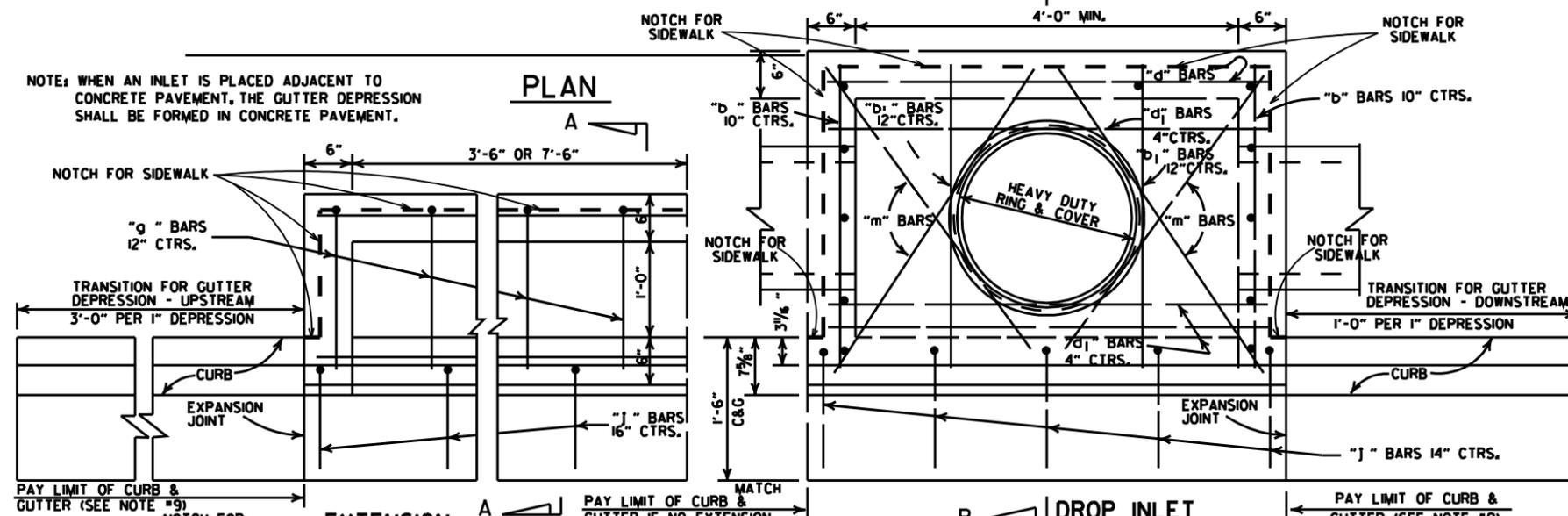
DATE	REV.	REVISION	DATE FILED
11-16-01		ADDED NOTE 10	
1-12-00		REVISED HEAVY DUTY RING & COVER	
7-02-98		CHANGED GRATE DETAIL, DELETED DI (TYPE D), REPLACED RING & COVER W/HEAVY DUTY RING & COVER, ADDED JUNCTION BOX (TYPE E)	
6-26-97		ADDED DIMENSION TO TYPE IV-A	
10-18-96		ADDED DETAIL OF YARD DRAIN	
8-15-91		DELETE TYPE IV GRATE	
7-15-88		REVISED STEP DETAIL	
5-20-83		REVISED DETAILS OF GRATES (TYPE IV & IV-A)	
2-4-83		ADDED GENERAL NOTE NO. 4	
3-2-81		ADDED TYPE IV-A GRATE	
5-22-74		DELETED INLET (TYPE F) & GRATE (TYPE III)	
10-2-72		REVISED AND REDRAWN	

ARKANSAS STATE HIGHWAY COMMISSION
DETAILS OF DROP INLETS & JUNCTION BOXES
STANDARD DRAWING FPC-9

4'-0" LENGTH DROP INLET DROP INLET EXTENSION

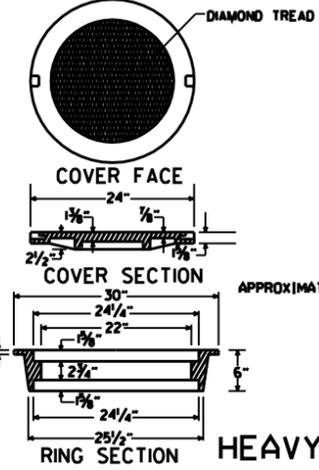
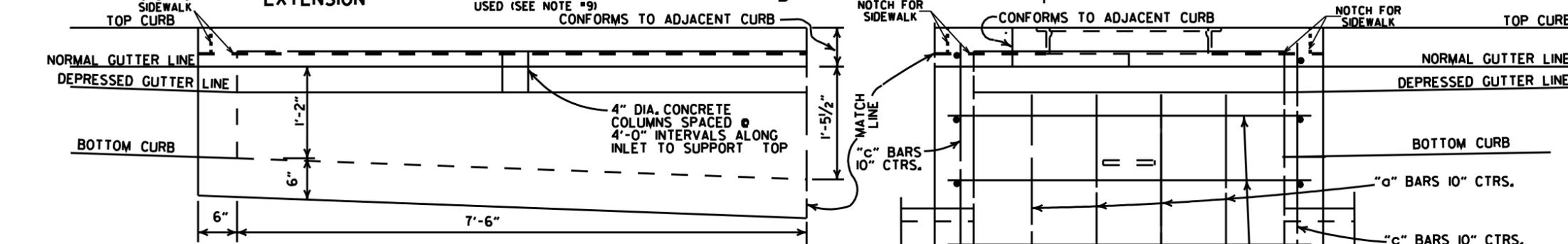
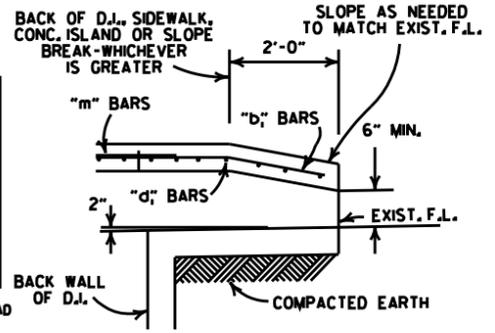
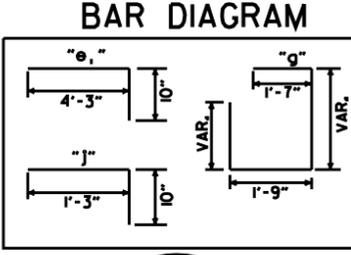
PIPE SIZE	MIN. WIDTH	HEIGHT 5'-0"		PLUS OR MINUS PER LIN. FT. OF HEIGHT		4'-0"		8'-0"	
		CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS
18"	2'-6"	1.77	156	0.28	22	0.58	38	0.87	72
24"	2'-6"	1.79	156	0.28	22				
30"	3'-2"	2.39	205	0.30	26				
36"	3'-8"	2.63	236	0.32	28				
42"	4'-4"	2.95	250	0.34	30				
48"	4'-10"	3.21	265	0.36	32				
						DEDUCT FROM QUANTITY COMPUTED FOR EACH EXTENSION ADDED.			
						0.04	3		

NOTE: QUANTITIES ARE APPROXIMATE AND ARE SHOWN FOR BIDDER INFORMATION ONLY.



DEDUCT FROM QUANTITY COMPUTED FOR EACH PIPE ENTERING INLET

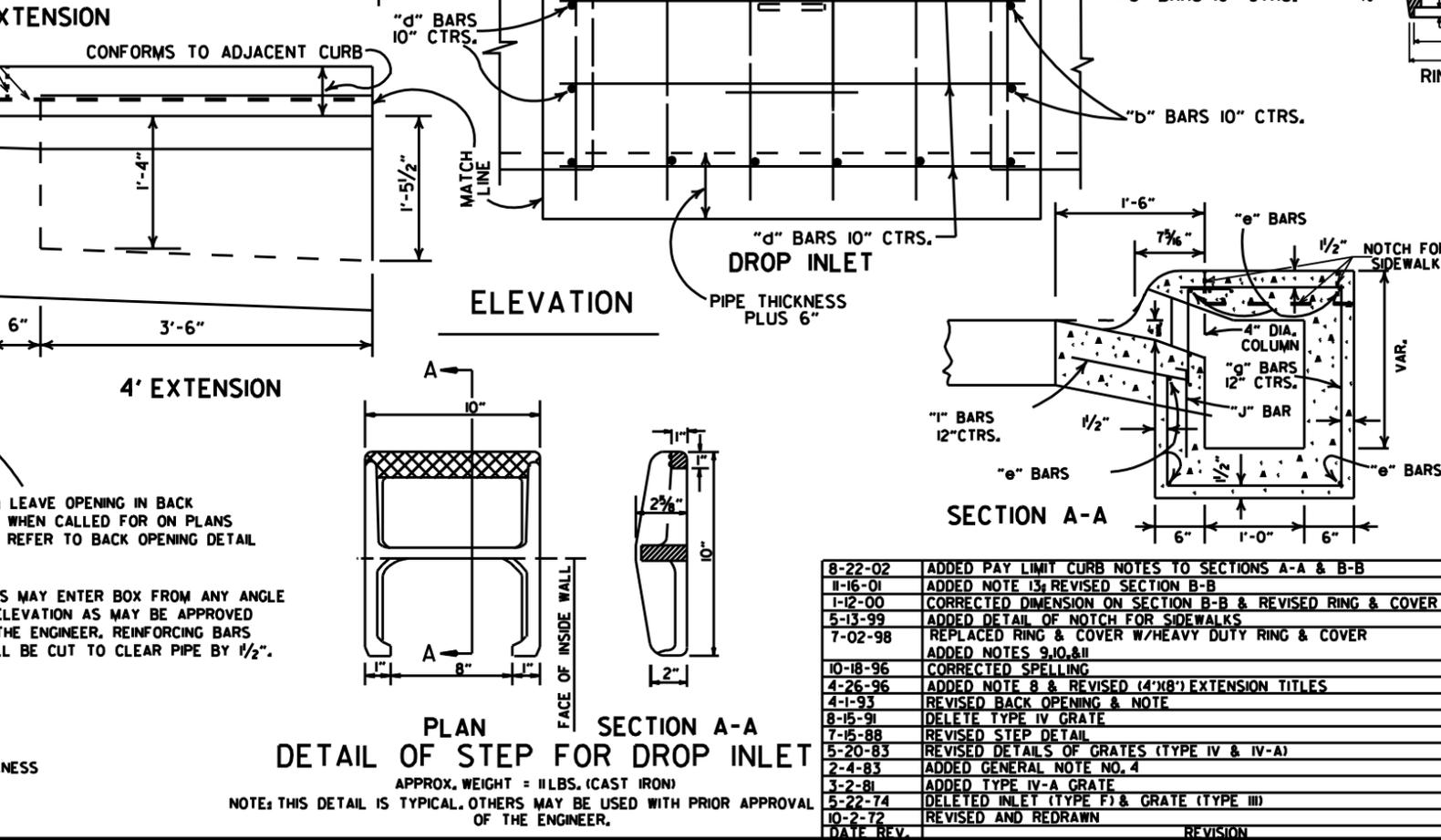
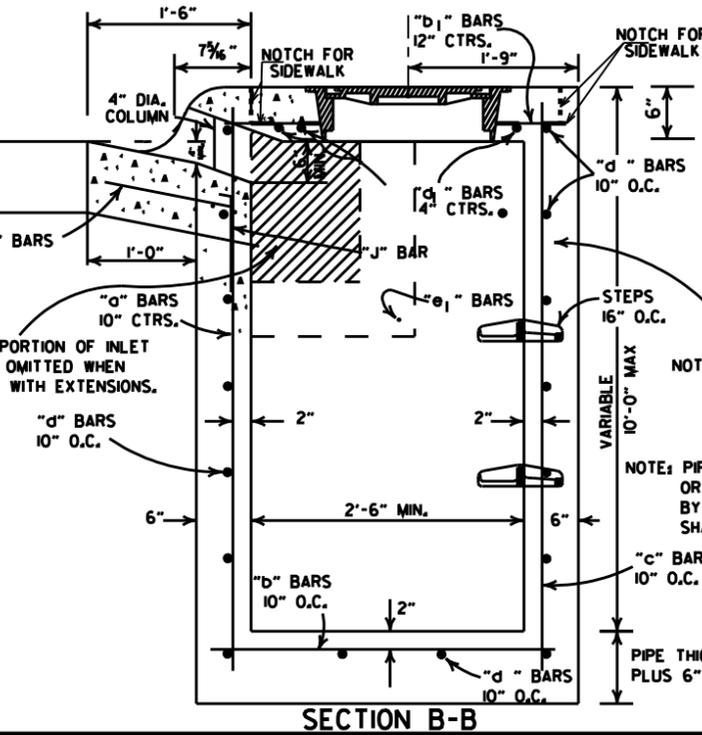
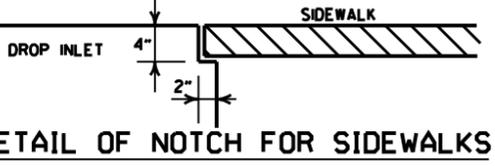
INSIDE DIA. PIPE INCHES	CLASS A CONC. CU. YDS.	REINF. STEEL POUNDS
18	0.05	2
24	0.09	3
30	0.13	4
42	0.24	8



APPROXIMATE TOTAL WEIGHT = 333 LBS.

HEAVY DUTY RING & COVER

- GENERAL NOTES:
- ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER.
 - STEPS SHALL BE INSTALLED IN ALL INLETS 4'-0" HIGH AND OVER AS APPROVED BY THE ENGINEER.
 - ALL REINF. BARS SHALL BE #4 AND HAVE 1/2" COVER.
 - DROP INLETS AND EXTENSION ON CURVED SECTIONS SHALL CONFORM TO THE CURVATURE OF THE CURB.
 - THIS DROP INLET MAY BE CONSTRUCTED ON NEW OR EXISTING R.C. BOX CULVERT AS SHOWN ON F.P.C.-9.
 - WHEN PLANS CALL FOR DROP INLET OVER 10'-0" HIGH, FLOOR AND WALLS SHALL BE CONSTRUCTED AS SHOWN FOR TYPE "RM" DROP INLET (F.P.C.-9D).
 - HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.
 - DURING CONSTRUCTION OF THE ROADWAY THE CONTRACTOR SHALL MAINTAIN DRAINAGE INTO OR AROUND THE DROP INLET AS APPROVED BY THE ENGINEER.
 - PAYMENT FOR CURB AND/OR CURB AND GUTTER WITHIN THE LIMITS OF DROP INLETS AND DROP INLET EXTENSIONS SHALL BE CONSIDERED INCLUDED IN PAYMENT MADE FOR DROP INLETS AND/OR DROP INLET EXTENSIONS.
 - HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M105 CLASS 35B & AASHTO M306.
 - HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
 - 4"x2" NOTCH SHALL BE FORMED IN ALL DROP INLETS TO SUPPORT SIDEWALK CONSTRUCTION, REFER TO DETAIL OF NOTCH FOR SIDEWALKS.
 - DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.

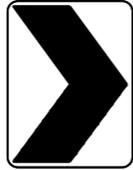
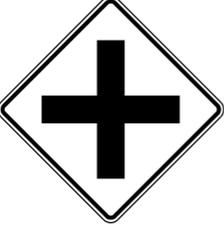
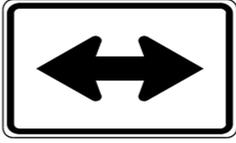
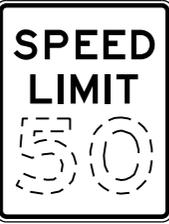
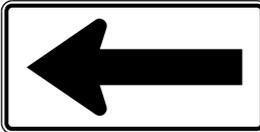
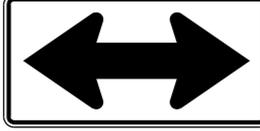
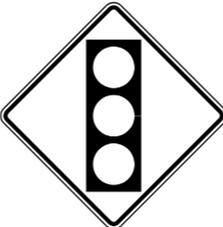
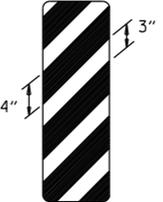


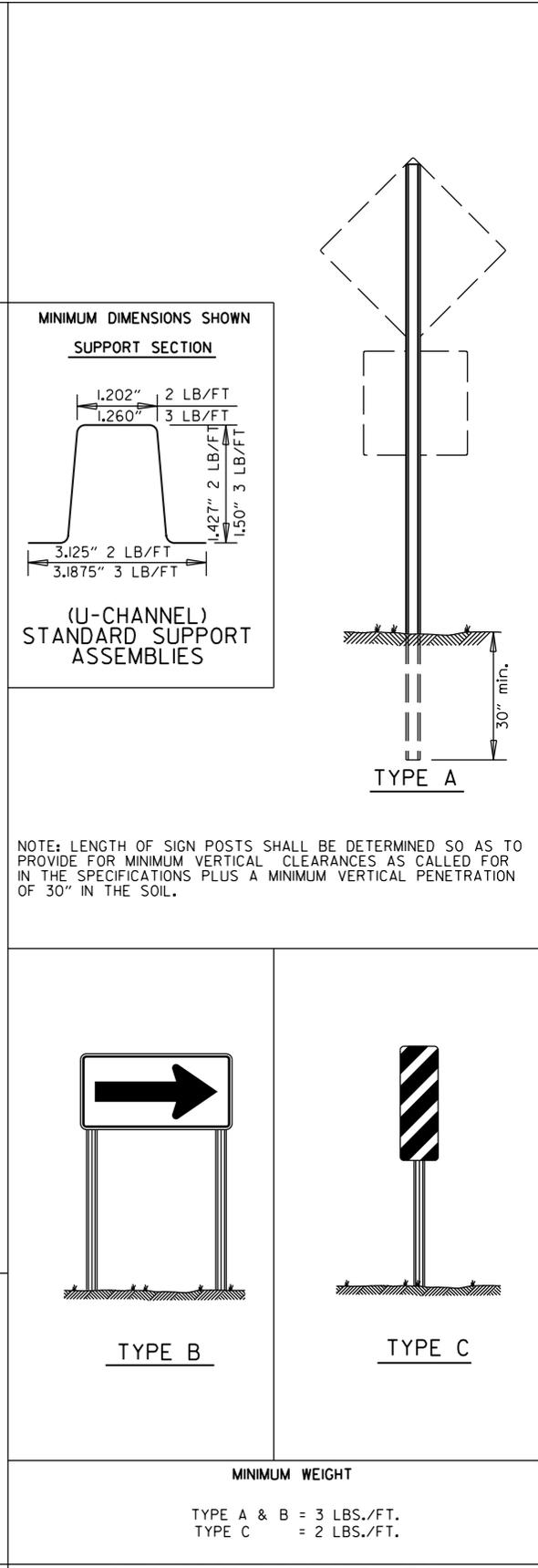
DATE	REV.	REVISION	DATE FILMED
8-22-02		ADDED PAY LIMIT CURB NOTES TO SECTIONS A-A & B-B	
11-16-01		ADDED NOTE 13; REVISED SECTION B-B	
1-12-00		CORRECTED DIMENSION ON SECTION B-B & REVISED RING & COVER	
5-13-99		ADDED DETAIL OF NOTCH FOR SIDEWALKS	
7-02-98		REPLACED RING & COVER W/HEAVY DUTY RING & COVER ADDED NOTES 9,10,&11	
10-18-96		CORRECTED SPELLING	
4-26-96		ADDED NOTE 8 & REVISED (4'x8') EXTENSION TITLES	10-18-96
4-1-93		REVISED BACK OPENING & NOTE	
8-15-91		DELETE TYPE IV GRATE	
7-15-88		REVISED STEP DETAIL	
5-20-83		REVISED DETAILS OF GRATES (TYPE IV & IV-A)	
2-4-83		ADDED GENERAL NOTE NO. 4	
3-2-81		ADDED TYPE IV-A GRATE	
5-22-74		DELETED INLET (TYPE F) & GRATE (TYPE III)	
10-2-72		REVISED AND REDRAWN	

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF DROP INLETS (TYPE C)

STANDARD DRAWING FPC-9E

 RI-1 30"X30"	 WI-3 30"X30" (LT. OR RT.)	 WI-8 18"X24"	 W2-5 30"X30"	 W3-1 36"X36"	 W5-1 36"X36"	 M6-3 21"X15"
 RI-2 36"X36"X36"	 WI-4 30"X30" (LT. OR RT.)	 W2-1 30"X30"	 SI-1 36"X36"	 W3-2 36"X36"	 County Route Marker MI-6 24"X24" <small>NOTE: REFLECTORIZED YELLOW LEGEND (COUNTY NAME, ROUTE LETTER & NUMBER) & BORDER ON A BLUE BACKGROUND.</small>	 M6-4 21"X15"
 R2-1 24"X30"	 WI-5 30"X30" (LT. OR RT.)	 W2-2 30"X30"	 W5-2 36"X36"	 W8-3 36"X36"	 RI-3P 18"X6"	 M6-5 21"X15"
 WI-1 30"X30" (LT. OR RT.)	 WI-6 48"X24"	 W2-3 30"X30" (LT. OR RT.)	 W5-3 36"X36"	 WI3-1P 18"X18"	 M6-1 21"X15" <small>NOTE: ALL M6 SIGNS TO BE MADE WITH REFLECTORIZED YELLOW ARROW & BORDER WITH BLUE BACKGROUND.</small>	 M6-6 21"X15"
 WI-2 30"X30" (LT. OR RT.)	 WI-7 48"X24"	 W2-4 30"X30"	 W10-1 36" DIAMETER	 W3-3 36"X36"	 M6-2 21"X15"	 S4-3P 24"X8"  S4-2P 24"X10"  OM-3 12"X36" (LT. OR RT.)



STANDARD HIGHWAY SIGNS

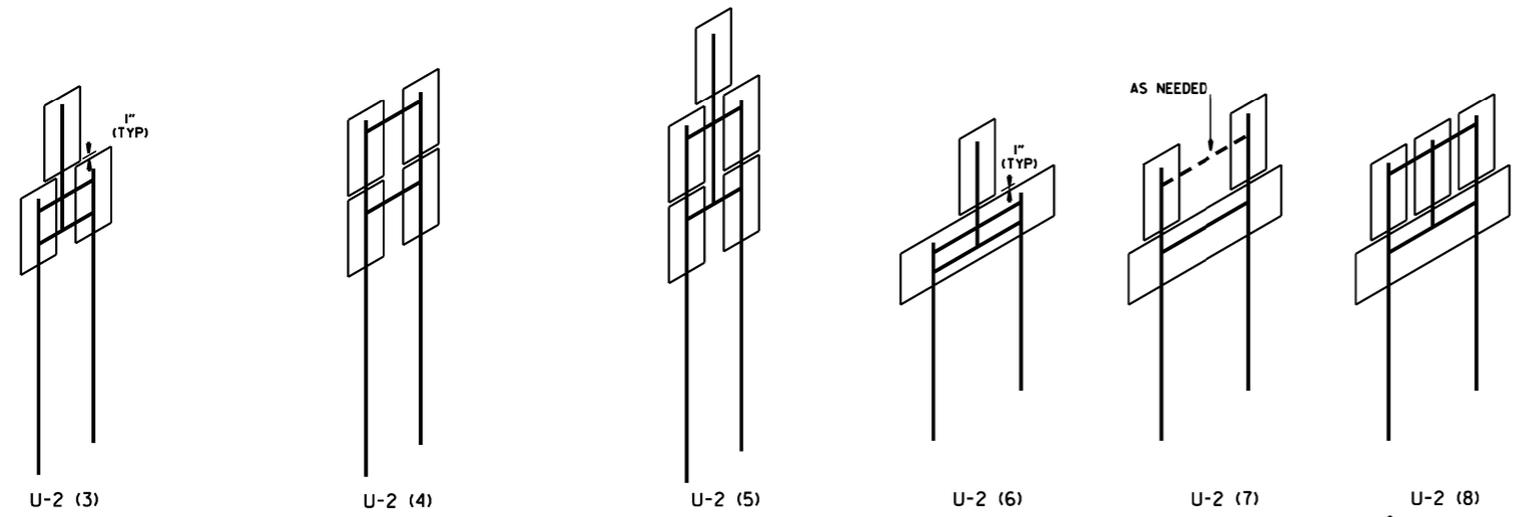
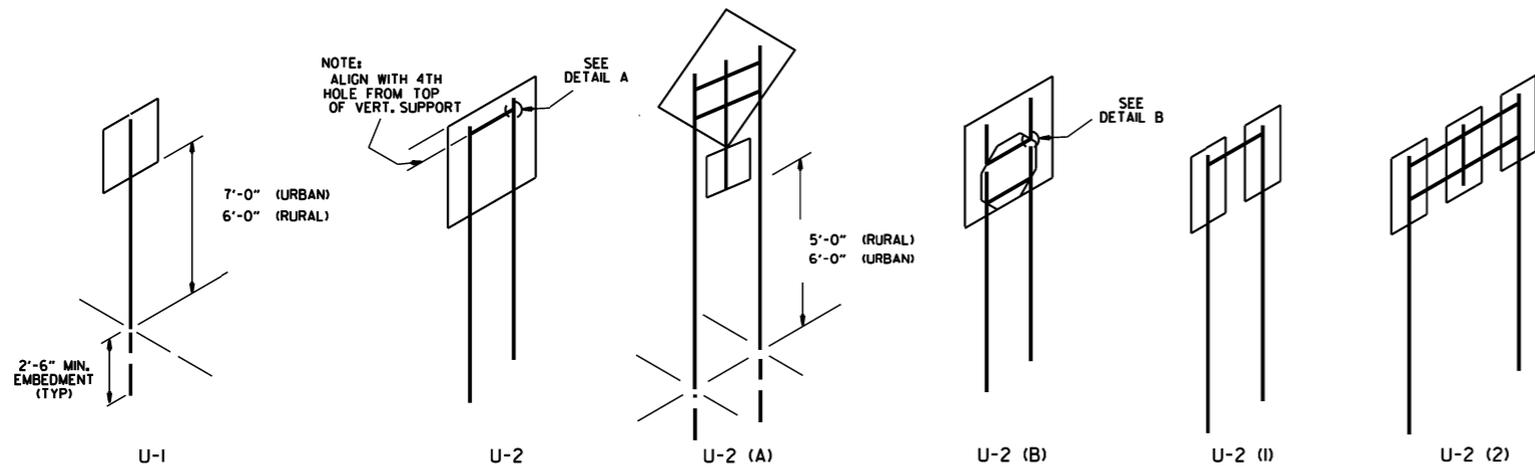
9-12-13	DELETED JOB NO. BLOCK; REVISED RI-3 TO RI-3P	
4-17-08	REVISED SIGN DESIGNATION - W3-1 & W3-2	
4-10-03	REVISED W5-2, W8-3, OM-3; ADDED WI-8	
1-5-81	REDRAWN	960-1-15-81
9-15-78	ADDED WI-4-3	877-9-15-78
9-2-76	POST WT.	623-9-3-76
5-3-76	STEEL POST WT. FROM 2*3*; ADDED S4-2 & S4-3	504-5-3-76
8-12-74	REV. HT. TYPE "C" ASSEMBLY	500-8-21-74
12-21-72	ADDED M6-2,3,4,5,6	500-12-21-72
12-1-72	ISSUED	562-12-1-72
DATE	REVISION	DATE FILMED

SUPPORT ASSEMBLIES

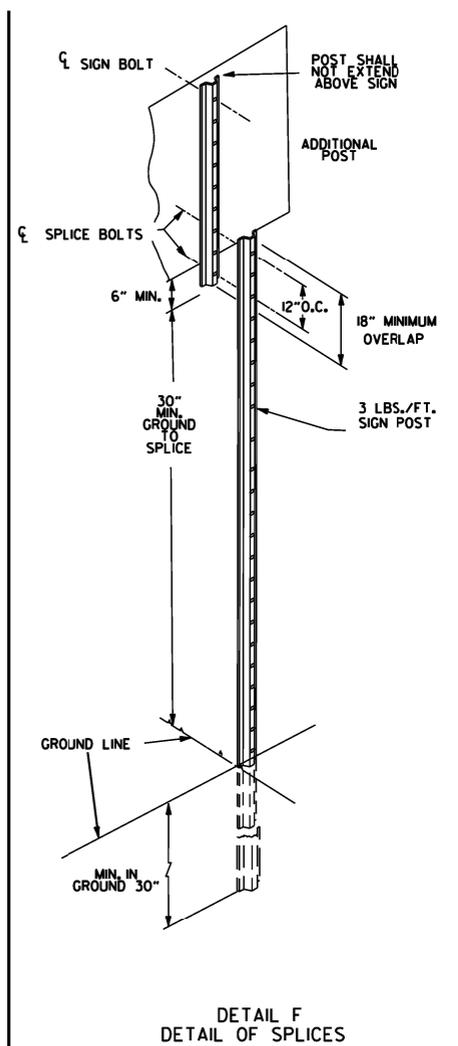
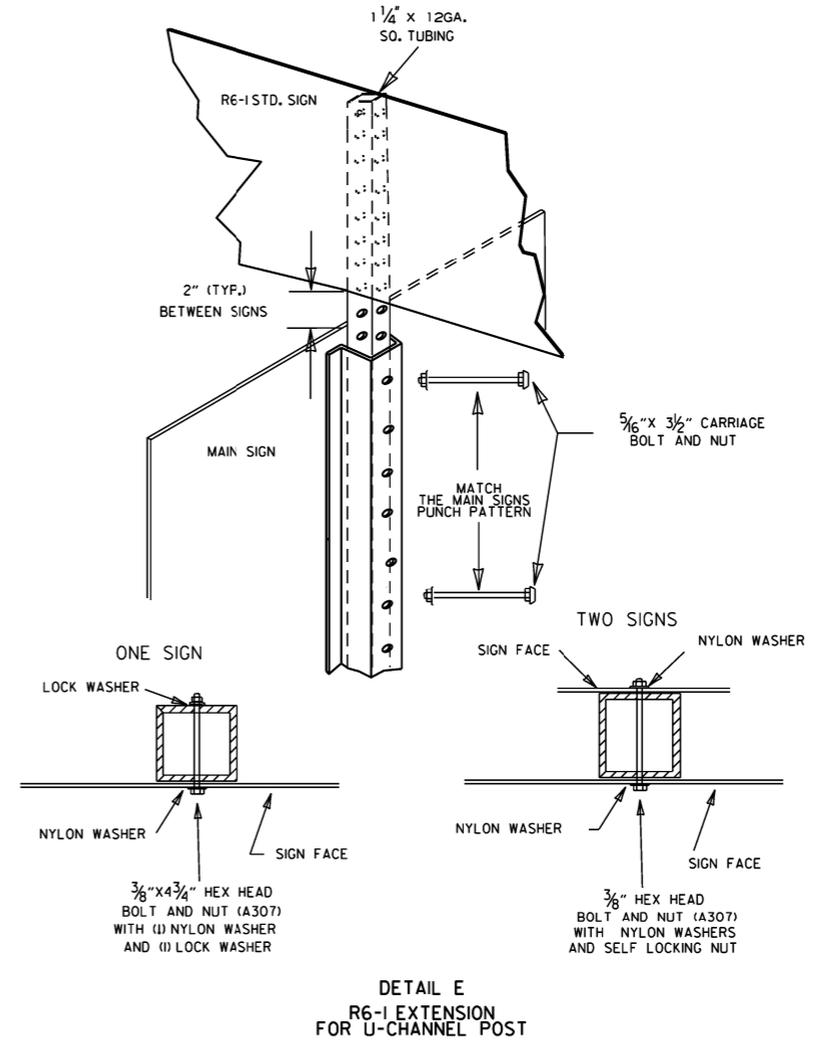
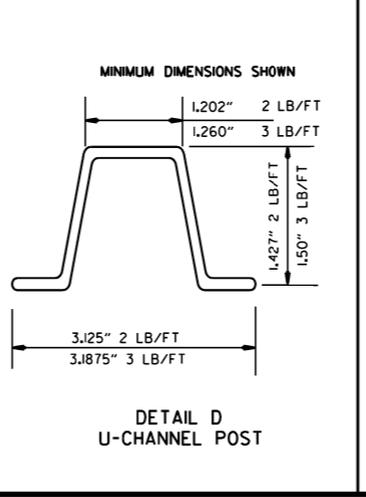
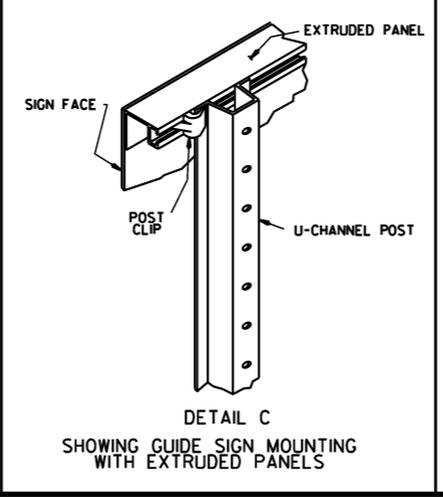
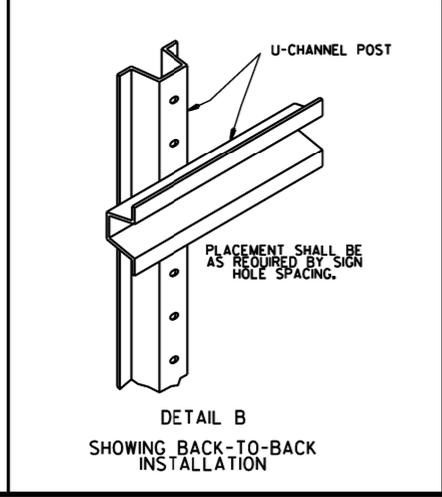
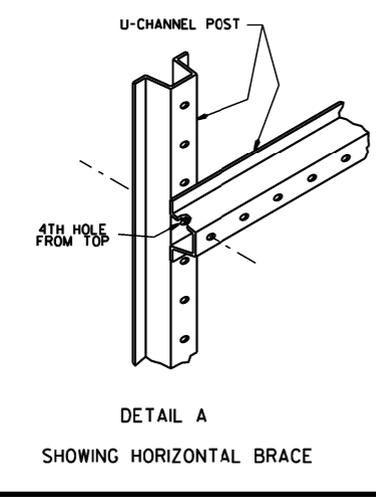
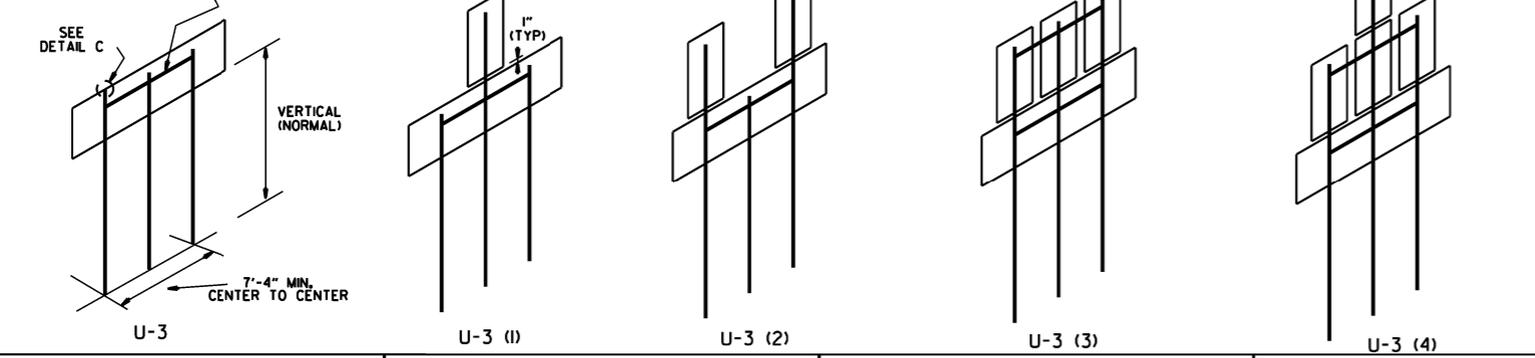
ARKANSAS STATE HIGHWAY COMMISSION

STANDARD HIGHWAY SIGNS AND SUPPORT ASSEMBLIES

STANDARD DRAWING SHS-1



HORIZONTAL BRACE
(FOR ALL MULTIPLE POST ASSEM.
WITH FLAT SHEET SIGNS)



NOTES:

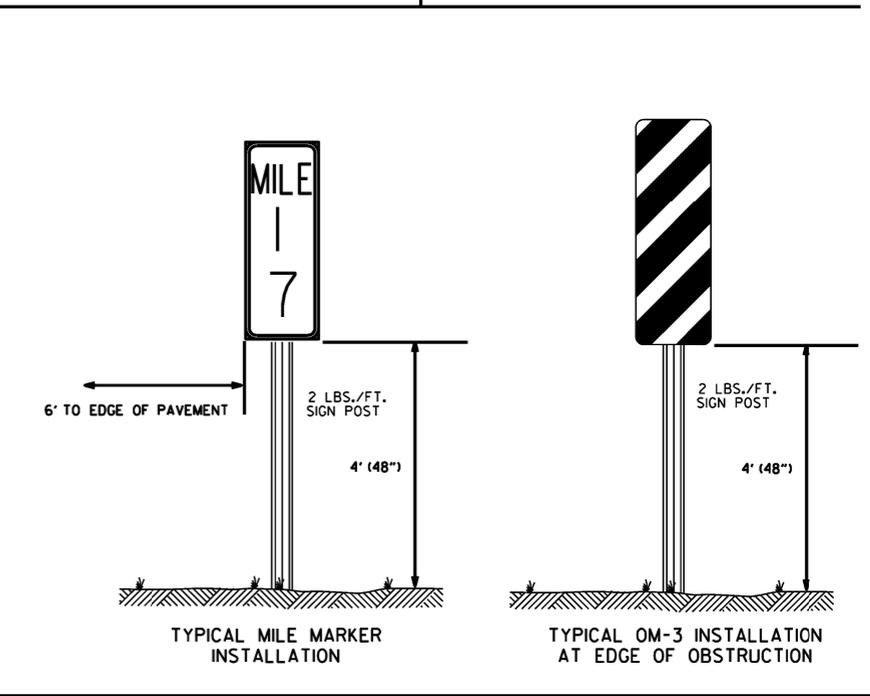
SIGNS AT LEAST 8' IN LENGTH MAY BE INSTALLED ON THREE 3 LB. POST. IN NO CASE SHALL THERE BE MORE THAN TWO 3 LB. POSTS WITHIN A 7' PATH.

SPLICES NECESSARY TO ATTAIN PROPER MOUNTING HEIGHT SHALL BE AS SHOWN IN DETAIL (F).

NORMAL INSTALLATIONS WILL REQUIRE 5/16" DIA. CARRIAGE BOLTS TO MOUNT SIGNS TO POST AND TO ASSEMBLE THE VARIOUS POST SUPPORTS.

ALL SIGN POSTS SHALL BE PLUMB.

THE POST FOR "TYPE U" SUPPORTS SHALL BE HOT DIP GALVANIZED.

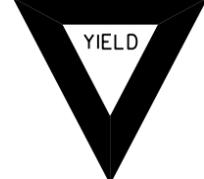
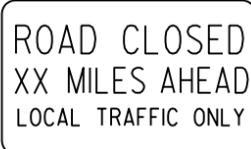
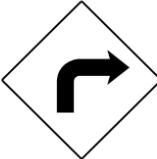
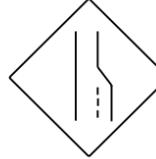
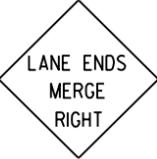
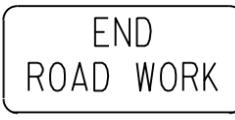
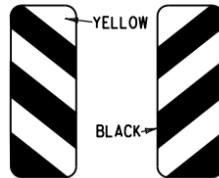
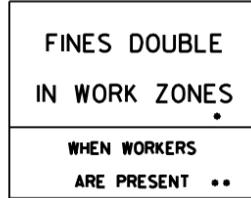


DATE	REVISION	
2-27-14	REVISED NOTES.	
9-12-13	REVISED U-2(3), U-2(6), U-3(1), DETAIL D; ADDED DETAILS E & F; ADDED TYPICAL MARKERS	
10-9-03	REMOVED ROUND POST & REVISED SPACING	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL	6-8-95
2-2-95	REDRAWN	2-2-95
		FILMED

ARKANSAS STATE HIGHWAY COMMISSION

U-CHANNEL POST ASSEMBLIES

STANDARD DRAWING SHS-2

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

ADVANCE DISTANCES (XXXX)

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

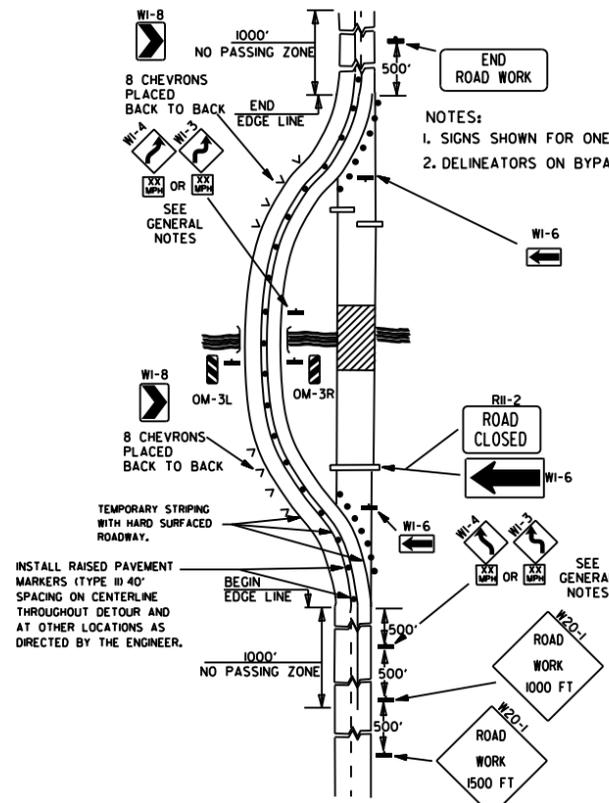
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

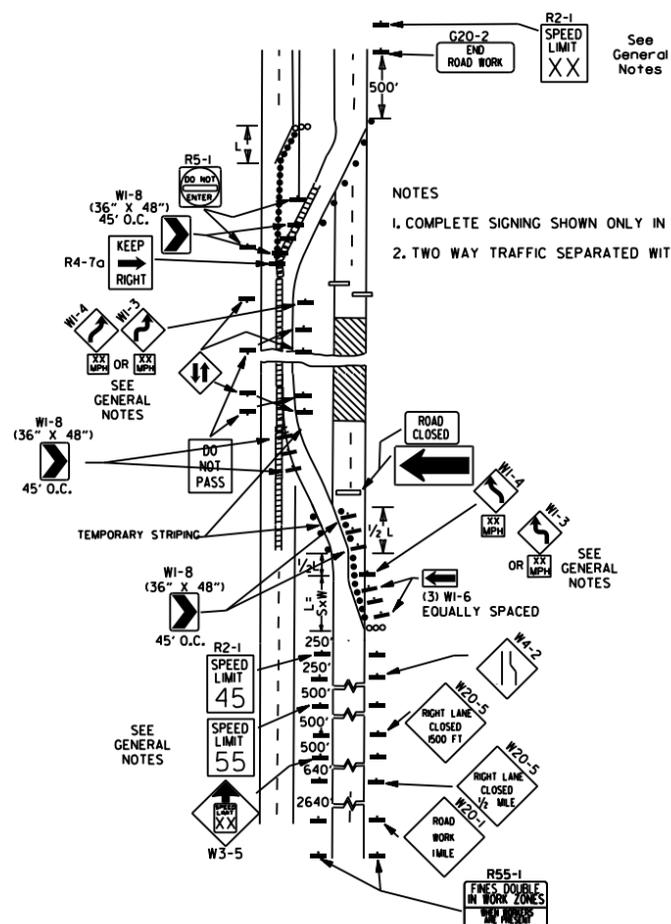
• NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS	
	REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

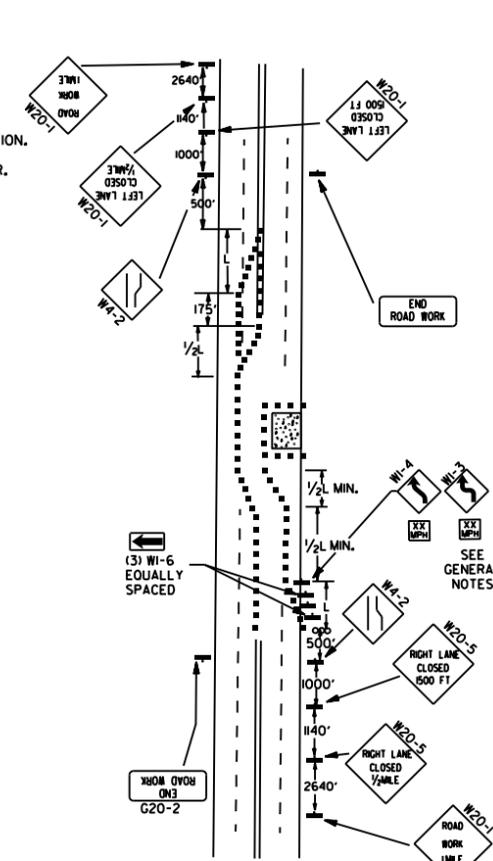
ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-1



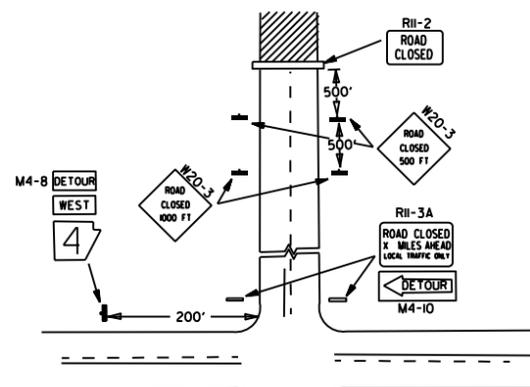
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.

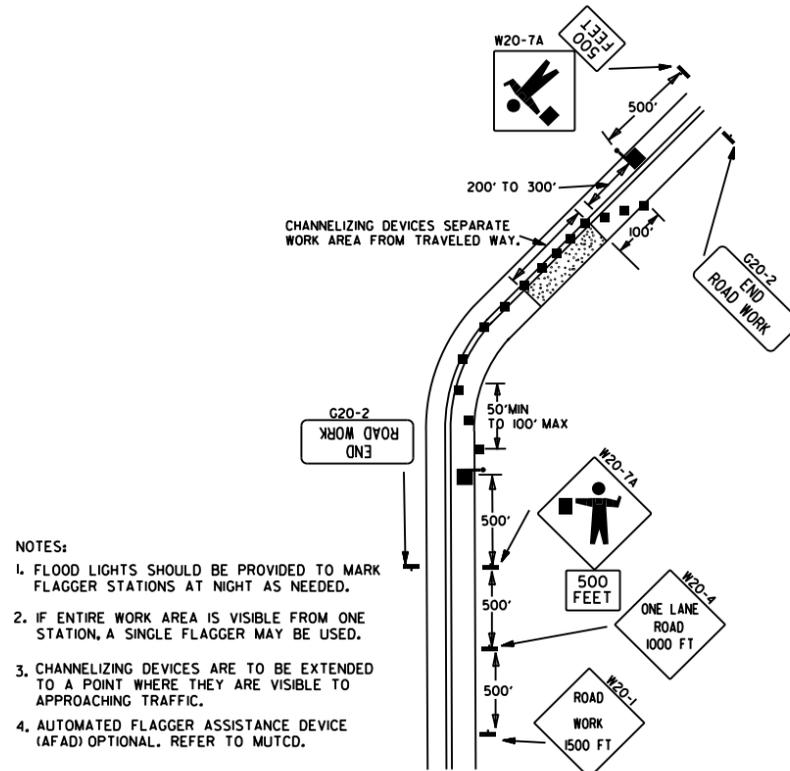


(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



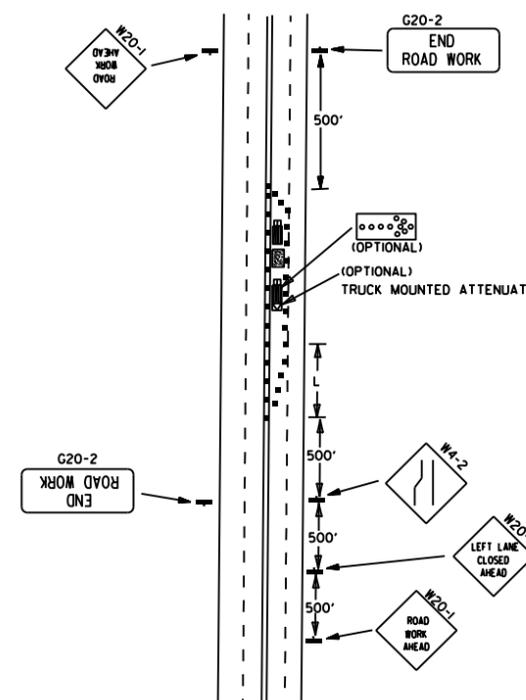
NOTES:
 1. REGULATORY TRAFFIC CONTROL DEVICES TO BE MODIFIED AS NEEDED FOR THE DURATION OF THE DETOUR.
 2. STREET NAMES MAY BE USED WHEN DESIRABLE FOR DIRECTING DETOURED TRAFFIC.

(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

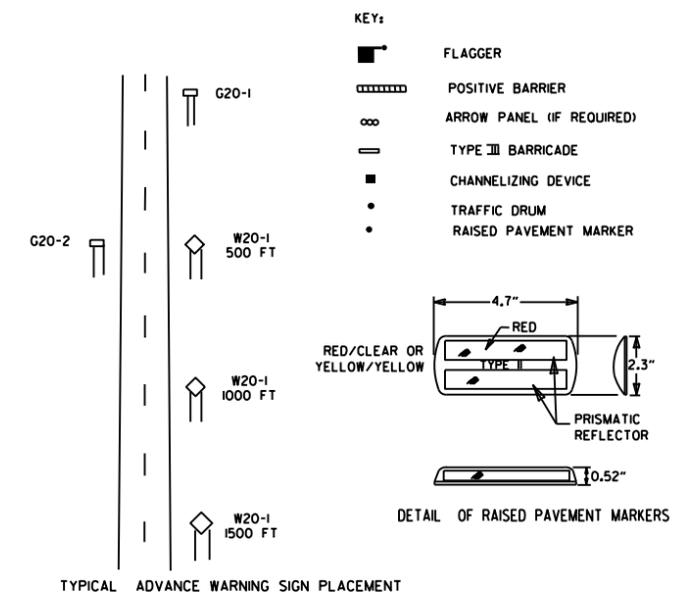


NOTES:
 1. FLOOD LIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
 2. IF ENTIRE WORK AREA IS VISIBLE FROM ONE STATION, A SINGLE FLAGGER MAY BE USED.
 3. CHANNELIZING DEVICES ARE TO BE EXTENDED TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.
 4. AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) OPTIONAL. REFER TO MUTCD.

(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.



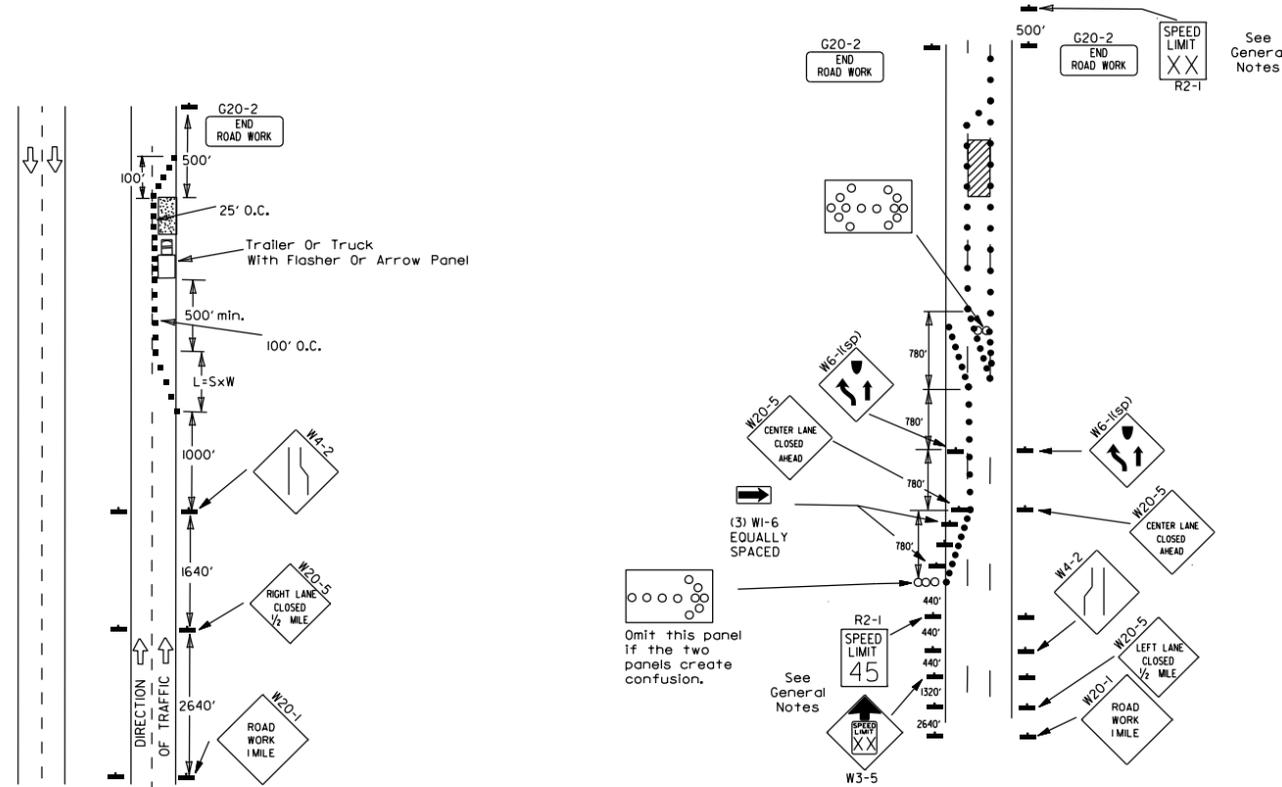
TYPICAL ADVANCE WARNING SIGN PLACEMENT

TAPER FORMULAE:
 L=SW FOR SPEEDS OF 45MPH OR MORE.
 L= $\frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L= MINIMUM LENGTH OF TAPER.
 S= NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W= WIDTH OF OFFSET.

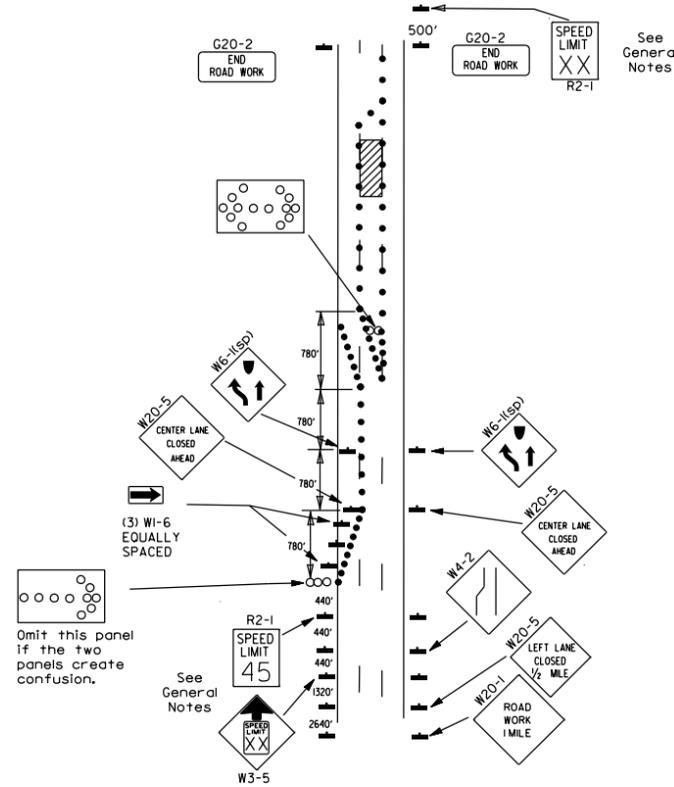
- GENERAL NOTES:
- ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 - WHEN THE EXISTING SPEED LIMIT IS 45MPH, THE R2-(K55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(KXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(K45) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(KXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 - PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
 - DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

DATE	REVISION	FILMED
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

Channelizing devices



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

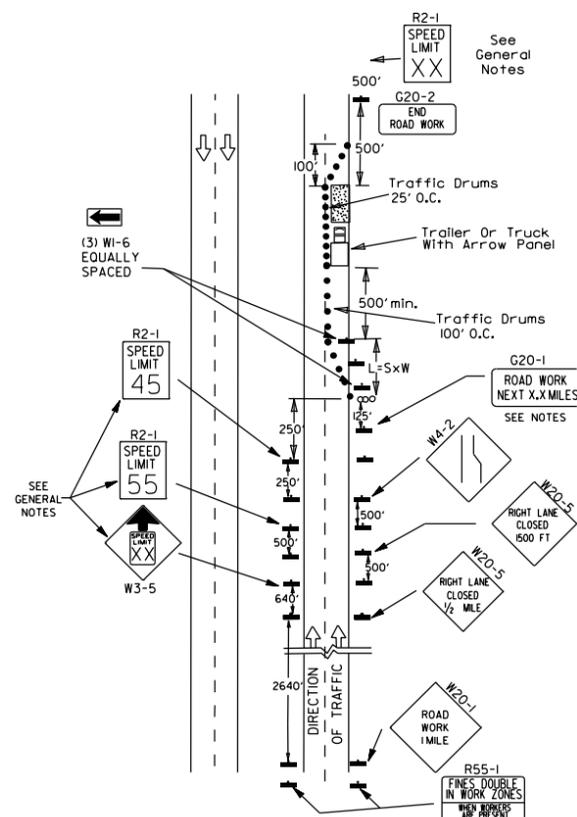


(B) Typical application - 3-lane oneway roadway where center lane is closed.

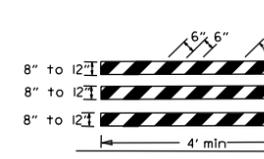
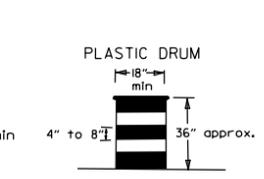
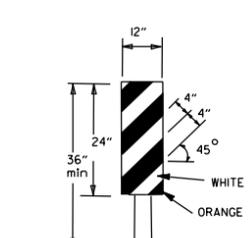
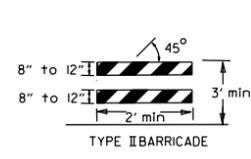
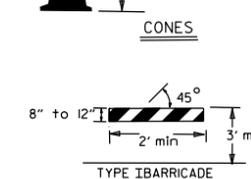
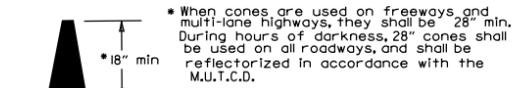
- KEY:
- Arrow Panel (if Required)
 - Channelizing Device
 - Traffic drum

GENERAL NOTES:

1. A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the W3-5 shall be installed at that location. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-1(55) speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
7. The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1(1/2 MILE) signs are not required in advance of lane closures that begin inside the project limits.
8. Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
9. All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
10. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

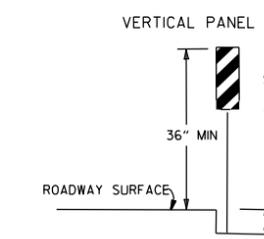


(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.



NOTE:

For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



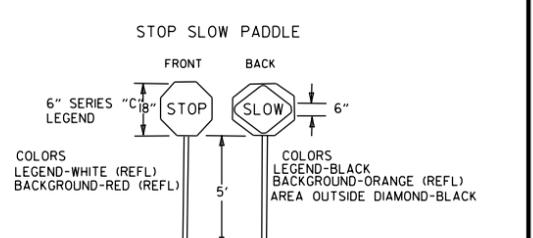
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

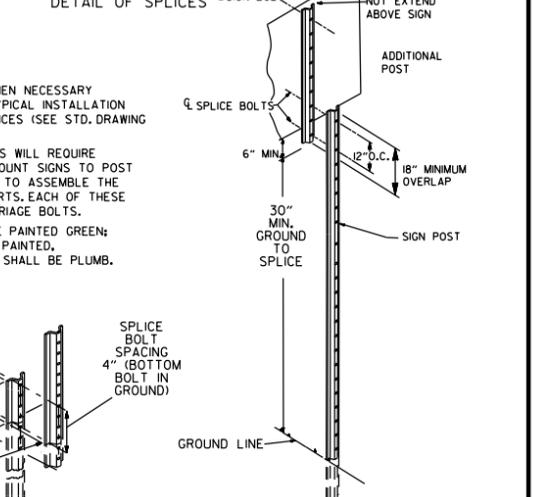
* When shown on the plans concrete barrier will be used. When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



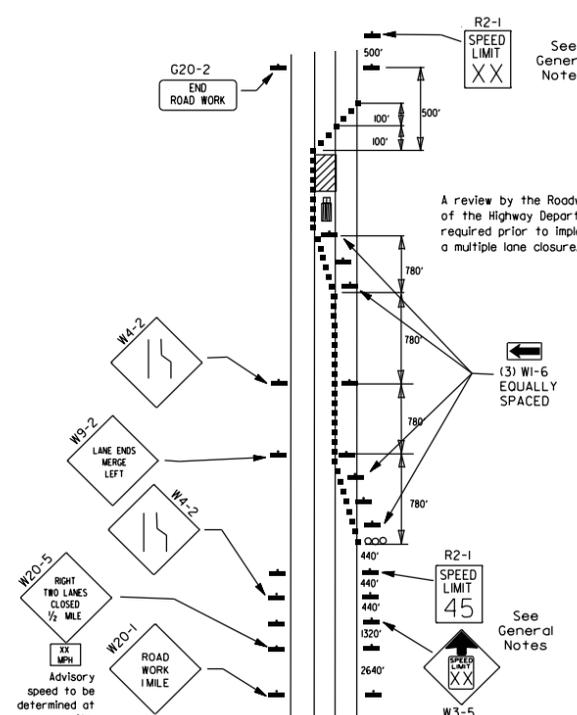
Flag shall be of good grade red material



COLORS LEGEND-WHITE (REFL) BACKGROUND-RED (REFL) LEGEND-BLACK BACKGROUND-ORANGE (REFL) AREA OUTSIDE DIAMOND-BLACK

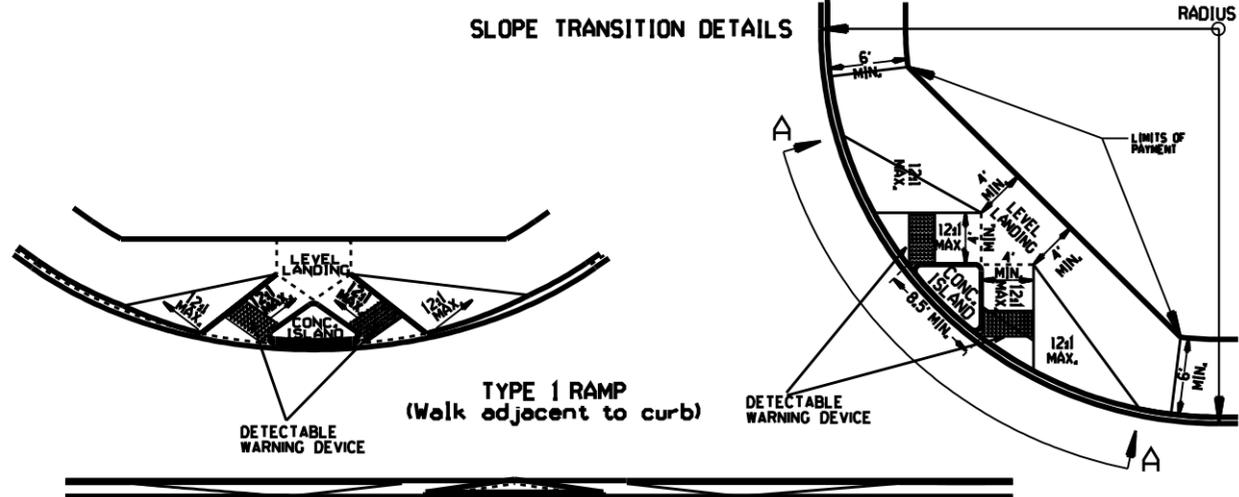
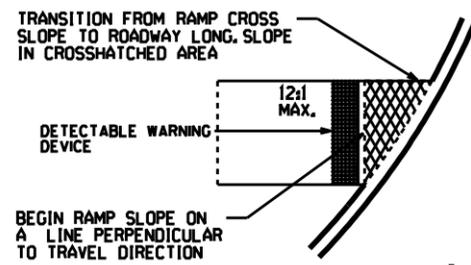
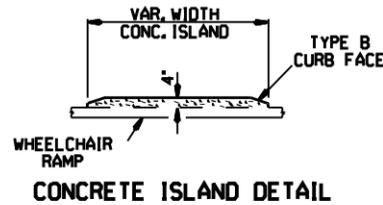


NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2) NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



(D) Typical application - closing multiple lanes of a multilane highway.

DATE	REVISION	FILED
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5	
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

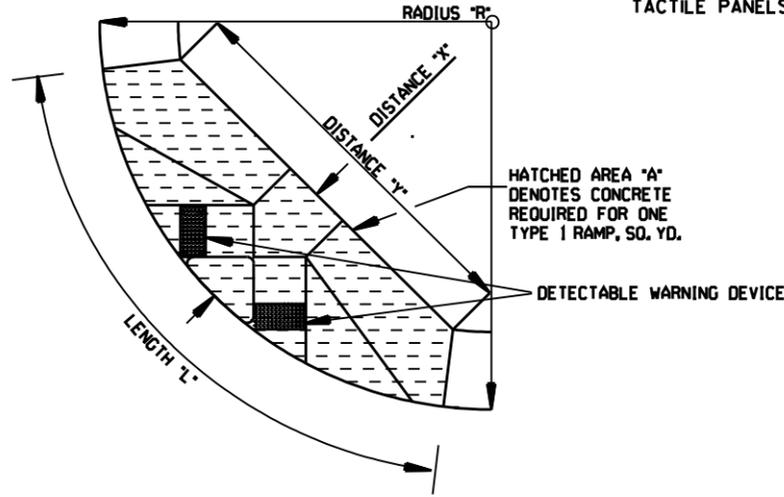
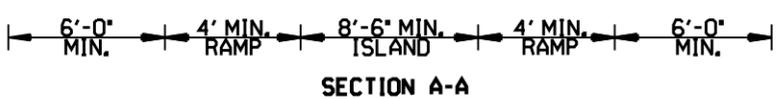
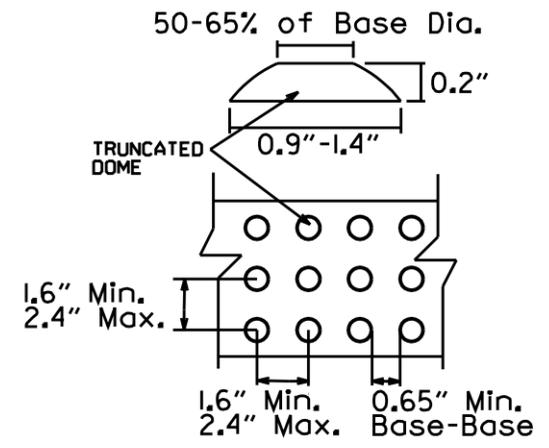


TYPE 1 RAMP DIMENSIONS AND QUANTITIES

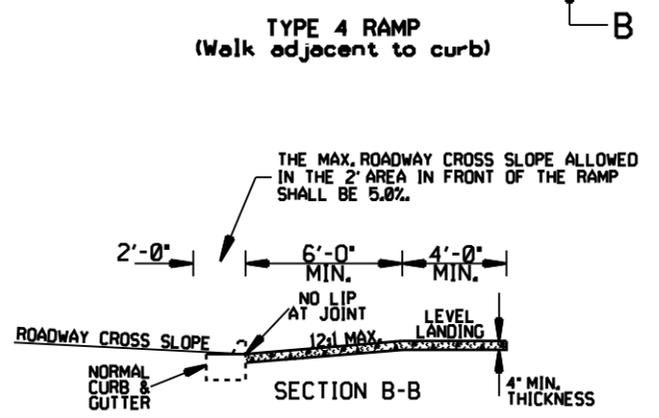
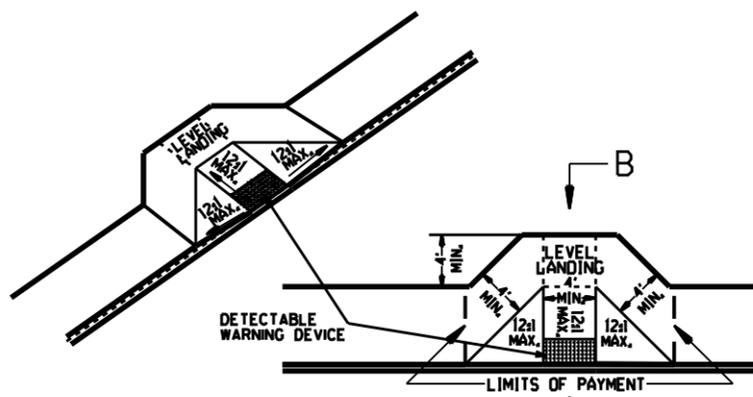
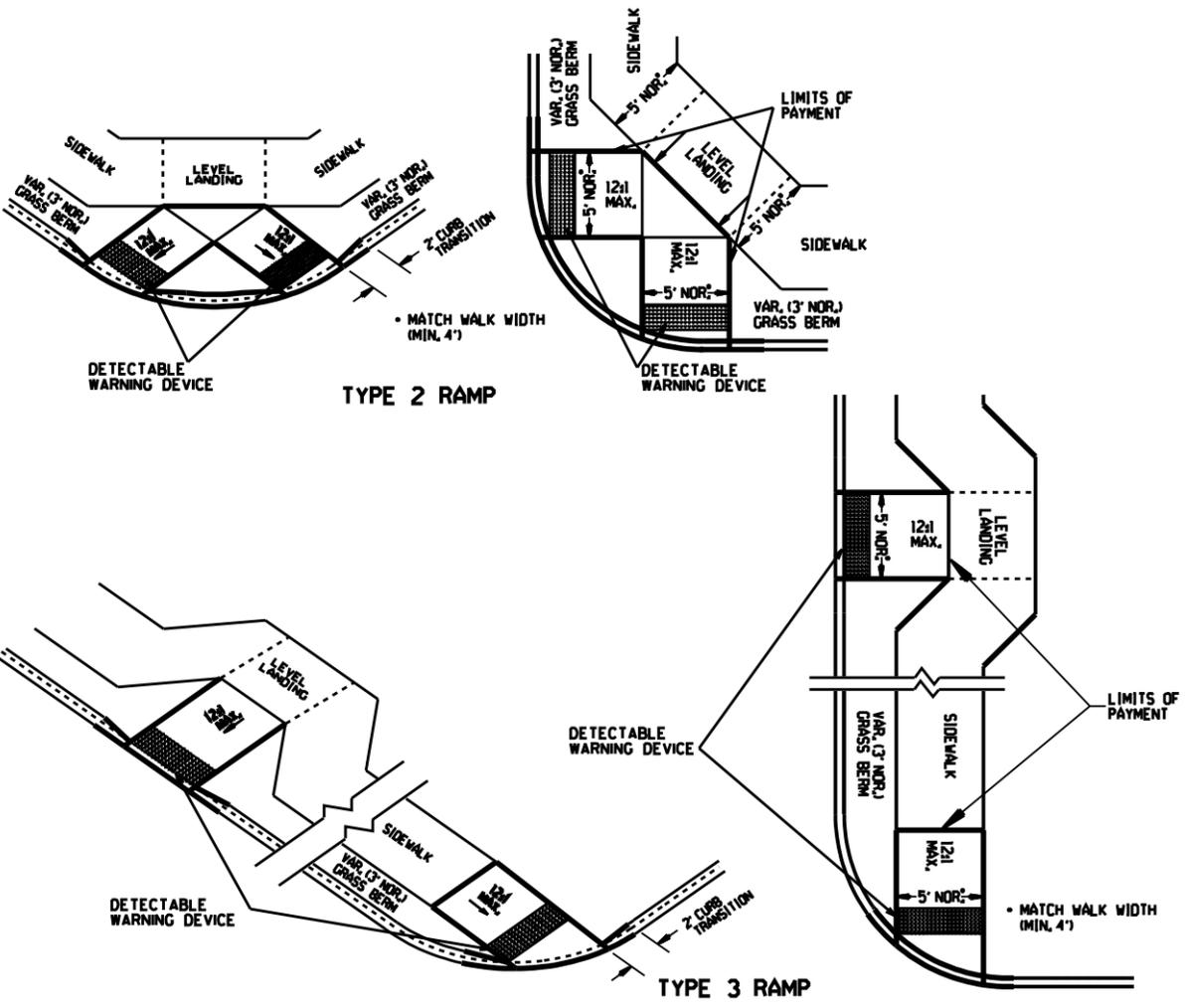
RADIUS "R"	DISTANCE "Y"	DISTANCE "Y"	LENGTH "L"	RAMP AREA "A"
FEET	FEET	FEET	FEET	SQ. YD.
15	11.67	18.82	32.18	26.21
20	11.52	22.28	35.46	30.07
25	11.43	26.60	38.77	33.80
30	11.37	30.26	40.93	36.90
35	11.33	33.51	43.11	39.77
40	11.30	36.45	45.26	42.45
45	11.27	39.16	47.34	44.97
50	11.25	41.69	49.36	47.35
55	11.24	44.07	51.31	49.63
60	11.22	46.33	53.21	51.80

GENERAL NOTES FOR DETECTABLE WARNING DEVICES

THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SO THAT THE NEAREST EDGE OF THE DEVICE IS 6 TO 8 INCHES FROM THE FACE OF THE CURB. TRUNCATED DOMES IN THE DETECTABLE WARNING SURFACE SHALL MEET THE REQUIREMENTS OF THE GEOMETRIC CONFIGURATION SHOWN. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES. DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE. DETECTABLE WARNING DEVICE SHALL BE ON THE AHTD QUALIFIED PRODUCTS LIST FOR CAST-IN-PLACE TACTILE PANELS (ADA DETECTABLE WARNING).



NOTE: THE CROSS SLOPE OF THE RAMPS, LEVEL LANDINGS, AND SIDEWALKS SHALL NOT EXCEED 2.0% UNLESS REQUIRED TO MATCH STREET LONGITUDINAL GRADE.



GENERAL NOTES:

IN NEW CONSTRUCTION, UNLESS OTHERWISE INDICATED ON THE PLANS, WHEELCHAIR RAMPS ARE TO BE PROVIDED AT ALL CORNERS OF CURBED STREET INTERSECTIONS AND MID-BLOCK CROSSWALK LOCATIONS. IN ALTERATIONS WHEELCHAIR RAMPS ARE TO BE PROVIDED AT CURBED STREET INTERSECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS. THE LENGTH OF THE RAMP SHALL BE SUCH THAT THE SLOPE DOES NOT EXCEED 12:1, THE SURFACE TEXTURE OF THE RAMP SHALL CONFORM TO A CLASS 6 FINISH ACCORDING TO SECTION 802.19. THE NORMAL GUTTER GRADE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP. ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION. THE MINIMUM THICKNESS OF THE RAMP, WALK, & LANDING SHALL BE 4". THE MINIMUM WIDTH OF THE RAMPS SHALL BE THE WALK WIDTH OR 36", WHICHEVER IS GREATER. RAMPS SHALL BE MODIFIED AS NECESSARY TO INSURE THAT THEY ARE PARALLEL TO A LINE DRAWN FROM THE CENTER OF ONE RAMP TO THE CENTER OF THE RAMP ON THE OPPOSITE SIDE OF THE INTERSECTION. THE DIMENSIONS AND QUANTITIES SHOWN ON THIS DRAWING ARE FOR A 90° INTERSECTION ONLY, DIMENSIONS AND QUANTITIES FOR SKEWED INTERSECTIONS WILL VARY, AND ARE TO BE DETERMINED BY THE ENGINEER.

RAMP SELECTION CRITERIA

CHOICE	TYPE	DESCRIPTION
FIRST CHOICE	TYPE 1	CORNER LOCATIONS WITH THE WALK ADJACENT TO THE CURB (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 2	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE INSUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 3	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE SUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 4	TANGENT LOCATIONS (BOTH NEW CONSTRUCTION AND ALTERATIONS).
SECOND CHOICE	TYPE 5	TANGENT LOCATIONS (ALTERATIONS ONLY).
THIRD CHOICE	TYPE 6	CORNER LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE TYPE 5 RAMPS CANNOT BE PLACED AT THE ENDS OF THE RADIUS.
FOURTH CHOICE		IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF ANY OF THE TYPES LISTED, THEN AND ONLY THEN CAN THE 12:1 MAX. SLOPE ON THE RAMP BE EXCEEDED TO PROVIDE ACCESS TO THE STREET LEVEL (ALTERATIONS ONLY). THE SLOPE CAN BE STEEPENED TO A 10:1 MAX. FOR A MAX. LENGTH OF 5' OR A 8:1 MAX. FOR A MAX. LENGTH OF 2'. SLOPES STEEPER THAN 8:1 ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.

NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE, AND ON THE PRESENCE OF OTHER SITE CONSTRAINTS (UTILITIES, BUILDINGS, ETC.). THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED. AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) BUT DOES NOT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. ALL PROJECTS THAT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY WILL USUALLY BE CONSIDERED NEW CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.

DATE	REVISION	DATE FILED
11-10-05	REVISED TO NEW SIDEWALK POLICY	
10-9-03	REVISED GEN. NOTES & ADDED NOTE	
4-10-03	REV. DETECTABLE WARNING DEVICES	
8-22-02	ADD DETECTABLE WARNING DEVICES	
3-30-00	ADD SLOPE TRANS. & REV. ISL. DIMS.	
11-18-98	REVISED NOTES	
8-12-98	REVISED TEXTURE	
7-02-98	REORDERED & REISSUED	
10-18-96	CORRECTED DIMENSIONS	10-18-96
5-24-90	FROM 8:1 TO 12:1 MAX. SLOPES	5-24-90
7-5-88	ADJUSTED MAX. SLOPE	652-7-5-88
7-14-88	INCLUDED CONC. ISLAND IN PAY ITEM	
6-02-76	ISSUED - P.A.D.	299-7-28-76
	DATE	REVISION
		DATE FILED

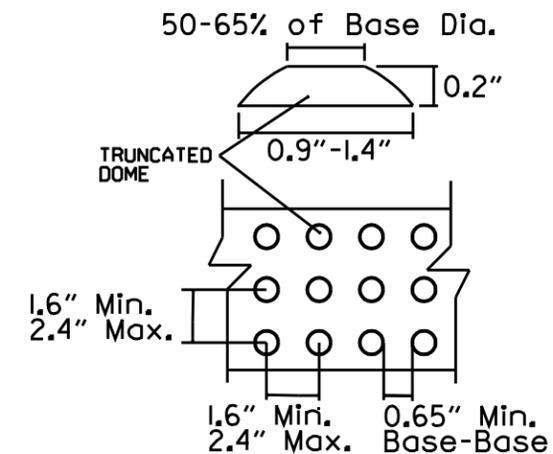
ARKANSAS STATE HIGHWAY COMMISSION

**WHEELCHAIR RAMPS
NEW CONSTRUCTION
AND ALTERATIONS**

STANDARD DRAWING WR-1

GENERAL NOTES FOR DETECTABLE WARNING DEVICES

THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SO THAT THE NEAREST EDGE OF THE DEVICE IS 6 TO 8 INCHES FROM THE FACE OF THE CURB. TRUNCATED DOMES IN THE DETECTABLE WARNING SURFACE SHALL MEET THE REQUIREMENTS OF THE GEOMETRIC CONFIGURATION SHOWN. DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES. DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE. DETECTABLE WARNING DEVICE SHALL BE ON THE AHTD QUALIFIED PRODUCTS LIST FOR CAST-IN-PLACE TACTILE PANELS (ADA DETECTABLE WARNING).



DETECTABLE WARNING DEVICE DETAIL

GENERAL NOTES:

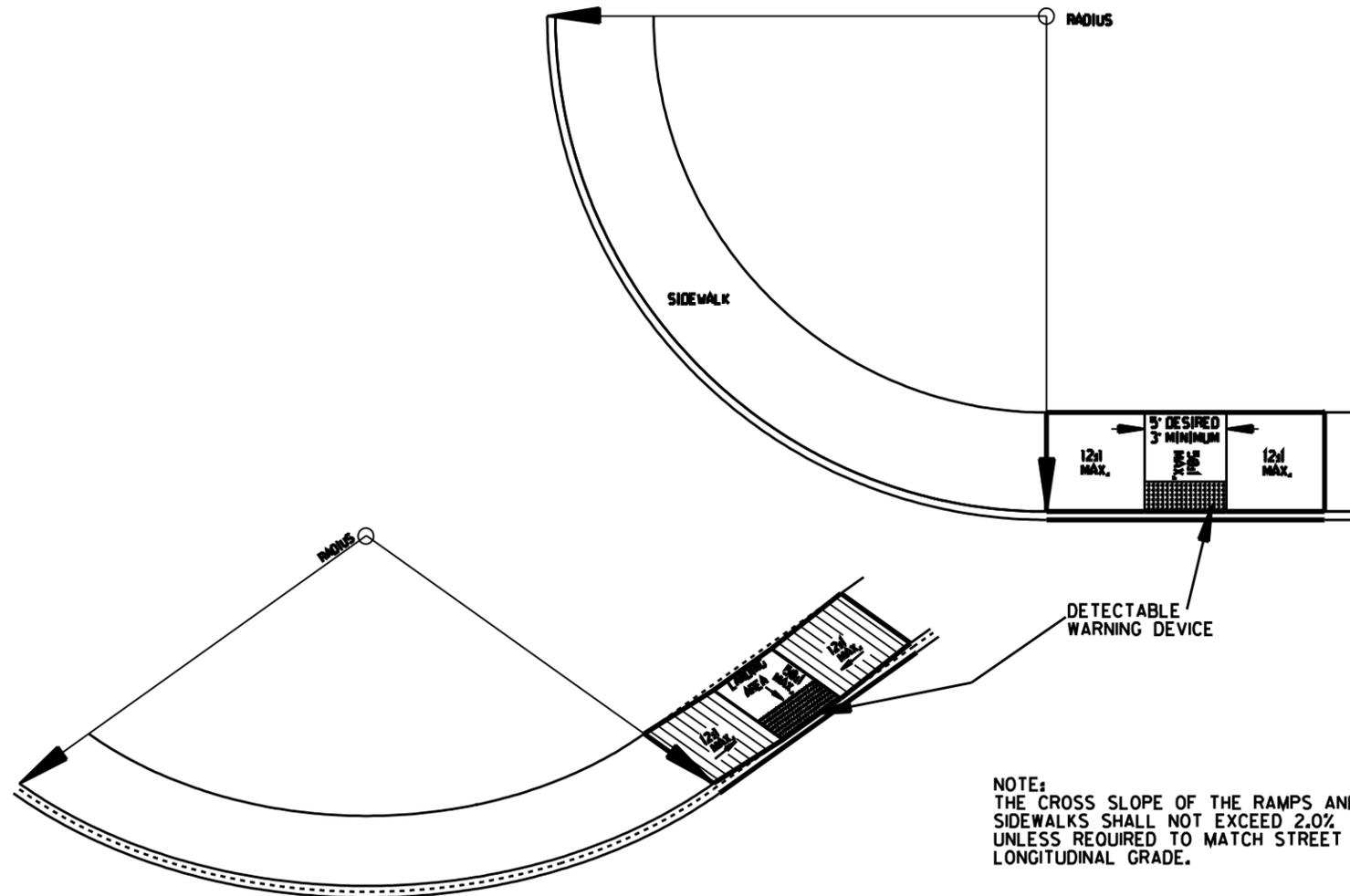
IN ALTERATIONS WHEELCHAIR RAMPS ARE TO BE PROVIDED AT CURBED STREET INTERSECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS. THE LENGTH OF THE RAMP SHALL BE SUCH THAT THE SLOPE DOES NOT EXCEED 12:1. THE SURFACE TEXTURE OF THE RAMP SHALL CONFORM TO A CLASS 6 FINISH ACCORDING TO SECTION 802.19. THE NORMAL GUTTER GRADE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP. ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION. THE MINIMUM THICKNESS OF THE RAMP, WALK, & LANDING SHALL BE 4". THE MINIMUM WIDTH OF THE RAMPS SHALL BE THE EXISTING WALK WIDTH OR 36", WHICHEVER IS GREATER. MINOR MODIFICATIONS OF THESE DETAILS, AS APPROVED BY THE ENGINEER, MAY BE MADE TO ADJUST TO LOCAL CONDITIONS.

RAMP SELECTION CRITERIA

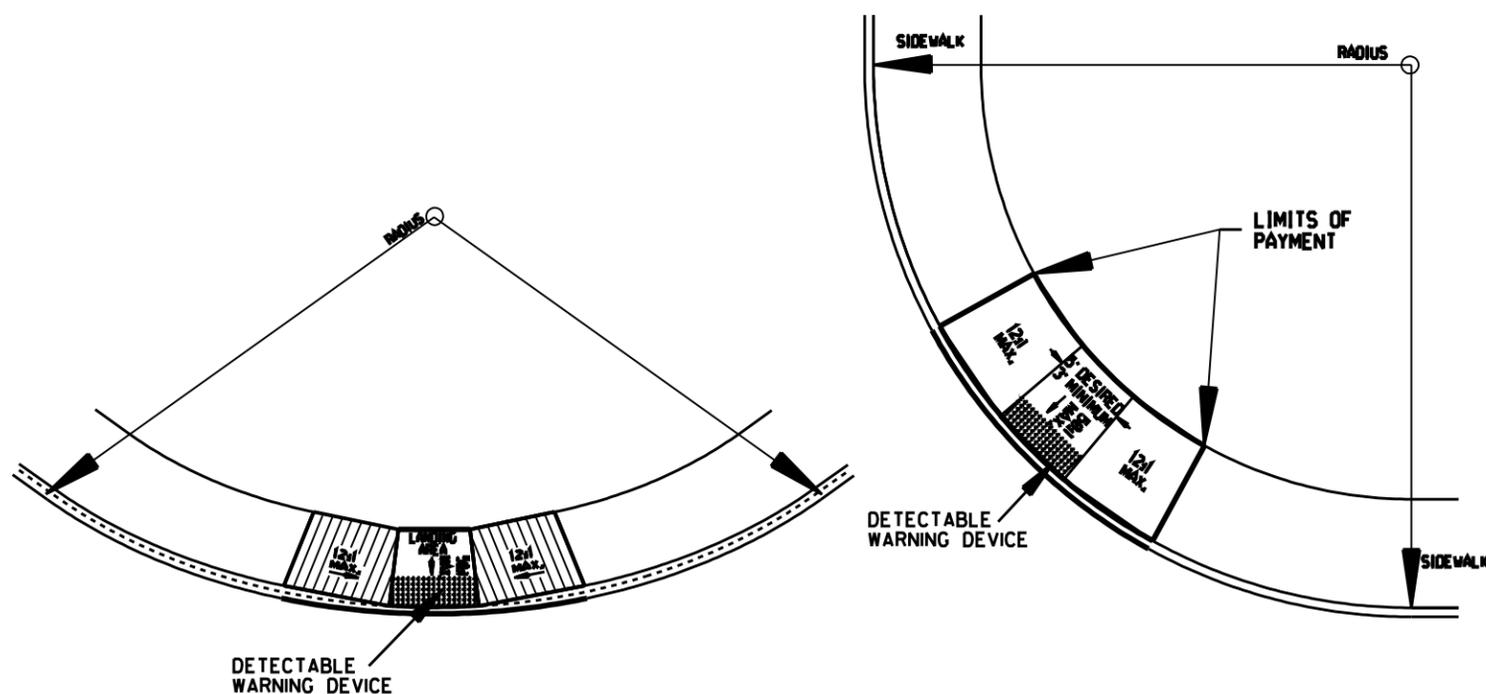
FIRST CHOICE	TYPE 1	CORNER LOCATIONS WITH THE WALK ADJACENT TO THE CURB (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 2	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE INSUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 3	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE SUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 4	TANGENT LOCATIONS (BOTH NEW CONSTRUCTION AND ALTERATIONS).
SECOND CHOICE	TYPE 5	TANGENT LOCATIONS (ALTERATIONS ONLY).
THIRD CHOICE	TYPE 6	CORNER LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE TYPE 5 RAMPS CANNOT BE PLACED AT THE ENDS OF THE RADIUS.
FOURTH CHOICE		IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF ANY OF THE TYPES LISTED, THEN AND ONLY THEN CAN THE 12:1 MAX. SLOPE ON THE RAMP BE EXCEEDED TO PROVIDE ACCESS TO THE STREET LEVEL (ALTERATIONS ONLY). THE SLOPE CAN BE STEEPENED TO A 10:1 MAX. FOR A MAX. LENGTH OF 5' OR A 8:1 MAX. FOR A MAX. LENGTH OF 2'. SLOPES STEEPER THAN 8:1 ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.

NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE, AND ON THE PRESENCE OF OTHER SITE CONSTRAINTS (UTILITIES, BUILDINGS, ETC.). THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED.

AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) BUT DOES NOT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. ALL PROJECTS THAT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY WILL USUALLY BE CONSIDERED NEW CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.



TYPE 5 RAMP



TYPE 6 RAMP

DATE	REVISION	DATE FILM
10-9-03	REVISED GENERAL NOTES & ADDED NOTE.	
4-10-03	REVISED DETECTABLE WARNING DEVICE DETAIL	
8-22-02	ADDED DETECTABLE WARNING DEVICES DETAILS	
11-18-98	REV. FOURTH CHOICE NOTE	
8-12-98	REVISED TEXTURE	
7-02-98	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

WHEELCHAIR RAMPS ALTERATIONS ONLY

STANDARD DRAWING WR-2

DATE: 2017-04-01

ESTIMATE OF MATERIAL AND FORCE ACCOUNT WORK
 BY THE
 UNION PACIFIC RAILROAD

THIS ESTIMATE GOOD FOR 6 MONTHS EXPIRATION DATE IS :2017-09-30

DESCRIPTION OF WORK:

JONESBORO, AR / PEDESTRIAN XING / DOT#440571H-1 / WYNNE SUB MP 238.44
 INSTALL 8' CROSSING SURFACE TO FUNCTION AS A PEDESTRIAN CROSSING,
 INCLUDING TIES AND OTM
 PROJECT WAS BUILT USING FED ADDITIVE W/ OVERHEAD AND INDIRECT 234%
 UPRR WILL BE REIMBURSED FOR 100% OF THE PROJECT COSTS BY CITY/STATE/FED.

PID: 100352 AWO: MP,SUBDIV: 238.42, WYNNE
 SERVICE UNIT: 06 CITY: JONESBORO STATE: AR

DESCRIPTION	QTY	UNIT	LABOR	MATERIAL	RECOLL	UPRR	TOTAL
ENGINEERING WORK							
ENGINEERING			640		640		640
FLAGGING			859		859		859
LABOR ADDITIVE 234%			3837		3837		3837
TRACK			140		140		140
TOTAL ENGINEERING			5476		5476		5476
SIGNAL WORK							
LABOR ADDITIVE 234%			1106		1106		1106
MATL STORE EXPENSE				21	21		21
SALES TAX				21	21		21
SIGNAL			644	549	1193		1193
TOTAL SIGNAL			1750	591	2341		2341
TRACK & SURFACE WORK							
BILL PREP FEE				900	900		900
ENVIRONMENTAL PERMIT				10	10		10
FOREIGN LINE FREIGHT				101	101		101
HOMELINE FREIGHT				900	900		900
LABOR ADDITIVE 234%			3340		3340		3340
MATL STORE EXPENSE				28	28		28
OTM			351	60	411		411
RDXING	8.00	TF	415	1789	2204		2204
SALES TAX				100	100		100
XTIE	6.00	EA	1498	683	2181		2181
TOTAL TRACK & SURFACE			5604	4571	10175		10175
LABOR/MATERIAL EXPENSE			12830	5162			
RECOLLECTIBLE/UPRR EXPENSE					17992	0	
ESTIMATED PROJECT COST							17992

THE ABOVE FIGURES ARE ESTIMATES ONLY AND SUBJECT TO FLUCTUATION. IN THE EVENT OF AN INCREASE OR DECREASE IN THE COST OR QUANTITY OF MATERIAL OR LABOR REQUIRED, UPRR WILL BILL FOR ACTUAL CONSTRUCTION COSTS AT THE CURRENT EFFECTIVE RATE.

**IMPROVEMENTS TO EXISTING PUBLIC ROAD AT GRADE CROSSING
FOR PUBLIC PEDESTRIAN CROSSING, DOT 440571H
M.P. 238.44 WYNNE SUBDIVISION
JONESBORO, ARKANSAS**

THIS AGREEMENT, executed in duplicate this ___ day of _____, _____, by and between UNION PACIFIC RAILROAD COMPANY, a Delaware corporation ("Railroad"), and CITY OF **Jonesboro**, a municipal corporation of the State of **Arkansas** ("Political Body").

RECITALS:

Political Body has requested Railroad to improve the existing **Public Pedestrian** crossing, at grade, along, over and across Railroad's track and right of way at Railroad Mile Post **238.44**, on Railroad's **Wynne** Subdivision, DOT No. 440571, in City of **Jonesboro, Craighead** County, State of Arkansas (the "Crossing"), to which Railroad is agreeable, but solely upon terms and conditions hereinafter set forth.

The location of the Crossing is generally shown on the print marked **Exhibit A**, attached hereto and hereby made a part hereof.

AGREEMENT:

NOW THEREFORE, in consideration of the premises and of the promises and conditions hereinafter set forth, the parties hereto agree as follows:

1. Railroad shall furnish all labor, material, equipment and supervision for, and shall (a) remove the existing timber crossing, (b) install an **8** ft. precast concrete crossing, (c) renew the crossties, and (d) surface the track with new ballast.

2. Political Body agrees to reimburse the Railroad for one hundred percent (100%) of Railroad's actual labor and material costs associated with the work and materials described in Section 1 above. Railroad estimates such cost to be **Seventeen** Thousand **Nine** Hundred and **Ninety-Two** Dollars (**\$17,992.00**), as set forth in Estimate of Material and Force Account dated April 01, 2017 marked **Exhibit B**, hereto attached and hereby made a part hereof. During the performance of such work Railroad will provide progressive billing to Political Body based on Railroad's actual costs. Actual costs to Railroad shall include customary additives (which includes its overhead and indirect construction costs) to materials, services and labor provided by Railroad. Within One Hundred Twenty (120) days after Railroad has completed its work, Railroad will submit a final billing to Political Body for any balance owed. Political Body shall pay Railroad within thirty (30) days of its receipt of progressive and final bills submitted by Railroad.

3. Railroad, at its cost, shall maintain the Crossing between the track tie ends and Political Body, at its cost, shall maintain all other portions of the Crossing. If, in the future, Political Body elects to have the surfacing material between the track tie ends replaced with paving or some surfacing material other than concrete, Railroad, at the Political Body's expense, shall install such replacement surfacing.

4. Political Body, at its cost, shall provide all labor, material and equipment associated with traffic control, barricades, detour signing, advanced warning signs and pavement markings. All such work shall be in compliance with the current Manual on Uniform Traffic Control Devices.

5. If Political Body's contractor(s) is/are performing any work described in Section 4 above, then Political Body shall require its contractor(s) to execute Railroad's standard and current form of

Contractor's Right of Entry Agreement. Political Body acknowledges receipt of a copy of the Contractor's Right of Entry Agreement and understanding of its terms, provisions, and requirements, and will inform its contractor(s) of the need to execute the Agreement and to obtain and provide to Railroad the insurance policies, binders, certificates and endorsements that are required in the Contractor's Right of Entry Agreement. Under no circumstances will the Political Body's contractor(s) be allowed onto the Railroad's premises without first executing the Contractor's Right of Entry Agreement and providing the aforesaid insurance documents.

6. Fiber optic cable systems may be buried on Railroad's property. Protection of the fiber optic cable systems is of extreme importance since any break could disrupt service to users resulting in business interruption and loss of revenue and profits. Political Body or its contractor(s) shall telephone Railroad during normal business hours (7:00 a.m. to 9:00 p.m., Central Time, Monday through Friday, except holidays) at 1-800-336-9193 (also a 24-hour number, 7 day number for emergency calls) to determine if fiber optic cable is buried anywhere on Railroad's premises to be used by Political Body or its contractor(s). If it is, Political Body or its contractor(s) will telephone the telecommunications company(ies) involved, arrange for a cable locator, and make arrangements for relocation or other protection of the fiber optic cable prior to beginning any work on Railroad's premises.

7. Political Body, for itself and for its successors and assigns, hereby waives any right of assessment against Railroad, as an adjacent property owner, for any and all improvements made under this Agreement.

8. Covenants herein shall inure to or bind each party's successors and assigns; provided, no right of Political Body shall be transferred or assigned, either voluntarily or involuntarily, except by express written agreement acceptable to Railroad.

9. Political Body hereby confirms that funds have been appropriated for the work set forth in this Agreement.

10. The person signing this Agreement on behalf of Political Body hereby confirms that he/she is authorized under Political Body policies and procedures to execute this Agreement and to bind Political Body to the obligations under this Agreement.

IN WITNESS WHEREOF, the parties hereto have duly executed this Agreement as of the date first herein written.

UNION PACIFIC RAILROAD COMPANY

CITY OF JONESBORO

By _____

By: _____

Title: _____

Title: _____

ATTEST:

By: _____

Title: _____